



JAMESTOWN S'KLALLAM TRIBE

2020

STRATEGIC TRANSPORTATION

SAFETY PLAN UPDATE

Prepared by:



December 2020

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"Preserving Our Future Generations"

Improve Roadway Safety and Move Toward Zero Deaths and Zero Serious Injuries

INTRODUCTION

For thousands of years, the S'Klallam Tribes (meaning "strong people") lived and prospered in a community based on strength, pride and survival on the lands now known as the Olympic Peninsula in Northwest Washington State. Their culture was rich in art, song, spirituality, traditional knowledge and social structure.

By 1855, S'Klallam Tribal leaders signed the Point No Point Treaty. By 1874, friction with settlers made it clear that the Jamestown S'Klallam community would not be able to remain in their traditional lands near Dungeness in Washington Territory. Under the leadership of Lord James Balch, a group of approximately 100 members pooled their money to purchase 210 acres of land rather than move to the reservation in Skokomish territory. They supported themselves by farming, fishing and working in local pulp mills, and received some aid from the federal government. In 1939, the Port Gamble band, located along the eastern end of S'Klallam territory accepted a reservation and became an officially recognized Tribe. The Jamestown people, not wanting to lose the land they had purchased or the independence they had worked so hard to obtain, continued to refuse to move to a reservation. In 1968, the Lower Elwha band located west of Port Angeles accepted reservation lands and was recognized by the federal government. Though "officially unrecognized", the Jamestown Tribe continued to receive services from the federal government until 1953, when those services were discontinued. Tribal leaders realized that without federal recognition, their rights to fish, hunt and gather in their usual and accustomed places were becoming increasingly limited by policy and by a growing local population. This affected their overall economic conditions and their ability to provide for the basic needs of their citizens, such as healthcare and education. In the mid 1970's, the Tribe began its effort to regain federal recognition and, after a lengthy legal struggle, was finally re-recognized on February 10, 1981. In 1988, the Jamestown S'Klallam Tribe became one of the first 20 tribes to participate in the Self-Governance Demonstration Project, showing that Tribes could manage their own finances, programs and services.

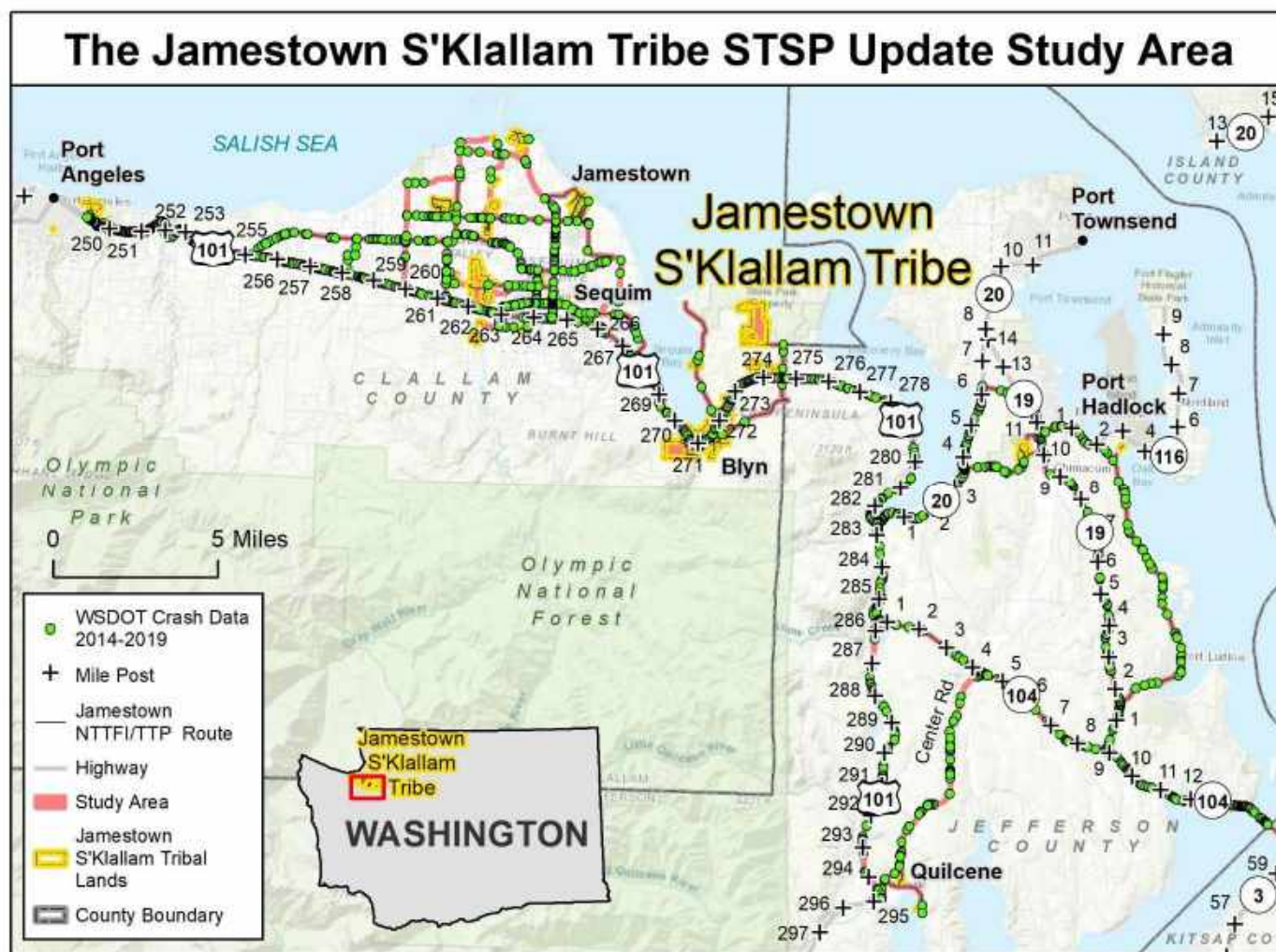
Today, the Jamestown S'Klallam Tribal campus is located in the village of Blyn, in Clallam County, WA, on a naturally beautiful landscape along the southern shores of the Sequim Bay, approximately 70 miles northwest of Seattle. In this area you will find the Tribal government administration, natural resources, social and community services departments, the Tribal Library, Jamestown Family Dental Clinic, 7 Cedars Resort and Hotel, and the Longhouse Market Deli & Fueling Station, and the Jamestown Public Safety and Justice Center. The total Tribal property in both Clallam and Jefferson counties is 1,153.5 acres. Just as it always has been, an abundance of fish, wildlife and vegetation inhabit this region. The importance and ongoing desire of preservation, conservation, and restoration by the Jamestown S'Klallam people will continue to allow this cohesion of people with their surroundings to exist for many, many more years to come.

This document represents the second Strategic Transportation Safety Plan (STSP) for The Jamestown S'Klallam Tribe and tells the story of the needs and strategies to support the health and well-being of their community. The Plan will also set the stage for future funding applications, safety initiatives, and safety campaigns. Implementation of the safety improvements identified in this Plan will work towards reducing the risk of death and serious injuries that may occur to transportation users within and near tribal lands. The Plan's development and future updates are the responsibility of The Jamestown S'Klallam Tribe Department of Administration with input from the Tribe's Safety Partners.

One of the goals of the Jamestown S'Klallam STSP is to raise awareness about transportation safety by providing crash data that supports the prioritization of needed transportation safety improvements. The focus is on the specific transportation network affecting safe ingress and egress to and from Tribal facilities and the community. The STSP will be used as a tool to apply for and justify future Tribal Transportation Plan Safety Funding and other grant funding.

PLAN DEVELOPMENT

The Jamestown S'Klallam Tribe issued a Request for Proposal for a Strategic Transportation Safety Plan Update (STSP) in May 2020. Red Plains Professional was selected for the project; the contract was signed and executed June 2020. Crash data received from Washington State Department of Transportation (WSDOT), analyzed and mapped. The STSP study area includes roadways connecting Jamestown S'Klallam Tribal Lands, Tribal Enterprises, Tribal Government and main community housing areas. The 2016 study area included main arterials: US 101 MP 250-295, SR 19 MP 0-14, SR 20 MP 0-12, SR 116 MP 0-9, SR 104 MP 0-13, Anderson Lake Road, and Oak Bay Road. The 2020 study area was updated to include all Tribal Transportation Program Inventory (TTP) and non-TTP routes connecting to main tribal properties, and main arterials: US 101 MP 250-295, SR 19 MP 0-14, SR 20 MP 0-6.3, SR 116 MP 0-2, SR 104 MP 0-13, Anderson Lake Road, Center Road, and Oak Bay Road.



All project data is viewable with the following Web Mapping Application:

<https://red-plains.xyz/JamestownSKlallam>

SAFETY STAKEHOLDERS

The following entities contributed to this plan. The partnerships developed during this process represent good resources for the Tribe, as the plan is evaluated, implemented and updated.

Stakeholders were contacted, sent the Web Mapping Application with all crash data and a copy of the Emphasis Areas. Both the Clallam and Jefferson County Contacts were appreciative to receive the information and expressed appreciation for the continued cooperation with the Jamestown S’Klallam Tribe. They identified several areas where they might be able to collaborate on some of the suggested improvements and would be contacting Annette Nesse with the Tribe directly. See Appendix D for Clallam County 2021-2026 Transportation Improvement Program (TIP) and Appendix E for Jefferson County 2021-2026 Transportation Improvement Program (TIP).

Jamestown S’Klallam Tribe

- Tribal Leadership
- Tribal Staff:
 - Transportation Program Manager
 - Planner

State of Washington

- Washington State Department Of Transportation (WSDOT)

City of Sequim

- Sequim Police Department

Clallam County

- Sheriff
- Planning Department

Jefferson County

- Sheriff
- Planning Department

EXISTING EFFORTS

Jamestown S’Klallam Tribe

The Tribe is dedicated to the betterment of transportation safety. These documents address a variety of safety concerns the Tribe has with their transportation system in and around the Tribal properties, facilities and roads accessing them. The main safety concern is that US 101 divides the North and South Tribal campuses and enterprise properties and there are serious challenges that exist with access to and from US 101. The activities, programs, projects and policies listed below, to address transportation safety, were completed. Additionally, the Jamestown S’Klallam Tribal Council and Administration has adopted high standards and expectations for all development projects, requiring consideration of transportation safety. They dedicate significant effort to enhance the environment and its safety for both Tribal citizens and the traveling public.

- A Roadway Safety Audit (RSA) conducted, in April 2014, on intersections with US 101, MP 267 – MP 274.6, reviewed fifteen intersections and developed countermeasures to improve the safety and reduce the crashes at each intersection. As a result, the realignment of the Chicken Coop Road/Zaccardo Road/US 101 intersection was completed in 2018, a partnership between WSDOT and the Jamestown S’Klallam Tribe (See Appendix A - Jamestown S’Klallam SR 101 RSA).
- US 101, East Sequim Bay Road Access Point Intersection Justification Report - September 2009
- Intersection Justification Report Chicken Coop Road and Zaccardo Road Realignment Traffic Analysis - March 2012
- Traffic Analysis Report for the Jamestown S’Klallam Tribe Long Range Transportation Plan - August 2014
- 2014 Tribal Transportation Program Roadway Inventory & Long Range Transportation Plan - March 2016
- Olympic Discovery Trail Development and Enhancements
- Old Blyn Highway Traffic Calming Project – road and campus improvements (with pedestrian safety focus) – completed in 2014
- The US 101 Tribal Pedestrian Underpass Project - completed in 2004
- Jamestown S’Klallam Long Range Transportation Plan – completed in 2016

Washington State Transportation Improvement Projects (STIP) 2020-2023

The 2020-2023 Statewide Transportation Improvement Program (STIP), approved in January 2020, as required by the Federal Transportation Act. Approximately 1,200 statewide transportation improvement projects utilizing \$3.3 billion in federal funds are in the 2020-23 STIP statewide, representing Washington's highest priority projects.

The STIP is a four-year, fiscally constrained prioritized multimodal transportation program of state, local, tribal, and public transportation (transit) projects, which includes highways, streets, roads, railroads, transit-hubs, park-and-ride lots, bridges, sidewalks, bike lanes, ferry terminals, trails and safety projects funded with federal, state, tribal and local sources. The STIP is a calendar year document, developed on an annual basis, with monthly updates from January through October. The STIP comprises local, metropolitan and regional transportation improvement programs (TIPs). Through state, metropolitan, regional, tribal and local planning processes, projects are identified. An environment of community engagement is woven into each stage of the planning process merging public dialogue forums with practical solutions to deliver transportation projects that "fit" into the communities and augment their transportation needs. Projects programmed in the STIP are the highest priority for the available funding, to preserve and improve the state's transportation network and achieving the national goals established in the Moving Ahead for Progress in the 21st Century Act (MAP-21) and continued in the Fixing America's Surface Transportation Act (FAST). Only those projects programmed in the approved STIP are allowable to utilize federal funds by the Federal Highway Administration (FHWA) and the Federal Transit Administration (FTA). <https://wsdot.wa.gov/LocalPrograms/ProgramMgmt/STIP.htm>

The 2020-23 STIP has 44 projects- with 4 specifically addressing transportation safety were in the Jamestown S'Klallam study area. Project 310101E, US 101 - Morse Creek Vicinity - Safety Improvements is currently in progress.

Washington State Transportation Improvement Projects (STIP) 2020-2023 Jamestown S'Klallam STSP Study Area								
Project #	Project Title	Description	Region	County	Subprogram 1	Subcategory 1	Add Date	Date Complete
310101E	US 101/Morse Creek Vicinity - Safety Improvements	Through a statewide analysis, two locations within the project limits have been identified as Collision Analysis Locations (CAL).	Olympic	Clallam	Safety	Collision Reduction	2020-09-21	2021-07-15
310101V	US 101/Vic Deer Park Rd to Dungeness River Br - Install Cable Barrier	Through a statewide analysis, it has been determined that placing median barrier on limited access divided highways is a cost effective method of reducing the severity of cross over collisions.	Olympic	Clallam	Safety	Collision Prevention	2019-10-28	2020-03-12
310153E	US 101/Golf Course Rd to N of W Uncas Rd - Rumble Strips	Collision data indicates run off the road incidents occur along higher speed highways. By installing shoulder rumble strips or profiled plastic line, the frequency of run off the road incidents may be reduced.	Olympic	Clallam, Jefferson	Safety	Collision Prevention	2021-12-20	2022-09-30
310401D	SR 104/Paradise Bay-Shine Rd - Intersection Safety Improvement	The intersection of SR 104/Shine Road is an Intersection Analysis Location (IAL) for the year 2016. 90% of the crashes at this intersection were entering at angle type crashes from Paradise Road. This project will provide intersection control.	Olympic	Jefferson	Safety	Collision Reduction	2021-10-04	2022-09-20

Figure 1 – Washington State Transportation Projects (STIP) 2020-2023 within study area.

US 101 - Morse Creek Vicinity - Safety Improvements - Construction beginning spring 2021

'US 101 between Sequim and Port Angeles is a main arterial highway that connects Olympic Peninsula travelers, freight and local businesses. At Morse Creek specifically, the existing roadway alignment has seen several crashes over the years and needs to be addressed to improve the safety for all travelers. In collaboration with local stakeholders and elected officials, this project will improve safety by constructing a raised, landscaped median, implementing a reduced speed limit to 40 mph and other proven traffic calming measures to reduce the probability of median cross-over incidents and severity of crashes along this curved highway.'

<https://wsdot.wa.gov/projects/us101/morse-creek/home>



Figure 2 – Design Concept (Source: WSDOT)

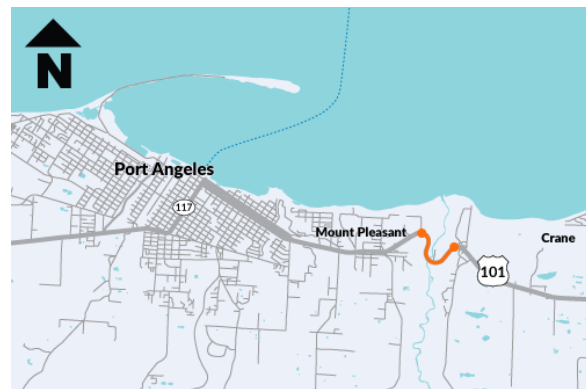
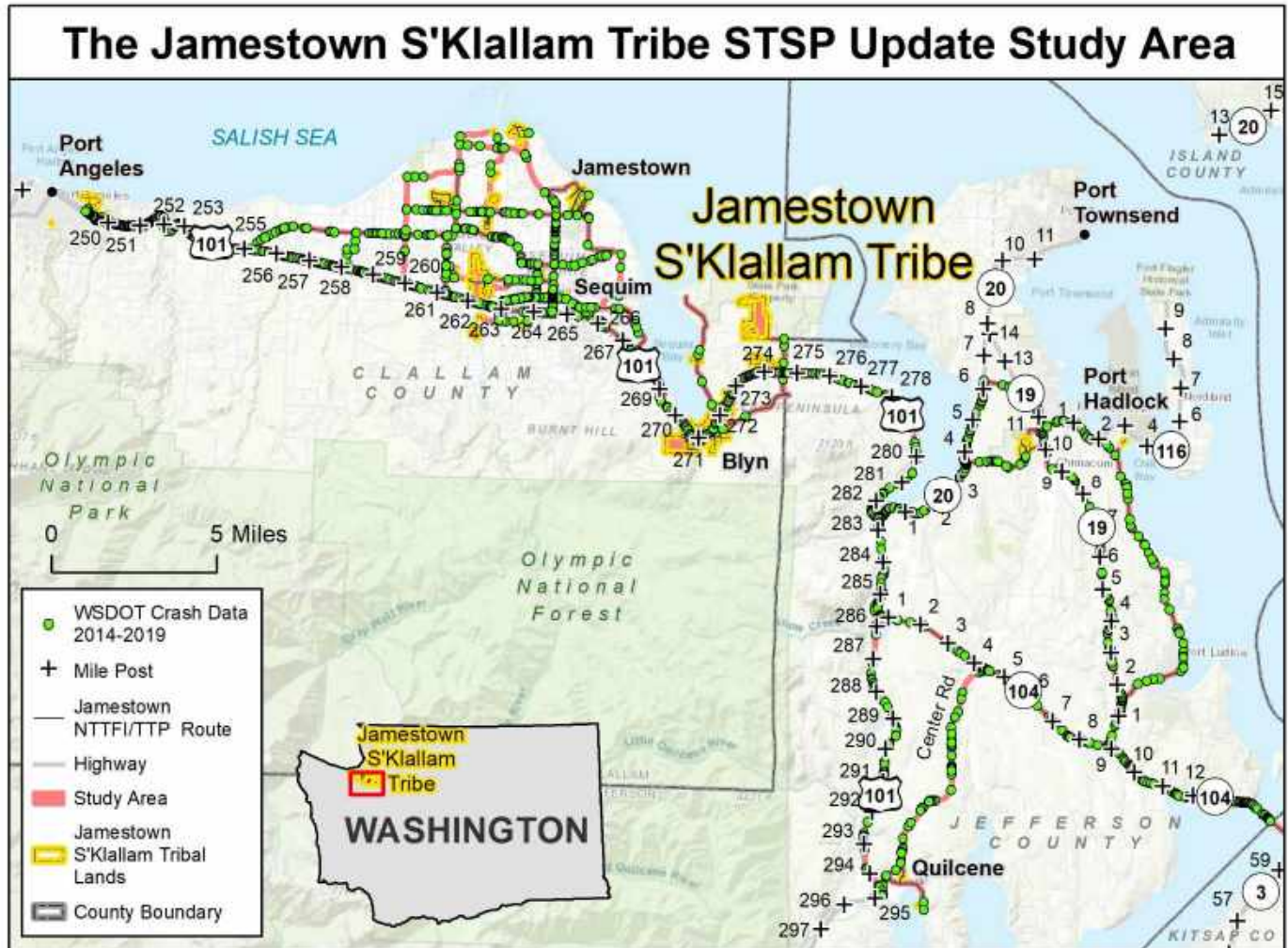


Figure 3 – Project Location Map (Source WSDOT).

MOTOR VEHICLE CRASH DATA REVIEW AND ANALYSIS

The STSP update study area includes roadways connecting Jamestown S'Klallam Tribal Lands, Tribal Enterprises, Tribal Administration and main community housing areas. The study area includes all Tribal Transportation Program Inventory (TTP) and non TTP routes connecting to main tribal properties, and main arterials: US 101 MP 250-295, SR 19 MP 0-14, SR 20 MP 0-6.3, SR 116 MP 0-2, SR 104 MP 0-13, Anderson Lake Road, Center Road, and Oak Bay Road.



The data source for the following analysis was the WSDOT Crash Data for the timeframe January 1, 2014 to December 31, 2019. All project data is viewable with the following Web Mapping Application:

<https://red-plains.xyz/JamestownSKlallam>.

During the 6-year period from 2014 to 2019 within the study area, a total of 2,842 reported crashes resulted in 25 fatality crashes, 85 major (suspected serious) injury, 801 minor injury crashes, and 1,885 property damage only crashes. In comparison, the 2016 STSP reported a 4-year period from 2010-2013, a total of 1,237 reported crashes resulted in 7 fatality crashes, 202 major injuries, 282 minor injuries and 730 property damage only crashes. See Crash Severity – Sequim and Blyn Map in Appendix B for distribution of crashes in the main tribal community. Traffic volumes increased in Blyn WA from 14,000 in 2013 to 17,000 in 2019, a 21% increase.

The statistics on the following page tell the story of crashes occurring within the Jamestown S'Klallam Transportation System.

CRASH FACTORS

- 2017 reported the highest number of all crashes with 501. 2014 reported the lowest number with only 437. Trend is showing increase in crash frequency throughout the timeframe. Same trend as in 2016 plan.
- 40% of all crashes involved 1 vehicle, less than the 47% in 2016 plan.
- 54% involved 2 vehicles, higher than the 47% in 2016 plan.
- 32% of all crashes resulted in either injury or fatality, 25 (1%) fatalities reported. Lower than the 40% in 2016 plan. Percentage of fatalities was the same, although numbers of fatalities were higher 25 vs 7.

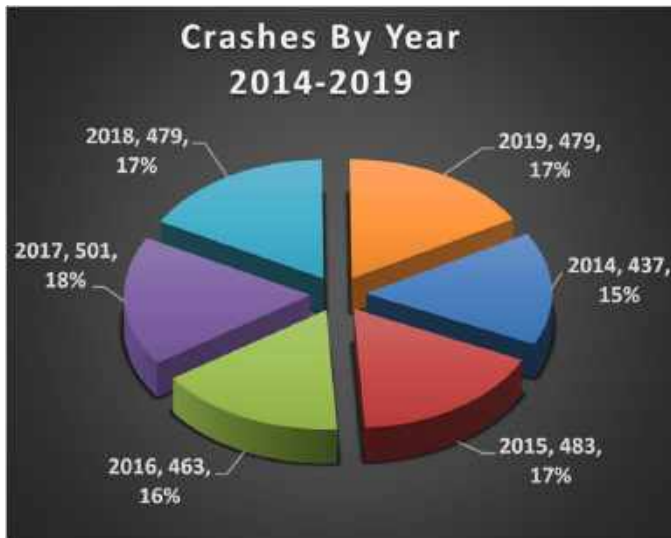


Figure 4 – All crashes by Year percentiles.

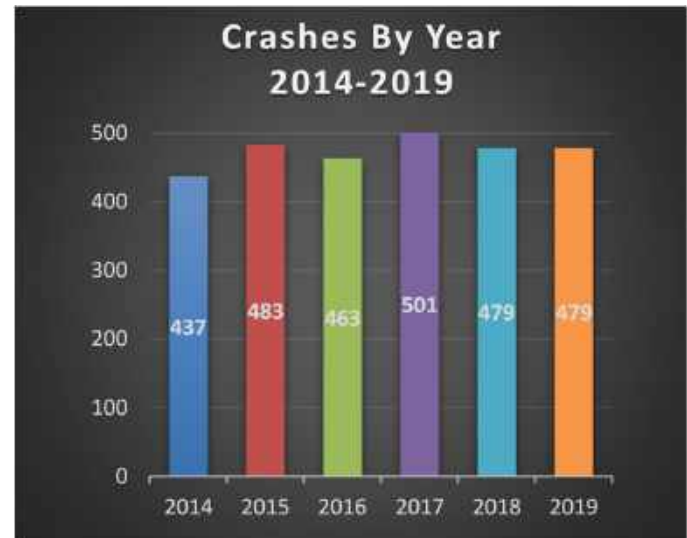


Figure 5 – All crashes by Year totals.

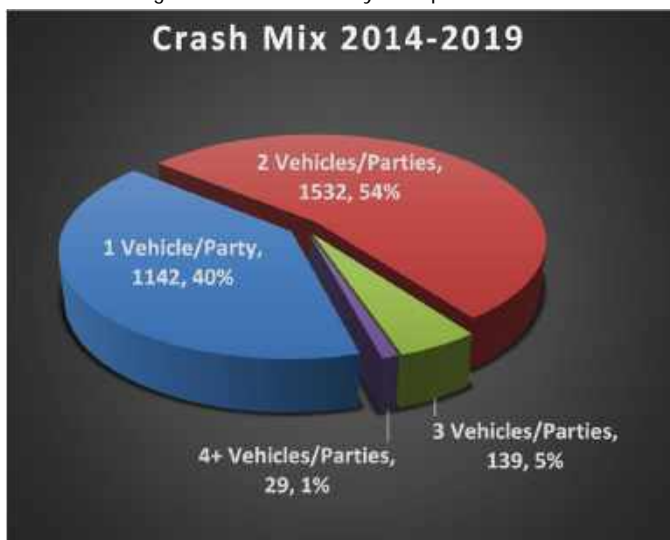


Figure 6 – All Crash Mix percentiles.

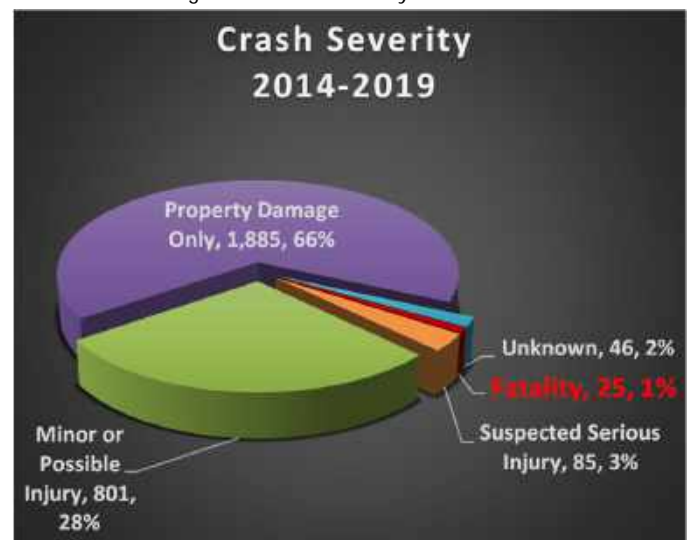


Figure 7 – All Crash Severity percentiles.

- 25% of crashes were Rear-End, the highest reported crash type in both 2014-2019 and 2010-2013 data sets.
- 12% of crashes involved Animal/Wildlife, 5% lower rate than 17% reported in 2016 plan.
- 30 crashes involved Pedestrian, and 27 involved Pedalcyclist, 2% of all crashes. There were 18 pedestrian/pedalcyclist crashes, 1% of all crashes in the 2016 plan.

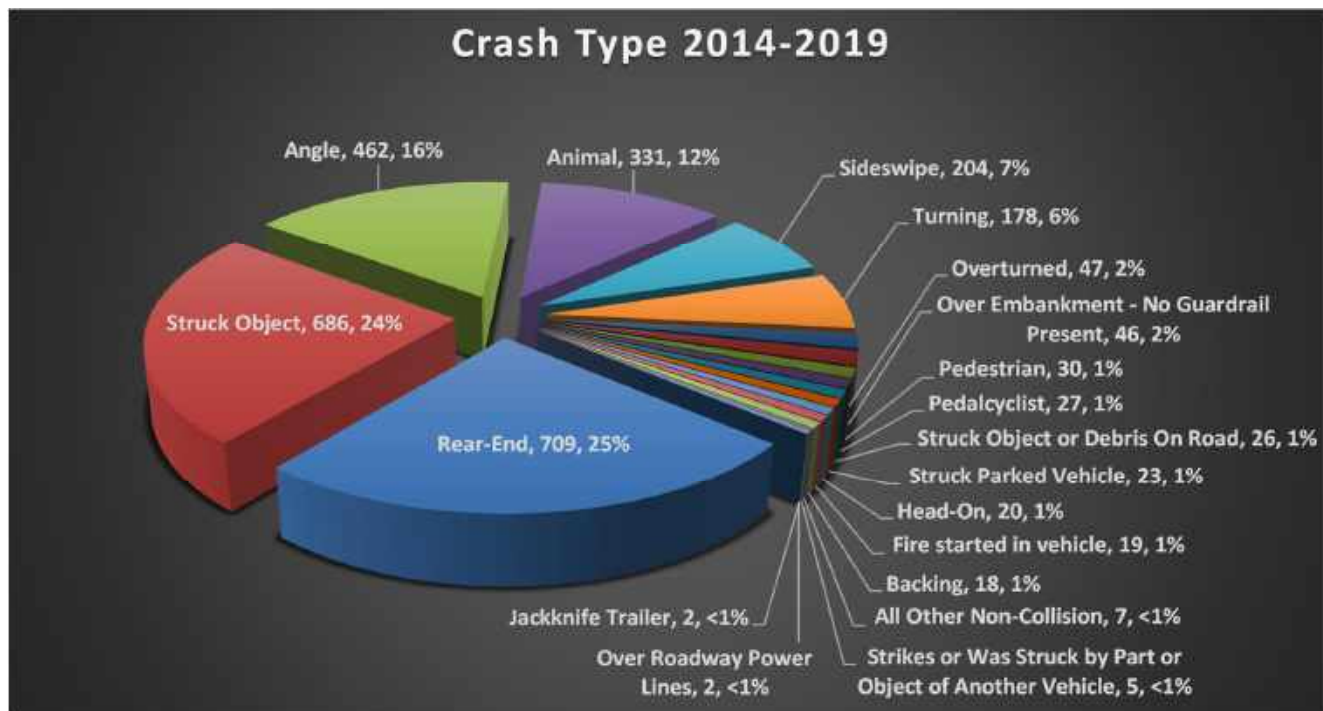


Figure 8 – All Crash Type percentiles.

ENVIRONMENT FACTORS

- 30% of all crashes occurred at night or in low light conditions. The same as in 2016 plan.
- 28% of all crashes occurred in Ice, Snow, Slush or Wet driving conditions. Higher than in 2016 plan at 25%.

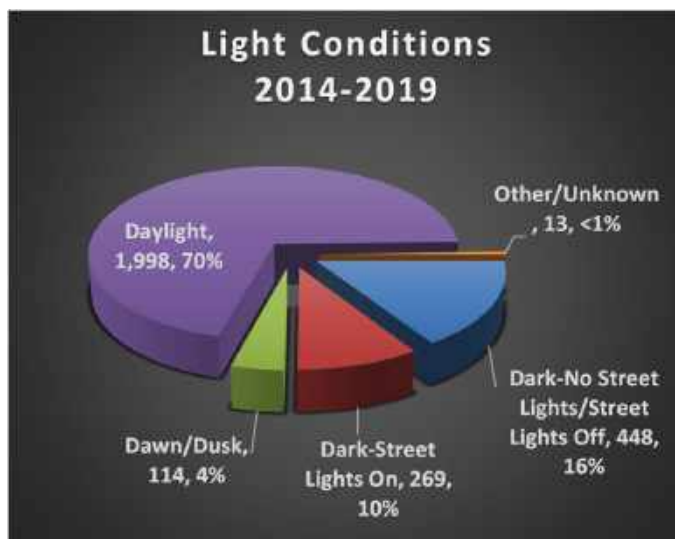


Figure 9 – All Light Conditions percentiles.

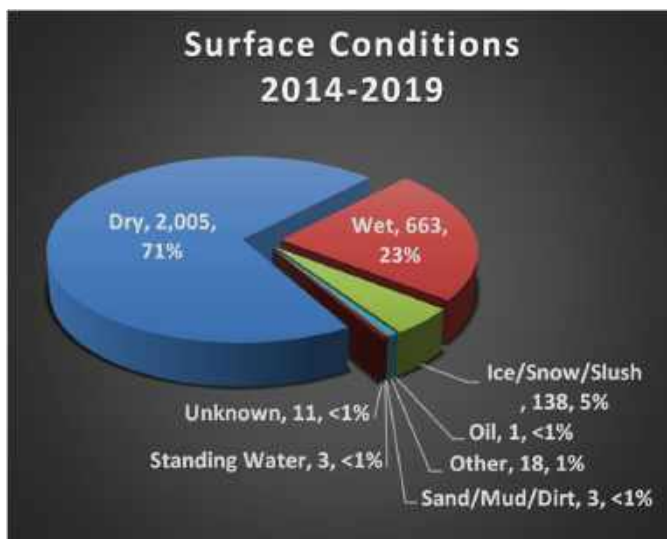


Figure 10 – All Surface Conditions percentiles.

ROADWAY FACTORS

- 48% of all crashes occurred at or were related to Intersections, Driveways or Roundabouts. Same as in 2016 plan.
- 43% of all crashes occurred on roadway geometries of Grade (Hill), Sag, Hillcrest or Curve. Not reported in 2016 plan.

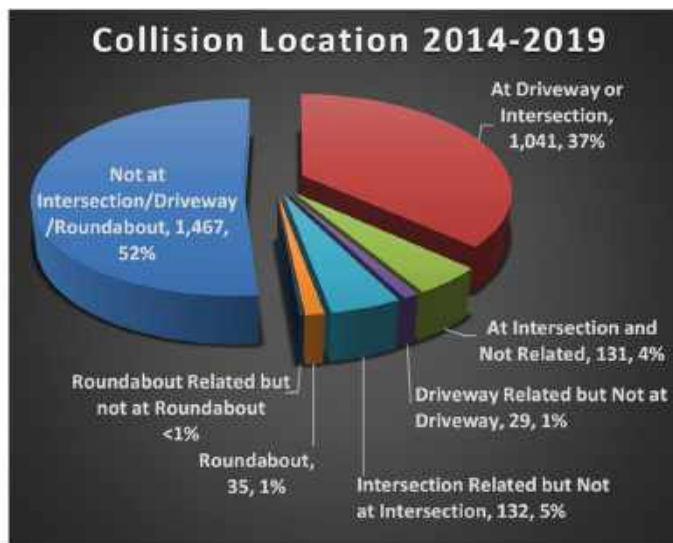


Figure 11 – All Crash Location percentiles.

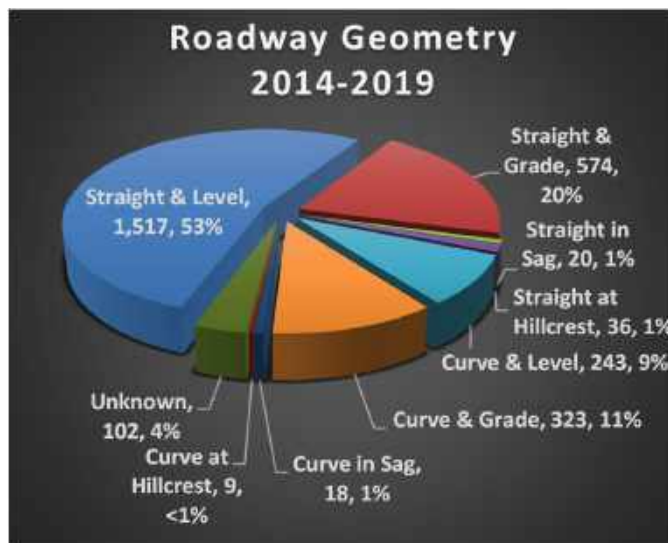


Figure 12 – All Roadway Geometry percentiles.

DRIVER FACTORS AND ROADWAY DEPARTURE

- 25% of all crashes involved Drivers 65 years of age or older age group at 25%. Higher than the 16% in the 2016 plan.
- 18% of all crashes involved Inattention as the primary human factor preceding crash. Higher than the 6% in the 2016 plan.
- 6% of all crashes, 8% of injury crashes and 28% of fatal crashes involved drivers Under Influence of Alcohol, Drugs or Medication. About the same as in the 2016 plan at 5%.
- 10% of all crashes and 13% of injury crashes. This was lower than the 19% in the 2016 plan.

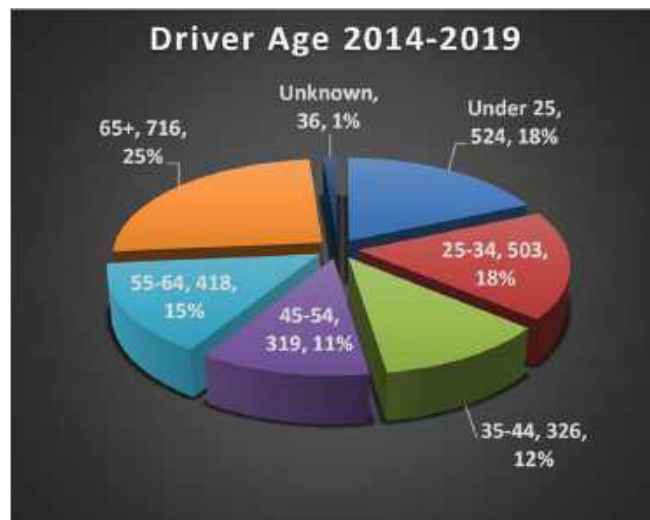


Figure 13 – All Driver Age percentiles.

- 9% of all crashes, 11% of injury crashes and 4% of fatal crashes involved Fail to Yield. Less than the 12% in 2016 plan.
- 8% of all crashes involved Follow Too Closely. 9% in the 2016 plan.
- 6% of all and injury crashes, and 4% of fatal crashes involved Distracted Driving. Not studied in 2016 plan.
- 5% of all crashes, 6% of injury crashes and 16% of all fatal crashes involved drivers Apparently Asleep, Fatigued or Ill. Not studied in 2016 plan
- 11% of all crashes and 25% of all fatal crashes involved Other Improper Driving, Parking Violation. Not studied in 2016 plan.

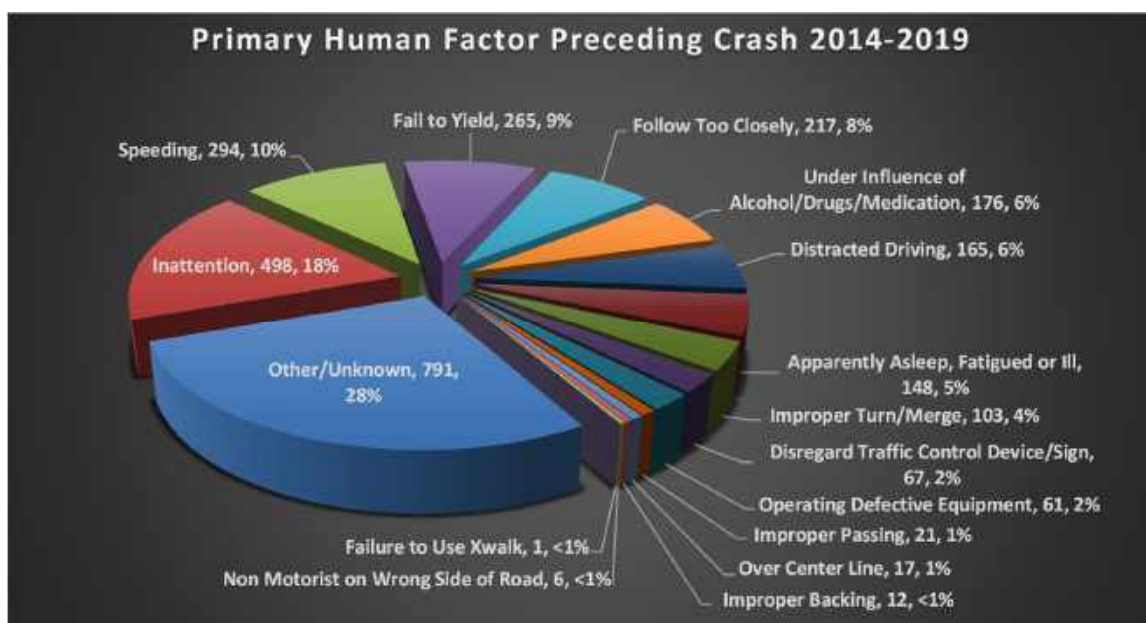


Figure 14 – All Human Factor Preceding Crash percentiles.

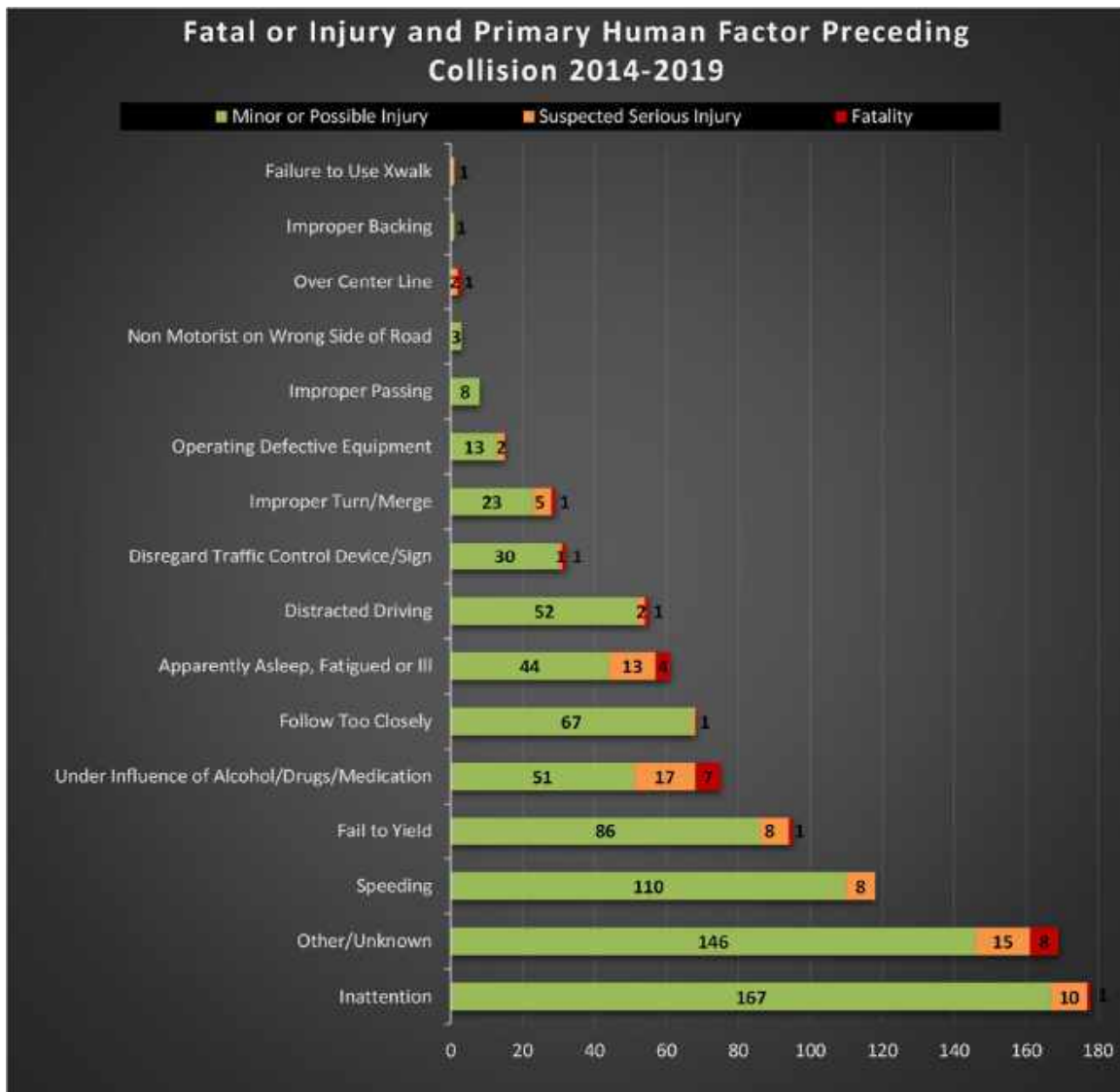


Figure 15 – All Fatal or Injury and Primary Factor Preceding Crash Totals.

The FHWA (Federal Highway Administration) defines Roadway Departure as a crash which a vehicle crosses an edge line, center line or otherwise leaves the traveled way. For this study, Roadway Departure crashes include those identified by WSDOT as Crash Types involving lane departure: Head-On, Over Embankment-No Guardrail Present, Overturned, Sideswipe, Struck Object, Struck Parked Vehicle and Primary Human Factor Preceding Crash of Over Center Line.

- 36% crashes involved Roadway Departure. 33% in 2016 plan.
- 76% of all fatal crashes and 56% of Suspected Serious injury crashes were Roadway Departure. Not studied in 2016 plan.

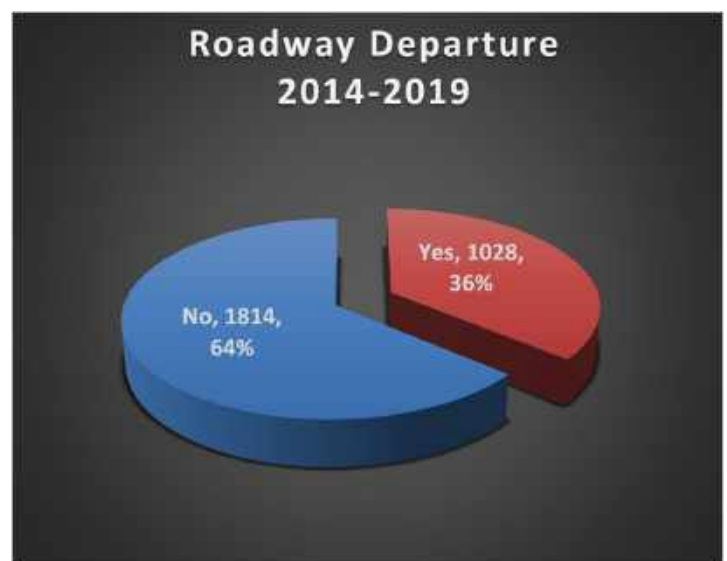


Figure 16 – All Crash Type percentiles.

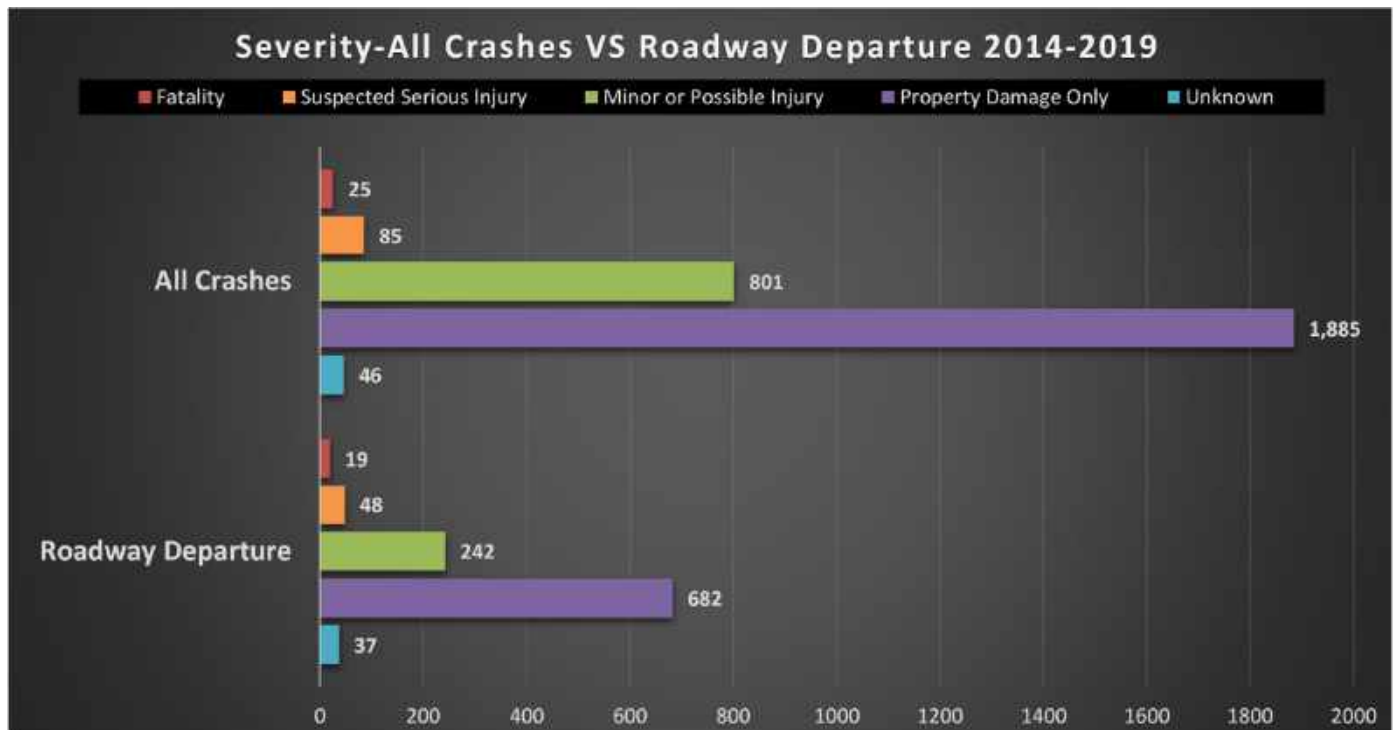


Figure 17 – Severity - All crashes vs Roadway Departure Involved totals.

- 11% of all crashes involved impaired driving. This is double the 5% in 2016 plan.
- 54% of impaired drivers were Under Influence of Alcohol, Drugs or Medication, 46% were Asleep, Fatigued or Ill. Not studied in 2016 plan
- 20% of impaired drivers were 65+ or 25-34 years of age. This compares to 5% for age 65+ and 22% for 25-34 on 2016 plan.
- 56% of all fatal crashes, 14% of injury crashes and 11% of all crashes involve Driver Impairment (Apparently Asleep, Fatigued, Ill, or Under Influence of Alcohol, Drugs or Medication). Not studied in 2016 plan.

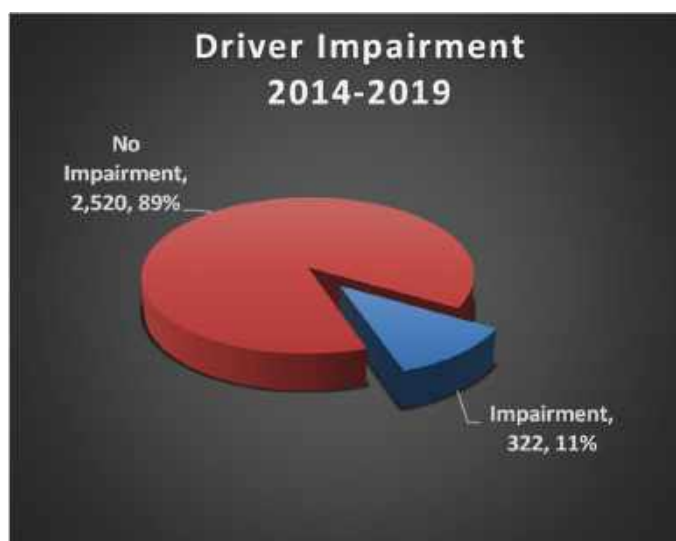


Figure 18 – All Driver Impairment percentiles.

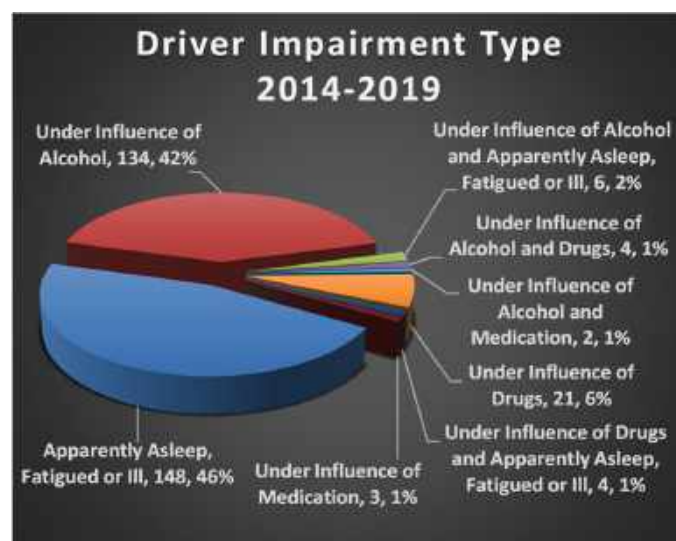


Figure 19 – All Driver Impairment Type percentiles.

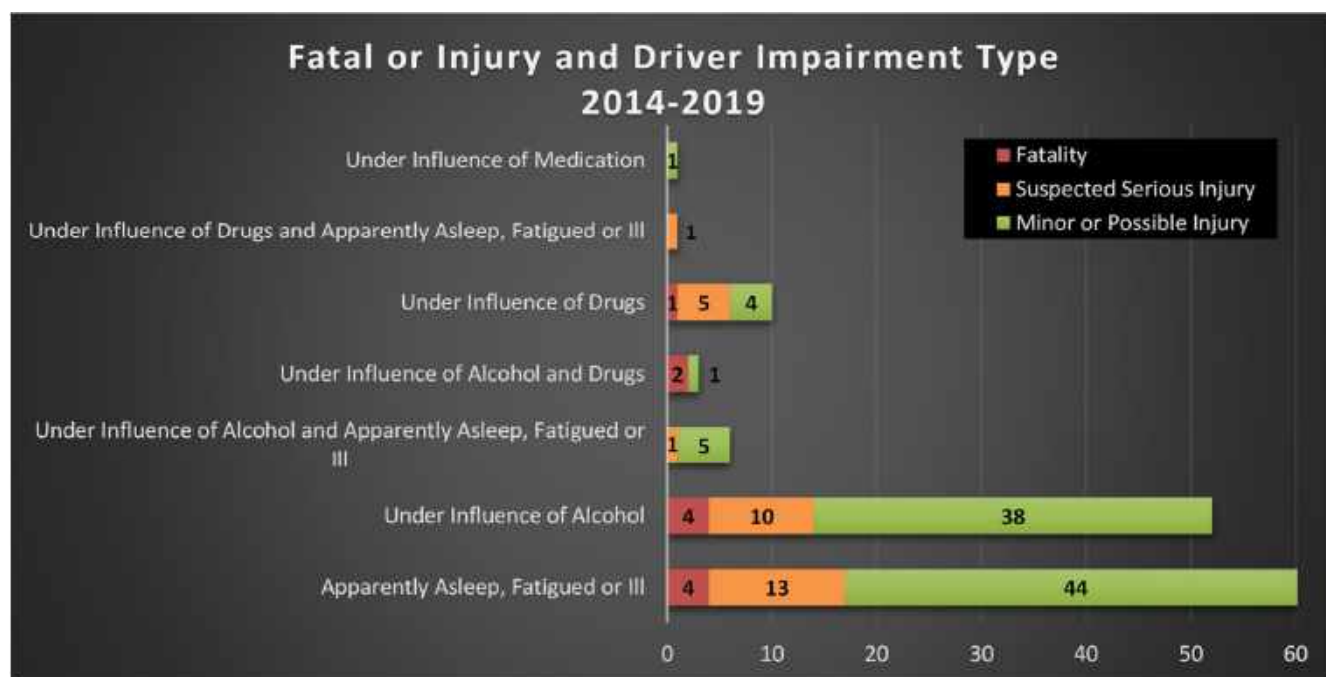


Figure 20 – All Driver Impairment percentiles.

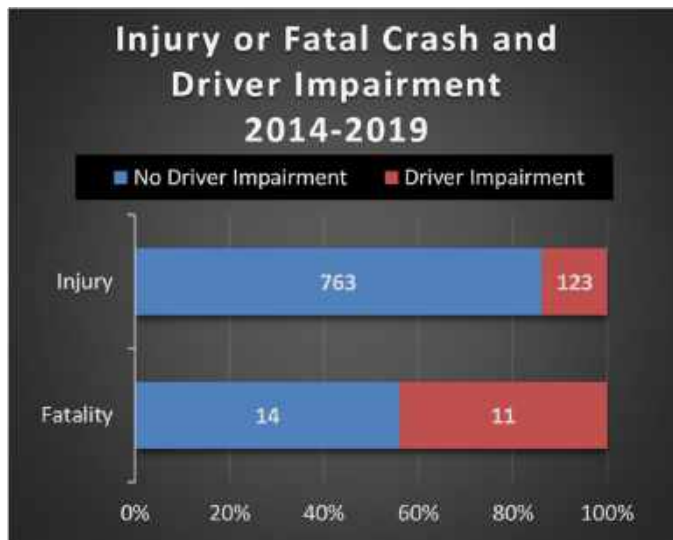


Figure 21 – Injury or Fatal Crash and Driver Impairment totals.

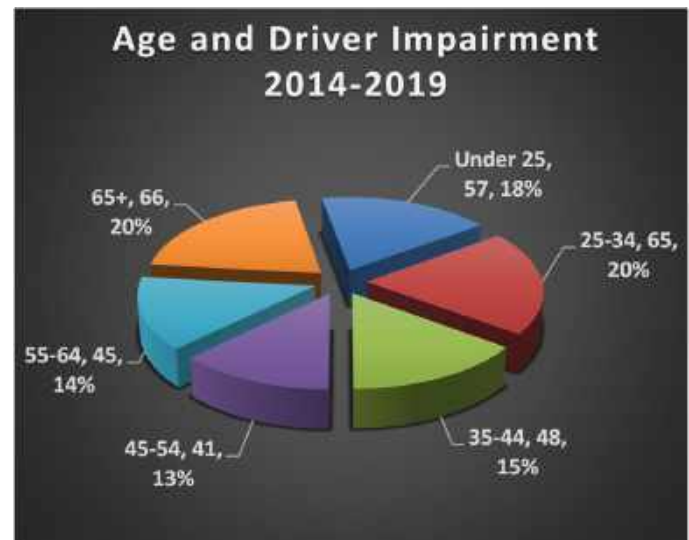


Figure 22 – All Age and Driver Impairment percentiles.

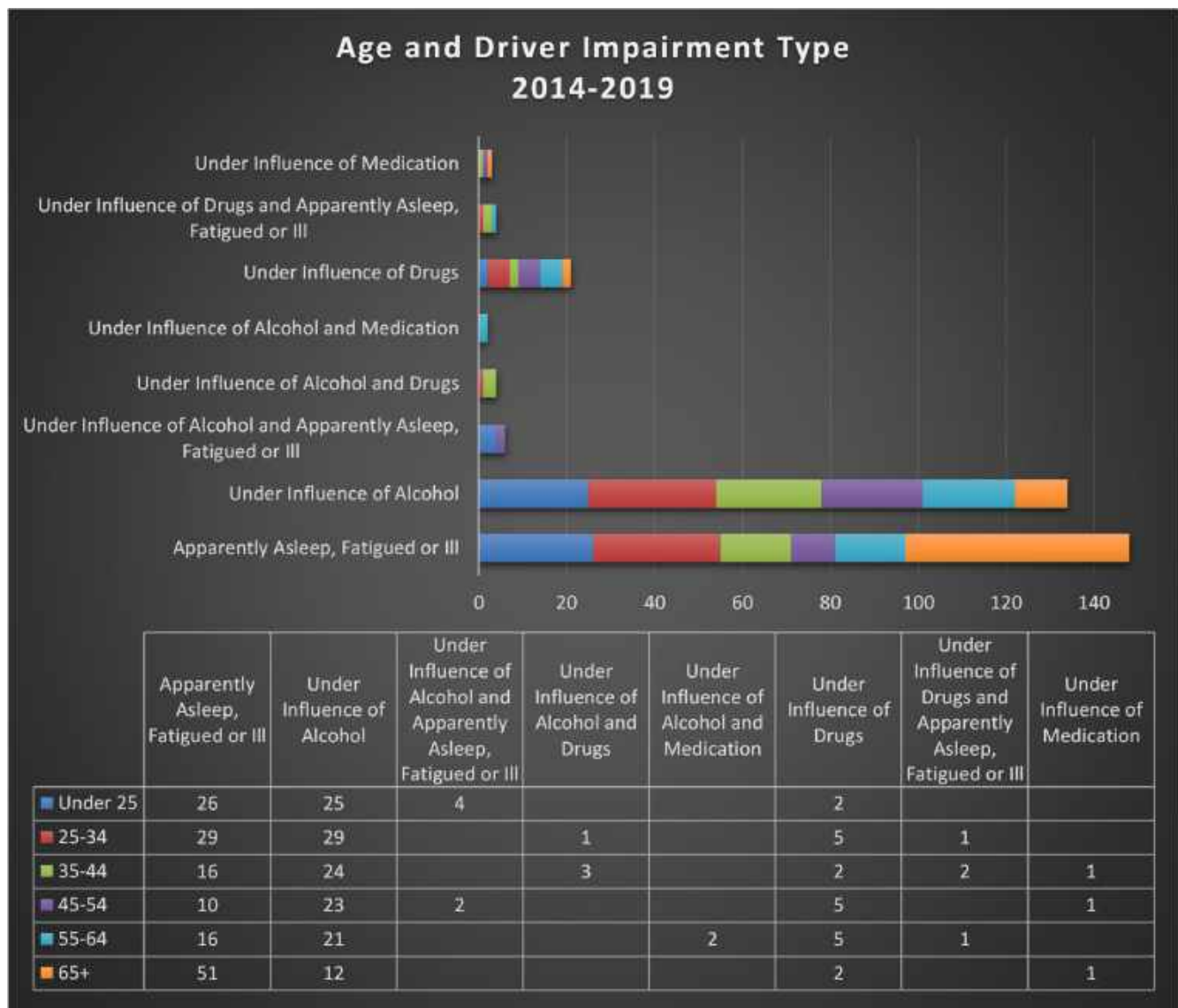


Figure 23 – All Driver Impairment percentiles.

RISKY DRIVING BEHAVIORS

The following factors were used to define Risky Driving Behaviors (RDB), Primary Human Factor Preceding Crash due to the high occurrence of crash fatalities or injuries: Inattention, Driver Impairment (Under Influence of Alcohol, Drugs or Medication or Apparently Asleep, Fatigued or Ill), Speeding, Fail to Yield, Follow Closely, and Distracted Driving. Further analysis was completed on RDBs to understand these crashes. Not studied in 2016 plan.

- 62% of all crashes were RDB's, including 56% of fatal crashes and 72% of injury crashes.
- 24% of all crashes were Both Roadway Departure and RDB's, including 48% of fatal crashes, and 24% of injury crashes.

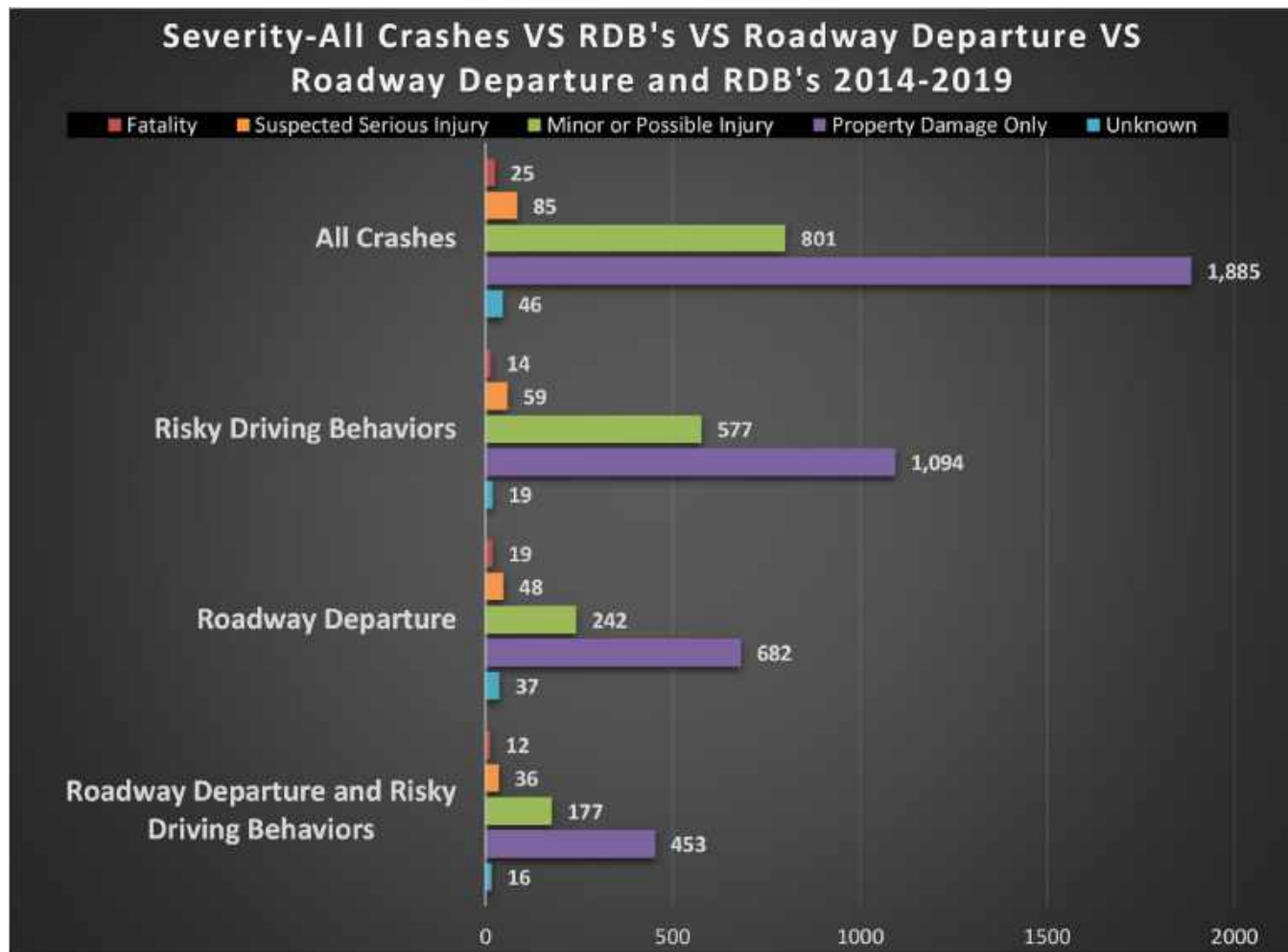


Figure 24 – Severity – All Crashes VS RDB's VS Roadway Departure totals.

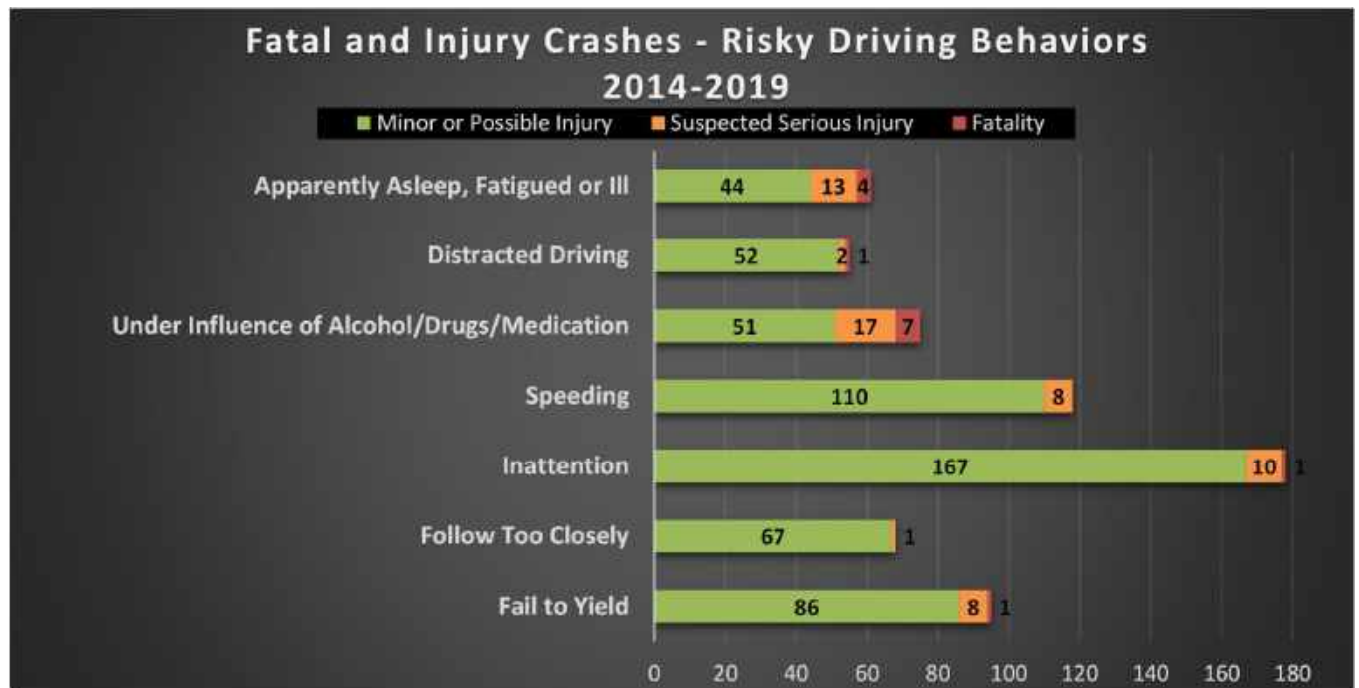


Figure 25 – All Fatal and Injury crashes - Risky Driving Behavior totals.

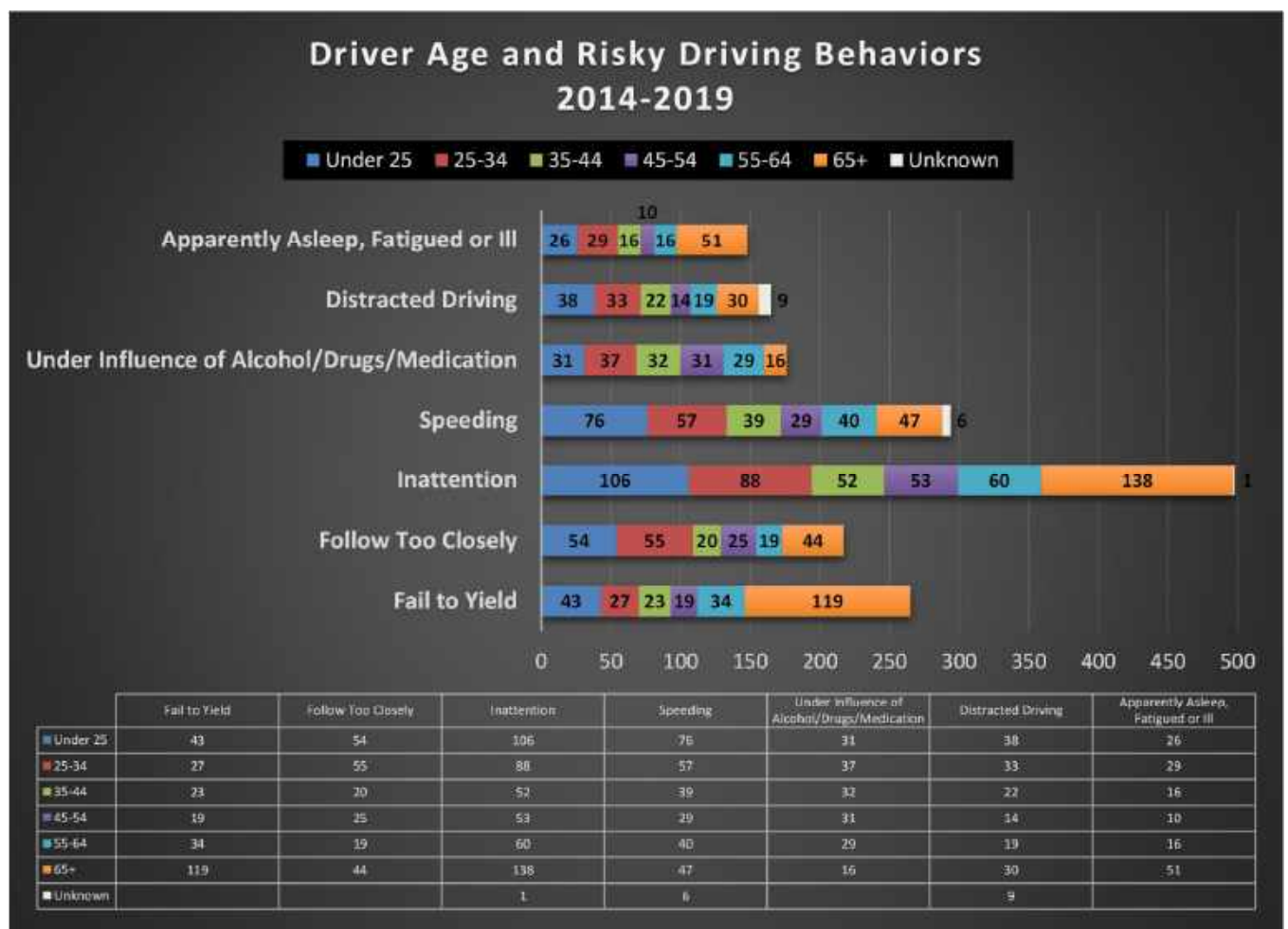


Figure 26 – All Driver Age and Risky Driving Behavior totals.

- Drivers 65 or older had the highest rate of crashes involving Fail to Yield at 45%, Inattention at 28%, and Driver Impairment-Apparently Asleep, Fatigued or Ill at 34%.
- Drivers under 25 and 25-34 had the highest rate of crashes involving Follow Too Closely at 25% each.
- Drivers under 25 had the highest rate of crashes involving Speeding at 26%, and Distracted Driving at 23%.
- Drivers 25-34 had the highest rate of crashes involving Driver Impairment-Under Influence of Alcohol, Drugs or Medication.

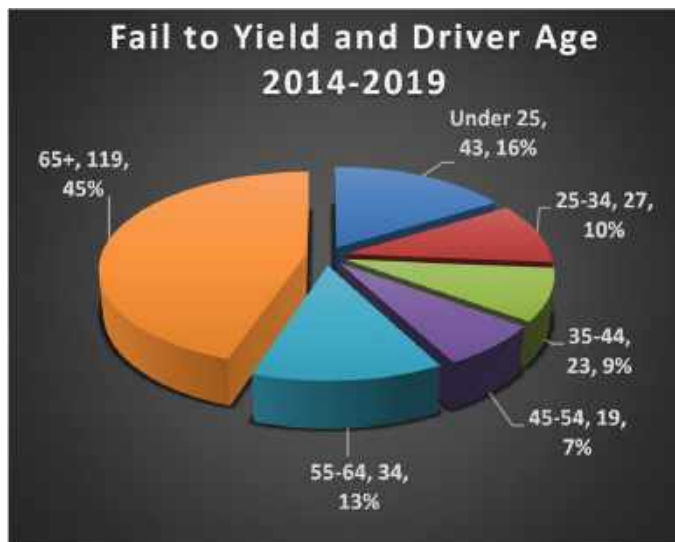


Figure 27 – All Fail to Yield and Driver Age percentiles.

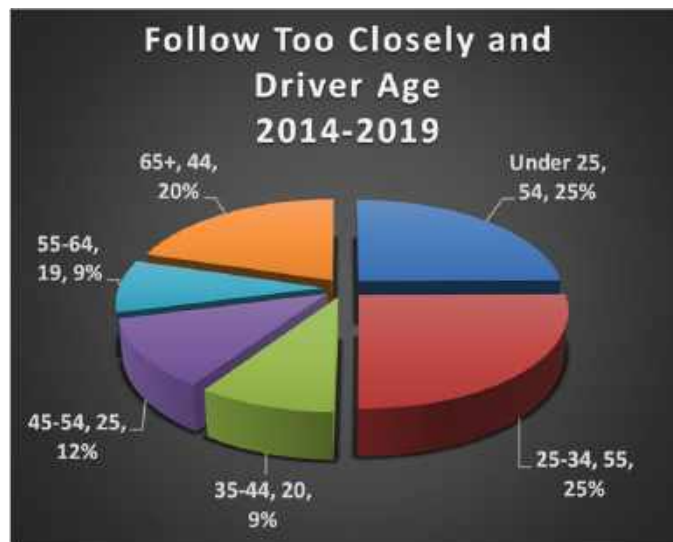


Figure 28 – All Follow Too Closely and Driver Age percentiles.

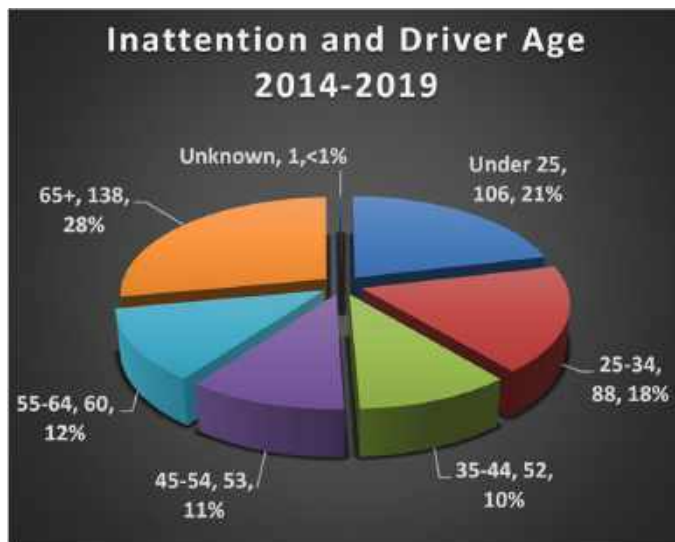


Figure 29 – All Inattention and Driver Age percentiles.

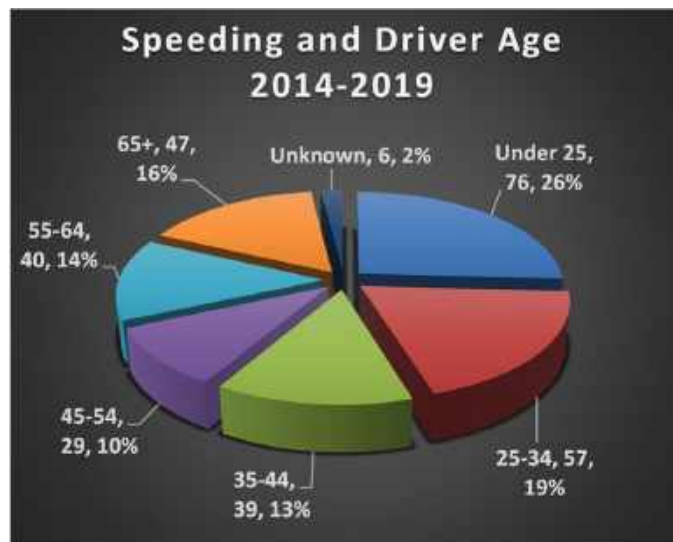


Figure 30 – All Speeding and Driver Age percentiles.

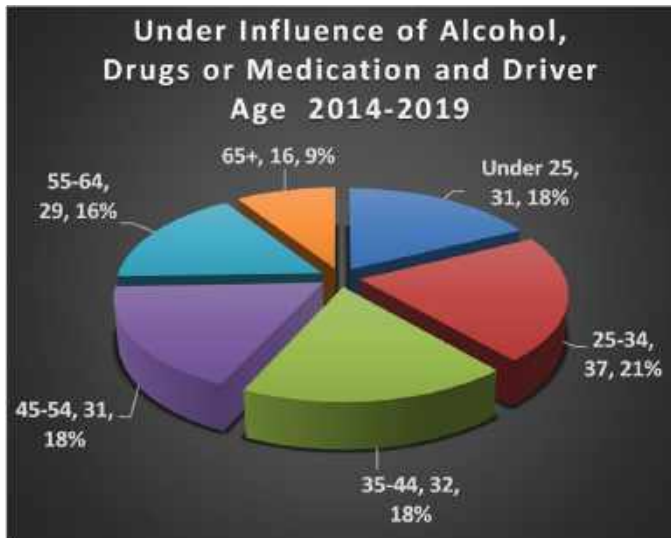


Figure 31 – All Under Influence of Alcohol, Drugs or Medication and Driver Age percentiles.

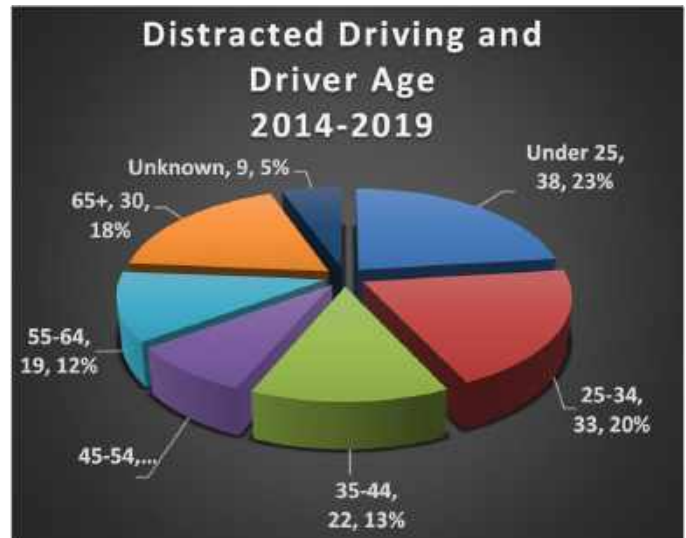


Figure 32 – All Distracted Driving and Driver Age percentiles.

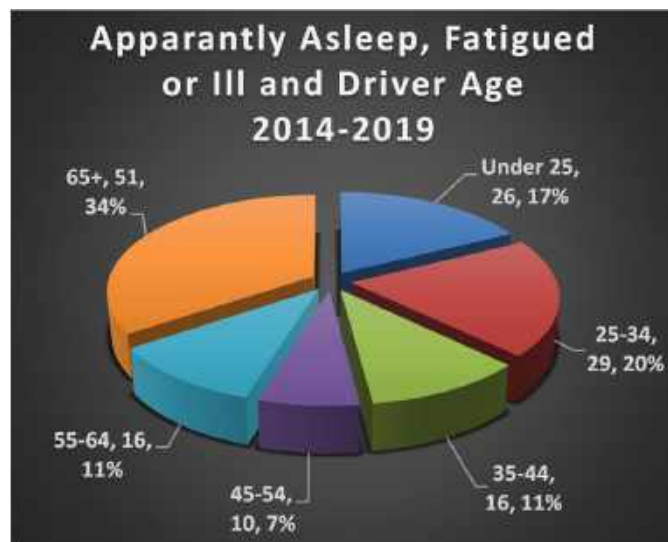


Figure 33 – All Apparently Asleep, Fatigued or Ill and Driver Age percentiles.

VULNERABLE ROAD USERS

Vulnerable Road Users include pedestrians, pedalcyclists, motorcyclists and road users 65 or older. Vulnerable road users are more exposed than other drivers operating vehicles, making them more susceptible to injury in the event of a crash. Not studied in 2016 plan.

- 28% of all crashes involved Pedestrian, Pedalcyclist, Motorcyclist or 65 or older road users.
- 41% of crashes involving Vulnerable Road Users resulted in injury or death. Only 34% of all crashes resulted in injury or death, the rates are much higher for Vulnerable Road Users.
- There were 13 fatal crashes involving Vulnerable Road Users. 6 (46%) of the fatal crashes involved Drivers 65 or older.
- Drivers 65 or older represent 24% of all drivers and were in 84% of crashes involving Vulnerable Road Users, with 46% of fatal crashes, 65% of suspected serious injury, 78% of minor or possible injury.



Figure 34 – Crashes involving Vulnerable Road User percentiles.

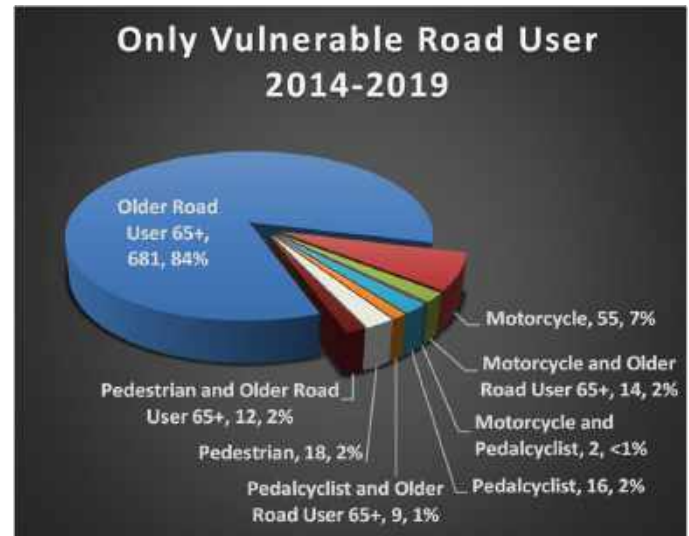


Figure 35 – Crashes only Involving Vulnerable Road User percentiles.

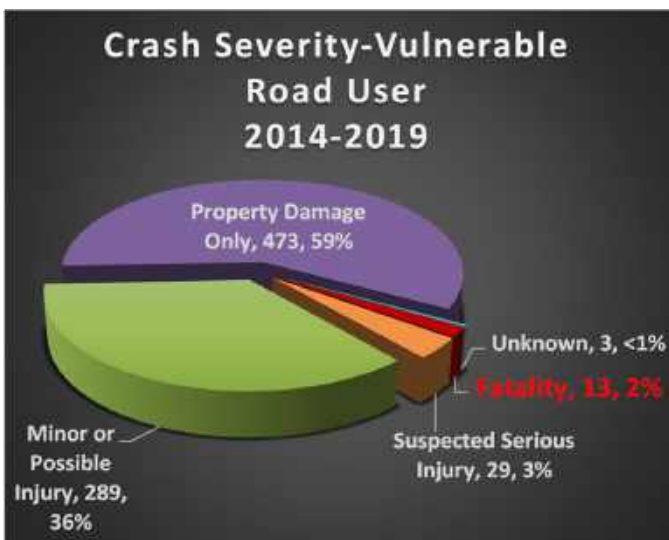


Figure 36 – Crash Severity - Vulnerable Road User percentiles.

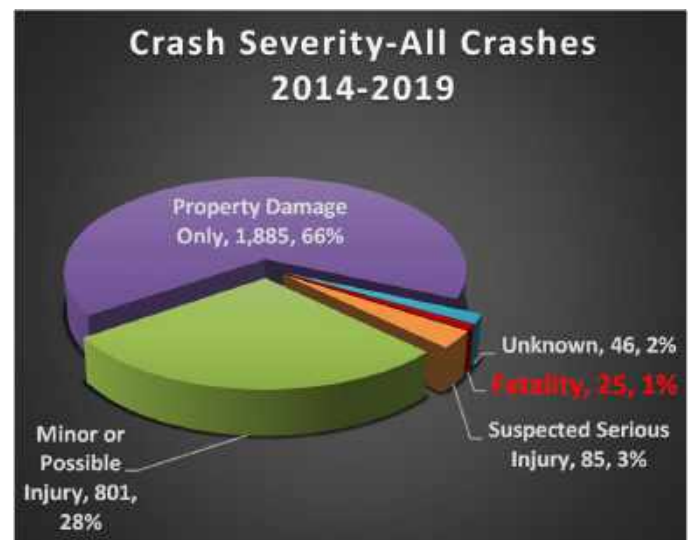


Figure 37 – Crashes Severity - All Crashes percentiles.



Figure 38 – Fatality crash and Vulnerable Road User totals.

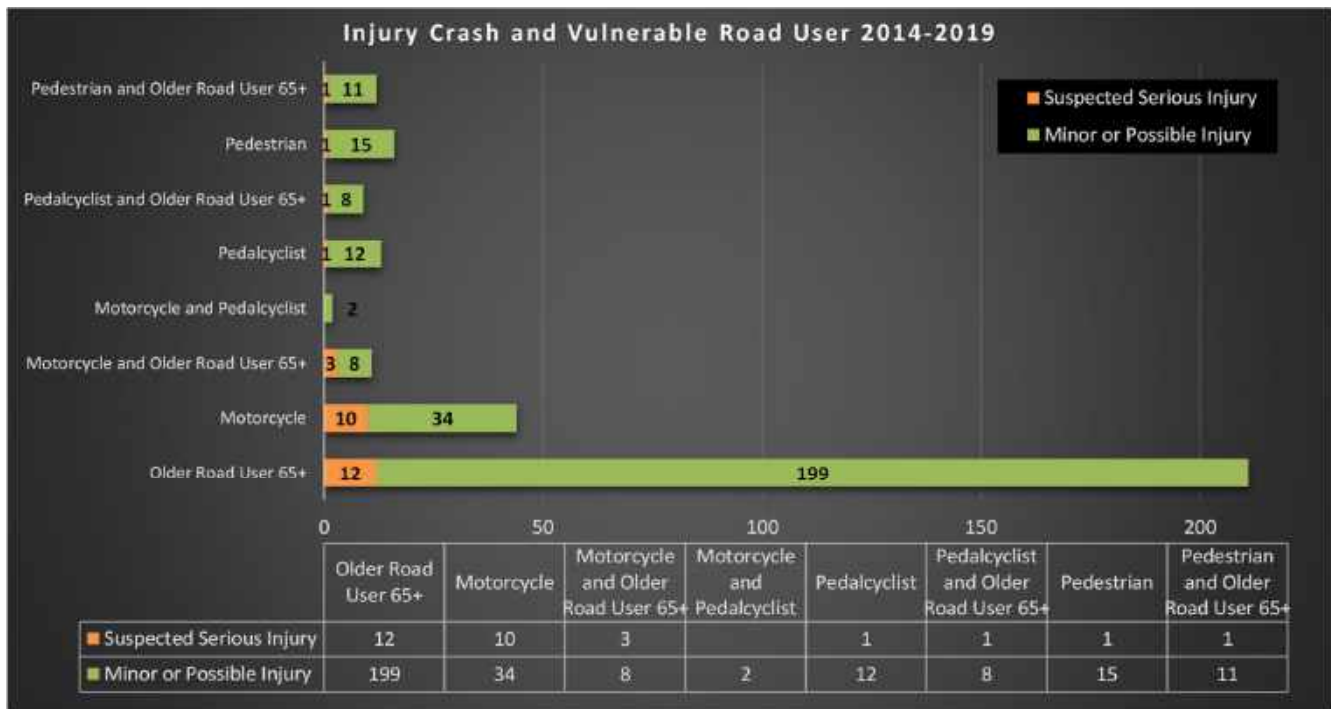


Figure 39 – Crashes only Involving Vulnerable Road User percentiles.



Figure 40 – Fatal crashes Involving Vulnerable Road Users and RDBs totals.

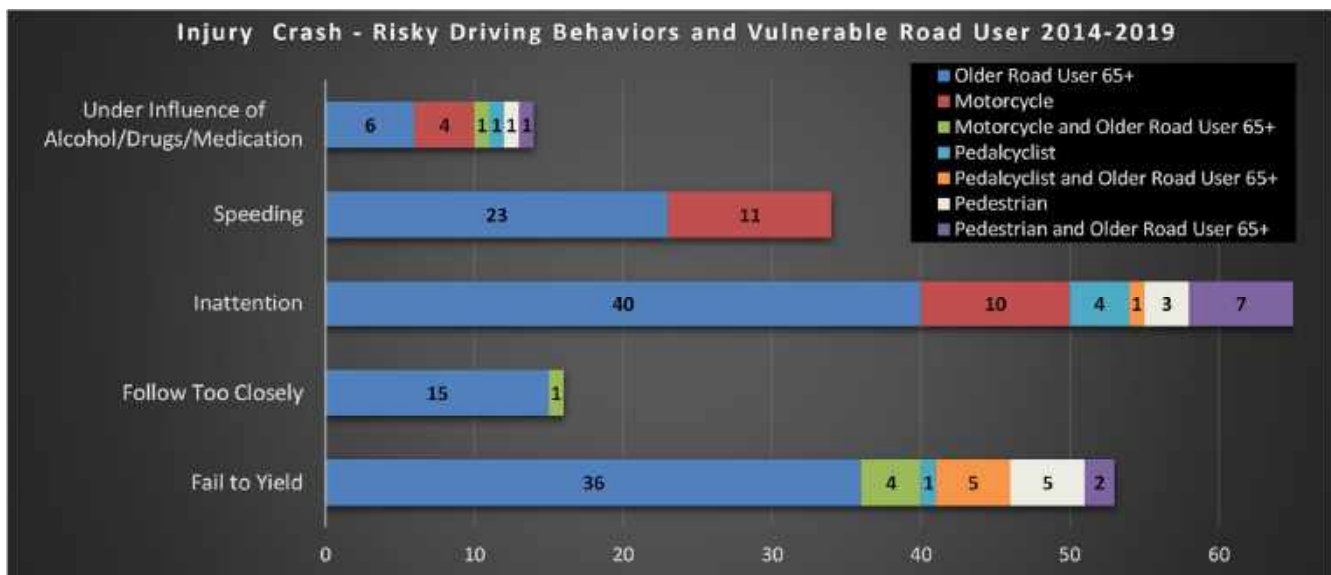


Figure 41 – Fatal crashes Involving Vulnerable Road Users and RDBs totals.

EMPHASIS AREAS

After reviewing the available data, 6 emphasis areas are selected for added attention in the transportation safety efforts of the Jamestown S’Klallam Tribe. These emphasis areas represent the most significant opportunities to accomplish the Tribe’s vision:

Emphasis Area 1 - Vulnerable Road Users

Emphasis Area 2 – Risky Driving Behaviors and Roadway Departure

Emphasis Area 3 – Rear-End Crashes

Emphasis Area 4 - Wildlife/Animal Crashes

Emphasis Area 5 – US 101 and Intersections with Local Roads

Emphasis Area 6 – Blyn Corridor Road improvements

Each emphasis area is described below and accompanied by a list of strategies that, if implemented, are expected to reduce the associated crashes and enhance safety. Each strategy is assigned to a department or task force that is responsible for implementation and evaluation.

EMPHASIS AREA 1 – VULNERABLE ROAD USERS

DESCRIPTION

Vulnerable Road Users include pedestrians, pedalcyclists, motorcyclists and drivers 65 or older. They are of concern to the Tribe. There is an increase in pedestrian and pedalcyclist usage on the Olympic Peninsula with the recent enhancements to the Olympic Discovery Trail.

From 2014-2019, 30 crashes involved pedestrians and 27 involved pedalcyclist (2.0% of total crashes). Of the 57 crashes, 3 were fatalities, 4 were serious injuries, 48 were minor injuries and 2 resulted in no injury.

In the 2016 Plan, 8 crashes involved pedestrians and 10 involved pedalcyclists (1.4% of total crashes). Of the 18 crashes, 4 were serious injuries, 13 were minor injuries and 1 resulted in no injury.

- 28% of all crashes involved Pedestrian, Pedalcyclist, Motorcyclist or 65 or older road users.
- 41% of crashes involving Vulnerable Road Users resulted in injury or death. Only 34% of all crashes resulted in injury or death, the rates are much higher for Vulnerable Road Users.
- There were 13 fatal crashes involving Vulnerable Road Users. 6 (46%) of the fatal crashes involved Drivers 65 or older.
- Drivers 65 or older represent 24% of all drivers and were in 84% of crashes involving Vulnerable Road Users, with 46% of fatal crashes, 65% of suspected serious injury, 78% of minor or possible injury.



Figure 42 – Crashes involving Vulnerable Road User percentiles.

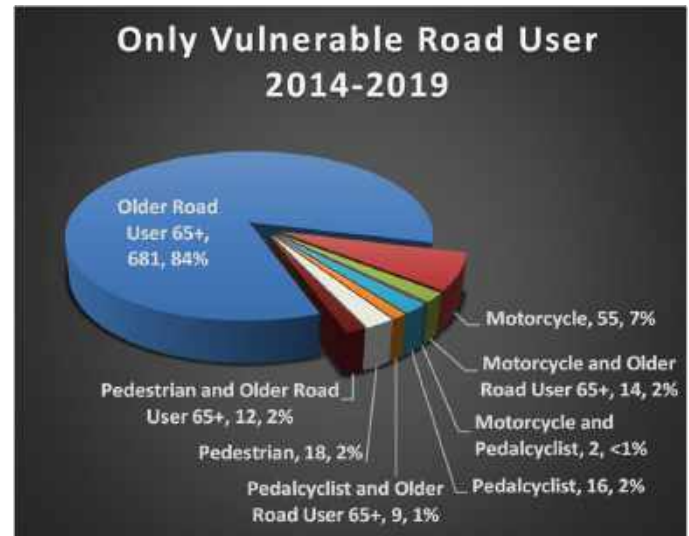


Figure 43 – Crashes only Involving Vulnerable Road User percentiles.

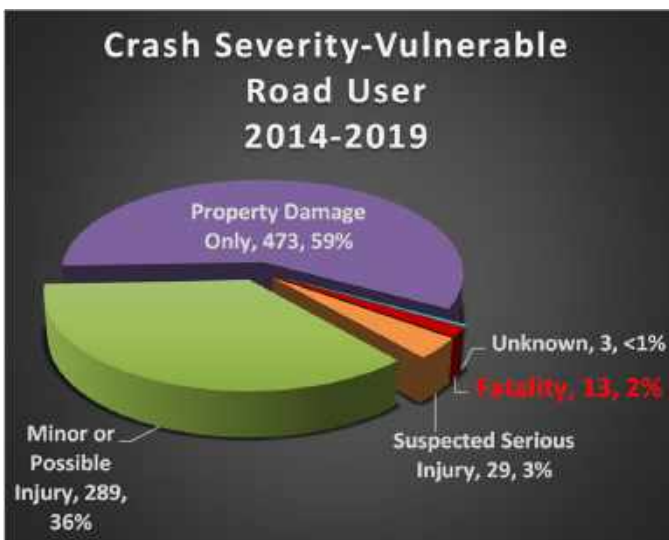


Figure 44 – Crash Severity - Vulnerable Road User percentiles.



Figure 45 – Crashes Severity - All Crashes percentiles.



Figure 46 – Fatality crash and Vulnerable Road User totals.

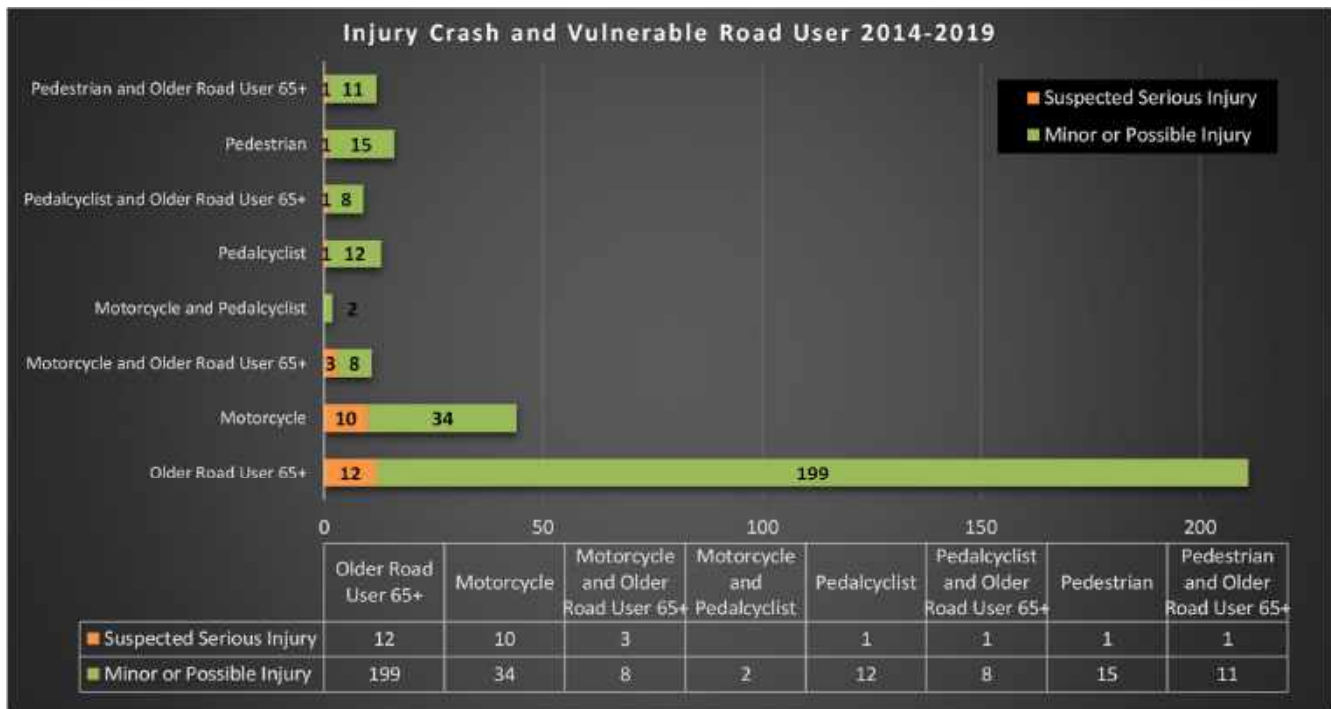


Figure 47 – Crashes only Involving Vulnerable Road User percentiles.



Figure 48 – Fatal crashes Involving Vulnerable Road Users and RDBs totals.

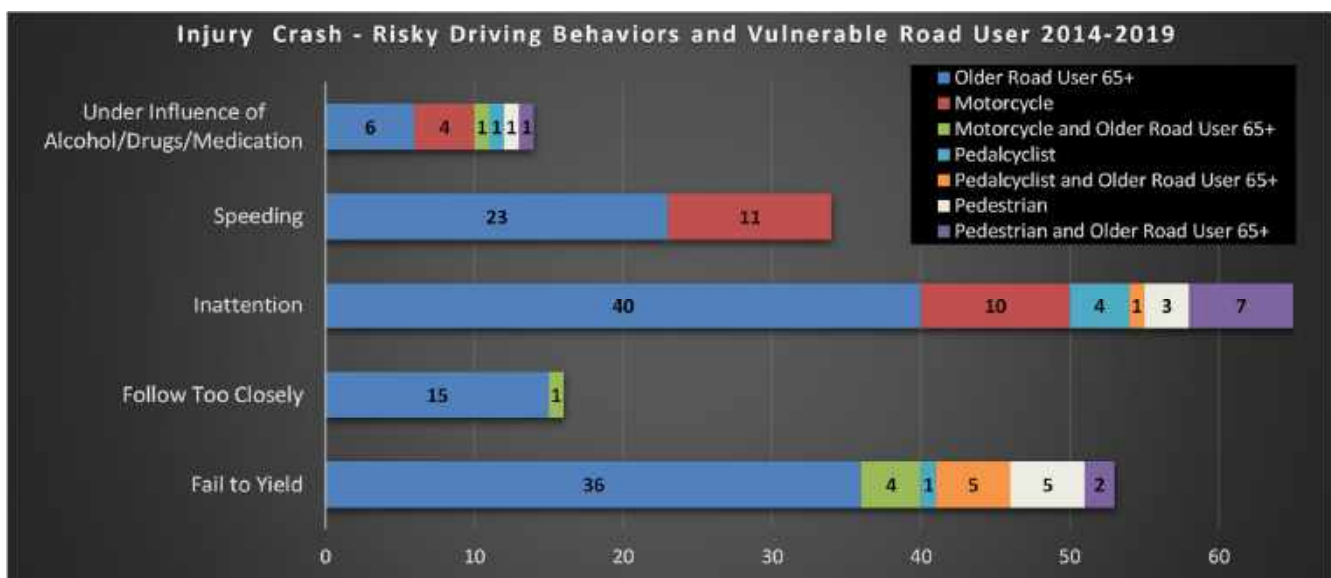


Figure 49 – Injury crashes Involving Vulnerable Road Users and RDBs totals.

Additional data analysis will be necessary to determine trends in the pedestrian and bicycle crashes. The following common factors need to be identified:

- Location Factors. Were there any hazards that need to be addressed in the crash locations?
- Human Factors. What were the human factors that contributed to the crash?
- Environmental Factors. What were the environmental factors in play?

This emphasis area identified in the Tribe's 2016 Long Range Transportation Plan as Prioritized Project Number 8:

8. *Comprehensive Pedestrian and Bicycle Plan (System Wide): With the many-programmed Tribal and area development plans, it is apparent that the connection of the tribal community members to the local goods and services (as supported by foot or bicycle travel) requires significant planning, expansion, and coordination. The pedestrian plan must focus on the condition rating of the existing trails and pathways utilized by the tribal citizens (youth to elders) to access services by foot. Additional study and community involvement will be required to also identify the desired connections that currently do not exist. In this plan, recreational trails should be strongly considered not only for the health and benefit of the Tribal and non-Tribal local communities but also for potential enhancement of commercial developments for the visiting public. Walking, hiking and biking trails provide a great opportunity for Tribal community enhancement by incorporating cultural education and preservation through interpretive signage, planned bench seating location with educational placards and interactive stations, the display of traditional tribal art, and environmental enhancement and education of plant and animal species. For extended hours of operation and use, path lighting should certainly be considered particularly here in the Pacific NW where we experience short days and overcast low light conditions seasonally. The plan should strongly consider connectivity to other internal and external paths and trails. There are many planned phases, for instance, of the Olympic Discovery Trail, which are identified as separate projects. In instances where the potential exists for vehicle-pedestrian conflicts, well-designed safe crossings and walkways should be implemented to discourage accidents. A significant challenge that will be faced by the Tribe through the study will be to gain project support and buy-in from those local, state, and regional agencies responsible for many of the roads traveling through the pedestrian trails and pathways transportation system. The LRTP identified significant challenges to pedestrian safety such as the Tribal community, administration, Tribal services, and economic develop areas bisection on either side of US101. Several local and county roads will likely require pedestrian facility upgrades required to improved driver education and warning of the presence of pedestrians and pedestrian facilities. Traffic calming measures must be implemented to warn the un-expecting vehicle driver of the proximity to the tribal community and increased likeliness of pedestrian traffic. Where possible and practical, designs must be implemented to reduce the potential for conflicts even if that includes the development of pathways and trails that do not share the same alignments of existing roads. This project is one of the highest priorities of the Tribal Public and Administration as supported by the recent pedestrian improvements near the tribal administration area, tunnel under US101, and extensions of the ODT. The effort must continue as new area developments and expansion occurs. The Tribe may utilize TTP funding to support this continued pedestrian transportation safety effort which significantly enhances the community and area.*

GOALS

Reduce the number of pedestrian and bicycle involved crashes by 10% by 2025.

PEDESTRIAN AND BICYCLE STRATEGIES

- Conduct a multidisciplinary Road Safety Audit (RSA) to analyze the locations identified as high pedestrian crash areas or areas with increased risk of crashes due to high pedestrian use and congestion. (Strategy Champion: Jamestown S'Klallam Transportation and Planning staff (JST Staff))
- Coordinate with the State DOT Safe Routes to School Program to develop a school walking plan. (Strategy Champions: JST Staff)
- Develop a Pedestrian and Bicycle Plan. (Strategy Champion: JST Staff)
- Continue to implement Pedestrian safety improvements to the transportation network as the Tribe has been through the last 10 years. (Strategy Champion: JST Staff)
- Seek funding to design pedestrian facility enhancements. (Strategy Champion: JST Staff).
- Apply for a Federal Highway Tribal Transportation Safety Grant in 2022 to secure funding for design and construction of improvements. (Strategy Champion: JST Staff)
- Work with Clallam County on the Olympic Discovery Trail (ODT) to develop a connector trail through Tribal Property on the East side of Diamond Point Road at the intersection with US 101 to tie in with a section, recently completed by the county, of the ODT that ties into Old Gardiner Road. This would tie into the existing ODT at the trailhead on the Westside of Diamond Point Road. (Strategy Champion: JST Staff and Clallam County Transportation Program Manager)

OLDER DRIVER AGE 65+ STRATEGIES

- Implement education program for the community encouraging enrollment in driving safety classes such as AAA's Roadwise Driver Course. These classes focus on tips and techniques for addressing factors typical among aging drivers including changing vision, reduced response times, and effects of prescription medications. (Strategy Champions: JST Staff, Sequim City Police, WSP)

EMPHASIS AREA 2 – RISKY DRIVING BEHAVIORS AND ROADWAY DEPARTURE

DESCRIPTION

Unsafe behaviors - Risky Driving Behaviors are becoming an accepted part of the culture on many tribal roads (based on an increase in crashes) and often cause serious injury or death. When analyzing the data, the Jamestown S'Klallam Tribe is, unfortunately, no exception.

Reducing fatal and serious injury crashes can be accomplished through identifying and deterring unsafe or risky behaviors by drivers and other transportation users, and preventing roadway departure.

The FHWA (Federal Highway Administration) defines Roadway Departure as a crash which a vehicle crosses an edge line, center line or otherwise leaves the traveled way. For this study, Roadway Departure crashes include those identified by WSDOT as Crash Types involving lane departure: Head-On, Over Embankment-No Guardrail Present, Overturned, Sideswipe, Struck Object, Struck Parked Vehicle and Primary Human Factor Preceding Crash of Over Center Line.

- 36% crashes involved Roadway Departure. Compared to 33% in 2016 plan.
- 76% of all fatal crashes and 56% of Suspected Serious injury crashes were Roadway Departure.



Figure 50 – All Crash Type percentiles.

The following Primary Human Factor Preceding Crash factors were used to define Risky Driving Behaviors (RDB) due to the high occurrence of crash fatalities or injuries: Inattention, Driver Impairment (Under Influence of Alcohol, Drugs or Medication or Apparently Asleep, Fatigued or Ill), Speeding, Fail to Yield, Follow Closely, and Distracted Driving. RDB's can have devastating consequences as a result of these behaviors are evident in the Crash data and result in severe consequences. WSDOT Crash data showed that from 2014-2019, 1,763 or 62% of all crashes involved RDB's, including 56% of fatal crashes and 72% of Injury crashes. Inattention and distracted driving was the highest RDB reported by far, involving 24% of All Primary Human Factors Preceding.

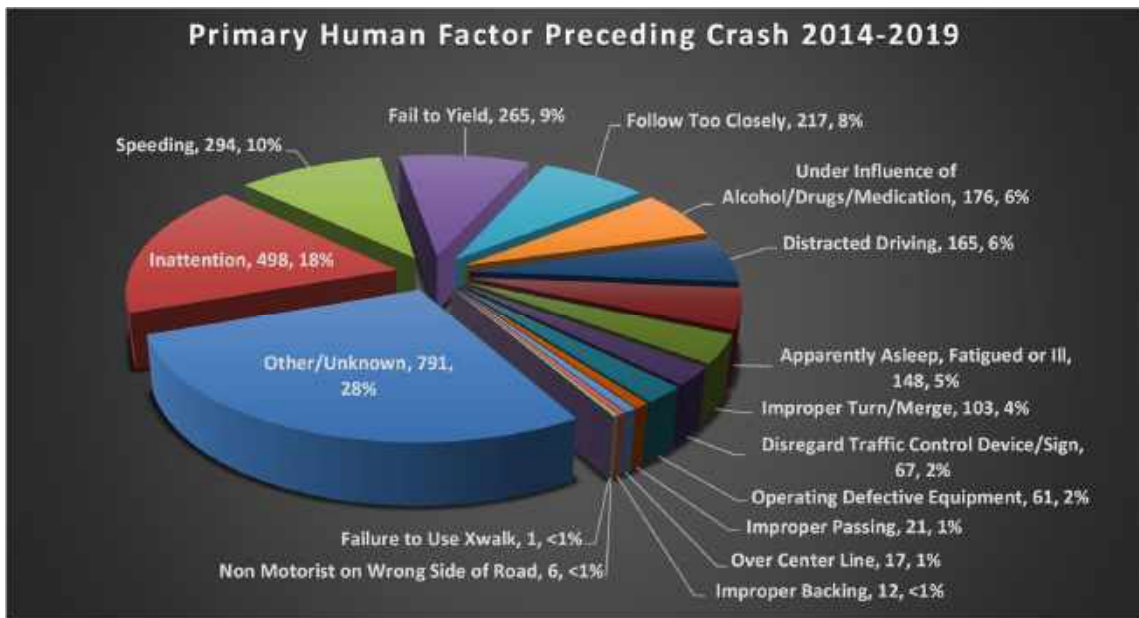


Figure 51 – All Human Factor Preceding Crash percentiles.

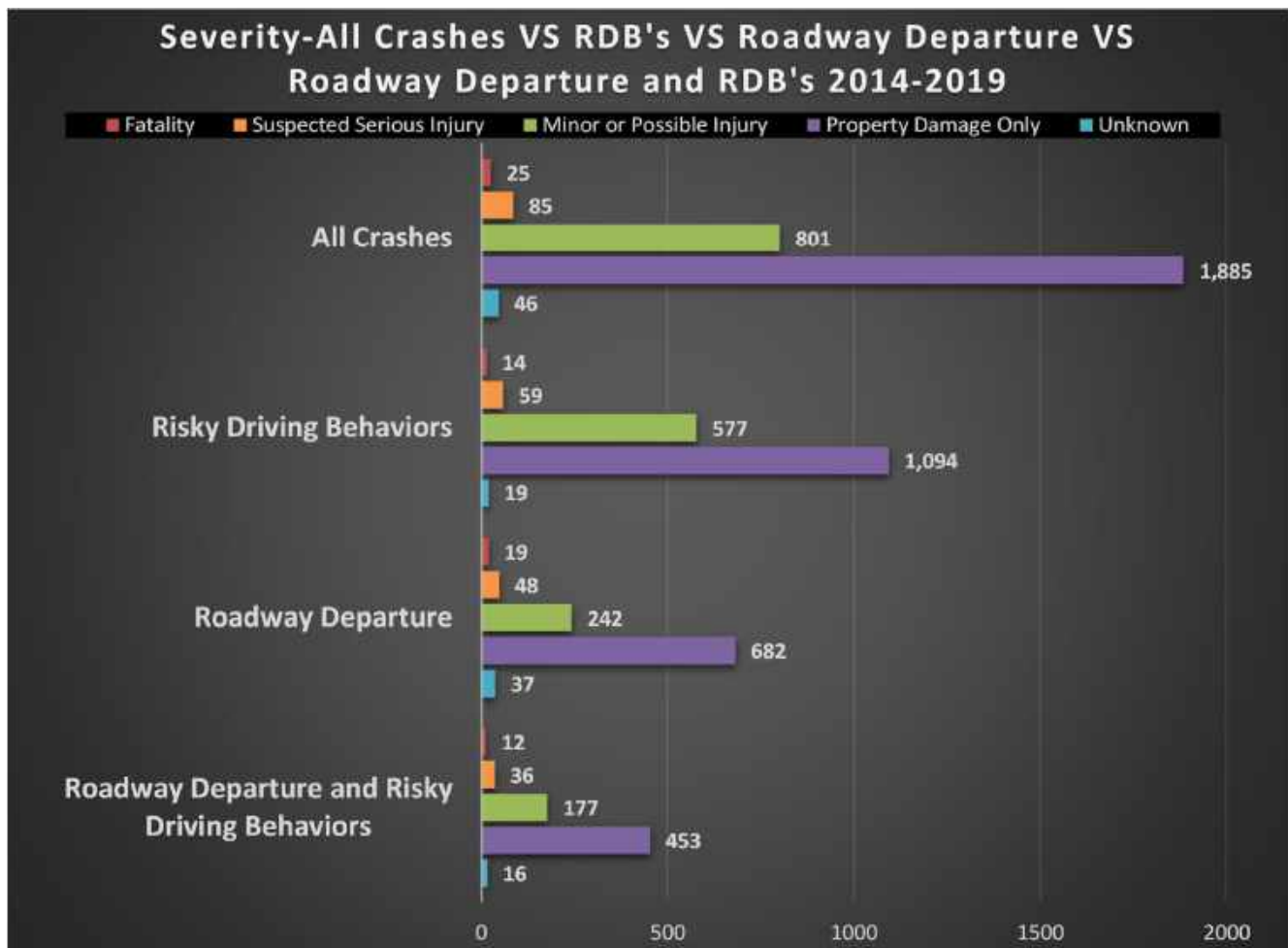


Figure 52 – Severity – All Crashes VS RDB's VS Roadway Departure totals.



Figure 53 – All Fatal and Injury crashes - Risky Driving Behavior totals.

- Drivers 65 or older had the highest rate of crashes involving Fail to Yield at 45%, Inattention at 28%, and Driver Impairment-Apparently Asleep, Fatigued or Ill at 34%.
- Drivers under 25 and 25-34 had the highest rate of crashes involving Follow Too Closely at 25% each.
- Drivers under 25 had the highest rate of crashes involving Speeding at 26%, and Distracted Driving at 23%.
- Drivers 25-34 had the highest rate of crashes involving Driver Impairment-Under Influence of Alcohol, Drugs or Medication.

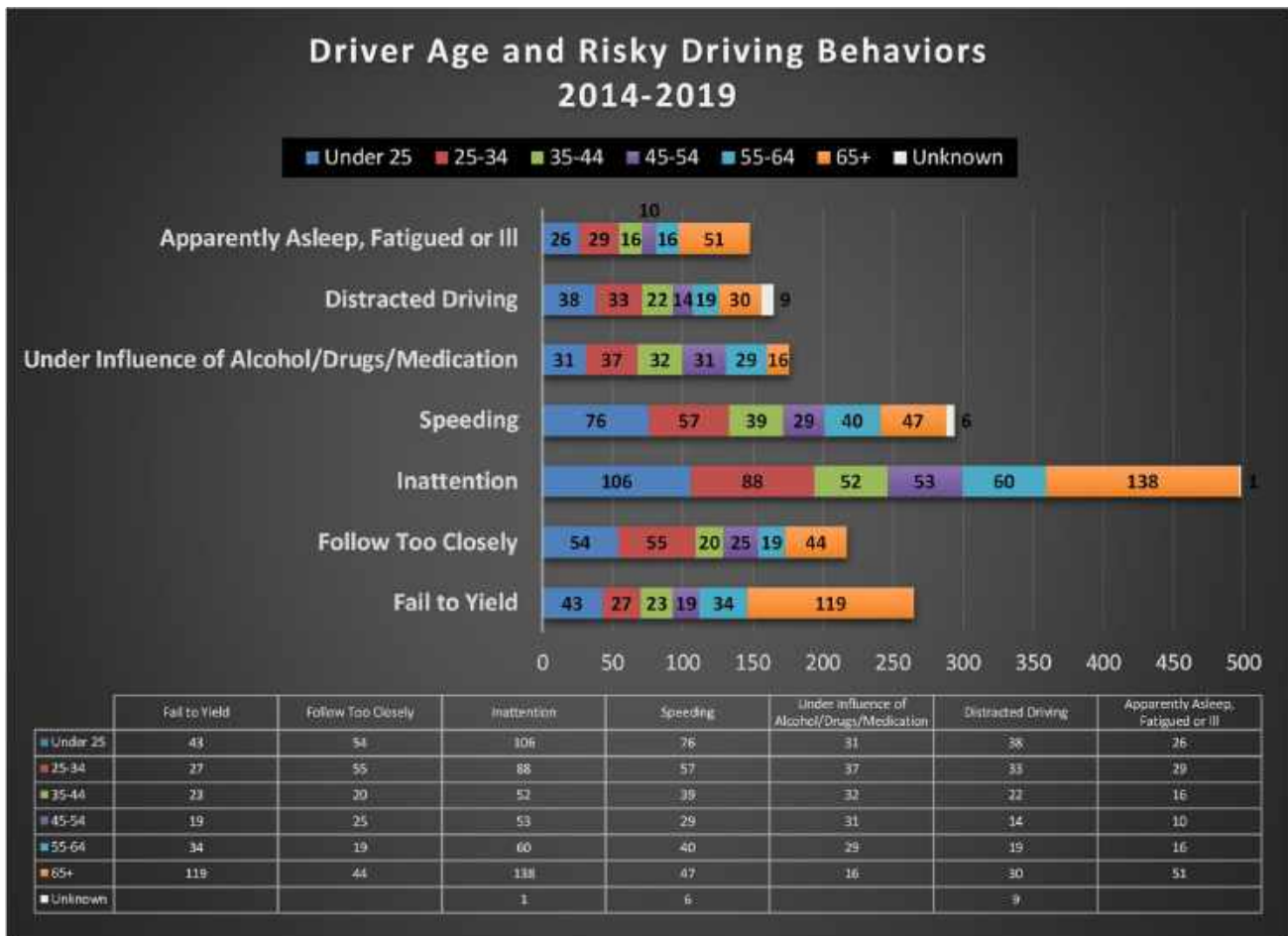


Figure 54 – All Driver Age and Risky Driving Behavior totals.

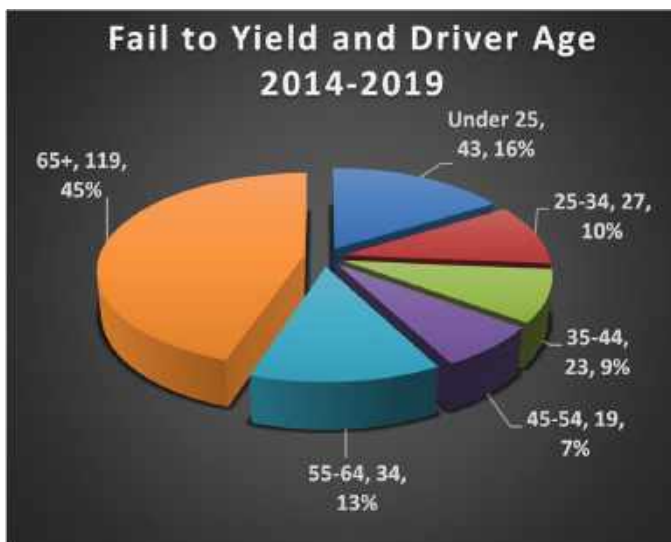


Figure 55 – All Fail to Yield and Driver Age percentiles.

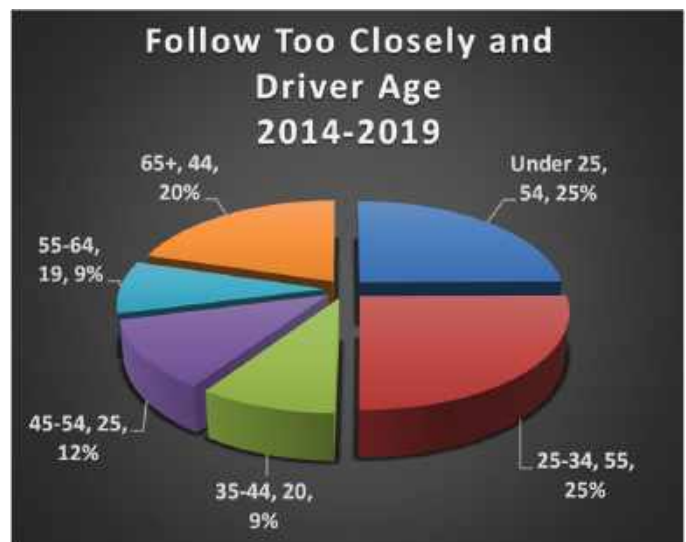


Figure 56 – All Follow Too Closely and Driver Age percentiles.

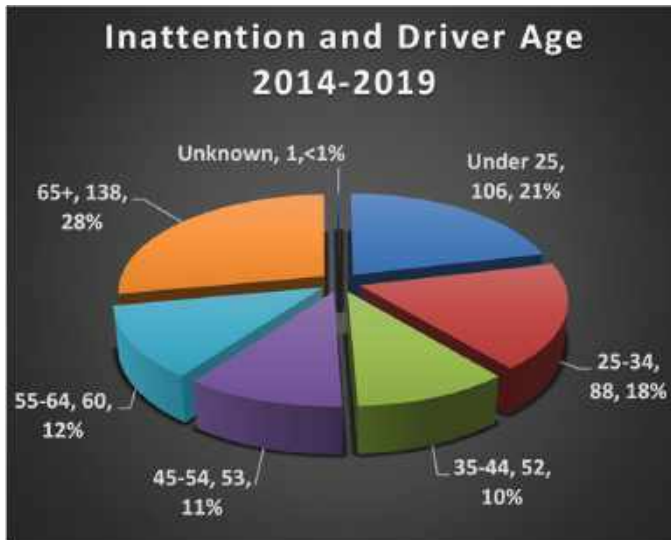


Figure 57 – All Inattention and Driver Age percentiles.

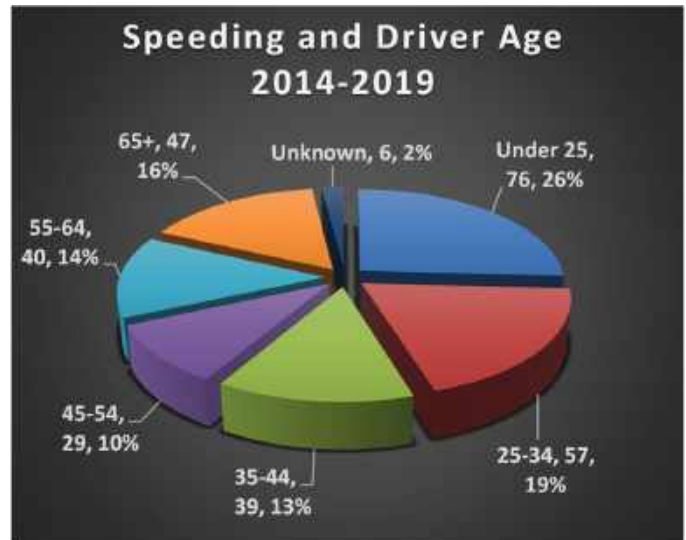


Figure 58 – All Speeding and Driver Age percentiles.

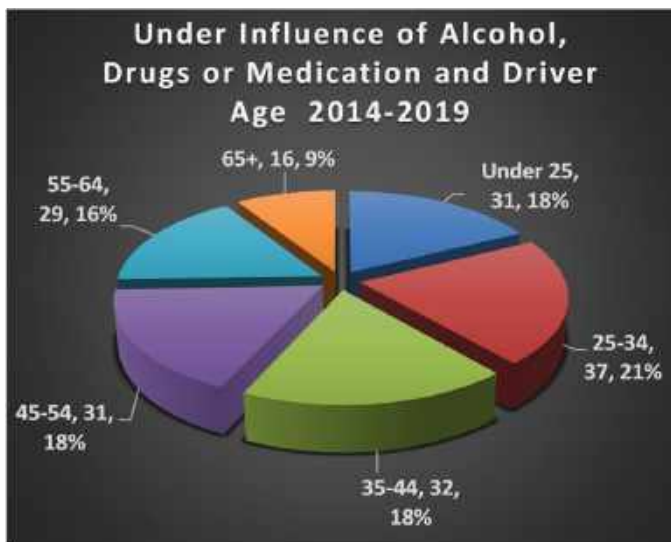


Figure 59 – All Under Influence of Alcohol, Drugs or Medication and Driver Age percentiles.

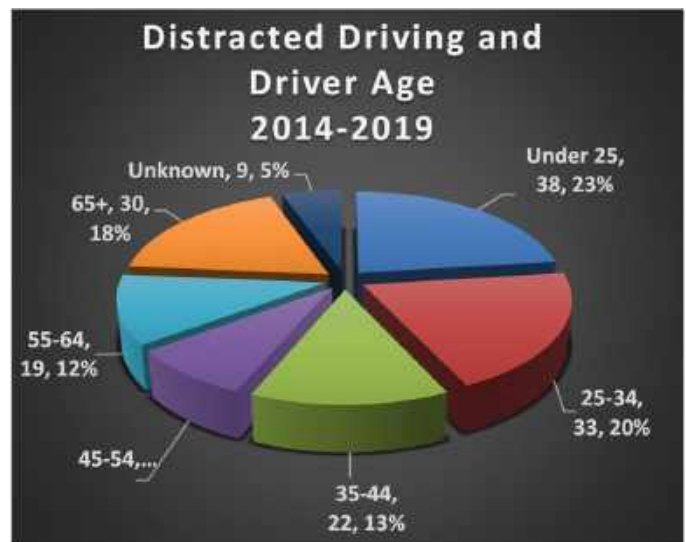


Figure 60 – All Distracted Driving and Driver Age percentiles.

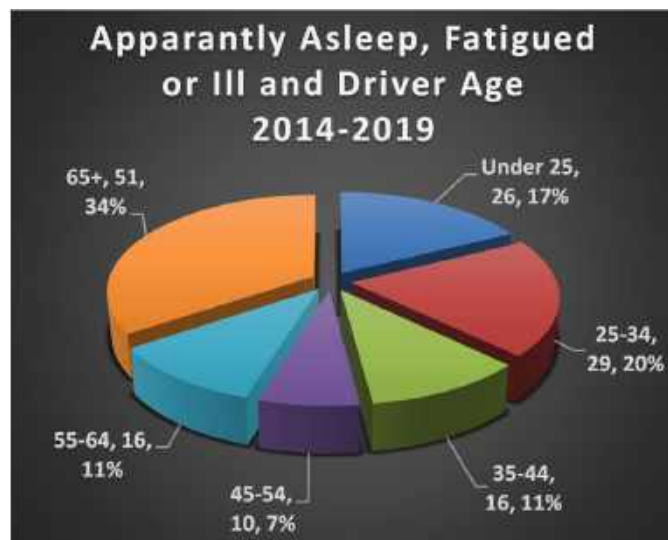


Figure 61 – All Apparently Asleep, Fatigued or Ill and Driver Age percentiles.

GOALS

Reduce Fatalities and Serious Injuries that involve Roadway Departure and Risky Driving Behaviors by at least 10% by 2025.

STRATEGIES

- Conduct a Systemic RSA that evaluates Roadway Departure risks including roadway condition, visibility, striping and signage in the community. The RSA should specifically evaluate locations with reported crashes involving Roadway Departure including Crash types: Head-On, Over Embankment-No Guardrail Present, Overturned, Sideswipe, Struck Object, and Struck Parked Vehicle. (Strategy Champions: JST Staff, City of Sequim, Clallam and Jefferson Counties, Sequim City Police, WSP)

ENFORCEMENT AND POLICY STRATEGIES

- Enforce Laws related to Inattention, Driver Impairment (Under Influence of Alcohol, Drugs or Medication or Apparently Asleep, Fatigued or Ill), Speeding, Fail to Yield, Follow Closely, and Distracted Driving. Renewed police patrols and presence at community concern locations for driving enforcement. (Strategy Champions: City of Sequim Police, WSP)
- Create a task force to evaluate, refine, and improve laws and policies for traffic enforcement in the community. The task force should specifically evaluate laws and policies concerning the following topics: Risky Driving Behaviors and Roadway Departure. Risky Driving Behaviors include Inattention, Driver Impairment (Under Influence of Alcohol, Drugs or Medication or Apparently Asleep, Fatigued or Ill), Speeding, Fail to Yield, Follow Closely, and Distracted Driving. (Strategy Champions: JST Staff, Clallam and Jefferson Counties, Sequim City Police, WSP)
- Intersection and Speed Management. Develop a policy for conducting studies to monitor intersection-driving behaviors and set speed limits. (Strategy Champions: JST Staff, Clallam and Jefferson Counties, Sequim City Police, WSP)

EDUCATION AND SAFETY CULTURE STRATEGIES

- Conduct additional data analysis to determine target audience for educational efforts. The data in this plan identifies that a significant percentage of crashes occurring are caused by drivers under 25 years of age, 25-34 and 65 years of age or older. This would be the initial target audience for a safety campaign marketed to the community to raise awareness that these behaviors are unacceptable. (Strategy Champions: JST Staff, City of Sequim, Clallam and Jefferson Counties, Sequim City Police, WSP)
- Create a media campaign using billboards and displays at the Tribe's Casino and Gas Station to encourage drivers to adopt a culture of safe driving. (Strategy Champions: JST Staff, Casino Public Relations)
- Implement an education program for the community that involves safety signage and a mock crash like the "Every 15-Minutes" program. (Strategy Champions: Sequim City Police, WSP)



EMPHASIS AREA 3– REAR-END CRASHES

DESCRIPTION

Approximately 25%, or 709, of all crashes were rear-end and were primarily caused by: speeding, following too close, and/or driver inattention. Of the 709 crashes, 270 serious/minor injuries (38%) occurred.

GOAL

Reduce rear end crashes by 10% by 2025.

STRATEGIES

- Develop a media campaign to emphasize: Slowing Down, Backing Off, and Paying Attention (Strategy Champions: JST Staff, WSP)
- Work with the WSP to increase enforcement patrols to reduce speeding. (Strategy Champions: JST Staff, WSP)
- Work with WSDOT to highlight signage needs on curves and areas within the Jamestown S’Klallam Study area. (Strategy Champions: JST Staff WSDOT)

EMPHASIS AREA 4 – WILDLIFE/ANIMAL CRASHES

DESCRIPTION

In the six years analyzed from January 2014 to December 2019, 331 reported crashes (12%) occurred involving animals and vehicles. While this percentage is lower than in 2016 plan at 233 crashes (19%) the actual number of crashes is higher by 98 crashes due to an increase in the ADT. Vehicle wildlife crashes nationwide cause approximately 200 deaths per year and the estimated cost to society is \$1.2 billion per year. Research completed by the Western Transportation Institute at Montana State University calculated the average total costs associated with an animal/vehicle crash for two species: \$7,890 for deer and \$17,100 for elk. Based on these numbers, the combined cost of these crashes is approximately \$4.1 million per year. The focus of this emphasis area is to identify how to reduce animal/vehicle crashes and to minimize the consequences when they occur. Two specific sections of US 101 were identified within 6.0 miles either side of Blyn, WA; approximately MP 266 to 270 (25 crashes) and MP 270.5 to 276.5 (20 crashes).

GOAL

Reduce crashes involving animals within reservation service area by 10% by 2023.



STRATEGIES

- Conduct a Road Safety Audit (RSA) on those areas with a higher incidence of animal/vehicle crashes (Strategy Champion: JST Staff)
- Work with the WSDOT and Washington Department of Fish and Wildlife (WDFW) to delineate and sign these areas to warn drivers of the hazards (Strategy Champion: JST Staff)

EMPHASIS AREA 5– US 101 AND INTERSECTIONS WITH LOCAL ROADS

DESCRIPTION

A Road Safety Audit (RSA) was performed in April 2014 on intersections with US 101, from MP 267 to MP 274.6. The RSA team reviewed fifteen intersections and developed countermeasures to improve the safety and reduce the crashes at each intersection. The realignment of the Chicken Coop Road/Zaccardo Road/US 101 intersection, completed in 2018, through a partnership between WSDOT and the Jamestown S’Klallam Tribe, was one of the projects. (See Appendix A - Jamestown S’Klallam SR 101 RSA).

Current crash data (2014-2019) was analyzed and compared with the crash data set (2009-2013) used in the 2014 RSA study for eight (8) of those intersections. The data sets included intersection, driveway or related crashes. Overall, the trend shows an increase in the total number of crashes, from thirty-two (32) from 2009-2013 to sixty-four (64) from 2014-2019. This is an annual average increase of 7 to 10.5 crashes per year from 2009-2019. Fatal/Injury crashes changed from 1 fatality and nineteen (19) injuries from 2009-2013, to no fatalities and twenty-nine (29) injuries from 2014-2019. Five of the intersections showed significantly higher crash rates: Palo Alto Rd, Louella Rd, Sophus Rd, Knapp/Guiles Rd, and Chicken Coop-Diamond Point Rd. These intersections remain a major concern to the Tribe. The increase in injury crashes at these intersections further reinforces the need for improvements to reduce crashes.

US 101 Roadway Safety Audit Intersection Crash History (2009-2013) vs Current Trends (2014-2019)																			
Cross road with US 101	Crash History 2009-2013 (5 years)								Crash History 2014-2019 (6 years)										
	# Crashes	Severity			Type				# Crashes	Severity			Type						
		Fatal	Injury	Property Damage Only	Angle	Head-On	Rear End	Animal		Fatal	Injury	Property Damage Only	Angle	Head-On	Rear End	Animal	Struck Object	Overturned	Turning
Palo Alto Rd	3	1	2		2		1		8		2	6	2		2	1	1	1	
Louella Rd	4		3	1	2		2		13		7	6	2	1	5		2		3
Schoolhouse Point Ln	3		2	1			2	1	5		3	2			4		1		
7 Cedars Casino Driveway	4		1	3	4				4		4		2		1				1
Sophus/Silva Rd	8		6	2	5	1		1	11		2	9	6		1		1		2
Pierce Rd	3		2	1			3		3		2	1			1		1		1
Knapp/Guiles Rd	1			1			1		9		6	3	2		4	1	2		
Chicken Coop-Diamond Point Rd	6		3	3	2	1	1		10		3	7	5		1	1			2
Total # Crashes	32	1	19	12	15	2	10	2	63	0	29	34	19	1	19	3	8	1	8

Figure 62 – US 101 Intersection Crash Data comparison 2009-2019.

GOAL

Reduce left turn crashes by 10% by 2025.

STRATEGIES

- Work with WSDOT and Clallam County to implement the countermeasures identified in the Jamestown S’Klallam SR 101 RSA. (Strategy Champions: JST Staff, WSDOT, Clallam County Transportation Program Manager)
- Apply for TTPSF funding to assist in the implementation of the improvements. (Strategy Champion: JST Staff)

- Work with WSDOT to add additional signs on curves and areas within the Jamestown S’Klallam Study area. (Strategy Champions: JST Staff, WSDOT)

EMPHASIS AREA 6– BLYN CORRIDOR TRANSPORTATION IMPROVEMENTS

DESCRIPTION

Old Blyn Road, MP 272.26, to the west side of the Casino/Resort at MP 270.6 where the Old Blyn Road ties back into US 101 is a very crowded corridor, with very high vehicle and pedestrian use. The area includes the Tribal government campus, Longhouse Market, and Casino/Hotel complex and many other driver distractions. There is heavy foot traffic within this area as Tribal members and visitors access the many Tribal facilities, creating a potential conflict between pedestrians, pedalcyclists and vehicles. There have been several near misses in the corridor. The realignment of the Chicken Coop Rd and Zaccardo Rd intersection reduced the congestion some, however more work needs to be done.

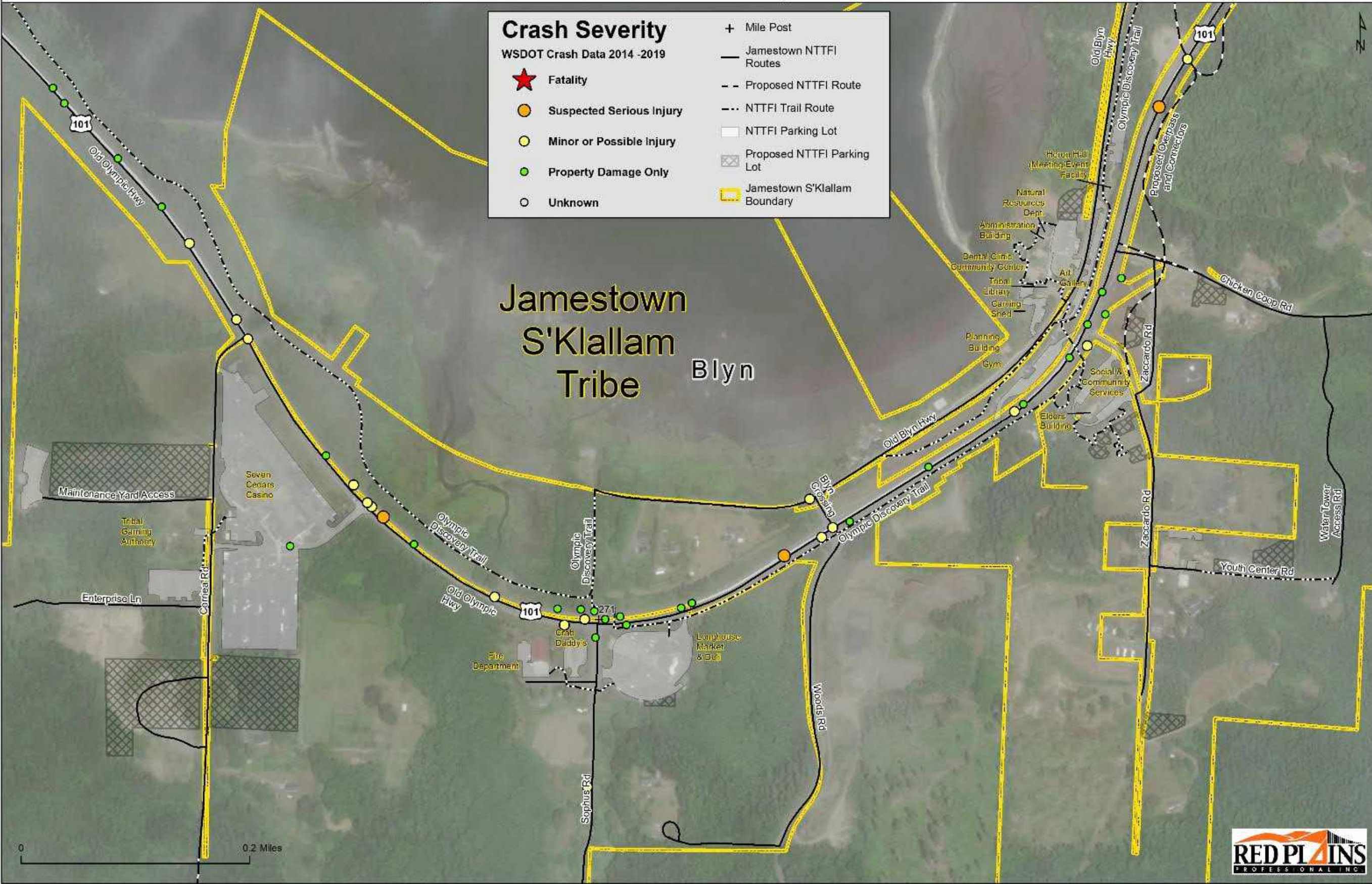
GOAL

Improve safe access for pedestrians, pedalcyclists, and vehicles within this corridor by 2023.

STRATEGIES

- Perform a traffic analysis through comparative analysis of the physical roadway conditions and the traffic, pedestrian and pedalcyclists behavior and movement conditions. The traffic analysis should include intersection level of service during peak hours, traffic volumes, traffic speeds, and general modeled traffic patterns. (Strategy Champions: JST Staff, WSDOT)

Blyn Corridor Road Improvements



IMPLEMENTATION AND EVALUATION

THE JAMESTOWN S'KLALLAM TRANSPORTATION SAFETY MANAGEMENT COMMITTEE

For this plan to be successful, it must be implemented and monitored, revisions to this plan will be necessary, as success will mandate change. The Safety Stakeholders will be interviewed on a yearly basis to discuss the Safety Plan and get their concerns/input. The JST Staff will review the STSP annually to evaluate progress toward each goal, discuss the progress of strategies that are being implemented, and consider any needed revisions/updates to this plan.

STRATEGY IMPLEMENTATION CHAMPIONS

The strategies listed above designate a champion for each strategy and this champion has the lead on implementation of that particular strategy. Many of the strategies may require an implementation plan be created that is separate from this document. As needed, the strategy champions will build an action plan for their strategy that outlines the implementation steps, schedule, and needed resources. The strategy champions will report to JST Staff on their strategy when updates are available or as requested.

APPENDICES

Jamestown S’Klallam

SR 101 Road Safety Audit

Prepared for
Jamestown S’Klallam Tribe

April 2014

Prepared by
Parametrix

Jamestown S’Klallam SR 101 Road Safety Audit

Prepared for

Jamestown S’Klallam
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Sequim, WA 98382

Prepared by

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CITATION

Parametrix. 2014. Jamestown S’Klallam
SR 101 Road Safety Audit. Prepared by Parametrix,
Bellevue, Washington. April 2014.

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- A Road Safety Audit Team
- B Maps

ACRONYMS

FHWA	Federal Highway Administration
HSM	Highway Safety Manual
MEV	million entering vehicles
MVM	million vehicle miles
RSA	road safety audit
WSDOT	Washington State Department of Transportation

1. INTRODUCTION

Jamestown S’Klallam Tribe (Tribe) initiated a road safety audit (RSA) along US 101 between Happy Valley Road (mile post 267) and Chicken Coop/Diamond Point Road (mile post 274.6). The Tribal governmental facilities and main economic enterprises are located adjacent to US 101. The Tribe is interested in improving the safety and mobility of this particular intersection for our Tribal Citizens accessing services, for residents living in the vicinity and for visitors to the Olympic Peninsula.

The RSA is a study that supplements WSDOT past safety improvements along mainline US 101, with the RSA focusing on County roads and driveways intersecting with US 101. The intent of the RSA is to understand the deficiencies and driver behaviors that contribute to collisions, and to identify the best safety improvements to reduce the frequency and severity of collisions in the future.

The Washington State Department of Transportation (WSDOT) has completed safety improvements along US 101 including this stretch of roadway over the past 10 years. These safety improvements were completed in an effort to reduce the collision rate. The eight mile section of US 101 corridor between Happy Valley Road and Diamond Point Road has a rate of 0.71 collisions per million vehicle miles (MVM); however some sections have much higher rates. The mile stretch of roadway for example approaching and departing Chicken Coop Road/Zaccardo Road intersection area has a collision rate of 1.34 per MVM. The state average for a similar roadway is 1.11 collisions per MVM. WSDOT recently reduced the posted speed limit for a portion of the study area from 55 mph to 50 mph to address concerns raised by the public and the Tribe related to near miss collisions, and to address major collisions that resulted in loss of property or injury.

2. OVERVIEW OF THE STUDY AREA

The US 101 corridor loops around the Olympic Peninsula and Olympic National Park, providing access to dozens of cities, communities, parks and recreational land uses. The corridor runs through the Clallam, Jefferson, Grays Harbor, and Mason counties. Tourism is one of the Tribe’s and the North Olympic Peninsula’s primary economic drivers. US 101 supports over 5 million vehicles annually, mainly visitors to Olympic National Park, and to the North Olympic Peninsula’s other increasingly popular attractions such as river and saltwater fishing, local community festivals (e.g. Sequim’s Lavender Festival, Port Angeles’ Crab Festival, etc.), as well as to the nine tribal nations located on the Olympic Peninsula.

Within the nearly eight mile study area, the US 101 corridor land use includes single family residences, commercial, and community center land uses including Jamestown S’Klallam 7 Cedars resort, gas station and deli, and visitor centers.

The study area included the following intersections with US 101:

- Happy Valley Road
- Palo Alto Road
- W Sequim Bay Road
- Barker Road
- Louella Road
- Schoolhouse Point Lane
- Corriea Road

- 7 Cedars Casino Driveway
- Sophus Road
- Chicken Coop-Zaccardo Road
- Blyn Road
- Old Blyn Highway
- Pierce Road
- Knapp/Guiles Road
- Chicken Coop-Diamond Point Road

3. ROAD SAFETY AUDIT PROCESS AND TEAM

As described by the Federal Highway Administration (FHWA), an RSA is the formal safety performance examination of an existing or future road or intersection by an independent, multidisciplinary team. The RSA identifies potential existing safety issues and opportunities for improvement.

An RSA differs from a standard safety analysis in that it is performed by an independent team of experts representing multiple disciplines, considers human factors (such as driving at excessive speeds or nighttime visibility), and findings are documented in a formal report.

The RSA process includes the following steps and is described in FHWA Road Safety Audit Guidelines (FHWA publication FHWA-SA-06-06).

- Identify project
- Select RSA team
- Conduct start-up meeting
- Collect historical data for collisions and traffic volumes
- Perform field reviews
- Conduct analysis and prepare report
- Present findings to Project Owner
- Prepare formal response
- Incorporate findings

The RSA study team for the Jamestown S'Klallam US 101 project includes a Tribe representative, a WSDOT traffic safety and operations engineer, and three consultant engineers. The engineers completing the audit each have over fifteen years of experience in roadway design or traffic operations analysis.

Clallam County was invited to participate but were unavailable for the field work. Traffic volumes, collision history, and local operational issues were provided by the County to supplement data collected.

4. FINDINGS AND RECOMMENDATIONS

The following summarizes for each intersection, the existing conditions and the countermeasures identified by the RSA team. The countermeasures are rated as low/medium/high effort (or cost) and benefit. The level of benefit for some improvements was determined using the Highway Safety Manual (HSM).

The following summary table (Exhibit 1) summarizes the existing issues and opportunities for safety improvements. Exhibits 2 through 16 describe the collision

history (rates, type, and severity), volumes, and countermeasures for the study intersections. The most typical collision types are vehicles rear-ending another vehicle turning from US 101 onto a side road (where a left or right turn pocket did not exist) and collisions as vehicles enter US 101 from side roads. Countermeasures identified include installing left or right turn lanes, and adding illumination. There are a few locations where all capacity improvements are already installed. The countermeasures for these locations could include signalization, closing access, or constructing a roundabout to reduce conflict points and speeds.

Deer and elk regularly cross the US 101 corridor within the study area. Reviewing the collision trends along US 101, collisions with wildlife is a frequent occurrence both at intersections and along US 101 between intersections. However this RSA focuses on vehicular collisions with other vehicles or persons rather than minimizing wildlife collisions.

Exhibit 1. Summary of Existing Conditions and Opportunities

Cross road with US 101		Existing Conditions (X) and Opportunities (O)											
		US 101							Cross Road		Intersection		
		Improve sight distance	Illumination	Left turn lane	Left turn receiving lane (refuge)	Right turn lane	Right turn receiving	Shoulder widening	Modify approach skew	Modify approach grade	Signal	Roundabout	Close access
1	Happy Valley Rd	x	O	O	O				O				
2	Palo Alto Rd	x	O	O	O				O				
3	W Sequim Bay Rd	O	O						O				
4	Barker Rd	x	O	O	O					O			
5	Louella Rd	x	O	O	O			O					
6	Schoolhouse Point Ln	x	O	O	O								
7	Corriea Rd	x	O	O	O	O							O
8	7 Cedars Casino Driveway	x	x	x	x	x						O	
9	Sophus Rd	x	x	x	x	x						O	
10	Chicken Coop-Zaccardo Rd	x	O	O	O	O		O	O				O
11	Blyn Rd	x											O
12	Old Blyn Hwy	x		O	O								
13	Pierce Rd	x		O	O								
14	Knapp/Guiles Rd	x		O									
15	Chicken Coop-Diamond Point Rd	x	O									O	

Exhibit 2. Happy Valley Road Detail

Happy Valley Road / US 101

Collision History (2009 – 2013)

Rate	0.13 collisions per million entering vehicles
Severity	4 collisions, 1 resulted in injury
Type	3 rear end (left turns from main road to side road) 1 entering at angle

Volume on major road: 17,000 ADT

Volume on minor road: 350 ADT

Existing Conditions	Countermeasures
<p>Adequate sight distance is available.</p> <p>Collisions typically occur as vehicles exceed safe speeds.</p> <p>There is no left turn pocket provided from westbound US 101 onto Happy Valley Road.</p> <p>Happy Valley Road skewed (70 degrees) at intersection with US 101. This is within the guidelines of 60 to 120 degrees.</p> <p>No illumination at intersection.</p>	<ol style="list-style-type: none"> Widen section to provide a left turn pocket from westbound US 101 onto Happy Valley Road and receiving refuge lane for turns onto US 101. <i>Level of Effort: Medium</i> <i>Potential Safety Benefit: High (44% reduction in collisions)</i> Install illumination <i>Level of Effort: Medium</i> <i>Potential Safety Benefit: Low (5% reduction in collisions)</i> Reconstruct intersection approach to minimize skew. <i>Level of Effort: Medium</i> <i>Potential Safety Benefit: Medium (10% reduction in collisions)</i>

View North onto US 101



View South onto US 101



Exhibit 3. Palo Alto Road Detail

Palo Alto Road / US 101

Collision History (2009 – 2013)

Rate	0.10 collisions per million entering vehicles
Severity	3 collisions, 2 resulted in injury and 1 resulted in fatality
Type	2 entering at angle 1 rear end (left turn from main road to side road)

Volume on major road: 16,000 ADT

Volume on minor road: 800 ADT

Existing Conditions	Countermeasures
<p>Adequate sight distance is available.</p> <p>Collisions typically occur as vehicles exceed safe speeds.</p> <p>There is no left turn pocket provided from westbound US 101 onto Palo Alto Road.</p> <p>Palo Alto Road intersects US 101 with a skew of 30 degrees.</p> <p>No illumination at intersection.</p>	<ol style="list-style-type: none"> Widen section to provide a left turn pocket from westbound US 101 onto Palo Alto Road and receiving refuge lane for turns onto US 101. <i>Level of Effort: Medium</i> <i>Potential Safety Benefit: High (44% reduction in collisions)</i> Install illumination <i>Level of Effort: Medium</i> <i>Potential Safety Benefit: Low (no reduction, all collisions during the day)</i> Reconstruct intersection approach to minimize skew. <i>Level of Effort: Medium</i> <i>Potential Safety Benefit: Medium (28% reduction in collisions)</i>

View North onto US 101



View South onto US 101



Exhibit 4. W Sequim Bay Road Detail

W Sequim Bay Road / US 101

Collision History (2009 – 2013)

Rate	0.03 collisions per million entering vehicles
Severity	1 collision resulted in fatality
Type	1 vehicle strikes object

Volume on major road: 16,000 ADT

Volume on minor road: 2,000 ADT

Existing Conditions	Countermeasures
<p>Adequate sight distance is available. However, the guardrail on the north side of the roadway, east of W Sequim Bay Road, potentially obstructing sight distance for southbound vehicles turning onto US 101.</p> <p>W Sequim Bay road intersects US 101 with a skew (40 degrees).</p> <p>Stop bar located 8-9 feet back from roadway.</p> <p>Steep grade on W Sequim Bay road approaching US 101.</p> <p>No illumination at intersection.</p>	<ol style="list-style-type: none"> Remove barrier to improve sight distance. <i>Level of Effort: Low</i> <i>Potential Safety Benefit: Medium</i> Install illumination <i>Level of Effort: Medium</i> <i>Potential Safety Benefit: Medium (15% reduction in collisions)</i> Reconstruct intersection approach to minimize skew. <i>Level of Effort: Medium</i> <i>Potential Safety Benefit: Medium (24% reduction in collisions)</i>

View North onto US 101



View South onto US 101



Exhibit 5. Barker Road Detail

Barker Road / US 101

Collision History (2009 – 2013)

Rate	0.07 collisions per million entering vehicles
Severity	2 collisions with no injury
Type	1 rear end (left turn from main road to side road) 1 vehicle strikes deer

Volume on major road: 16,000 ADT

Volume on minor road: < 50 ADT

Existing Conditions	Countermeasures
<p>Two-way left turn lane tapers out at the intersection. (Merging movements potentially occurring at intersection).</p> <p>Steep grade on Barker Road approaching US 101.</p> <p>Adequate sight distance is available.</p> <p>Collisions typically occur as vehicles exceed safe speeds.</p> <p>No illumination at intersection.</p>	<ol style="list-style-type: none"> 1. Widen roadway to allow left turn lane to extend south of intersection. <i>Level of Effort: High</i> <i>Potential Safety Benefit: High (56% reduction in collisions)</i> 2. Restripe so left turn lane fully merges north of the intersection. <i>Level of Effort: Low</i> <i>Potential Safety Benefit: Medium</i> 3. Install illumination <i>Level of Effort: Medium</i> <i>Potential Safety Benefit: Low (all collisions occurred during the day)</i> 4. Reduce grade approaching US 101 <i>Level of Effort: Medium</i> <i>Potential Safety Benefit: Medium (14% reduction in collisions)</i>

View North onto US 101



View South onto US 101



Exhibit 6. Louella Road Detail

Louella Road / US 101

Collision History (2009 – 2013)

Rate	0.14 collisions per million entering vehicles
Severity	4 collisions with 3 resulted in injury
Type	2 entering at angle (entering from side road to main road) 2 rear end (left turns from main road to side road)

Volume on major road: 16,000 ADT

Volume on minor road: 450 ADT

Existing Conditions	Countermeasures
<p>No left turn pocket for northbound vehicles turning from US 101 to Louella Road. Guardrail exists along the east side of roadway. No refuge for northbound vehicles approaching a northbound left-turn vehicle.</p> <p>Stream crossing exists south of intersection.</p> <p>Adequate sight distance is available.</p> <p>Collisions typically occur as vehicles exceed safe speeds.</p> <p>No illumination at intersection.</p>	<ol style="list-style-type: none"> 1. Widen roadway to construct a northbound left turn lane and receiving refuge lane. <i>Level of Effort: High</i> <i>Potential Safety Benefit: High (44% reduction in collisions)</i> 2. Remove guardrail and widen shoulder to allow for refuge or recovery area. <i>Level of Effort: Medium</i> <i>Potential Safety Benefit: Medium (13% reduction in collisions)</i> 3. Install illumination <i>Level of Effort: Medium</i> <i>Potential Safety Benefit: Low (6% reduction in collisions)</i>

View North onto US 101



View South onto US 101



Exhibit 7. Schoolhouse Point Lane

Schoolhouse Point Lane / US 101

Collision History (2009 – 2013)

Rate	0.10 collisions per million entering vehicles
Severity	3 collisions with 2 resulted in injury
Type	2 rear end (left turns from main road to side road) 1 vehicle strikes deer

Volume on major road: 16,000 ADT

Volume on minor road: 400 ADT

Existing Conditions	Countermeasures
<p>Wide shoulders exist on the north side of US 101 through the Schoolhouse Point Lane intersection.</p> <p>No left turn lane is provided for vehicles turning off US 101 onto Schoolhouse Point Lane.</p> <p>Adequate sight distance is available.</p> <p>Collisions typically occur as vehicles exceed safe speeds.</p> <p>No illumination at intersection.</p>	<ol style="list-style-type: none"> Widen roadway to construct a south-eastbound left turn lane and receiving refuge lane. <i>Level of Effort: Medium</i> <i>Potential Safety Benefit: High (44% reduction in collisions)</i> Install illumination <i>Level of Effort: Medium</i> <i>Potential Safety Benefit: Low (collisions occurred during the day)</i>

View West onto US 101



View East onto US 101



Exhibit 8. Corriea Road Detail

Corriea Road / US 101

Collision History (2009 – 2013)

Rate	0.03 collisions per million entering vehicles
Severity	2 collisions with 1 resulted in injury
Type	1 entering at angle (entering from side road to main road) 1 rear end (right turn from main road to side road)

Volume on major road: 16,000 ADT

Volume on minor road: 200 ADT

Existing Conditions	Countermeasures
<p>Corriea Road west of US 101 is a low volume road providing a second access point to and from the 7 Cedars Casino and two residences.</p> <p>A solid white stripe exists along US 101 as it intersects with Corriea Road. This indicates only right in/right out movements are allowed although left turn movements are observed.</p> <p>Corriea Road east of US 101 provides access to a single family residence and a commercial lot.</p> <p>Adequate sight distance is available.</p> <p>Collisions typically occur as vehicles exceed safe speeds.</p> <p>No illumination at intersection.</p>	<ol style="list-style-type: none"> 1. Close access to US 101. <i>Level of Effort: Low</i> <i>Potential Safety Benefit: High</i> 2. Widen roadway to construct an eastbound right turn pocket. <i>Level of Effort: Medium</i> <i>Potential Safety Benefit: Medium (14% reduction in collisions)</i> 3. Widen roadway to construct an eastbound left turn lane and receiving refuge lane. <i>Level of Effort: Medium</i> <i>Potential Safety Benefit: High (44% reduction in collisions)</i> 3. Install illumination <i>Level of Effort: Medium</i> <i>Potential Safety Benefit: Medium (15% reduction in collisions)</i>

View West onto US 101



View East onto US 101



Exhibit 9. 7 Cedars Casino Driveway

7 Cedars Casino Driveway / US 101

Collision History (2009 – 2013)

Rate	0.13 collisions per million entering vehicles
Severity	4 collisions with 1 resulted in injury
Type	4 entering at angle (entering from side road to main road)

Volume on major road: 16,000 ADT

Volume on minor road: 1,000 ADT

Existing Conditions	Countermeasures
<p>Left and right turn lanes are provided at the driveway and along US 101. There is a refuge or receiving lane on US 101 for vehicles exiting the driveway northbound and turning westbound onto US 101.</p> <p>Adequate sight distance is available.</p> <p>Collisions typically occur as vehicles exceed safe speeds.</p>	<p>1. Install a roundabout.</p> <p><i>Level of Effort: High</i> <i>Potential Safety Benefit: High</i> <i>(71% reduction in collisions)</i></p>

View West onto US 101



View East onto US 101



View North from Driveway



Exhibit 10. Sophus Road Detail

Sophus Road / US 101

Collision History (2009 – 2013)

Rate	0.29 collisions per million entering vehicles
Severity	8 collisions with 6 resulted in injury
Type	5 entering at angle (entering from side road to main road)
	1 WB L hit by EB vehicle
	1 vehicle strikes deer
	1 vehicle strikes object

Volume on major road: 14,000 ADT

Volume on minor road: 2,000 ADT (estimated)

Existing Conditions	Countermeasures
<p>Left and right turn lanes are provided on Sophus Road and along US 101. There is a refuge or receiving lane on US 101 for vehicles entering westbound US 101 from Sophus Road.</p> <p>Adequate sight distance is available.</p> <p>Collisions typically occur as vehicles exceed safe speeds.</p>	<p>1. Install a roundabout.</p> <p><i>Level of Effort: High</i> <i>Potential Safety Benefit: High (71% reduction in collisions)</i></p>

View West onto US 101



View East onto US 101



Exhibit 11. Chicken Coop Road/Zaccardo Road Detail

Chicken Coop Road / Zaccardo Road / US 101

Collision History (2009 – 2013)

Rate	0.16 collisions per million entering vehicles (0.15 for 2004 to 2013)
Severity	4 collisions with 1 resulted in injury
Type	2 rear ends (turn from main road to side roads) 1 entering at angle (entering from side road to main road) 1 vehicle strikes object

Volume on major road: 14,000 ADT

Volume on minor road: 300 ADT (Chicken Coop Road), 100 ADT (Zaccardo)

Existing Conditions	Countermeasures
<p>Within a few hundred feet there are four access points to US 101. This includes Zaccardo Road, the driveway to the Tribal center, and Chicken Coop Road. The Chicken Coop Road intersection includes a “spur” or two locations to turn onto and off of US 101.</p> <p>Chicken Coop Road intersects US 101 with a skew (angle).</p> <p>Adequate sight distance is available.</p> <p>Collisions typically occur as vehicles exceed safe speeds.</p> <p>No illumination at intersection.</p>	<ol style="list-style-type: none"> 1. Realign Zaccardo Road to T with Chicken Coop Road. Reduces access points. <i>Level of Effort: Medium / Potential Safety Benefit: High</i> 2. Install illumination <i>Level of Effort: Medium / Potential Safety Benefit: Low (8% reduction in collisions)</i> 3. Add left turn lane from US 101 onto Chicken Coop Road <i>Level of Effort: Medium / Potential Safety Benefit: High (44% reduction in collisions)</i> 4. Add right turn lane from US 101 onto Chicken Coop Road <i>Level of Effort: Medium / Potential Safety Benefit: Medium (14% reduction in collisions)</i> 5. Minor shoulder widening <i>Level of Effort: Medium / Potential Safety Benefit: Medium (13% reduction in collisions)</i> 6. Fix roadway skew <i>Level of Effort: Medium / Potential Safety Benefit: Medium (29% reduction in collisions)</i>

Chicken Coop Road / Zaccardo Road / US 101 (continued)

View West onto US 101



View East onto US 101



Exhibit 12. Blyn Road Detail

Blyn Road / US 101

Collision History (2009 – 2013)

Rate	0.08 collisions per million entering vehicles
Severity	2 collisions with 1 resulted in injury
Type	1 rear end (left turn from main road to side road) 1 vehicle strikes pedestrian

Volume on major road: 14,000 ADT

Volume on minor road: 100 ADT

Existing Conditions	Countermeasures
<p>Turn pockets do not exist at this location. (The low volume accessing Blyn Road would not warrant additional turn lanes).</p> <p>Adequate sight distance is available.</p> <p>Collisions typically occur as vehicles exceed safe speeds.</p> <p>No illumination at intersection.</p>	<p>1. Close access.</p> <p><i>Level of Effort: Low</i> <i>Potential Safety Benefit: High</i></p>
View Southwest onto US 101	View Northeast onto US 101



Exhibit 13. Old Blyn Highway

Old Blyn Highway / US 101

Collision History (2009 – 2013)

Rate	0.12 collisions per million entering vehicles
Severity	3 collisions with 1 resulted in injury
Type	2 rear end (turns from main road to side road) 1 entering at angle (entering from side road to main road)

Volume on major road: 14,000 ADT

Volume on minor road: 400 ADT

Existing Conditions	Countermeasures
<p>Turn lanes are not provided at the intersection.</p> <p>Adequate sight distance is available.</p> <p>Collisions typically occur as vehicles exceed safe speeds.</p> <p>No illumination at intersection.</p>	<p>1. Widen roadway to construct a north-eastbound left turn lane and receiving refuge lane.</p> <p><i>Level of Effort: Medium</i></p> <p><i>Potential Safety Benefit: High (44% reduction in collisions)</i></p>

View Southwest onto US 101



View Northeast onto US 101



Exhibit 14. Pierce Road Detail

Pierce Road / US 101

Collision History (2009 – 2013)

Rate	0.12 collisions per million entering vehicles
Severity	3 collisions with 2 resulted in injury
Type	3 rear end (left turns from main road to side road)

Volume on major road: 14,000 ADT

Volume on minor road: 600 ADT

Existing Conditions	Countermeasures
<p>Turn lanes are not provided at the intersection.</p> <p>Adequate sight distance is available.</p> <p>Collisions typically occur as vehicles exceed safe speeds.</p> <p>No illumination at intersection.</p>	<p>1. Widen roadway to construct a north-eastbound left turn lane and receiving refuge lane.</p> <p><i>Level of Effort: Medium</i></p> <p><i>Potential Safety Benefit: High (44% reduction in collisions)</i></p>

View Southwest onto US 101



View Northeast onto US 101



Exhibit 15. Knapp/Guiles Road Detail

Knapp / Guiles Road / US 101

Collision History (2009 – 2013)

Rate	0.04 collisions per million entering vehicles
Severity	1 collision with no injury
Type	1 rear end (turn from main road to side road)

Volume on major road: 14,000 ADT

Volume on minor road: 300 ADT

Existing Conditions	Countermeasures
<p>Turn lanes are not provided at the intersection.</p> <p>Adequate sight distance is available.</p> <p>Collisions typically occur as vehicles exceed safe speeds.</p> <p>No illumination at intersection.</p>	<p>1. Widen roadway to construct west and eastbound left turn lanes.</p> <p><i>Level of Effort: Medium</i></p> <p><i>Potential Safety Benefit: High (44% reduction in collisions)</i></p>

From Guiles Road (south side of US 101)

View West onto US 101



View East onto US 101



Exhibit 16. Chicken Coop/Diamond Point Road

Chicken Coop Road / Diamond Point Road / US 101

Collision History (2009 – 2013)

Rate	0.22 collisions per million entering vehicles
Severity	6 collisions with 3 resulted in injury
Type	2 entering at angle (entering from side road to main road) 1 rear end (turn from main road to side road) 2 roadway/ditch 1 head on collision

Volume on major road: 14,000 ADT

Volume on minor road: 2,000 ADT

Existing Conditions	Countermeasures
East and westbound left turn lanes are provided along US 101. Adequate sight distance is available. Collisions typically occur as vehicles exceed safe speeds. No illumination.	1. Install illumination <i>Level of Effort: Medium</i> <i>Potential Safety Benefit: Low (9% reduction in collisions)</i> 2. Install a roundabout <i>Level of Effort: High</i> <i>Potential Safety Benefit: High (71% reduction in collisions)</i>

From Diamond Point Road (north side of US 101)

View West onto US 101



View East onto US 101



Appendix A

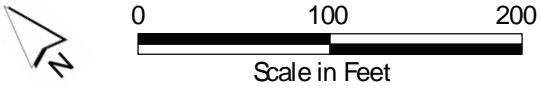
Road Safety Audit Team

A-1 – Road Safety Audit

Name	Organization	Participation in
Annette Nesse, Chief Operations Officer	Jamestown S'Klallam	Workshop
Steve Bennett, P.E.	WSDOT	Workshop, Field visit
Happy Longfellow, P.E.	Parametrix, Roadway Design	Workshop, Field visit
Tresia Hammonds	Parametrix, Traffic Operations and Planning	Workshop, Field visit
Greg Stidham, P.E.	Parametrix, Roadway Design	Workshop, Field visit

Appendix B

Maps



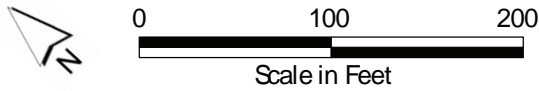
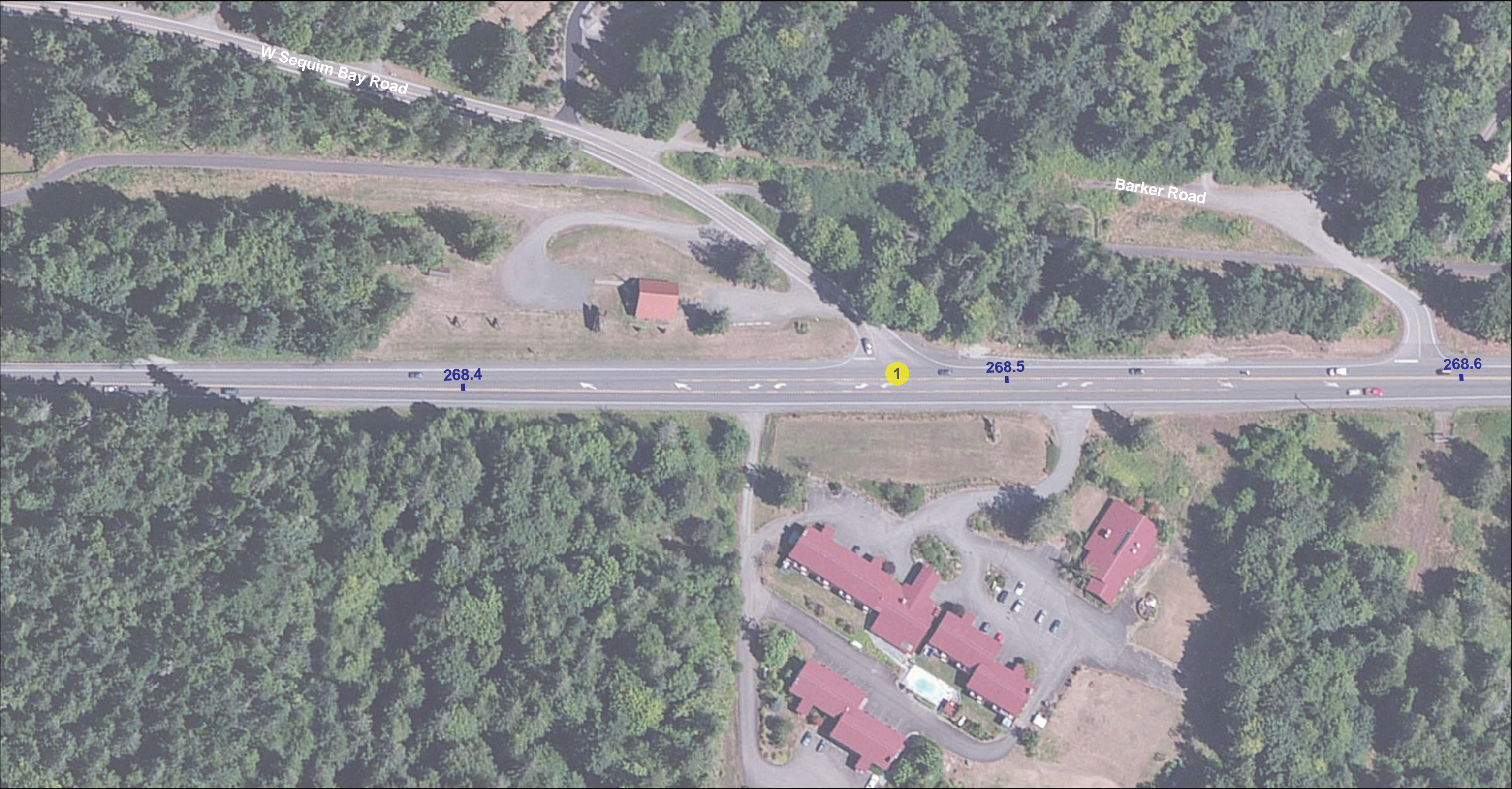
#	Description	Severity
1	Entering at angle	No Injury
2	NWB vehicle rear-ended	No Injury
3	NWB vehicle rear-ended	Injury
4	NWB vehicle rear-ended	No Injury

Represents general collision location



#	Description	Severity
1	Entering at angle	Fatality
2	Entering at angle	No Injury
3	NB rear-ended	No Injury

Represents general collision location



#	Description	Severity
1	Vehicle Strikes Object	Fatality

Represents general collision location



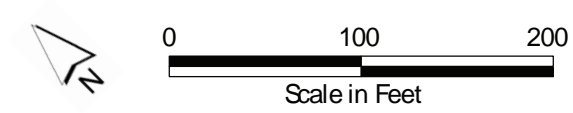
#	Description	Severity
1	SB R rear-ended	No Injury
2	Vehicle strikes deer	No Injury

Represents general collision location



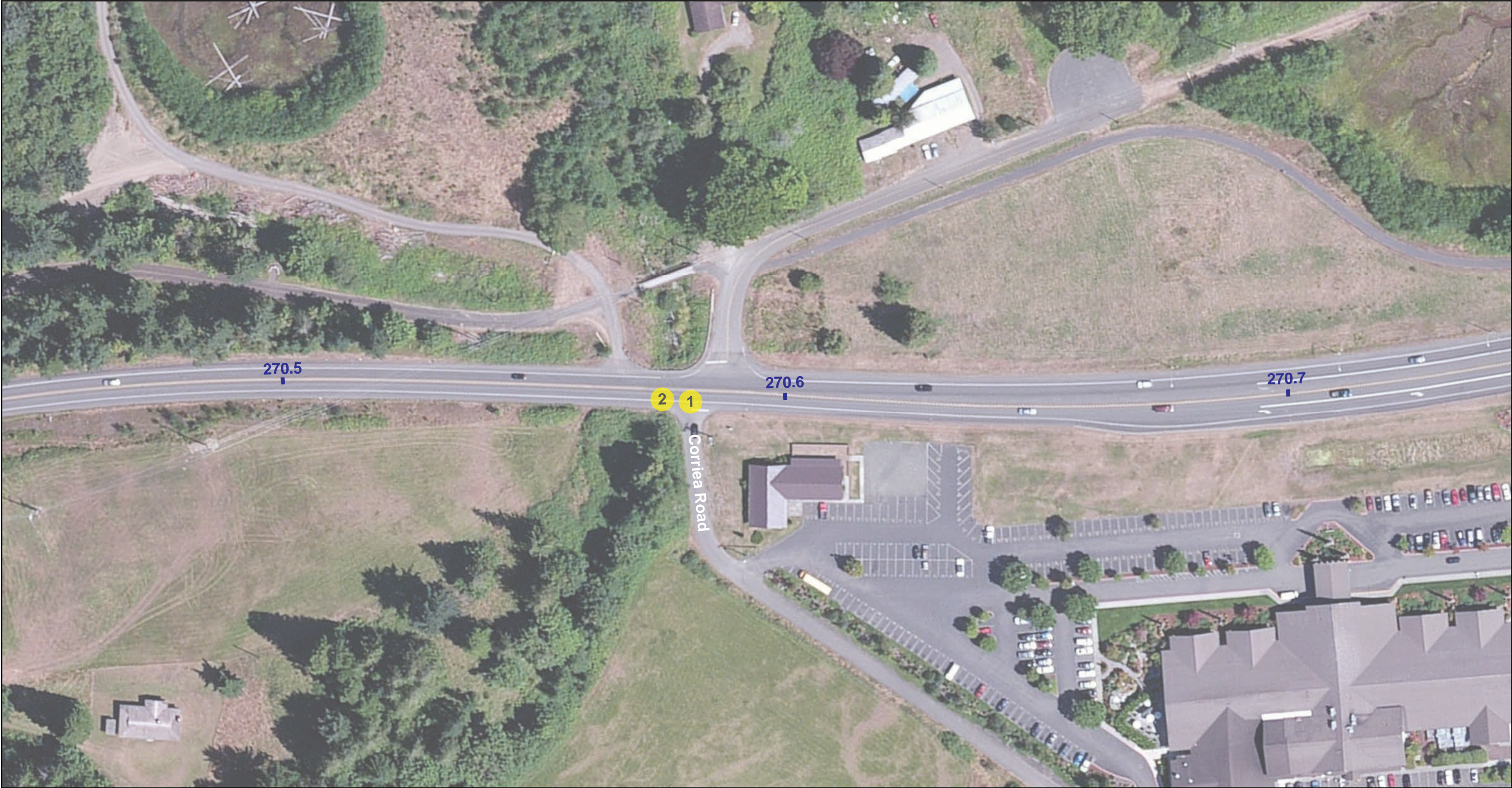
#	Description	Severity
1	Entering at angle	No Injury
2	Entering at angle	Injury
3	NB L rear-ended	Injury
4	NB L rear-ended	Injury

Represents general collision location



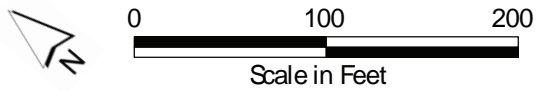
#	Description	Severity
1	SBL rear-ended	Injury
2	SBL rear-ended	Injury
3	Vehicle strikes deer	No Injury

Represents general collision location



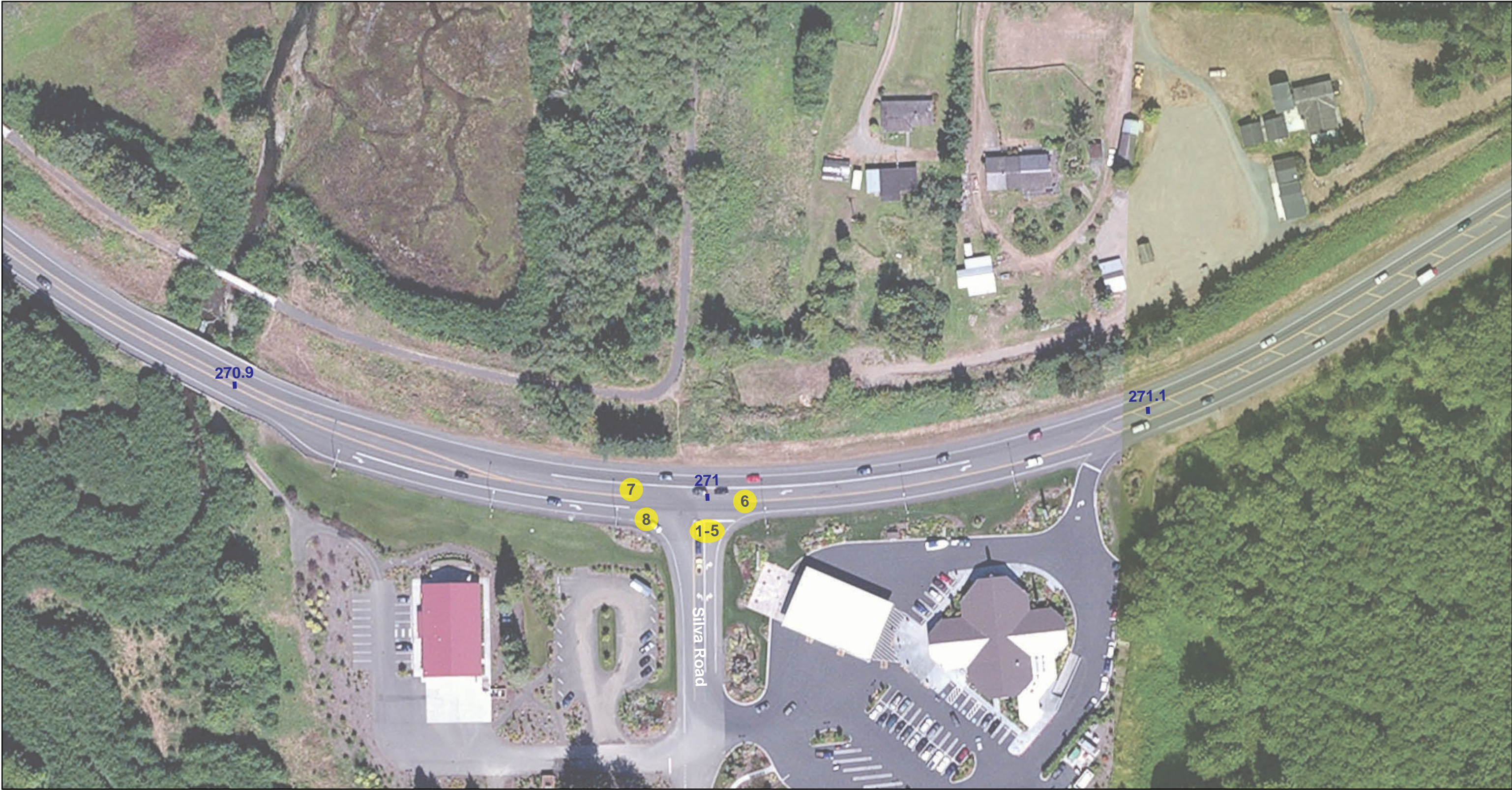
#	Description	Severity
1	Entering at angle	Injury
2	SEB rear-ended	No Injury

Represents general collision location



#	Description	Severity
1	Entering at angle	No Injury
2	Entering at angle	No Injury
3	Entering at angle	No Injury
4	Entering at angle	Injury

Represents general collision location



#	Description	Severity
1	Entering at angle	Injury
2	Entering at angle	Injury
3	Entering at angle	Injury
4	Entering at angle	Injury

#	Description	Severity
5	Entering at angle	No Injury
6	WB L hit by EB vehicle	Injury
7	Vehicle strikes deer	Injury
8	Vehicle strikes object	No Injury

Represents general collision location



0 100 200
Scale in Feet



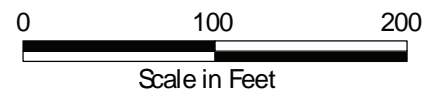
#	Description	Severity
1	SB R rear-ended	Injury
2	Vehicle strikes object	No Injury
3	Entering at angle	No Injury
4	SB L rear-ended	No Injury

Represents general collision location



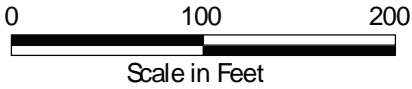
#	Description	Severity
1	NEB rear-ended	No Injury
2	Vehicle strikes pedestrian	Injury

Represents general collision location



#	Description	Severity
1	SWB R rear-ended	Injury
2	Entering at angle	No Injury
3	NEB L rear-ended	No Injury

Represents general collision location



#	Description	Severity
1	NEB L rear-ended	No Injury
2	NEB L rear-ended	Injury
3	NEB L rear-ended	Injury

Represents general collision location



#	Description	Severity
1	WB rear-ended	No Injury

Represents general collision location

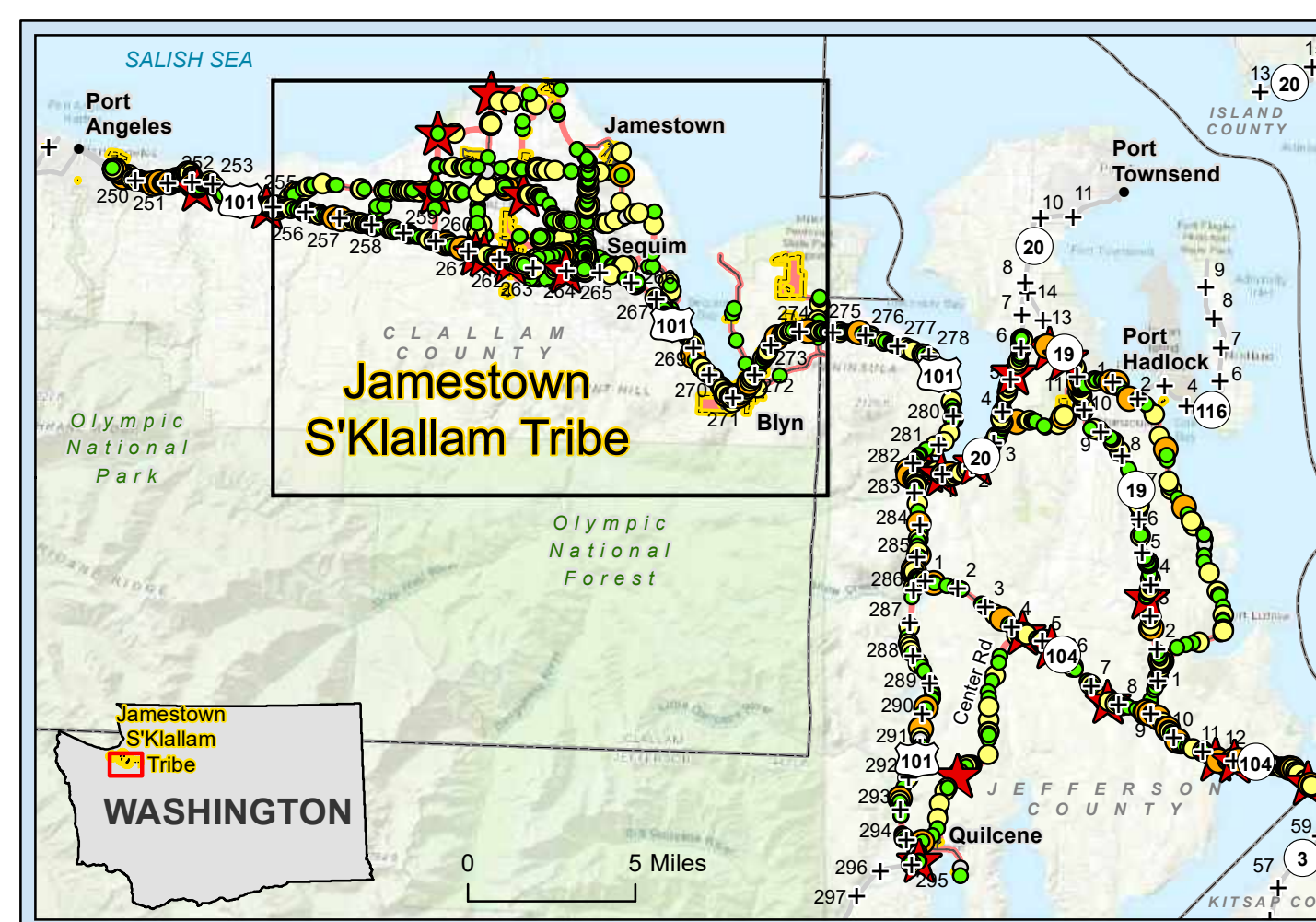



#	Description	Severity
1	WB rear-ended	No Injury
2	Entering at angle	Injury
3	Entering at angle	Injury

Represents general collision location

#	Description	Severity
4	Roadway ditch	Injury
5	Roadway ditch	No Injury
6	Head on collision	No Injury

APPENDIX B – CRASH SEVERITY – SEQUIM AND BLYN MAP





Jamestown S'Klallam Tribe Strategic Transportation Safety Plan Update

Crash Severity

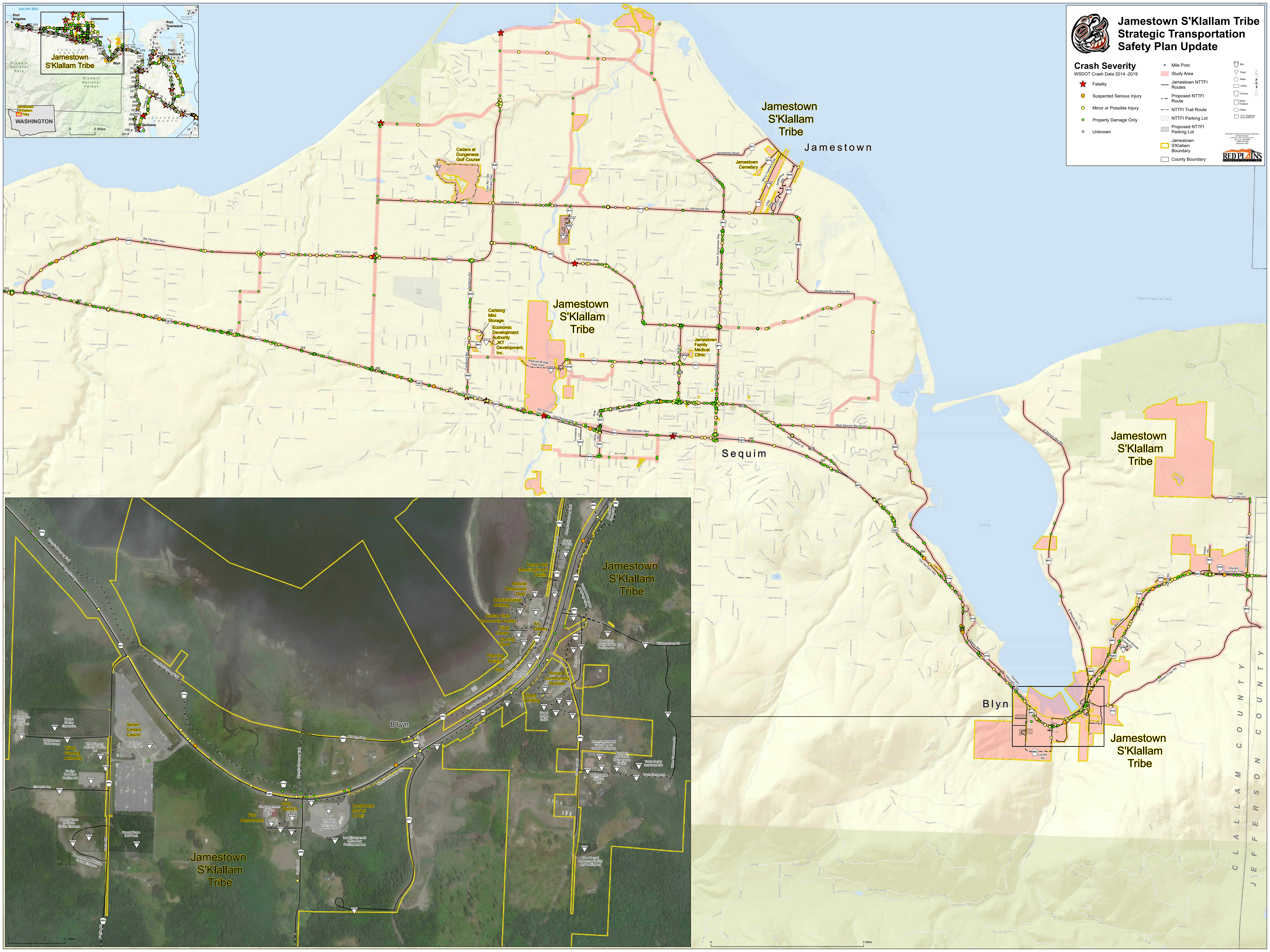
WSDOT Crash Data 2014-2019

- Fatality
- Suspected Serious Injury
- Minor or Possible Injury
- Property Damage Only
- Unknown

- Mile Post
- Study Area
- Jamestown NTTFI Routes
- Proposed NTTFI Route
- NTTFI Trail Route
- NTTFI Parking Lot
- Proposed NTTFI Parking Lot
- Jamestown S'Klallam Boundary
- County Boundary

- RA
- State
- County
- Other
- U.S. Highway

RED PLAINS



APPENDIX C – WSDOT CRASH DATA 2014-2019 (DIGITAL)

OBJECTID	JURISDICTION	COUNTY	CITY	PRIMARY THRAFFWAY	BLOCK NUMBER	INTERSECTING THRAFFWAY	DIST FROM REF. POINT	MI OF FT.	COMP. OR FROM FT.	REFERENCE POINT NAME	MILEPOST	A 6	SR. ONLY SUSPENSE INJURY	REPORT DATE	DATE	TIME	MO. MOST SEVERE INJURY TYPE	NM INU	NM FATAL	NM FATAL	NM PEDS	NM BIKES	VEH. 1 TYPE	VEH. 2 TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD SURFACE CONDITION	ROADWAY CHARACTERISTIC	LIGHTING CONDITION	FIRST COLLISION TYPE, CAUSE, STRUCK	VEH. 1 ACTION	VEH. 2 ACTION	VEH. 1 COMPASS DIRECTION FROM	VEH. 1 COMPASS DIRECTION TO	VEH. 1 COMPASS DIRECTION FROM	VEH. 1 COMPASS DIRECTION TO
5399	City Street	Chatham	Port Angeles	N ENNS ST	200				NE	GEORGINA ST				C150696	2014-08-21	21:20	No Apparent Injury	0	2	0	0	0	0	Other	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Shoulder L Level	Dark No Street Lights	One parked - one moving	Other*	Legally Parked, Unoccupied	South	West	South	North
5400	City Street	Chatham	Sequim	PTH ST	100				NE	S SEQUIM AVE				C608937	2014-03-13	20:08	No Apparent Injury	0	2	0	0	0	0	Other	At Intersection and Related	Clear or Partly Cloudy	Dry	Shoulder L Level	Dark No Street Lights	From same direction - one left turn - one straight	Other*	Legally Parked, Unoccupied	South	West	South	North
5401	City Street	Chatham	Sequim	E BELL ST	100				NE	S SEQUIM AVE				C592128	2016-08-02	05:15	No Apparent Injury	0	2	0	0	0	0	Other	At Intersection and Not Related	Clear or Partly Cloudy	Dry	Shoulder L Level	Dark No Street Lights	From same direction - one left turn - one straight	Other*	Legally Parked, Unoccupied	South	West	South	North
5402	City Street	Chatham	Sequim	TH ST	100				NE	S SEQUIM AVE				C159331	2014-08-12	12:15	No Apparent Injury	0	2	0	0	0	0	Other	At Intersection and Not Related	Clear or Partly Cloudy	Dry	Shoulder L Level	Dark No Street Lights	From same direction - one left turn - one straight	Other*	Legally Parked, Unoccupied	South	West	South	North
5403	City Street	Chatham	Sequim	E MAPLE ST	100				NE	S SEQUIM AVE				C496769	2015-12-19	13:10	No Apparent Injury	0	2	0	0	0	0	Other	At Intersection and Related	Clear or Partly Cloudy	Wet	Shoulder L Level	Daylight	From same direction - all others	Other*	Legally Parked, Unoccupied	South	West	South	North
5404	City Street	Chatham	Sequim	E SEQUIM AVE	100				NE	S SEQUIM AVE				C151007	2015-07-07	13:45	No Apparent Injury	0	2	0	0	0	0	Other	At Intersection and Related	Clear or Partly Cloudy	Wet	Shoulder L Level	Daylight	From same direction - all others	Other*	Legally Parked, Unoccupied	South	West	South	North
5405	City Street	Chatham	Sequim	E SILVERHORN RD	0				NE	S SEQUIM AVE				C120164	2015-02-10	17:45	No Apparent Injury	0	2	0	0	0	0	Other	At Intersection and Related	Clear or Partly Cloudy	Wet	Shoulder L Level	Daylight	From same direction - one left turn - one straight	Other*	Legally Parked, Unoccupied	South	West	South	North
5406	City Street	Chatham	Sequim	E SILVERHORN RD	0				NE	S SEQUIM AVE				C361506	2014-07-04	09:30	No Apparent Injury	0	2	0	0	0	0	Other	At Intersection and Related	Clear or Partly Cloudy	Wet	Shoulder L Level	Daylight	From same direction - one left turn - one straight	Other*	Legally Parked, Unoccupied	South	West	South	North
5407	City Street	Chatham	Sequim	E SILVERHORN RD	0				NE	S SEQUIM AVE				C127284	2015-02-12	24:00	No Apparent Injury	0	2	0	0	0	0	Other	At Intersection and Related	Clear or Partly Cloudy	Wet	Shoulder L Level	Daylight	From same direction - one left turn - one straight	Other*	Legally Parked, Unoccupied	South	West	South	North
5408	City Street	Chatham	Sequim	E SILVERHORN RD	0				NE	S SEQUIM AVE				C608944	2015-12-29	10:00	No Apparent Injury	0	2	0	0	0	0	Other	At Intersection and Related	Clear or Partly Cloudy	Wet	Shoulder L Level	Daylight	From same direction - one left turn - one straight	Other*	Legally Parked, Unoccupied	South	West	South	North
5409	City Street	Chatham	Sequim	E WASHINGTON ST	0				NE	S SEQUIM AVE				C272039	2014-03-27	20:09	No Apparent Injury	0	2	0	0	0	0	Other	At Intersection and Related	Clear or Partly Cloudy	Wet	Shoulder L Level	Daylight	From same direction - one left turn - one straight	Other*	Legally Parked, Unoccupied	South	West	South	North
5410	City Street	Chatham	Sequim	E WASHINGTON ST	0				NE	S SEQUIM AVE				C449106	2014-06-17	17:55	No Apparent Injury	0	2	0	0	0	0	Other	At Intersection and Related	Clear or Partly Cloudy	Wet	Shoulder L Level	Daylight	From same direction - one left turn - one straight	Other*	Legally Parked, Unoccupied	South	West	South	North
5411	City Street	Chatham	Sequim	E WASHINGTON ST	0				NE	S SEQUIM AVE				C454006	2014-07-07	18:48	No Apparent Injury	0	2	0	0	0	0	Other	At Intersection and Related	Clear or Partly Cloudy	Wet	Shoulder L Level	Daylight	From same direction - one left turn - one straight	Other*	Legally Parked, Unoccupied	South	West	South	North
5412	City Street	Chatham	Sequim	E WASHINGTON ST	0				NE	S SEQUIM AVE				C151680	2																					

	MY_DRIVER_CONTRIBUTING_CIRCUMSTANCE_1_UNIT_1	MY_DRIVER_CONTRIBUTING_CIRCUMSTANCE_2_UNIT_1	MY_DRIVER_CONTRIBUTING_CIRCUMSTANCE_3_UNIT_1	MY_DRIVER_AGE_UNIT_1	MY_DRIVER_GENDER_UNIT_1	MY_DRIVER_CONTRIBUTING_CIRCUMSTANCE_1_UNIT_2	MY_DRIVER_CONTRIBUTING_CIRCUMSTANCE_2_UNIT_2	MY_DRIVER_CONTRIBUTING_CIRCUMSTANCE_3_UNIT_2	MY_DRIVER_AGE_UNIT_2	MY_DRIVER_GENDER_UNIT_2	BICYCLIST_CONTRIBUTING_CIRCUMSTANCE_1_UNIT_1	BICYCLIST_CONTRIBUTING_CIRCUMSTANCE_2_UNIT_1	BICYCLIST_CONTRIBUTING_CIRCUMSTANCE_3_UNIT_1	BICYCLIST_AGE_UNIT_1	BICYCLIST_GENDER_UNIT_1	BICYCLIST_CONTRIBUTING_CIRCUMSTANCE_1_UNIT_2	BICYCLIST_CONTRIBUTING_CIRCUMSTANCE_2_UNIT_2	BICYCLIST_CONTRIBUTING_CIRCUMSTANCE_3_UNIT_2	BICYCLIST_AGE_UNIT_2	BICYCLIST_GENDER_UNIT_2	PEDESTRIAN_CONTRIBUTING_CIRCUMSTANCE_1_UNIT_1	PEDESTRIAN_CONTRIBUTING_CIRCUMSTANCE_2_UNIT_1	PEDESTRIAN_CONTRIBUTING_CIRCUMSTANCE_3_UNIT_1	PEDESTRIAN_AGE_UNIT_2	PEDESTRIAN_GENDER_UNIT_2		FIRST_IMPACT_LOCATION
5399	None					None																					Outside Shoulder of Primary Trafficway
5400	None			16 Male	None	None			65 Male																		Lane of Primary Trafficway
5401	Eating or Drinking			18 Male	None	None																					Outside Shoulder of Primary Trafficway
5402	Intention			79 Male	None	None									None			23	Female								Lane of Primary Trafficway
5403	Distractions Outside Vehicle			79 Male	Driver Not Distracted	67 Female																					Lane of Primary Trafficway
5404	Intention			50 Female	Intention	31 Female																					Lane of Primary Trafficway
5405	Did Not Grant RW to Vehicle	Intention		67 Male	None	67 Male																					Lane of Primary Trafficway
5406	None			28 Female	Operating Defective Equipment	19 Female																					Lane of Primary Trafficway
5407	Distractions Outside Vehicle			74 Female	None	72 Female																					Lane of Primary Trafficway
5408	Intention	Follow Too Closely		74 Female	None	72 Female															Intention			13 Male			Lane of Primary Trafficway
5409	Intention			74 Female	None	68 Female																					Lane of Primary Trafficway
5410	Intention			65 Male	Driver Not Distracted	65 Male																					Lane of Primary Trafficway
5411	Follow Too Closely			23 Female	None	60 Female																					Lane of Primary Trafficway
5412	Distractions Outside Vehicle			41 Male	Driver Not Distracted	61 Female																					Lane of Primary Trafficway
5413	Follow Too Closely			25 Male	None	61 Female																					Lane of Primary Trafficway
5414	Did Not Grant RW to Vehicle	Disregard Stop Sign - Flashing Red		56 Male	None	60 Male																					Lane of Primary Trafficway
5415	Disregard Stop Sign - Flashing Red	Intention		66 Male	None	66 Male																					Lane of Primary Trafficway
5416	Did Not Grant RW to Vehicle			68 Female	None	68 Male																					Lane of Primary Trafficway
5417	Unknown Distraction			64 Female	None	41 Female																					Lane of Primary Trafficway
5418	Intention			14 Female	None	14 Female																					Outside Shoulder of Primary Trafficway
5419	Other Contributing Circ Not Listed			78 Female	None	43 Male																					Lane of Primary Trafficway
5420	None			51 Male	Other Contributing Circ Not Listed	78 Male		Intention																			Lane of Primary Trafficway
5421	Intention			60 Female	None	62 Male																					Lane of Primary Trafficway
5422	Distractions Outside Vehicle			18 Female	None	66 Female																					Lane of Primary Trafficway
5423	Apparently Alone or Fatigued			71 Male	None	71 Male																					Lane of Primary Trafficway
5424	Did Not Grant RW to Vehicle	Over Center Line</																									

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OBJECT ID	JURISDICTION	COUNTY	CITY	PRIMARY THRUWAY	BLOCK NUMBER	INTERSECTING THRUWAY	DET. FROM REF. POINT	ML OF FT.	COMP. DIR. FROM REF. POINT	REFERENCE POINT NAME	MLPOST	IS ONLY SURVEYED	REPORT NUMBER	DATE	TIME	MOST SEVERE INJURY TYPE	NM INJ. FU	NM INJ. FU	NM INJ. FU	NM INJ. FU	NM INJ. FU	VEH. 1 TYPE	VEH. 2 TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD SURFACE CONDITION	ROADWAY CHARACTERISTIC	LIGHTING CONDITION	FIRST COLLISION TYPE OBJECT STRUCK	VEH. 1 ACTION	VEH. 2 ACTION	VEH. 1 COMPASS DIRECTION FROM	VEH. 1 COMPASS DIRECTION TO	VEH. 2 COMPASS DIRECTION FROM	VEH. 2 COMPASS DIRECTION TO
5579	Cy Street	Clallam	Sequim	SEQUIM DUNDENESS WAY	0	OLD OLIMPIC HWY	377 F	W		PORT WILLIAM RD		No	EA37606	2015-08-07 11:41		No Apparent Injury	0	2	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car	Entering Roundabout	Clear or Partly Cloudy	Dry	Cure & Level	Daylight	Entering at angle	Merging (Entering Traffic)	Going Straight Ahead	West	Southwest	North	South
5580	Cy Street	Clallam	Sequim	SEQUIM DUNDENESS WAY	0	N 5TH AVE	370 F	W		PORT WILLIAM RD		No	EA37607	2015-07-20 12:20		No Apparent Injury	0	2	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car	Roundabout Related but not at Roundabout	Clear or Partly Cloudy	Dry	Cure & Level	Daylight	Entering at angle	Merging (Entering Traffic)	Going Straight Ahead	West	Southwest	North	South
5581	Cy Street	Clallam	Sequim	SEQUIM DUNDENESS WAY	1300	N 5TH AVE	330 F	W		RAINVIEW LN		No	EA49008	2015-03-09 12:03		No Apparent Injury	0	3	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car	Driveway Related but Not at Driveway	Clear or Partly Cloudy	Dry	Streights & Level	Daylight	From same direction - both going straight - both moving - near-end	Going Straight Ahead	Slowing	South	South	South	North
5582	Cy Street	Clallam	Sequim	SEQUIM DUNDENESS WAY	1300	N 5TH AVE	190 F	W		RAINVIEW LN		No	EA49009	2015-03-09 12:03		No Apparent Injury	0	3	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car	Driveway Related but Not at Driveway	Clear or Partly Cloudy	Dry	Streights & Level	Daylight	From same direction - both going straight - both moving - near-end	Going Straight Ahead	Slowing	South	South	South	North
5583	Cy Street	Clallam	Sequim	SEQUIM DUNDENESS WAY	1300	N 5TH AVE	190 F	W		RAINVIEW LN		No	EA49009	2015-03-09 12:03		No Apparent Injury	0	3	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car	Driveway Related but Not at Driveway	Clear or Partly Cloudy	Dry	Streights & Level	Daylight	From same direction - both going straight - both moving - near-end	Going Straight Ahead	Slowing	South	South	South	North
5584	Cy Street	Clallam	Sequim	SEQUIM DUNDENESS WAY	1300	N 5TH AVE	190 F	W		RAINVIEW LN		No	EA49009	2015-03-09 12:03		No Apparent Injury	0	3	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car	Driveway Related but Not at Driveway	Clear or Partly Cloudy	Dry	Streights & Level	Daylight	From same direction - both going straight - both moving - near-end	Going Straight Ahead	Slowing	South	South	South	North
5585	Cy Street	Clallam	Sequim	SEQUIM DUNDENESS WAY	1300	N 5TH AVE	190 F	W		RAINVIEW LN		No	EA49009	2015-03-09 12:03		No Apparent Injury	0	3	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car	Driveway Related but Not at Driveway	Clear or Partly Cloudy	Dry	Streights & Level	Daylight	From same direction - both going straight - both moving - near-end	Going Straight Ahead	Slowing	South	South	South	North
5586	Cy Street	Clallam	Sequim	SEQUIM DUNDENESS WAY	1300	N 5TH AVE	190 F	W		RAINVIEW LN		No	EA49009	2015-03-09 12:03		No Apparent Injury	0	3	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car	Driveway Related but Not at Driveway	Clear or Partly Cloudy	Dry	Streights & Level	Daylight	From same direction - both going straight - both moving - near-end	Going Straight Ahead	Slowing	South	South	South	North
5587	Cy Street	Clallam	Sequim	SEQUIM DUNDENESS WAY	1300	N 5TH AVE	190 F	W		RAINVIEW LN		No	EA49009	2015-03-09 12:03		No Apparent Injury	0	3	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car	Driveway Related but Not at Driveway	Clear or Partly Cloudy	Dry	Streights & Level	Daylight	From same direction - both going straight - both moving - near-end	Going Straight Ahead	Slowing	South	South	South	North
5588	Cy Street	Clallam	Sequim	SEQUIM DUNDENESS WAY	1300	N 5TH AVE	190 F	W		RAINVIEW LN		No	EA49009	2015-03-09 12:03		No Apparent Injury	0	3	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car	Driveway Related but Not at Driveway	Clear or Partly Cloudy	Dry	Streights & Level	Daylight	From same direction - both going straight - both moving - near-end	Going Straight Ahead	Slowing	South	South	South	North
5589	Cy Street	Clallam	Sequim	SEQUIM DUNDENESS WAY	1300	N 5TH AVE	190 F	W		RAINVIEW LN		No	EA49009	2015-03-09 12:03		No Apparent Injury	0	3	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car	Driveway Related but Not at Driveway	Clear or Partly Cloudy	Dry	Streights & Level	Daylight	From same direction - both going straight - both moving - near-end	Going Straight Ahead	Slowing	South	South	South	North
5590	Cy Street	Clallam	Sequim	SEQUIM DUNDENESS WAY	1300	N 5TH AVE	190 F	W		RAINVIEW LN		No	EA49009	2015-03-09 12:03		No Apparent Injury	0	3	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car	Driveway Related but Not at Driveway	Clear or Partly Cloudy	Dry	Streights & Level	Daylight	From same direction - both going straight - both moving - near-end	Going Straight Ahead	Slowing	South	South		

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FILE NO.	JURISDICTION	COUNTY	CITY	PRIMARY TRAFFIC	BLOCK NUMBER	INTERSECTING TRAFFICWAY	DIST FROM REF POINT	HE OF TRF	COMP DIR FROM REF POINT	REFERENCE POINT NAME	MULPOST A	IS ONLY HISTORY SUSPENSE	REPORT NUMBER	DATE	TIME	MOST SEVERE VIOLATION TYPE	NUM OF	NUM OF	NUM OF	NUM OF	NUM OF	NUM OF	VEH 1 TYPE	VEH 2 TYPE	VEH 1 ACTION	VEH 2 ACTION	VEH 1 COMPASS DIRECTION FROM	VEH 1 COMPASS DIRECTION TO	VEH 2 COMPASS DIRECTION FROM	VEH 2 COMPASS DIRECTION TO
5769	City Street	Chatham	Sequim	W WASHINGTON ST	800		100	W	S	S BAYVIEW		No	633990	2018-01-05	10:02	No Apparent Injury	0	0	2	0	0	0	Passenger Car	Passenger Car	W	W	W	W	W	W
5770	City Street	Chatham	Sequim	W WASHINGTON ST	800		100	W	S	S BAYVIEW		No	633990	2018-01-05	10:02	No Apparent Injury	0	0	2	0	0	0	Passenger Car	Passenger Car	W	W	W	W	W	W
5771	City Street	Chatham	Sequim	W WASHINGTON ST	700		133F	E	W	W BRACKETT RD		No	272820	2018-05-17	17:40	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	W	W	W	W	W	W
5772	City Street	Chatham	Sequim	W WASHINGTON ST	700		142F	E	W	W BRACKETT RD		No	633990	2018-05-17	17:40	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	W	W	W	W	W	W
5773	City Street	Chatham	Sequim	W WASHINGTON ST	700		142F	E	W	W BRACKETT RD		No	633990	2018-05-17	17:40	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	W	W	W	W	W	W
5774	City Street	Chatham	Sequim	W WASHINGTON ST	700		142F	E	W	W BRACKETT RD		No	633990	2018-05-17	17:40	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	W	W	W	W	W	W
5775	City Street	Chatham	Sequim	W WASHINGTON ST	700		142F	E	W	W BRACKETT RD		No	633990	2018-05-17	17:40	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	W	W	W	W	W	W
5776	City Street	Chatham	Sequim	W WASHINGTON ST	700		142F	E	W	W BRACKETT RD		No	633990	2018-05-17	17:40	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	W	W	W	W	W	W
5777	City Street	Chatham	Sequim	W WASHINGTON ST	700		142F	E	W	W BRACKETT RD		No	633990	2018-05-17	17:40	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	W	W	W	W	W	W
5778	City Street	Chatham	Sequim	W WASHINGTON ST	700		142F	E	W	W BRACKETT RD		No	633990	2018-05-17	17:40	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	W	W	W	W	W	W
5779	City Street	Chatham	Sequim	W WASHINGTON ST	700		142F	E	W	W BRACKETT RD		No	633990	2018-05-17	17:40	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	W	W	W	W	W	W
5780	City Street	Chatham	Sequim	W WASHINGTON ST	700		142F	E	W	W BRACKETT RD		No	633990	2018-05-17	17:40	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	W	W	W	W	W	W
5781	City Street	Chatham	Sequim	W WASHINGTON ST	700		142F	E	W	W BRACKETT RD		No	633990	2018-05-17	17:40	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	W	W	W	W	W	W
5782	City Street	Chatham	Sequim	W WASHINGTON ST	700		142F	E	W	W BRACKETT RD		No	633990	2018-05-17	17:40	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	W	W	W	W	W	W
5783	City Street	Chatham	Sequim	W WASHINGTON ST	700		142F	E	W	W BRACKETT RD		No	633990	2018-05-17	17:40	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	W	W	W	W	W	W
5784	City Street	Chatham	Sequim	W WASHINGTON ST	700		142F	E</																						

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JURISDICTION	COUNTY	CITY	PRIMARY TRAFFICWAY	BLOCK NUMBER	INTERSECTING TRAFFICWAY	DIR FROM REF POINT	MI OF RT	COMP DIR FROM REF POINT	REFERENCE POINT NAME	MILEPOST	IS ONLY HISTORY SUSPENSE	REPORT NUMBER	DATE	TIME	MOST SEVERE VIOLATION TYPE	NUM PL	NUM FV	NUM INV	NUM FV	NUM FV	NUM FV	NUM FV	VEH 1 TYPE	VEH 2 TYPE	VEH 1 ACTION	VEH 2 ACTION	VEH 1 COMPASS DIRECTION FROM	VEH 1 COMPASS DIRECTION TO	VEH 2 COMPASS DIRECTION FROM	VEH 2 COMPASS DIRECTION TO	
5939	County Road	Chatham	94120	1494	2716795	2016-12-28	07:36	Suspected Serious Injury		1.94	No	2716795	2016-12-28	07:36	Suspected Serious Injury	3	0	2	0	0	0	0	Pickup/Panel Truck or Vanette under 10,000 lb	Pickup/Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Ice	Going Straight Ahead	Going Straight Ahead	East	West
5940	County Road	Chatham	94120	1493	62171729	2016-11-20	18:46	Not Apparent Injury		1.93	No	62171729	2016-11-20	18:46	Not Apparent Injury	1	0	0	0	0	0	0	Pickup/Panel Truck or Vanette under 10,000 lb	Pickup/Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Ice	Going Straight Ahead	Going Straight Ahead	East	West
5941	County Road	Chatham	94120	1491	2716795	2017-11-02	17:53	No Apparent Injury		1.90	No	2716795	2017-11-02	17:53	No Apparent Injury	1	0	0	0	0	0	0	Passenger Car	Passenger Car	Intersection and Related	Rainning	Wet	Going Straight Ahead	Going Straight Ahead	North	West
5942	County Road	Chatham	94120	1490	2716795	2017-11-02	18:03	No Apparent Injury		1.89	No	2716795	2017-11-02	18:03	No Apparent Injury	1	0	0	0	0	0	0	Pickup/Panel Truck or Vanette under 10,000 lb	Pickup/Panel Truck or Vanette under 10,000 lb	Intersection and Related	Clear or Partly Cloudy	Ice	Going Straight Ahead	Going Straight Ahead	North	West
5943	County Road	Chatham	94120	1490	3615578	2015-11-25	14:47	Dead in Hospital		1.90	No	3615578	2015-11-25	14:47	Dead in Hospital	1	0	0	0	0	0	0	Passenger Car	Passenger Car	Intersection and Related	Clear or Partly Cloudy	Ice	Going Straight Ahead	Going Straight Ahead	South	West
5944	County Road	Chatham	94120	1490	3615584	2016-05-28	13:05	No Apparent Injury		1.90	No	3615584	2016-05-28	13:05	No Apparent Injury	1	0	0	0	0	0	0	Passenger Car	Passenger Car	Intersection and Related	Clear or Partly Cloudy	Ice	Going Straight Ahead	Going Straight Ahead	East	West
5945	County Road	Chatham	94120	1490	2017-08-25	09:12	15:05	No Apparent Injury		1.90	No	2017-08-25	09:12	15:05	No Apparent Injury	1	0	0	0	0	0	0	Passenger Car	Passenger Car	Intersection and Related	Clear or Partly Cloudy	Ice	Going Straight Ahead	Going Straight Ahead	East	West
5946	County Road	Chatham	94120	1490	3616032	2014-07-01	13:16	No Apparent Injury		1.90	No	3616032	2014-07-01	13:16	No Apparent Injury	1	0	0	0	0	0	0	Passenger Car	Passenger Car	Intersection and Related	Clear or Partly Cloudy	Ice	Going Straight Ahead	Going Straight Ahead	South	West
5947	County Road	Chatham	94120	1490	2016-08-02	14:53	15:05	No Apparent Injury		1.90	No	2016-08-02	14:53	15:05	No Apparent Injury	1	0	0	0	0	0	0	Passenger Car	Passenger Car	Intersection and Related	Clear or Partly Cloudy	Ice	Going Straight Ahead	Going Straight Ahead	South	West
5948	County Road	Chatham	94120	1490	3716384	2017-06-27	12:40	Suspected Minor Injury		1.90	No	3716384	2017-06-27	12:40	Suspected Minor Injury	2	0	0	0	0	0	0	Pickup/Panel Truck or Vanette under 10,000 lb	Pickup/Panel Truck or Vanette under 10,000 lb	Intersection and Related	Clear or Partly Cloudy	Ice	Going Straight Ahead	Going Straight Ahead	East	West
5949	County Road	Chatham	94120	1490	3602552	2016-09-29	16:26	Suspected Minor Injury		1.90	No	3602552	2016-09-29	16:26	Suspected Minor Injury	1	0	0	0	0	0	0	Passenger Car	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Ice	Going Straight Ahead	Going Straight Ahead	East	West
5950	County Road	Chatham	94120	1490	3616780	2017-07-27	10:10	No Apparent Injury		1.90	No	3616780	2017-07-27	10:10	No Apparent Injury	1	0	0	0	0	0	0	Pickup/Panel Truck or Vanette under 10,000 lb	Pickup/Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Ice	Going Straight Ahead	Going Straight Ahead	East	West
5951	County Road	Chatham	94120	1490	2716717	2016-05-18	05:00	No Apparent Injury		1.90	No	2716717	2016-05-18	05:00	No Apparent Injury	1	0	0	0	0	0	0	Passenger Car	Passenger Car	Intersection and Related	Rainning	Wet	Going Straight Ahead	Going Straight Ahead	East	West
5952	County Road	Chatham	94120	1490	2016-05-18	05:00	15:05	No Apparent Injury		1.90	No	2016-05-18	05:00	15:05	No Apparent Injury	1	0	0	0	0	0	0	Passenger Car	Passenger Car	Intersection and						

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	A STATE PLANE SOUTH Y 2010 FORWARD	A STATE PLANE SOUTH Y 2010 FORWARD	COL YEAR	COL MILE	COL TYPE	COL SEVERITY	COL_HM FACTOR	COL AGE	COL SURFACE CONDITIONS	COL_LIGHT CONDITIONS	COL_LOCATION	COL_GEOMETRY	STUDY AREA	COL_VIL_USER	COL_TPE TEXT	COL_SEVERITY TEXT	COL_HUMAN FACTOR, RECORDED	COL_MWV	COL_LIGHT CONDITIONS Decoded	COL_SURFACE CONDITIONS Decoded	COL_LOCATION Decoded	COL_RISKY BEHAVIOR	Intersection Area, Name	Emphasis Area	Roadway Departure
9339	86028.78	102227.82	2016	2	Vehicles/Partes	Sidewalk	Suspected Serious Injury	Speeding	Unknown	25-34	Dry	Not at Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	Not at Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9340	86028.78	102227.82	2016	2	Vehicles/Partes	Street Object	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Curve & Grade	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9341	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9342	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Curve & Grade	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9343	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9344	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9345	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9346	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9347	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9348	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9349	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9350	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9351	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9352	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9353	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9354	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9355	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9356	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9357	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9358	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9359	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9360	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9361	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9362	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9363	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9364	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9365	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9366	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9367	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9368	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9369	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9370	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9371	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9372	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9373	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9374	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9375	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9376	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9377	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9378	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9379	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9380	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9381	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9382	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9383	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9384	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9385	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9386	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9387	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9388	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9389	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9390	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9391	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9392	86028.78	102227.82	2016	2	Vehicles/Partes	Angle	Property Damage Only	Inattention	Unknown	Under 25	Dry	At Intersection/Dnwy/Roundabout	Straight Level	No Vulnerable Road User Involved	Daylight	Other Contributing Circ Not Listed	Speeding	None	Ice/Snow/Icy	At Intersection/Dnwy/Roundabout	Speeding	No Risky Driving Behaviors Involved	Yes		
9393	86028.78	1																							

OBJECT ID	JURISDICTION	COUNTY	CITY	PRIMARY TRAFFICWAY	BLOCK NUMBER	INTERSECTING TRAFFICWAY	DET FROM REF POINT	ML OF PT	COMP DIR FROM REF POINT	REFERENCE POINT NAME	MILEPOST A	SR OR LOCAL SUSPENSE, IN	REPORT NUMBER	DATE	TIME	MOST SEVERE INJURY TYPE	NUM IN	NUM FATAL	NUM INJURY	NUM PEDES	NUM BREAST	VEH 1 TYPE	VEH 2 TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD SURFACE CONDITION	ROADWAY CHARACTERISTIC	LIGHTING CONDITION	FIRST COLLISION TYPE OBJECT STRUCK	VEH 1 ACTION	VEH 2 ACTION	VEH 1 COMPASS DIRECTION FROM	VEH 1 COMPASS DIRECTION TO	VEH 2 COMPASS DIRECTION FROM	VEH 2 COMPASS DIRECTION TO
6119	County Road	Clallam		95250	95000						2.62	No	E85878	2019-11-01	08:15	Possible Injury	0	0	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Pickup Panel Truck or Vanette under 10,000 lb.	At Intersection and Related	Raining	Wet	Shoulder & Level	Dusk	Entering at angle	Going Straight Ahead	West	East	North	South	
6120	County Road	Clallam		95250	95000						2.62	No	E85878	2019-11-01	08:15	Possible Injury	0	0	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Pickup Panel Truck or Vanette under 10,000 lb.	At Intersection and Related	Raining	Wet	Shoulder & Level	Dusk	Entering at angle	Going Straight Ahead	West	East	North	South	
6121	County Road	Clallam		95250							2.74	No	E97974	2019-12-24	17:24	No Apparent Injury	0	0	0	0	0	Passenger Car	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Shoulder & Level	Dusk	Vehicle Skids Deer	Going Straight Ahead	South	North			
6122	County Road	Clallam		95250							2.81	No	E84263	2019-08-20	10:00	No Apparent Injury	0	0	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Passenger Car	At Intersection and Related	Clear or Partly Cloudy	Dry	Shoulder & Level	Dusk	Vehicle Skids Deer	Going Straight Ahead	South	North			
6123	County Road	Clallam		95250							2.88	No	E748250	2017-11-27	18:49	No Apparent Injury	0	0	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Passenger Car	At Intersection and Related	Clear or Partly Cloudy	Dry	Shoulder & Level	Dusk	Vehicle Skids Deer	Going Straight Ahead	South	North			
6124	County Road	Clallam		95250							2.92	No	E89449	2019-02-09	12:49	No Apparent Injury	0	0	0	0	0	Passenger Car	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Shoulder & Level	Dusk	Vehicle Skids Deer	Going Straight Ahead	South	North			
6125	County Road	Clallam		95250							2.93	No	E748249	2017-11-27	18:49	No Apparent Injury	0	0	0	0	0	Passenger Car	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Shoulder & Level	Dusk	Vehicle Skids Deer	Going Straight Ahead	South	North			
6126	County Road	Clallam		95250							2.93	No	E89449	2019-02-09	12:49	No Apparent Injury	0	0	0	0	0	Passenger Car	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Shoulder & Level	Dusk	Vehicle Skids Deer	Going Straight Ahead	South	North			
6127	County Road	Clallam		95250							2.93	No	E89449	2019-02-09	12:49	No Apparent Injury	0	0	0	0	0	Passenger Car	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Shoulder & Level	Dusk	Vehicle Skids Deer	Going Straight Ahead	South	North			
6128	County Road	Clallam		95250	95590						2.97	No	E77773	2015-08-16	15:56	No Apparent Injury	0	0	0	0	0	Passenger Car	Passenger Car	At Intersection and Related	Clear or Partly Cloudy	Dry	Shoulder & Level	Dusk	Vehicle Skids Deer	Going Straight Ahead	South	North			
6129	County Road	Clallam		95250							3.01	No	E781857	2019-05-12	13:51	Unknown	0	0	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Pickup Panel Truck or Vanette under 10,000 lb.	At Intersection and Not Related	Clear or Partly Cloudy	Dry	Shoulder & Level	Dusk	Vehicle Skids Deer	Going Straight Ahead	South	North			
6130	County Road	Clallam		95250	95740						3.06	No	E778008	2017-04-17	13:51	Unknown	0	0	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Pickup Panel Truck or Vanette under 10,000 lb.	At Intersection and Not Related	Clear or Partly Cloudy	Dry	Shoulder & Level	Dusk	Vehicle Skids Deer	Going Straight Ahead	South	North			
6131	County Road	Clallam		95250							3.34	No	E920293	2014-12-24	17:56	No Apparent Injury	0	0	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Passenger Car	At Intersection and Not Related	Clear or Partly Cloudy	Dry	Shoulder & Level	Dusk	Vehicle Skids Deer	Going Straight Ahead	South	North			
6132	County Road	Clallam		95250							4.42	No	E778029	2017-01-21	09:50	No Apparent Injury	0	0	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Passenger Car	At Intersection and Not Related	Clear or Partly Cloudy	Dry	Shoulder & Level	Dusk	Vehicle Skids Deer	Going Straight Ahead	South	North			
6133	County Road	Clallam		95250							4.65	No																							

[illegible]

[illegible]

OBJECTID	JURISDICTION	COUNTY	CITY	PRIMARY TRACWAY	BLOCK NUMBER	INTERSECTING TRACWAY	DIST FROM REF. POINT	MI. OF RT. POINT	COMP DIR FROM RT. POINT	REFERENCE POINT NAME	MILEPOST A B	IS ONLY TRUCKS/ BUS/ TRAILER	REPORT NUMBER	DATE	TIME	MOST SEVERE INJURY TYPE	NUM INV.	NUM FATAL	NUM NON FATAL	NUM PERSONS	NUM BIKES	VEH. 1 TYPE	VEH. 2 TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD SURFACE CONDITION	ROADWAY CHARACTERISTIC	LIGHTING CONDITION	FIRST COLLISION TYPE, OBST. STRUCK	VEH. 1 ACTION	VEH. 2 ACTION	VEH. 1 COMPASS DIRECTION FROM	VEH. 1 COMPASS DIRECTION TO	VEH. 2 COMPASS DIRECTION FROM	VEH. 2 COMPASS DIRECTION TO
6299	State Route	Challam	Port Angeles	101			24577	No	6400760	2015-02-08 07:50	Possible Injury	2	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	East	West	Vehicle Stopped	Vehicle Stopped
6300	State Route	Challam	Port Angeles	101			24577	No	6411607	2015-04-14 13:48	Possible Injury	2	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	East	West	Vehicle Stopped	Vehicle Stopped
6301	State Route	Challam	Port Angeles	101			24577	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	East	West	Vehicle Stopped	Vehicle Stopped
6302	State Route	Challam	Port Angeles	101			24577	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	East	West	Vehicle Stopped	Vehicle Stopped
6303	State Route	Challam	Port Angeles	101			24577	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	East	West	Vehicle Stopped	Vehicle Stopped
6304	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6305	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6306	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6307	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6308	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6309	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6310	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6311	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6312	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6313	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6314	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6315	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6316	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6317	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6318	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6319	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6320	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6321	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6322	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6323	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6324	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6325	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6326	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6327	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6328	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6329	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6330	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6331	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6332	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6333	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6334	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6335	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6336	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6337	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10 10:10	Possible Injury	1	0	0	0	0	0	0	0	0	0	Pickup, Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
6338	State Route	Challam	Port Angeles	101			24578	No	64082248	2017-06-10																									

[illegible]

	JURISDICTION	COUNTY	CITY	PRIMARY THOROUGHFARE	BLOCK NUMBER	INTERSECTING THOROUGHFARE	DIST FROM REF POINT	MI OF RT	COMP DIR FROM REF POINT	REFERENCE POINT NAME	MILEPOST	IS ONLY HISTORY SUSPENSE	REPORT NUMBER	DATE	TIME	MOST SEVERE VIOLATION TYPE	NUM PL	NUM PL	NUM PL	NUM PL	NUM PL	NUM PL	VEH 1 TYPE	VEH 2 TYPE	VEH 1 ACTION	VEH 2 ACTION	VEH 1 COMPASS DIRECTION FROM	VEH 1 COMPASS DIRECTION TO	VEH 2 COMPASS DIRECTION FROM	VEH 2 COMPASS DIRECTION TO	
6470	State Route	Chatham	Sequim	101N126305			0.04	No		6855239	2018-10-31 16:45	No Apparent Injury	0	0	3	0	0	0	0	0	0	Passenger Car	Passenger Car	Intersection Related but Not at Intersection	Overcast	Going Straight Ahead	Stopped at Traffic	South	North	Vehicle Stopped	Vehicle Stopped
6471	State Route	Chatham	Sequim	101N126305			0.09	No		6822065	2018-07-11 12:40	No Apparent Injury	0	0	2	0	0	0	0	0	0	Passenger Car	Passenger Car	Clear or Partly Cloudy	Overcast	Going Straight Ahead	Making Left Turn	South	North	Vehicle Stopped	Vehicle Stopped
6481	State Route	Chatham	Sequim	101N126305			0.11	No		2727648	2015-12-19 16:59	No Apparent Injury	0	0	2	0	0	0	0	0	0	Passenger Car	Passenger Car	Intersection and Related	Raining	Going Straight Ahead	Making Left Turn	South	East	Vehicle Stopped	Vehicle Stopped
6482	State Route	Chatham	Sequim	101N126305			0.11	No		2018716	2018-08-18 14:47	Apparent Injury	0	0	0	0	0	0	0	0	0	Passenger Car	Passenger Car	Clear or Partly Cloudy	Overcast	Going Straight Ahead	Making Left Turn	South	East	Vehicle Stopped	Vehicle Stopped
6483	State Route	Chatham	Sequim	101N126305			0.11	No		7579370	2018-10-29 09:41	No Apparent Injury	0	0	2	0	0	0	0	0	0	Passenger Car	Passenger Car	Intersection and Related	Clear or Partly Cloudy	Going Straight Ahead	Making Left Turn	South	West	Vehicle Stopped	Vehicle Stopped
6484	State Route	Chatham	Sequim	101N126305			0.11	No		6400206	2015-02-08 15:48	No Apparent Injury	0	0	2	0	0	0	0	0	0	Passenger Car	Passenger Car	Intersection and Related	Clear or Partly Cloudy	Going Straight Ahead	Making Left Turn	South	West	Vehicle Stopped	Vehicle Stopped
6485	State Route	Chatham	Sequim	101N126456			0.09	No		6428565	2015-05-24 11:00	No Apparent Injury	0	0	2	0	0	0	0	0	0	Passenger Car	Passenger Car	Clear or Partly Cloudy	Clear or Partly Cloudy	Going Straight Ahead	Going Straight Ahead	North	South	Vehicle Stopped	Vehicle Stopped
6487	State Route	Chatham	Sequim	101N126456			0.09	No		6428565	2015-05-24 11:00	No Apparent Injury	0	0	2	0	0	0	0	0	0	Passenger Car	Passenger Car	Clear or Partly Cloudy	Clear or Partly Cloudy	Going Straight Ahead	Going Straight Ahead	North	South	Vehicle Stopped	Vehicle Stopped
6488	State Route	Chatham	Sequim	101N126456			0.06	No		2727885	2017-06-18 16:16	Possible Injury	1	0	1	0	0	0	0	0	0	Passenger Car	Passenger Car	Intersection Related but Not at Intersection	Overcast	Going Straight Ahead	Making Left Turn	West	North	Vehicle Stopped	Vehicle Stopped
6489	State Route	Chatham	Sequim	101N126456			0.08	No		2727873	2017-06-22 16:39	Possible Injury	0	0	2	0	0	0	0	0	0	Passenger Car	Passenger Car	Intersection and Related	Clear or Partly Cloudy	Going Straight Ahead	Going Straight Ahead	West	North	Vehicle Stopped	Vehicle Stopped
6490	State Route	Chatham	Sequim	101N126456			0.08	No		2015-08-10 16:51	No Apparent Injury	0	0	2	0	0	0	0	0	0	0	Passenger Car	Passenger Car	Intersection and Related	Clear or Partly Cloudy	Going Straight Ahead	Going Straight Ahead	West	North	Vehicle Stopped	Vehicle Stopped
6491	State Route	Chatham	Sequim	101N126456			0.08	No		7579389	2017-08-09 16:03	Suspected Minor Injury	1	0	2	0	0	0	0	0	0	Passenger Car	Passenger Car	Intersection and Related	Clear or Partly Cloudy	Going Straight Ahead	Going Straight Ahead	West	North	Vehicle Stopped	Vehicle Stopped
6492	State Route	Chatham	Sequim	101N126456			0.08	No		6428565	2015-05-24 11:00	No Apparent Injury	0	0	2	0	0	0	0	0	0	Passenger Car	Passenger Car	Intersection and Related	Clear or Partly Cloudy	Going Straight Ahead	Going Straight Ahead	West	North	Vehicle Stopped	Vehicle Stopped
6493	State Route	Chatham	Sequim	101N126456			0.08	No		7579418	2017-12-29 14:30	Suspected Minor Injury	1	0	2	0	0	0	0	0	0	Passenger Car	Passenger Car	Intersection and Related	Clear or Partly Cloudy	Going Straight Ahead	Going Straight Ahead	West	North	Vehicle Stopped	Vehicle Stopped
6494	State Route	Chatham	Sequim	101N126210			0.05	No		2017878	2018-10-20 16:51	No Apparent Injury	0	0	2	0	0	0	0	0	0	Passenger Car	Passenger Car	Intersection and Related	Clear or Partly Cloudy	Going Straight Ahead	Going Straight Ahead	East	West	Vehicle Stopped	Vehicle Stopped
6495	State Route	Chatham	Sequim	101N126210			0.13																								

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WA STATE PLANE SOUTH 2010 FORWARD	WA STATE PLANE SOUTH 2010 FORWARD	COL YEAR	COL ME	COL TYPE	COL SEVERITY	COL HUMAN FACTOR	COL AGE	COL IMPAIRED	COL SURFACE CONDITIONS	COL LIGHT CONDITIONS	COL LOCATION	COL GEOMETRY	STUDY AREA	COL VIL. USER	COL TYPE TEXT	COL SEVERITY TEXT	COL HUMAN FACTOR, Decoded	COL MEWN	COL LIGHT CONDITIONS Decoded	COL SURFACE CONDITIONS Decoded	COL LOCATION Decoded	COL RISKY BEHAVIOR	Intersection Acc. Name	Emphasis	Roadway Reporture
6479	998180.59	101047.88	2018	Vehicles Parties	Property Damage Only	Follow Too Closely	25-34		Dry	Dark/Dusk	Intersection Related but Not at Intersection	Straight Level	No Vulnerable Road User Involved		Follow Too Closely	Follow Too Closely			Dark/Dusk		Intersection Related but Not at Intersection	Follow Too Closely	No		
6480	998180.59	101047.88	2018	Vehicles Parties	Property Damage Only	Follow Too Closely	25-34		Dry	Dark/Dusk	Intersection Related but Not at Intersection	Straight Level	No Vulnerable Road User Involved		Follow Too Closely	Follow Too Closely			Dark/Dusk		Intersection Related but Not at Intersection	Follow Too Closely	No		
6481	998180.42	1010136.52	2015	Vehicles Parties	Property Damage Only	Other/Unknown	65+		Dry	Dark/Street Lights On	At/Through an Intersection	Unknown	Older Road User 65+		Other/Unknown	Other/Unknown			Dark/Street Lights On		At/Through an Intersection	No Risky Driving Behaviors Involved	Yes		
6482	998179.89	1010136.52	2015	Vehicles Parties	Property Damage Only	Other/Unknown	65+		Dry	Dark/Street Lights On	At/Through an Intersection	Unknown	Older Road User 65+		Other/Unknown	Other/Unknown			Dark/Street Lights On		At/Through an Intersection	No Risky Driving Behaviors Involved	Yes		
6483	998169.78	1010136.52	2015	Vehicles Parties	Property Damage Only	Other/Unknown	65+		Dry	Dark/Street Lights On	At/Through an Intersection	Unknown	Older Road User 65+		Other/Unknown	Other/Unknown			Dark/Street Lights On		At/Through an Intersection	No Risky Driving Behaviors Involved	Yes		
6484	998178.43	1010136.52	2015	Vehicles Parties	Property Damage Only	Other/Unknown	65+		Dry	Dark/Street Lights On	At/Through an Intersection	Unknown	Older Road User 65+		Other/Unknown	Other/Unknown			Dark/Street Lights On		At/Through an Intersection	No Risky Driving Behaviors Involved	Yes		
6485	998178.43	1010136.52	2015	Vehicles Parties	Property Damage Only	Other/Unknown	65+		Dry	Dark/Street Lights On	At/Through an Intersection	Unknown	Older Road User 65+		Other/Unknown	Other/Unknown			Dark/Street Lights On		At/Through an Intersection	No Risky Driving Behaviors Involved	Yes		
6486	100417.69	1009978.79	2015	Vehicles Parties	Property Damage Only	Follow Too Closely	25-34		Dry	Dark/Street Lights On	Intersection Related but Not at Intersection	Straight Level	No Vulnerable Road User Involved		Follow Too Closely	Follow Too Closely			Dark/Street Lights On		Intersection Related but Not at Intersection	Follow Too Closely	No		
6487	100417.69	1009978.79	2015	Vehicles Parties	Property Damage Only	Follow Too Closely	25-34		Dry	Dark/Street Lights On	Intersection Related but Not at Intersection	Straight Level	No Vulnerable Road User Involved		Follow Too Closely	Follow Too Closely			Dark/Street Lights On		Intersection Related but Not at Intersection	Follow Too Closely	No		
6488	100417.69	1009978.79	2015	Vehicles Parties	Property Damage Only	Follow Too Closely	25-34		Dry	Dark/Street Lights On	Intersection Related but Not at Intersection	Straight Level	No Vulnerable Road User Involved		Follow Too Closely	Follow Too Closely			Dark/Street Lights On		Intersection Related but Not at Intersection	Follow Too Closely	No		
6489	100417.69	1009978.79	2015	Vehicles Parties	Property Damage Only	Follow Too Closely	25-34		Dry	Dark/Street Lights On	Intersection Related but Not at Intersection	Straight Level	No Vulnerable Road User Involved		Follow Too Closely	Follow Too Closely			Dark/Street Lights On		Intersection Related but Not at Intersection	Follow Too Closely	No		
6490	100417.69	1009978.79	2015	Vehicles Parties	Property Damage Only	Follow Too Closely	25-34		Dry	Dark/Street Lights On	Intersection Related but Not at Intersection	Straight Level	No Vulnerable Road User Involved		Follow Too Closely	Follow Too Closely			Dark/Street Lights On		Intersection Related but Not at Intersection	Follow Too Closely	No		
6491	100417.69	1009978.79	2015	Vehicles Parties	Property Damage Only	Follow Too Closely	25-34		Dry	Dark/Street Lights On	Intersection Related but Not at Intersection	Straight Level	No Vulnerable Road User Involved		Follow Too Closely	Follow Too Closely			Dark/Street Lights On		Intersection Related but Not at Intersection	Follow Too Closely	No		
6492	100417.69	1009978.79	2015	Vehicles Parties	Property Damage Only	Follow Too Closely	25-34		Dry	Dark/Street Lights On	Intersection Related but Not at Intersection	Straight Level	No Vulnerable Road User Involved		Follow Too Closely	Follow Too Closely			Dark/Street Lights On		Intersection Related but Not at Intersection	Follow Too Closely	No		
6493	100417.69	1009978.79	2015	Vehicles Parties	Property Damage Only	Follow Too Closely	25-34		Dry	Dark/Street Lights On	Intersection Related but Not at Intersection	Straight Level	No Vulnerable Road User Involved		Follow Too Closely	Follow Too Closely			Dark/Street Lights On		Intersection Related but Not at Intersection	Follow Too Closely	No		
6494	100417.69	1009978.79	2015	Vehicles Parties	Property Damage Only	Follow Too Closely	25-34		Dry	Dark/Street Lights On	Intersection Related but Not at Intersection	Straight Level	No Vulnerable Road User Involved		Follow Too Closely	Follow Too Closely			Dark/Street Lights On		Intersection Related but Not at Intersection	Follow Too Closely	No		
6495	995169.22	1010510.63	2016	Vehicles Parties	Property Damage Only	Follow Too Closely	25-34		Dry	Dark/Street Lights On	Intersection Related but Not at Intersection	Straight Level	No Vulnerable Road User Involved		Follow Too Closely	Follow Too Closely			Dark/Street Lights On		Intersection Related but Not at Intersection	Follow Too Closely	No		
6496	995169.22	1010510.63	2016	Vehicles Parties	Property Damage Only	Follow Too Closely	25-34		Dry	Dark/Street Lights On	Intersection Related but Not at Intersection	Straight Level	No Vulnerable Road User Involved		Follow Too Closely	Follow Too Closely			Dark/Street Lights On		Intersection Related but Not at Intersection	Follow Too Closely	No		
6497	995169.22	1010510.63	2016	Vehicles Parties	Property Damage Only	Follow Too Closely	25																		

	JURISDICTION	COUNTY	CITY	PRIMARY TRAFFICWAY	BLOCK NUMBER	INTERSECTING TRAFFICWAY	DIST FROM REF POINT	MILE OF FT	COMP DIR FROM REF POINT	REFERENCE POINT NAME	MILEPOST A B	SR ONLY RAMP SUSPENSE LINE	REPORT NUMBER	DATE	TIME	MOST SEVERE INJURY TYPE	NUM INV	NUM FATAL	NUM NON FATAL	NUM PERSONS KILLED	NUM PERSONS INJURED	VEH 1 TYPE	VEH 2 TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD SURFACE CONDITION	ROADWAY CHARACTERISTIC	LIGHTING CONDITION	FIRST COLLISION TYPE OBJECT STRUCK	VEH 1 ACTION	VEH 2 ACTION	VEH 1 COMPASS DIRECTION FROM	VEH 1 COMPASS DIRECTION TO	VEH 2 COMPASS DIRECTION FROM	VEH 2 COMPASS DIRECTION TO	
6659	State Route	Chatham	01								251.01	No	E607541	2016-01-15	15:18	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Overcast	Dry	Straight & Level	Dark Street Lights On	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Slowing	From opposite direction - one left turn - one straight	East	West	East	West
6660	State Route	Chatham	01								251.01	No	E606244	2016-01-08	08:24	No Apparent Injury	0	0	2	0	0	Passenger Car	Truck Tractor & Semi-Trailer	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Slowing	From opposite direction - one left turn - one straight	East	West	East	West
6661	State Route	Chatham	01								251.01	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6662	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6663	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6664	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6665	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6666	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6667	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6668	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6669	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6670	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6671	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6672	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6673	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6674	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6675	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6676	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6677	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6678	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6679	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6680	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6681	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6682	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6683	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6684	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6685	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6686	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6687	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6688	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6689	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6690	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6691	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6692	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6693	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6694	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6695	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	Vehicle Shopped
6696	State Route	Chatham	01								251.02	No	E612508	2016-08-01	01:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	R Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	From same direction - both going straight - one stopped - rear-end	East	West	Vehicle Shopped	

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CRASH ID	JURISDICTION	COUNTY	CITY	PRIMARY TRAFFICWAY	BLOCK NUMBER	INTERSECTING TRAFFICWAY	DIST FROM REF POINT	MI OF RT	COMP DIR FROM REF POINT	REFERENCE POINT NAME	MILEPOST A	STATE ONLY HISTORIC SUSPENSE NO	REPORT NUMBER	DATE	TIME	MOST SEVERE ACT	NUM INVOLVED	NUM FATAL	NUM INJURY	NUM NON INJURY	NUM PROP	VEH 1 TYPE	VEH 2 TYPE	VEH 1 ACTION	VEH 2 ACTION	VEH 1 COMPASS DIRECTION FROM	VEH 1 COMPASS DIRECTION TO	VEH 2 COMPASS DIRECTION FROM	VEH 2 COMPASS DIRECTION TO	
6339	State Route	Chatham	101				253.10	No	ET17655	2017-06-14 17:41	Possible Injury	1	0	1	0	0	1	0	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Other	At Intersection and Not Related	Clear or Partly Cloudy	Daylight	Earth Bank or Ledge	Going Straight Ahead	East	West
6340	State Route	Chatham	101				253.10	No	ET17655	2017-06-14 17:41	Possible Injury	1	0	1	0	0	1	0	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Other	At Intersection and Not Related	Clear or Partly Cloudy	Daylight	Earth Bank or Ledge	Going Straight Ahead	East	West
6341	State Route	Chatham	101				253.11	No	ET15006	2019-04-27 22:47	No Apparent Injury	0	0	1	0	0	1	0	0	0	0	Passenger Car	Passenger Car	At Intersection and Not Related	Clear or Partly Cloudy	Daylight	Domestic animal other (cat, dog, etc)	Going Straight Ahead	East	West
6342	State Route	Chatham	101				253.11	No	ET15006	2019-04-27 22:47	No Apparent Injury	0	0	1	0	0	1	0	0	0	0	Passenger Car	Passenger Car	At Intersection and Not Related	Clear or Partly Cloudy	Daylight	Domestic animal other (cat, dog, etc)	Going Straight Ahead	East	West
6343	State Route	Chatham	101				253.15	No	ET45877	2016-06-27 17:35	No Apparent Injury	0	0	1	0	0	1	0	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	At Intersection and Not Related	Clear or Partly Cloudy	Daylight	Vehicle Strikes Deer	Stopped at Signal or Stop Sign	East	West
6344	State Route	Chatham	101				253.18	No	ET15887	2017-05-01 02:21	No Apparent Injury	0	0	1	0	0	1	0	0	0	0	Passenger Car	Passenger Car	At Intersection and Not Related	Overcast	Daylight	Vehicle Strikes Deer	Going Straight Ahead	East	West
6345	State Route	Chatham	101				253.20	No	ET45877	2017-05-01 02:21	No Apparent Injury	0	0	1	0	0	1	0	0	0	0	Passenger Car	Passenger Car	At Intersection and Not Related	Overcast	Daylight	Vehicle Strikes Deer	Going Straight Ahead	East	West
6346	State Route	Chatham	101				253.30	No	ET60879	2018-03-30 12:30	No Apparent Injury	0	0	1	0	0	1	0	0	0	0	Passenger Car	Passenger Car	At Intersection and Not Related	Clear or Partly Cloudy	Daylight	Vehicle Strikes Deer	Going Straight Ahead	East	West
6347	State Route	Chatham	101				253.32	No	ET60879	2018-03-30 12:30	No Apparent Injury	0	0	1	0	0	1	0	0	0	0	Passenger Car	Passenger Car	At Intersection and Not Related	Clear or Partly Cloudy	Daylight	Vehicle Strikes Deer	Going Straight Ahead	East	West
6348	State Route	Chatham	101				253.40	No	ET04447	2016-11-02 18:13	No Apparent Injury	0	0	1	0	0	1	0	0	0	0	Passenger Car	Passenger Car	At Intersection and Not Related	Raining	Daylight	Vehicle Strikes Deer	Going Straight Ahead	East	West
6349	State Route	Chatham	101				253.44	No	ET60921	2018-11-14 17:30	No Apparent Injury	0	0	1	0	0	1	0	0	0	0	Passenger Car	Passenger Car	At Intersection and Not Related	Overcast	Daylight	Wood Sign Post	Going Straight Ahead	East	West
6350	State Route	Chatham	101				253.45	No	ET15887	2017-05-01 02:21	No Apparent Injury	0	0	1	0	0	1	0	0	0	0	Passenger Car	Passenger Car	At Intersection and Not Related	Overcast	Daylight	Wood Sign Post	Going Straight Ahead	East	West
6351	State Route	Chatham	101				253.45	No	ET67745	2016-12-28 08:38	No Apparent Injury	0	0	1	0	0	1	0	0	0	0	Passenger Car	Passenger Car	At Intersection and Not Related	Overcast	Daylight	Wood Sign Post	Going Straight Ahead	East	West
6352	State Route	Chatham	101				253.46	No	ET17702	2018-02-06 07:45	No Apparent Injury	0	0	1	0	0	1	0	0	0	0	Passenger Car	Passenger Car	At Intersection and Not Related	Overcast	Daylight	Wood Sign Post	Going Straight Ahead	East	West
6353	State Route	Chatham	101				253.70	No	ET52680	2019-06-08 13:42	No Apparent Injury	0	0	1	0	0	1	0	0	0	0	Passenger Car	Passenger Car	At Intersection and Not Related	Overcast	Daylight	Vehicle Strikes Deer	Going Straight Ahead	East	West
6354	State Route	Chatham	101				253.70	No	ET46176	2018-03-12 09:14	No Apparent Injury	0	0	1	0	0	1	0	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	At Intersection and Not Related	Overcast	Daylight	Vehicle Strikes Deer	Going Straight Ahead	East	

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OBJECTID	JURISDICTION	COUNTY	CITY	PRIMARY THRUWAY	BLOCK NUMBER	INTERSECTING THRUWAY	DET FROM REPT POINT	ML OF PT	COMP DIR FROM REPT POINT	REFERENCE POINT NAME	MILEPOST	A	B	SR ONLY SUSPENSE NO.	REPORT NUMBER	DATE	TIME	MOST SEVERE INJURY TYPE	NUM INV	NUM FATAL	NUM NON FATAL	NUM SUSP	NUM UNK	VEH 1 TYPE	VEH 2 TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD SURFACE CONDITION	ROADWAY CHARACTERISTIC	LIGHTING CONDITION	TRFCT COLLISION TYPE OBJECT STRUCK	VEH 1 ACTION	VEH 2 ACTION	VEH 1 COMPASS DIRECTION FROM	VEH 1 COMPASS DIRECTION TO	VEH 2 COMPASS DIRECTION FROM	VEH 2 COMPASS DIRECTION TO
7019	State Route	Chatham	101								258.54	No			E70492	2017-08-20 10:10	No Apparent Injury	0	0	1	0	0	0	Passenger Car		At Intersection and Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	Roadway Ditch	Vehicle Struck Deer	Going Straight Ahead	West	North	East	
7020	State Route	Chatham	101								258.59	No			E70493	2017-10-20 12:30	Possible Injury	0	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	Vehicle Struck Deer	Going Straight Ahead	West	North	East		
7021	State Route	Chatham	101								258.61	No			E54884	2014-10-10 05:19	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		Not at Intersection and Not Related	Overcast	Wet	Curve & Level	Daylight	Vehicle Struck Deer	Going Straight Ahead	South	North	East		
7022	State Route	Chatham	101								258.63	No			E54885	2014-10-10 05:19	No Apparent Injury	0	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	Vehicle Struck Deer	Going Straight Ahead	South	North	East		
7023	State Route	Chatham	101								258.64	No			E49622	2016-11-24 05:30	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Passenger Car	At Intersection and Related	Clear or Partly Cloudy	Dry	Curve & Level	Daylight	Vehicle Struck Deer	Going Straight Ahead	South	North	East		
7024	State Route	Chatham	101								258.65	No			E51787	2016-02-20 15:50	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Passenger Car	At Intersection and Related	Clear or Partly Cloudy	Dry	Curve & Level	Daylight	Vehicle Struck Deer	Going Straight Ahead	South	North	East		
7025	State Route	Chatham	101								258.65	No			E54886	2017-08-20 10:10	Suspected Minor Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		At Intersection and Not Related	Clear or Partly Cloudy	Dry	Roadway Ditch	Daylight	Vehicle Struck Deer	Going Straight Ahead	South	North	East		
7026	State Route	Chatham	101								258.68	No			E68081	2019-02-28 10:00	Unknown	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		Not at Intersection and Not Related	Snowing	Brown/Slush	Daylight	Roadway Ditch	Going Straight Ahead	West	East	West	East		
7027	State Route	Chatham	101								258.70	No			E54892	2016-05-20 17:16	Possible Injury	0	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Overcast	Dry	Straight & Grade	Daylight	Vehicle Struck Deer	Going Straight Ahead	East	West	East	West	
7028	State Route	Chatham	101								258.73	No			E54893	2016-05-20 17:16	No Apparent Injury	0	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Overcast	Dry	Straight & Grade	Daylight	Vehicle Struck Deer	Going Straight Ahead	East	West	East	West	
7029	State Route	Chatham	101								258.81	No			E46688	2018-10-04 18:00	No Apparent Injury	0	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Curve & Grade	Dark Street Lights On	Vehicle Struck Deer	Going Straight Ahead	West	East	West	East	
7030	State Route	Chatham	101								258.82	No			E54894	2018-10-04 18:00	No Apparent Injury	0	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Overcast	Dry	Straight & Grade	Daylight	Vehicle Struck Deer	Going Straight Ahead	West	East	West	East	
7031	State Route	Chatham	101								259.00	No			E74387	2018-01-26 06:48	No Apparent Injury	0	0	2	0	0	0	Passenger Car		Not at Intersection and Not Related	Raining	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - sideswipe	Going Straight Ahead	West	East	West	East	
7032	State Route	Chatham	101								259.00	No			E68670	2019-09-20 18:00	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Other	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	From same direction - both going straight - both moving - sideswipe	Going Straight Ahead	West	East	West	East	
7033	State Route	Chatham	101								259.12	No			E74388	2019-12-20 18:00	No Apparent Injury	0	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Dark No Street Lights	Vehicle Struck Deer	Going Straight Ahead	West	East	West	East	
7034	State Route	Chatham	101								259.16	No			E54452	2019-08-26 19:00	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		At Intersection and Not Related	Unknown	Unknown	Straight & Level	Unknown	Meal Sign Post	Going Straight Ahead	West	East	West	East	
7035	State Route	Chatham	101								259.18	No			E74389	2014-11-27 19:18	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		At Intersection and Not Related	Overcast	Wet	Straight & Level	Dark Street Lights On	Entering an angle	Going Straight Ahead	North	West	East	West	
7036	State Route	Chatham	101								259.18	No			E37865	2019-02-28 10:00	No Apparent Injury	0	0	2	0	0	0	Passenger Car		At Intersection and Not Related	Overcast	Wet	Straight & Level	Dark Street Lights On	Entering an angle	Going Straight Ahead	North	West	East	West	
7037	State Route	Chatham	101								259.18	No			E38893	2014-12-13 18:00	Suspected Minor Injury	1	0	2	0	0	0	Passenger Car		At Intersection and Related	Clear or Partly Cloudy	Dry	Straight & Level	Dark Street Lights On	Entering an angle	Going Straight Ahead	East	West	South	West	
7038	State Route	Chatham	101								259.19	No			E74390	2018-03-20 16:40	No Apparent Injury	0	0	2	0	0	0	Passenger Car		At Intersection and Not Related	Overcast	Dry	Straight & Level	Dark Street Lights On	From same direction - both turning right - one stopped - near end	Going Straight Ahead	South	West	South	West	
7039	State Route	Chatham	101								259.22	No			E72700	2014-06-26 12:00	No Apparent Injury	0	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	Vehicle Struck Deer	Going Straight Ahead	West	East	West	East	
7040	State Route	Chatham	101								259.23	No			E74391	2014-06-26 12:00	No Apparent Injury	0	0	1	0	0	0	Passenger Car		At Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - both moving - sideswipe	Going Straight Ahead	West	East	West	East	
7041	State Route	Chatham	101								259.30	No			E74392	2014-03-20 08:45	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Passenger Car	At Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From opposite direction - both going straight - both moving - sideswipe	Going Straight Ahead	West	East	West	East	
7042	State Route	Chatham	101								259.37	No			E81149	2019-01-08 09:18	No Apparent Injury	0	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Overcast	Wet	Straight & Grade	Daylight	Earth Bank or Edge	Going Straight Ahead	East	West	East	West	
7043	State Route	Chatham	101								259.41	No			E74393	2018-03-20 16:40	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Passenger Car	At Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - both moving - sideswipe	Going Straight Ahead	West	East	West	East	
7044	State Route	Chatham	101								259.59	No			E50388	2016-06-10 10:24	No Apparent Injury	0	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	Roadway Ditch	Going Straight Ahead	West	East	West	East	
7045	State Route	Chatham	101								259.60	No			E50389	2016-06-10 10:24	No Apparent Injury	0	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	Vehicle Struck Deer	Going Straight Ahead	West	East	West	East	
7046	State Route	Chatham	101								259.70	No			E38061	2014-11-20 14:23	No Apparent Injury	0	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Overcast	Dry	Straight & Grade	Daylight	From same direction - all others	Going Straight Ahead	West	East	West	East	
7047	State Route	Chatham	101								259.77	No			E414166	2015-04-06 13:15	Suspected Minor Injury	1	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Truck (Flatbed Van etc)	At Intersection and Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	From same direction - one right turn - one straight	Going Straight Ahead	West	East	South	West	
7048	State Route	Chatham	101								259.80	No			E414167	2015-04-06 13:15	Suspected Minor Injury	1	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Truck (Flatbed Van etc)	At Intersection and Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	West	East	South	West	
7049	State Route	Chatham	101								259.80	No			E37056	2014-06-13 06:51	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		Not at Intersection and Not Related	Raining	Wet	Straight & Grade	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	West	East	West	East	
7050	State Route	Chatham	101								259.80	No			E70498	2017-12-18 16:21	No Apparent Injury	0	0	2	0	0	0	Passenger Car		Not at Intersection and Not Related	Overcast	Dry	Straight & Level	Daylight	From same direction - both going straight - both moving - sideswipe	Going Straight Ahead	West	East	West	East	
7051	State Route	Chatham	101								259.80	No			E74394	2014-10-10 05:19	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Passenger Car	At Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	West	East	West	East	
7052	State Route	Chatham	101								259.86	No			E72801	2017-09-28 12:30	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		At Intersection and Related	Overcast	Dry	Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	North	South	Vehicle Stopped	West	
7053	State Route	Chatham	101								259.88	No			E74395	2017-09-28 12:30	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		At Intersection and Related	Overcast	Dry	Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	North	South	Vehicle Stopped	West	
7054	State Route	Chatham	101								259.95	No			E37050	2014-06-20 18:51	Suspected Minor Injury	2	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		At Intersection and Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	Entering an angle	Going Straight Ahead	North	South	Vehicle Stopped	West	
7055	State Route	Chatham	101								259.96	No			E38425	2014-09-24 14:40	Possible Injury	4	0	2	0	0	0	Passenger Car		At Intersection and Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	Entering an angle	Going Straight Ahead	North	South	Vehicle Stopped	West	
7056	State Route	Chatham	101								259.95	No			E74396	2018-03-20 16:40	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Passenger Car	At Intersection and Not Related	Overcast	Dry	Straight & Level	Daylight	From same direction - both going straight - both moving - sideswipe	Going Straight Ahead	West	East	West	East	
7057	State Route	Chatham	101								259.96	No			E66713	2017-01-10 08:30	No Apparent Injury	0	0	1	0	0	0	Passenger Car		At Intersection and Not Related	Raining	Wet	Straight & Level	Daylight	Entering an angle	Going Straight Ahead	North	South	Vehicle Stopped	West	
7058	State Route	Chatham	101								259.96	No			E74397	2018-03-20 16:40	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		At Intersection and Not Related	Overcast	Dry	Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	West	East	West	East	
7059	State Route	Chatham	101								259.95	No			E74398	2018-03-20 16:40	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		At Intersection and Not Related	Overcast	Dry	Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	West	East	West	East	
7060	State Route	Chatham	101								260.01	No			E72812	2017-11-24 04:17	Possible Injury	1	0	2	0	0	0	Passenger Car		Not at Intersection and Not Related	Raining	Wet	Straight & Level	Dark Street Lights On	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	West	East	West	East	
7061	State Route	Chatham	101								260.03	No			E74399	2018-03-20 16:40	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		At Intersection and Not Related	Overcast	Dry	Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	West	East	West	East	
7062	State Route	Chatham	101								260.10	No			E30367	2014-08-08 14:41	Possible Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	From same direction - both going						

MY_DRIVER CONTRIBUTING CIRCUMSTANCE 1, UNIT 1		MY_DRIVER CONTRIBUTING CIRCUMSTANCE 2, UNIT 1	MY_DRIVER CONTRIBUTING CIRCUMSTANCE 3, UNIT 1	MY_DRIVER AGE UNIT 1	MY_DRIVER GENDER UNIT 1	MY_DRIVER CONTRIBUTING CIRCUMSTANCE 1, UNIT 2	MY_DRIVER CONTRIBUTING CIRCUMSTANCE 2, UNIT 2	MY_DRIVER CONTRIBUTING CIRCUMSTANCE 3, UNIT 2	MY_DRIVER AGE UNIT 2	MY_DRIVER GENDER UNIT 2	BIICYCLIST CONTRIBUTING CIRCUMSTANCE 1, UNIT 1	BIICYCLIST CONTRIBUTING CIRCUMSTANCE 2, UNIT 1	BIICYCLIST CONTRIBUTING CIRCUMSTANCE 3, UNIT 1	BIICYCLIST AGE UNIT 1	BIICYCLIST GENDER UNIT 1	BIICYCLIST CONTRIBUTING CIRCUMSTANCE 1, UNIT 2	BIICYCLIST CONTRIBUTING CIRCUMSTANCE 2, UNIT 2	BIICYCLIST CONTRIBUTING CIRCUMSTANCE 3, UNIT 2	BIICYCLIST AGE UNIT 2	BIICYCLIST GENDER UNIT 2	PEDESTRIAN CONTRIBUTING CIRCUMSTANCE 1, UNIT 1	PEDESTRIAN CONTRIBUTING CIRCUMSTANCE 2, UNIT 2	PEDESTRIAN CONTRIBUTING CIRCUMSTANCE 3, UNIT 2	PEDESTRIAN AGE UNIT 2	PEDESTRIAN GENDER UNIT 2	FIRST IMPACT LOCATION City, St, County, Rd. & Misc. 2010, forward, Rt
7019	Improper Turn/Merge			18	Male																				Pass Right Shoulder Decreasing Misspost	
7020	None			22	Female																				Lane 2 Increasing Misspost	
7021	None			53	Female																				Lane 1 Increasing Misspost	
7022	None			29	None																				Lane 1 Decreasing Misspost	
7023	Follow Too Closely	Inattention		68	Female	None				48	Female														Intersecting Road Increasing Misspost	
7024	Inattention			73	Female	None				53	Female														Intersecting Road Increasing Misspost	
7025	Under Influence of Alcohol			49	Male																				Intersecting Road Increasing Misspost	
7026	Exceeding Reas. Safe Speed			30	Female																				Intersecting Road Increasing Misspost	
7027	None			19	Female																				Right Shoulder Increasing Misspost	
7028	None			25	Male																				Lane 1 Decreasing Misspost	
7029	None			25	Male																				Lane 2 Increasing Misspost	
7030	Apparently Asleep or Fatigued			31	Male																				In Median Decreasing Misspost	
7031	Apparently Fatigued	Follow Too Closely		34	None																				Lane 2 Increasing Misspost	
7032	Inattention			63	Male	None				18	Female														Lane 1 Increasing Misspost	
7033	None			71	Male					21	Male														Lane 2 Increasing Misspost	
7034	Apparently Ill			71	Male																				Increasing Other Location	
7035	Did Not Grant RW to Vehicle			24	Male					24	Female														Lane 1 Decreasing Misspost	
7036	Did Not Grant RW to Vehicle			19	Male	None				66	Male														Lane 1 Decreasing Misspost	
7037	None			35	Female	Other Contributing Circ Not Listed																			Lane 1 Decreasing Misspost	
7038	Follow Too Closely			51	Male	None																			Intersecting Road Decreasing Misspost	
7039	None			44	Female	Other Contributing Circ Not Listed				29	Female														Lane 1 Increasing Misspost	
7040	None			51	Male																				Lane 2 Decreasing Misspost	
7041	Operating Defective Equipment			39	Female	None				18	Male															Lane 1 Decreasing Misspost
7042	Apparently Fatigued	Over Center Line		24	Female																				In Median Decreasing Misspost	
7043	Inattention	Lapses or Drifting		34	Male																				In Median Decreasing Misspost	
7044	Inattention	Operating Hands-Free Cell Phone		34	Male																				Lane 1 Increasing Misspost	
7045	None			38	None																				Pass Right Shoulder Increasing Misspost	
7046	Exceeding Reas. Safe Speed			39	Female	None																				

[illegible]

	JURISDICTION	COUNTY	CITY	PRIMARY TRAVELWAY	BLOCK NUMBER	INTERSECTING TRAVELWAY	DIST FROM REF POINT	MILE OF RT	COMP DIR FROM RT	REFERENCE POINT NAME	MILEPOST A B	SA ONLY HOURS SUSPENSE LN	REPORT NUMBER	DATE RECEIVED	TIME	MOST SEVERE INJURY TYPE	NUM INV	NUM FATAL	NUM NON FATAL	NUM PERS	NUM UNK	VEH 1 TYPE	VEH 2 TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD SURFACE CONDITION	ROADWAY CHARACTERISTIC	LIGHTING CONDITION	FIRST COLLISION TYPE OBJECT STRUCK	VEH 1 ACTION	VEH 2 ACTION	VEH 1 COMPASS DIRECTION FROM	VEH 1 COMPASS DIRECTION TO	VEH 2 COMPASS DIRECTION FROM	VEH 2 COMPASS DIRECTION TO
7199	State Route	Chatham	001								265.00	No	E847161	2019-07-09	09:50	No Apparent Injury	0	0	1	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	Vehicle Strikes Deer	Going Straight Ahead		West	East		
7200	State Route	Chatham	001								265.00	No	E841200	2019-07-09	11:44	No Apparent Injury	0	0	1	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Roadway Ditch	Daylight	None	Going Straight Ahead		West	East		
7201	State Route	Chatham	001								265.05	No	E823597	2019-07-01	08:49	No Apparent Injury	0	0	1	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	All other non-collision	Going Straight Ahead		East	West		
7202	State Route	Chatham	001								265.05	No	E851800	2019-07-01	11:07	PT	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Pickup Panel Truck or Vanette under 10,000 lb.	Not at Intersection and Not Related	Overcast	Straight & Grade	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Other	West	East		
7203	State Route	Chatham	001								265.10	No	E72624	2019-11-11	16:25	No Apparent Injury	0	0	1	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	From same direction - both going straight - sideswipe	Going Straight Ahead	Other	West	East		
7204	State Route	Chatham	001								265.21	No	E478416	2019-11-01	20:30	Possible Injury	1	0	1	0	0	Passenger Car		Not at Intersection and Not Related	Overcast	Straight & Grade	Dark No Street Lights	Roadway Ditch	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead		West	East		
7205	State Route	Chatham	001								265.43	No	E855116	2019-11-16	08:40	No Apparent Injury	0	0	1	0	0	Passenger Car	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Dark No Street Lights	Interchange	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Going Straight Ahead	West	East		
7206	State Route	Chatham	001								265.43	No	E853001	2018-03-28	16:39	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Pickup Panel Truck or Vanette under 10,000 lb.	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Going Straight Ahead	West	East		
7207	State Route	Chatham	001								268.44	No	E878744	2019-02-08	12:07	No Apparent Injury	0	0	2	0	0	Passenger Car	Pickup Panel Truck or Vanette under 10,000 lb.	Not at Intersection and Not Related	Brownish	Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Going Straight Ahead	East	West			
7208	State Route	Chatham	001								268.52	No	E824149	2019-08-16	15:05	No Apparent Injury	0	0	1	0	0	Passenger Car	Pickup Panel Truck or Vanette under 10,000 lb.	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Curve & Grade	Daylight	From opposite direction - both going straight - sideswipe	Going Straight Ahead	Going Straight Ahead	Northwest	South		
7209	State Route	Chatham	001								268.58	No	E811295	2019-03-21	13:05	No Apparent Injury	0	0	1	0	0	Passenger Car	Pickup Panel Truck or Vanette under 10,000 lb.	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	Guardrail - Leading End	Going Straight Ahead	Going Straight Ahead	East	West		
7210	State Route	Chatham	001								268.60	No	E850106	2019-04-08	14:00	No Apparent Injury	0	0	1	0	0	Passenger Car	Pickup Panel Truck or Vanette under 10,000 lb.	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	Vehicle Strikes Deer	Going Straight Ahead	Going Straight Ahead	East	West		
7211	State Route	Chatham	001								268.64	No	E845431	2019-09-13	14:50	Possible Injury	1	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Pickup Panel Truck or Vanette under 10,000 lb.	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	Vehicle Strikes Deer	Going Straight Ahead	Going Straight Ahead	East	West		
7212	State Route	Chatham	001								267.70	No	E469521	2019-12-12	2:52	No Apparent Injury	0	0	3	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Curve & Level	Dark No Street Lights	Meal Sign Post	Going Straight Ahead		East	West		
7213	State Route	Chatham	001								267.71	No	E850106	2019-09-13	14:50	Possible Injury	1	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Pickup Panel Truck or Vanette under 10,000 lb.	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Curve & Level	Dark No Street Lights	Meal Sign Post	Going Straight Ahead		East	West		
7214	State Route	Chatham	001								268.80	No	E556193	2016-06-09	0:14	Possible Injury	3	0	2	0	0	Passenger Car	Passenger Car	Not at Intersection and Not Related	Overcast	Curve & Level	Daylight	From opposite direction - both going straight - sideswipe	Going Straight Ahead	Going Straight Ahead	West	East			
7215	State Route	Chatham	001								268.92	No	N01116	2016-12-01	12:35	No Apparent Injury	0	0	1	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Curve & Grade	Daylight	Vehicle Strikes Deer	Going Straight Ahead		West	East		
7216	State Route	Chatham	001								268.90	No	E824149	2019-08-16	15:05	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Pickup Panel Truck or Vanette under 10,000 lb.	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Curve & Grade	Daylight	From opposite direction - both going straight - sideswipe	Going Straight Ahead	Going Straight Ahead	West	East		
7217	State Route	Chatham	001								268.99	No	E818499	2019-12-08	17:19	No Apparent Injury	0	0	1	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Dark No Street Lights	Domestic animal other (cat, dog, etc)	Going Straight Ahead		West	East		
7218	State Route	Chatham	001								268.77	No	E847171	2019-08-21	12:11	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Pickup Panel Truck or Vanette under 10,000 lb.	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Curve & Level	Daylight	Vehicle Strikes Deer	Going Straight Ahead		West	East		
7219	State Route	Chatham	001								269.00	No	E465079	2019-07-17	14:24	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Pickup Panel Truck or Vanette under 10,000 lb.	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Going Straight Ahead	East	West		
7220	State Route	Chatham	001								269.20	No	E781615	2019-03-21	21:53	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Pickup Panel Truck or Vanette under 10,000 lb.	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Going Straight Ahead	West	East		
7221	State Route	Chatham	001								269.20	No	E850106	2019-11-11	16:25	No Apparent Injury	0	0	1	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Curve & Level	Daylight	From opposite direction - both going straight - sideswipe	Going Straight Ahead	Going Straight Ahead	West	East		
7222	State Route	Chatham	001								269.21	No	E344683	2014-07-23	13:17	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Pickup Panel Truck or Vanette under 10,000 lb.	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Curve & Grade	Daylight	Guardrail - Face	Going Straight Ahead		East	West		
7223	State Route	Chatham	001								269.21	No	E465079	2019-08-16	15:05	Suspected Minor Injury	2	0	2	0	0	Motorcycle		At Intersection and Related	Clear or Partly Cloudy	Dry	Curve & Grade	Daylight	From same direction - one left turn - one straight	Going Straight Ahead	Stopped at Roadway	East	West		
7224	State Route	Chatham	001								269.21	No	E850106	2019-07-01	11:07	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Pickup Panel Truck or Vanette under 10,000 lb.	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Curve & Grade	Daylight	From same direction - one left turn - one straight	Going Straight Ahead	Stopped at Roadway	East	West		
7225	State Route	Chatham	001								269.21	No	E462998	2019-09-17	1:55	Possible Injury	1	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Pickup Panel Truck or Vanette under 10,000 lb.	At Intersection and Related	Clear or Partly Cloudy	Dry	Curve & Grade	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	East	West		
7226	State Route	Chatham	001								269.31	No	E553181	2019-04-18	11:50	Possible Injury	1	0	1	0	0	Passenger Car	Passenger Car	At Intersection and Related	Clear or Partly Cloudy	Dry	Curve & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East		
7227	State Route	Chatham	001								269.21	No	E851343	2019-07-26	09:31	No Apparent Injury	0	0	2	0	0	Passenger Car	Passenger Car	At Intersection and Related	Clear or Partly Cloudy	Dry	Curve & Grade	Daylight	Entering at angle	Going Straight Ahead		South	North		
7228	State Route	Chatham	001								269.21	No	E813949	2017-03-09	1:00	Suspected Minor Injury	2	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Pickup Panel Truck or Vanette under 10,000 lb.	At Intersection and Not Related	Overcast	Curve & Level	Daylight	From same direction - both moving - head-on	Going Straight Ahead	Going Straight Ahead	West	East			
7229	State Route	Chatham	001								269.21	No	E721113	2017-11-13	19:19	Possible Injury	2	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Pickup Panel Truck or Vanette under 10,000 lb.	At Intersection and Not Related	Clear or Partly Cloudy	Dry	Curve & Level	Dark No Street Lights	From same direction - one left turn - one straight	Going Straight Ahead	Going Straight Ahead	West	East		
7230	State Route	Chatham	001								269.21	No	E774788	2017-12-28	16:27	Possible Injury	2	0	2	0	0	Passenger Car		Intersection Related but Not at Intersection	Overcast	Curve & Grade	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	East	West			
7231	State Route	Chatham	001								269.21	No	E769910	2019-08-08	09:22	No Apparent Injury	0	0	2	0	0	Passenger Car	Passenger Car	At Intersection and Related	Clear or Partly Cloudy	Dry	Curve & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	East	West		
7232	State Route	Chatham	001								269.21	No	E846884	2018-10-06	15:55	No Apparent Injury	0	0	2	0	0	Passenger Car	Passenger Car	At Intersection and Related	Clear or Partly Cloudy	Dry	Curve & Level	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Going Straight Ahead	West	East		
7233	State Route	Chatham	001								269.22	No	E868661	2019-11-16	09:09	No Apparent Injury	0	0	3	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Passenger Car	Intersection Related but Not at Intersection	Clear or Partly Cloudy	Dry	Curve & Grade	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	East	West		
7234	State Route	Chatham	001								269.22	No	E850106	2019-09-11	17:27	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Pickup Panel Truck or Vanette under 10,000 lb.	Intersection Related but Not at Intersection	Clear or Partly Cloudy	Dry	Curve & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	East	West		
7235	State Route	Chatham	001								269.23	No	E302922	2014-01-19	17:38	Possible Injury	1	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Pickup Panel Truck or Vanette under 10,000 lb.	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Curve & Level	Dark No Street Lights	Roadway Ditch	Going Straight Ahead		West	East		
7236	State Route	Chatham	001								269.24	No	E850106	2017-07-31	13:03	Possible Injury	2	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Pickup Panel Truck or Vanette under 10,000 lb.	Not at Intersection and Not Related	Overcast	Curve & Level	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Going Straight Ahead	West	East			
7237	State Route	Chatham	001								269.24	No	E850106	2019-08-16	15:05	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Pickup Panel Truck or Vanette under 10,000 lb.	At Intersection and Related	Clear or Partly Cloudy	Dry	Curve & Level	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Going Straight Ahead	West	East		
7238	State Route	Chatham	001								269.25	No	E378905	2014-11-16	16:45	Suspected Minor Injury	1	0	1	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Curve & Level	Dark	Vehicle Strikes Deer	Going Straight Ahead		West	East		
7239	State Route	Chatham	001								269.26	No	E564835	2019-10-01	10:40	No Apparent Injury	0	0	2	0	0	Passenger Car	Passenger Car	At Intersection and Not Related	Clear or Partly Cloudy	Dry	Curve & Level	Daylight	From opposite direction - both going straight - moving - head-on	Going Straight Ahead	Going Straight Ahead	West	East		
7240	State Route	Chatham	001								269.40	No	E732246	2017-10-04	16:52	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Passenger Car	Intersection Related but Not at Intersection	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Going Straight Ahead	West	East		
7241	State Route	Chatham	001								269.41	No	E830212	2018-08-13	13:43	No Apparent Injury	0	0	3	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Going Straight Ahead	West	East		
7242	State Route	Chatham	001								269.42	No	E830212	2018-08-13	13:43	No Apparent Injury	0	0	3	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Going Straight Ahead	West	East		
7243	State Route	Chatham	001								269.62	No	E897769	2019-09-03	09:25	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb.	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Going Straight Ahead	West	East		
7244	State Route	Chatham	001								269.62	No	E897769	2019-09-03	09:25	No Apparent Injury	0	0	2	0															

FILE NO.	MY_DRIVER CONTRIBUTING CIRCUMSTANCE, 1 UNIT, 1	MY_DRIVER CONTRIBUTING CIRCUMSTANCE, 2 UNIT, 1	MY_DRIVER CONTRIBUTING CIRCUMSTANCE, 3 UNIT, 1	MY_DRIVER AGE UNIT, 1	MY_DRIVER GENDER UNIT, 1	MY_DRIVER CONTRIBUTING CIRCUMSTANCE, 1 UNIT, 2	MY_DRIVER CONTRIBUTING CIRCUMSTANCE, 2 UNIT, 2	MY_DRIVER CONTRIBUTING CIRCUMSTANCE, 3 UNIT, 2	MY_DRIVER AGE UNIT, 2	MY_DRIVER GENDER UNIT, 2	BICYCLIST CONTRIBUTING CIRCUMSTANCE, 1 UNIT, 1	BICYCLIST CONTRIBUTING CIRCUMSTANCE, 2 UNIT, 1	BICYCLIST CONTRIBUTING CIRCUMSTANCE, 3 UNIT, 1	BICYCLIST AGE UNIT, 1	BICYCLIST GENDER UNIT, 1	BICYCLIST CONTRIBUTING CIRCUMSTANCE, 1 UNIT, 2	BICYCLIST CONTRIBUTING CIRCUMSTANCE, 2 UNIT, 2	BICYCLIST CONTRIBUTING CIRCUMSTANCE, 3 UNIT, 2	BICYCLIST AGE UNIT, 2	BICYCLIST GENDER UNIT, 2	PEDESTRIAN CONTRIBUTING CIRCUMSTANCE, 1 UNIT, 1	PEDESTRIAN CONTRIBUTING CIRCUMSTANCE, 2 UNIT, 1	PEDESTRIAN CONTRIBUTING CIRCUMSTANCE, 3 UNIT, 1	PEDESTRIAN AGE UNIT, 1	PEDESTRIAN GENDER UNIT, 1	PEDESTRIAN CONTRIBUTING CIRCUMSTANCE, 1 UNIT, 2	PEDESTRIAN CONTRIBUTING CIRCUMSTANCE, 2 UNIT, 2	PEDESTRIAN CONTRIBUTING CIRCUMSTANCE, 3 UNIT, 2	PEDESTRIAN AGE UNIT, 2	PEDESTRIAN GENDER UNIT, 2	FIRST IMPACT LOCATION City, St., County, Rd. & Misc. 2016 Forward_R
7199	None			71	Male																					Lane 1 Increasing Misspot					
7200	None			72	Female																					Past Right Shoulder Increasing Misspot					
7201	None			36	Male																					Lane 1 Decreasing Misspot					
7202	Following Too Closely			18	Male	None			17	Male																Lane 1 Decreasing Misspot					
7203	Apparently Asleep or Fatigued	Other Contributing Circ Not Listed	Inattention	19	Male	None			35	Male	None															Lane 1 Increasing Misspot					
7204	Smoking	Driver Interacting with Passengers, Anim		37	Female	None			37	Male	None															Lane 1 Increasing Misspot					
7205	Over Center Line			19	Male	None			29	Male	None															Past Right Shoulder Increasing Misspot					
7206	Driver Adjusting Audio or Entertainment	Follow Too Closely		44	Female	None			44	Female	None															Lane 1 Decreasing Misspot					
7207	Exceeding Reas. Safe Speed			30	Female	None			60	Male	None															Lane 1 Decreasing Misspot					
7208	Exceeding Reas. Safe Speed			60	Male	None																				Lane 1 Decreasing Misspot					
7209	Other Contributing Circ Not Listed	Distractors Outside Vehicle		60	Male	None																				Intersecting Road Decreasing Misspot					
7210	None			60	Female	None																				Past Right Shoulder Decreasing Misspot					
7211	None			46	Female	None																				Lane 1 Decreasing Misspot					
7212	Under Influence of Alcohol			52	Male	None																				Past Right Shoulder Decreasing Misspot					
7213	None			64	None	None			75	Female	None															Lane 1 Increasing Misspot					
7214	Inattention	Over Center Line		58	Female	None																				Lane 1 Decreasing Misspot					
7215	None			54	Male	None																				Lane 1 Increasing Misspot					
7216	Apparently ill			54	Male	None																				Lane 1 Decreasing Misspot					
7217	None			74	Female	None																				Lane 1 Increasing Misspot					
7218	None			21	Male	None			58	Male	None															Lane 1 Decreasing Misspot					
7219	Exceeding Reas. Safe Speed			34	Female	None			68	Female	None															Lane 1 Increasing Misspot					
7220	Follow Too Closely			66	Male	None			43	Male	None															Lane 1 Increasing Misspot					
7221	Over Center Line			22	Male	None																				Lane 1 Increasing Misspot					
7222	Inattention	Exceeding Reas. Safe Speed		60	Male	None			60	Male	None															Right Shoulder Decreasing Misspot					
7223	Exceeding Reas. Safe Speed			61	Male	None			33	Male	None															Lane 1 Decreasing Misspot					
7224	Driver Interacting with Passengers, Anim	Did Not Grant RW to Vehicle	Follow Too Closely	38	Female	None			38	Female	None															Lane 1 Decreasing Misspot					
7225	Under Influence of Alcohol			38	Female	None			38	Female	None															Lane 1 Increasing Misspot					
7226	Inattention	Did Not Grant RW to Vehicle		86	Male	None			86	Male	None															Lane 1 Increasing Misspot					
7227	Improper Turn/Merge			56	Female	None			26	Male	None															Intersecting Road Increasing Misspot					
7228	Inattention	Over Center Line		73	Male	None			73	Male	None															Lane 1 Increasing Misspot					
7229	Exceeding Reas. Safe Speed			31	Male	None			31	Male	None															Lane 1 Decreasing Misspot					
7230	Inattention			29	Male	Driver Not Distracted			29	Male																Right Shoulder Decreasing Misspot					
7231	Inattention	Exceeding Reas. Safe Speed		84	Female	None			84	Female	None															Right Shoulder Decreasing Misspot					
7232	Exceeding Reas. Safe Speed			52	Male	Driver Not Distracted			52	Male	None															Lane 1 Decreasing Misspot					
7233	Inattention	Driver Not Distracted		78	Male	None			78	Male	None															Lane 1 Increasing Misspot					
7234	None			78	Male	None			60	Male																Lane 1 Decreasing Misspot					
7235	Under Influence of Alcohol			19	Female	None																				Past Right Shoulder Increasing Misspot					
7236	Follow Too Closely			77	Male	None			77	Male																Lane 1 Decreasing Misspot					
7237	None			74	Male	None																				Lane 1 Decreasing Misspot					
7238	None			59	Male	None			49	Male																Lane 1 Increasing Misspot					
7239	Under Influence of Drugs	Inattention		49	Male	None			49	Male																Lane 1 Increasing Misspot					
7240	Distractors Outside Vehicle			33	Male	None			64	Female	None															Lane 1 Decreasing Misspot					
7241	Inattention	Exceeding Reas. Safe Speed		60	Female	None			60	Female	None															Lane 1 Increasing Misspot					
7242	Under Influence of Alcohol	Eating or Drinking		53	Male	None			53	Male	None															Lane 1 Increasing Misspot					
7243	None			50	Female	Under Influence of Alcohol	Over Center Line																			Lane 1 Increasing Misspot					
7244	Operating Defective Equipment	Other Contributing Circ Not Listed		34	Female	None			63	Male	None															Lane 1 Decreasing Misspot					
7245	Operating Other Electronic Devices (texting)	Follow Too Closely		28	Female	None			69	Male	None															Lane 1 Increasing Misspot					
7246	Inattention	Exceeding Reas. Safe Speed		73	Male	None			73	Male	None															Lane 1 Increasing Misspot					
7247	Inattention	Follow Too Closely		66	Female	None			33	Male	None															Lane 1 Increasing Misspot					
7248	Inattention	Follow Too Closely		41	Male	None																				Past Right Shoulder Increasing Misspot					
7249	Inattention	Follow Too Closely		68	Female	None			68	Female	None															Lane 1 Increasing Misspot					
7250	None			67	Female	None																				Lane 1 Decreasing Misspot					
7251	Exceeding Reas. Safe Speed			78	Female	None																				Past Right Shoulder Decreasing Misspot					
7252	Over Center Line			83	Male	Driver Not Distracted			26	Male																Lane 1 Increasing Misspot					
7253	Follow Too Closely			43	Female	None			43	Female	None															Lane 1 Increasing Misspot					
7254	Inattention	Follow Too Closely		26	Female	None			74	Male	None															Lane 1 Decreasing Misspot					
7255	Follow Too Closely			57	Female	None			64	Male																Lane 1 Decreasing Misspot					
7256	None			27	Female	None																				Lane 1 Increasing Misspot					
7257	Inattention			29	Male	None																				Past Right Shoulder Increasing Misspot					
7258	None			27	Female	None																				Lane 1 Decreasing Misspot					
7259	Driver Not Distracted			58	Female	None																				Lane 1 Decreasing Misspot					
7260	None			28	Male	Operating Defective Equipment			33	Male																Lane 1 Decreasing Misspot					
7261	None			71	Female	None			23	Male	None															Lane 1 Increasing Misspot					
7262	Exceeding Reas. Safe Speed			68	Female	None			54	Female	None															Lane 1 Increasing Misspot					
7263	Follow Too Closely			68	Male	None																				Past Right Shoulder Increasing Misspot					
7264	Exceeding Reas. Safe Speed			62	Male	Driver Not Distracted			56	Female	None															Lane 1 Increasing Misspot					
7265	Inattention	Did Not Grant RW to Vehicle		43	Female	None			43	Female	None															Left Turn Lane Increasing Misspot					
7266	Improper Turn/Merge			62	Female	None			78	Female	None															Lane 1 Increasing Misspot					
7267	Did Not Grant RW to Vehicle	Inattention		78	Female	None			46	Female	None															Lane 1 Increasing Misspot					
7268	Exceeding Reas. Safe Speed			60	Male	None			34	Female	None															Lane 1 Decreasing Misspot					
7269	None			27	Female	None																				Lane 1 Increasing Misspot					
7270	Inattention	Follow Too Closely		23	Female	None																				Past Right Shoulder Decreasing Misspot					
7271	Did Not Grant RW to Vehicle			62	Female	None			71	Male	None															Lane 1 Increasing Misspot					
7272	Other Contributing Circ Not Listed			62	Female	Other Contributing Circ Not Listed			30	Male	None															Lane 2 Increasing Misspot					
7273	Improper Turn/Merge	Inattention		18	Female	None			64	Female	None															Intersecting Road Increasing Misspot					
7274	Inattention			24	Male	None			24	Male	None															Left Turn Lane Decreasing Misspot					
7275	Did Not Grant RW to Vehicle			38	Female	None			38	Female	None															Lane 1 Increasing Misspot					
7276	Did Not Grant RW to Vehicle			72	Female	None			72	Female	None															Lane 1 Increasing Misspot					
7277	Did Not Grant RW to Vehicle			64	Female	None			64	Female	None															Lane 1 Increasing Misspot					
7278	Other Contributing Circ Not Listed			62	Male	None																				Lane 1 Increasing Misspot					
7279	Inattention	Driver Not Distracted		36	Female	None			36	Female	None															Past Right Shoulder Increasing Misspot					
7280	Disregard Stop Sign - Flashing Red	Did Not Grant RW to Vehicle		26	Male	None			56	Male	None															Lane 1 Increasing Misspot					
7281	Inattention			26	Male	None			26	Male	None																				

COL STATE PLATE SOUTH Y 2010 FORWARD	COL STATE PLATE SOUTH Y 2010 FORWARD	COL YEAR	COL MAKE	COL MODEL	COL TYPE	COL SEVERITY	COL_HUM FACTOR	COL AGE	COL IMPAIRED	COL SURFACE CONDITIONS	COL_LIGHT CONDITIONS	COL_LOCATION	COL_GEOMETRY	STREET AREA	COL_VEH USER	COL TYPE TEXT	COL SEVERITY TEXT	COL_HUMAN FACTOR_Record	COL_MVW	COL_LIGHT CONDITIONS Decoded	COL SURFACE CONDITIONS Decoded	COL_LOCATION Decoded	COL_RISKY BEHAVIOR	Intersection Area, Name	Emphasis Area	Roadway Departure
7199	1018214.32	101808.53	2019	1 Vehicle/Party	Animal	Property Damage Only	Other/Unknown	65+	No	Dry	Dawnlight	Not at Intersection/Dnwy/Roundabout	Straight & Grade	Older Road User 65+	Older Road User 65+	None	None	None	Daylight	Dry	Not at Intersection/Dnwy/Roundabout	None	No Risky Driving Behaviors Involved	No		No
7200	1018399.23	101775.38	2019	1 Vehicle/Party	Struck Object	Property Damage Only	Other/Unknown	35-44	No	Wet	Dawnlight	Not at Intersection/Dnwy/Roundabout	Straight & Grade	Older Road User 65+	Older Road User 65+	None	None	None	Daylight	Wet	Not at Intersection/Dnwy/Roundabout	None	No Risky Driving Behaviors Involved	No		No
7201	1018399.23	101620.56	2019	1 Vehicle/Party	At Other Non-Collision	Property Damage Only	Other/Unknown	35-44	No	Dry	Dawnlight	Not at Intersection/Dnwy/Roundabout	Straight & Grade	No Vulnerable Road User Involved	No Vulnerable Road User Involved	None	None	None	Daylight	Dry	Not at Intersection/Dnwy/Roundabout	None	No Risky Driving Behaviors Involved	No		No
7202	1018617.72	1018617.72	2019	1 Vehicle/Party	Struck Object	Property Damage Only	Other/Unknown	35-44	No	Wet	Dawnlight	Not at Intersection/Dnwy/Roundabout	Straight & Grade	Older Road User 65+	Older Road User 65+	None	None	None	Daylight	Wet	Not at Intersection/Dnwy/Roundabout	None	No Risky Driving Behaviors Involved	No		No
7203	1018617.72	1018617.72	2019	1 Vehicle/Party	Struck Object	Property Damage Only	Other/Unknown	35-44	No	Wet	Dawnlight	Not at Intersection/Dnwy/Roundabout	Straight & Grade	Older Road User 65+	Older Road User 65+	None	None	None	Daylight	Wet	Not at Intersection/Dnwy/Roundabout	None	No Risky Driving Behaviors Involved	No		No
7204	1018617.72	1018617.72	2019	1 Vehicle/Party	Struck Object	Property Damage Only	Other/Unknown	35-44	No	Wet	Dawnlight	Not at Intersection/Dnwy/Roundabout	Straight & Grade	Older Road User 65+	Older Road User 65+	None	None	None	Daylight	Wet	Not at Intersection/Dnwy/Roundabout	None	No Risky Driving Behaviors Involved	No		No
7205	1018617.72	1018617.72	2019	1 Vehicle/Party	Struck Object	Property Damage Only	Other/Unknown	35-44	No	Wet	Dawnlight	Not at Intersection/Dnwy/Roundabout	Straight & Grade	Older Road User 65+	Older Road User 65+	None	None	None	Daylight	Wet	Not at Intersection/Dnwy/Roundabout	None	No Risky Driving Behaviors Involved	No		No
7206	1018617.72	1018617.72	2019	1 Vehicle/Party	Struck Object	Property Damage Only	Other/Unknown	35-44	No	Wet	Dawnlight	Not at Intersection/Dnwy/Roundabout	Straight & Grade	Older Road User 65+	Older Road User 65+	None	None	None	Daylight	Wet	Not at Intersection/Dnwy/Roundabout	None	No Risky Driving Behaviors Involved	No		No
7207	1018617.72	1018617.72	2019	1 Vehicle/Party	Struck Object	Property Damage Only	Other/Unknown	35-44	No	Wet	Dawnlight	Not at Intersection/Dnwy/Roundabout	Straight & Grade	Older Road User 65+	Older Road User 65+	None	None	None	Daylight	Wet	Not at Intersection/Dnwy/Roundabout	None	No Risky Driving Behaviors Involved	No		No
7208	1018617.72	1018617.72	2019	1 Vehicle/Party	Struck Object	Property Damage Only	Other/Unknown	35-44	No	Wet	Dawnlight	Not at Intersection/Dnwy/Roundabout	Straight & Grade	Older Road User 65+	Older Road User 65+	None	None	None	Daylight	Wet	Not at Intersection/Dnwy/Roundabout	None	No Risky Driving Behaviors Involved	No		No
7209	1018617.72	1018617.72	2019	1 Vehicle/Party	Struck Object	Property Damage Only	Other/Unknown	35-44	No	Wet	Dawnlight	Not at Intersection/Dnwy/Roundabout	Straight & Grade	Older Road User 65+	Older Road User 65+	None	None	None	Daylight	Wet	Not at Intersection/Dnwy/Roundabout	None	No Risky Driving Behaviors Involved	No		No
7210	1018617.72	1018617.72	2019	1 Vehicle/Party	Struck Object	Property Damage Only	Other/Unknown	35-44	No	Wet	Dawnlight	Not at Intersection/Dnwy/Roundabout	Straight & Grade	Older Road User 65+	Older Road User 65+	None	None	None	Daylight	Wet	Not at Intersection/Dnwy/Roundabout	None	No Risky Driving Behaviors Involved	No		No
7211	1018617.72	1018617.72	2019	1 Vehicle/Party	Struck Object	Property Damage Only	Other/Unknown	35-44	No	Wet	Dawnlight	Not at Intersection/Dnwy/Roundabout	Straight & Grade	Older Road User 65+	Older Road User 65+	None	None	None	Daylight	Wet	Not at Intersection/Dnwy/Roundabout	None	No Risky Driving Behaviors Involved	No		No
7212	1018617.72	1018617.72	2019	1 Vehicle/Party	Struck Object	Property Damage Only	Other/Unknown	35-44	No	Wet	Dawnlight	Not at Intersection/Dnwy/Roundabout	Straight & Grade	Older Road User 65+	Older Road User 65+	None	None	None	Daylight	Wet	Not at Intersection/Dnwy/Roundabout	None	No Risky Driving Behaviors Involved	No		No
7213	1018617.72	1018617.72	2019	1 Vehicle/Party	Struck Object	Property Damage Only	Other/Unknown	35-44	No	Wet	Dawnlight	Not at Intersection/Dnwy/Roundabout	Straight & Grade	Older Road User 65+	Older Road User 65+	None	None	None	Daylight	Wet	Not at Intersection/Dnwy/Roundabout	None	No Risky Driving Behaviors Involved	No		No
7214	1018617.72	1018617.72	2019	1 Vehicle/Party	Struck Object	Property Damage Only	Other/Unknown	35-44	No	Wet	Dawnlight	Not at Intersection/Dnwy/Roundabout	Straight & Grade	Older Road User 65+	Older Road User 65+	None	None	None	Daylight	Wet	Not at Intersection/Dnwy/Roundabout	None	No Risky Driving Behaviors Involved	No		

OBJECTID	JURISDICTION	COUNTY	CITY	PRIMARY THRUWAY	BLOCK NUMBER	INTERSECTING THRUWAY	DET FROM REF POINT	ML OF PT	COMP DIR FROM REF POINT	REFERENCE POINT NAME	MILEPOST	A	B	SR ONLY SUSPENSE, NO	REPORT NUMBER	DATE	TIME	MOST SEVERE INJURY TYPE	NUM INV	NUM FATAL	NUM INJ	NUM PROP	NUM UNK	NUM SUSP	VEH 1 TYPE	VEH 2 TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD SURFACE CONDITION	ROADWAY CHARACTERISTIC	LIGHTING CONDITION	PRST COLLISION TYPE OBJECT STRUCK	VEH 1 ACTION	VEH 2 ACTION	VEH 1 COMPASS DIRECTION FROM	VEH 1 COMPASS DIRECTION TO	VEH 2 COMPASS DIRECTION FROM	VEH 2 COMPASS DIRECTION TO
7375	State Route	Jefferson	119								1.00	No			EC82762	2018-07-28 17:43	10:43	No Apparent Injury	0	0	1	0	0	0	Passenger Car	Pickup Panel Truck or Vanette under 10,000 lb.	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	Vehicle Strikes Deer	Going Straight Ahead		North	South		
7380	State Route	Jefferson	119								1.00	No			EC82762	2018-07-28 17:43	10:43	No Apparent Injury	0	0	1	0	0	0	Passenger Car	Pickup Panel Truck or Vanette under 10,000 lb.	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	Vehicle Strikes Deer	Going Straight Ahead		North	South		
7381	State Route	Jefferson	119								1.00	No			EC82762	2018-07-28 17:43	10:43	Possible Injury	1	0	1	0	0	0	Motorcycle		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	Vehicle overturned	Going Straight Ahead		South	North		
7382	State Route	Jefferson	119								1.05	No			EC15974	2017-12-24 19:44	19:44	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Come & Grade	Daylight	Tree or Shrub (stationary)	Going Straight Ahead		South	North		
7383	State Route	Jefferson	119								1.05	No			EC82922	2018-09-11 17:25	17:25	Suspected Minor Injury	0	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Come & Grade	Daylight	Tree or Shrub (stationary)	Going Straight Ahead		South	North		
7384	State Route	Jefferson	119								1.18	No			EC91205	2019-10-15 06:49	06:49	No Apparent Injury	0	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	Vehicle Strikes Deer	Going Straight Ahead		North	South		
7385	State Route	Jefferson	119								1.20	No			EC82922	2018-09-11 17:25	17:25	No Apparent Injury	0	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	Vehicle Strikes Deer	Going Straight Ahead		North	South		
7386	State Route	Jefferson	119								1.21	No			EC84992	2015-12-22 11:15	11:15	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Grade	Dark No Street Lights	Roadway Ditch	Going Straight Ahead		North	South		
7387	State Route	Jefferson	119								1.29	No			EC85957	2017-06-28 08:39	08:39	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	Roadway Ditch	Going Straight Ahead		North	South		
7388	State Route	Jefferson	119								1.47	No			EC85879	2016-06-04 08:39	08:39	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	Rock Bank or ledge	Going Straight Ahead		South	North		
7389	State Route	Jefferson	119								1.47	No			EC85859	2016-11-04 08:39	08:39	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Come & Grade	Dark No Street Lights	Vehicle Strikes Deer	Going Straight Ahead		South	North		
7390	State Route	Jefferson	119								1.53	No			EC85859	2016-11-04 08:39	08:39	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Come & Grade	Dark No Street Lights	Vehicle Strikes Deer	Going Straight Ahead		South	North		
7391	State Route	Jefferson	119								1.63	No			EC86308	2019-06-01 07:38	07:38	Possible Injury	1	0	1	0	0	0	Motorcycle		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	Roadway Ditch	Going Straight Ahead		North	South		
7392	State Route	Jefferson	119								1.63	No			EC84941	2019-06-01 07:38	07:38	Possible Injury	1	0	2	0	0	0	Truck (Flatbed Van etc)		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Come & Grade	Daylight	From same direction - both going straight - both moving - sideswipe	Going Straight Ahead		North	South		
7393	State Route	Jefferson	119								1.68	No			EC84941	2019-06-01 07:38	07:38	Possible Injury	1	0	2	0	0	0	Truck (Flatbed Van etc)		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Come & Grade	Daylight	From same direction - both going straight - both moving - sideswipe	Going Straight Ahead		North	South		
7394	State Route	Jefferson	119								1.69	No			EC83959	2018-05-24 08:43	08:43	Suspected Serious Injury	1	0	1	0	0	0	Truck (Flatbed Van etc)		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Come & Level	Dark Street Lights On	Over Embankment - No Guadrail Present	Going Straight Ahead		North	South		
7395	State Route	Jefferson	119								1.69	No			EC83959	2018-05-24 08:43	08:43	Suspected Serious Injury	1	0	1	0	0	0	Truck (Flatbed Van etc)		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Come & Level	Dark Street Lights On	Over Embankment - No Guadrail Present	Going Straight Ahead		North	South		
7396	State Route	Jefferson	119								1.63	No			EC83236	2014-07-07 16:14	16:14	No Apparent Injury	0	0	2	0	0	0	Passenger Car	Pickup Panel Truck or Vanette under 10,000 lb.	Intersection Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead		North	South		
7397	State Route	Jefferson	119								1.63	No			EC83236	2014-07-07 16:14	16:14	No Apparent Injury	0	0	2	0	0	0	Passenger Car	Pickup Panel Truck or Vanette under 10,000 lb.	Intersection Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead		North	South		
7398	State Route	Jefferson	119								1.63	No			EC83236	2014-07-07 16:14	16:14	No Apparent Injury	0	0	2	0	0	0	Passenger Car	Pickup Panel Truck or Vanette under 10,000 lb.	Intersection Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead		North	South		
7399	State Route	Jefferson	119								1.63	No			EC84704	2018-04-16 08:20	08:20	Possible Injury	1	0	2	0	0	0	Passenger Car	Pickup Panel Truck or Vanette under 10,000 lb.	Intersection Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead		North	South		
7400	State Route	Jefferson	119								1.70	No			EC84704	2018-04-16 08:20	08:20	Possible Injury	1	0	2	0	0	0	Passenger Car	Pickup Panel Truck or Vanette under 10,000 lb.	Intersection Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead		North	South		
7401	State Route	Jefferson	119								1.76	No			EC85958	2019-07-17 17:58	17:58	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	Vehicle Strikes Deer	Going Straight Ahead		North	South		
7402	State Route	Jefferson	119								1.89	No			EC85957	2017-06-24 08:39	08:39	No Apparent Injury	0	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Come & Level	Daylight	Vehicle Strikes Deer	Going Straight Ahead		North	South		
7403	State Route	Jefferson	119								2.08	No			EC17001	2014-11-22 21:00	21:00	No Apparent Injury	0	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	Vehicle Strikes Deer	Going Straight Ahead		North	South		
7404	State Route	Jefferson	119								2.59	No			EC84133	2015-06-26 13:45	13:45	Possible Injury	2	0	2	0	0	0	Passenger Car	Pickup Panel Truck or Vanette under 10,000 lb.	Intersection Related but Not at Intersection	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead		South	North		
7405	State Route	Jefferson	119								2.60	No			EC85862	2016-09-08 15:30	15:30	No Apparent Injury	0	0	2	0	0	0	Truck (Flatbed Van etc)		Intersection Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead		South	North		
7406	State Route	Jefferson	119								2.69	No			EC85862	2016-09-08 15:30	15:30	No Apparent Injury	0	0	2	0	0	0	Truck (Flatbed Van etc)		Intersection Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead		South	North		
7407	State Route	Jefferson	119								2.81	No			EC83238	2014-04-27 10:50	10:50	Suspected Minor Injury	2	0	2	0	0	0	Truck Tractor		Intersection Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead		South	North		
7408	State Route	Jefferson	119								2.91	No			EC84884	2016-03-14 14:08	14:08	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		Intersection Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - one left turn - one straight	Going Straight Ahead		South	North		
7409	State Route	Jefferson	119								2.93	No			EC69394	2016-11-06 20:05	20:05	Possible Injury	1	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Come & Grade	Dark No Street Lights	Vehicle overturned	Going Straight Ahead		North	South		
7410	State Route	Jefferson	119								2.93	No			EC84721	2019-09-28 08:30	08:30	Possible Injury	1	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Come & Level	Daylight	Roadway Ditch	Going Straight Ahead		North	South		
7411	State Route	Jefferson	119								2.71	No			EC84721	2019-09-28 08:30	08:30	Possible Injury	1	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Come & Level	Daylight	Tree or Shrub (stationary)	Going Straight Ahead		North	South		
7412	State Route	Jefferson	119								2.80	No			EC84880	2016-10-30 18:45	18:45	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Come & Level	Dark No Street Lights	Vehicle Strikes Deer	Going Straight Ahead		North	South		
7413	State Route	Jefferson	119								2.81	No			EC84880	2016-10-30 18:45	18:45	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Come & Level	Dark No Street Lights	Vehicle Strikes Deer	Going Straight Ahead		North	South		
7414	State Route	Jefferson	119								3.00	No			EC82893	2016-08-26 21:00	21:00	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	Vehicle Strikes Deer	Going Straight Ahead		South	North		
7415	State Route	Jefferson	119								3.00	No			EC82893	2016-08-26 21:00	21:00	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	Vehicle Strikes Deer	Going Straight Ahead		South	North		
7416	State Route	Jefferson	119								3.10	No			EC82893	2016-08-26 21:00	21:00	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb.		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	Vehicle Strikes Deer	Going Straight Ahead		South	North		
7417	State Route	Jefferson	119								3.10	No			EC84544	2019-08-11 12:48	12:48	Suspected Minor Injury	1	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	Tree or Shrub (stationary)	Going Straight Ahead		North	South		
7418	State Route	Jefferson	119								3.11	No			EC84544	2019-08-11 12:48	12:48	Suspected Minor Injury	1	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	Tree or Shrub (stationary)	Going Straight Ahead		North	South		
7419	State Route	Jefferson	119								3.18	No			EC87264	2017-01-28 07:33	07:33	No Apparent Injury	0	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	Roadway Ditch	Going Straight Ahead		South	North		
7420	State Route	Jefferson	119								3.32	No			EC86969	2018-06-07 07:37	07:37	No Apparent Injury	0	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	Roadway Ditch	Going Straight Ahead		South	North		
7421	State Route	Jefferson	119								3.32	No			EC86969	2018-06-07 07:37	07:37	No Apparent Injury	0	0	1	0	0	0	Passenger Car		Not at Intersection and Not Related	Clear or										

OBJECTID	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE 1, UNIT 1	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE 2, UNIT 1	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE 3, UNIT 1	MV_DRIVER AGE UNIT 1	MV_DRIVER GENDER UNIT 1	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE 1, UNIT 2	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE 2, UNIT 2	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE 3, UNIT 2	MV_DRIVER AGE UNIT 2	MV_DRIVER GENDER UNIT 2	BICYCLIST_CONTRIBUTING CIRCUMSTANCE 1, UNIT 1	BICYCLIST_CONTRIBUTING CIRCUMSTANCE 2, UNIT 1	BICYCLIST_CONTRIBUTING CIRCUMSTANCE 3, UNIT 1	BICYCLIST AGE UNIT 1	BICYCLIST GENDER UNIT 1	BICYCLIST_CONTRIBUTING CIRCUMSTANCE 1, UNIT 2	BICYCLIST_CONTRIBUTING CIRCUMSTANCE 2, UNIT 2	BICYCLIST_CONTRIBUTING CIRCUMSTANCE 3, UNIT 2	BICYCLIST AGE UNIT 2	BICYCLIST GENDER UNIT 2	PEDESTRIAN_CONTRIBUTING CIRCUMSTANCE 1, UNIT 2	PEDESTRIAN_CONTRIBUTING CIRCUMSTANCE 2, UNIT 2	PEDESTRIAN_CONTRIBUTING CIRCUMSTANCE 3, UNIT 2	PEDESTRIAN AGE UNIT 2	PEDESTRIAN GENDER UNIT 2	PEDESTRIAN GENDER UNIT 2	FIRST IMPACT LOCATION City, St, County, Rd, A, Misc, 2019, Forward, Rt	
7375	None				64 Female																						Lane 1 Decreasing Misspot	
7380	Exceeding Reas. Safe Speed				36 Male																						Past Right Shoulder Increasing Misspot	
7381	Under Influence of Alcohol	Exceeding Reas. Safe Speed			36 Male																						Right Shoulder Increasing Misspot	
7384	Exceeding Reas. Safe Speed				64 Female																						Past Right Shoulder Increasing Misspot	
7385	Operating Handheld Cell Phone				64 Female																						Past Right Shoulder Increasing Misspot	
7386	None				36 Female																						Lane 1 Increasing Misspot	
7386	Operating Defective Equipment				36 Female																						Past Right Shoulder Decreasing Misspot	
7387	Intention	Distractions Outside Vehicle			36 Female																						Past Right Shoulder Decreasing Misspot	
7388	Apparently Asleep or Fatigued				64 Female																						Past Right Shoulder Decreasing Misspot	
7389	None				64 Female																						Lane 1 Increasing Misspot	
7390	Other Contributing Circ Not Listed	Apparently Fatigued			64 Female																						Past Right Shoulder Decreasing Misspot	
7391	Other Contributing Circ Not Listed				64 Male																						Past Right Shoulder Increasing Misspot	
7392	None				40 Male	Other Contributing Circ Not Listed																					Lane 2 Decreasing Misspot	
7393	Exceeding Reas. Safe Speed				40 Male																						Past Right Shoulder Decreasing Misspot	
7394	Apparently Fatigued				71 Male																						Past Right Shoulder Decreasing Misspot	
7395	Follow Too Closely				63 Male	Follow Too Closely				72 Male																	Lane 1 Decreasing Misspot	
7396	Driver Adjusting Audio or Entertainment				20 Male	None				63 Male	None																Lane 1 Decreasing Misspot	
7397	Improper Turn/Merge	Intention			67 Female	None				37 Female																	Past Right Shoulder Decreasing Misspot	
7398	Intention	Exceeding Reas. Safe Speed			12 Male	Driver Not Distracted				12 Male																		Lane 1 Decreasing Misspot
7399	Follow Too Closely				79 Female	Driver Not Distracted				78 Male																		Lane 1 Decreasing Misspot
7400	Apparently Asleep or Fatigued				61 Male																						Past Right Shoulder Decreasing Misspot	
7401	None				61 Male																							
7402	None				69 Female																						Lane 1 Decreasing Misspot	
7403	Other Contributing Circ Not Listed				69 Female																						Lane 1 Increasing Misspot	
7404	Distractions Outside Vehicle				69 Female	None				76 Male																	Lane 1 Increasing Misspot	
7405	Intention	Follow Too Closely			36 Male	None				26 Male																	Lane 1 Increasing Misspot	
7406	Intention				17 Female	None				44 Male																	Lane 1 Increasing Misspot	
7407	Distractions Outside Vehicle	Follow Too Closely			43 Male	None				43 Male	None																Lane 1 Increasing Misspot	
7408	Exceeding Reas. Safe Speed				60 Female	None				60 Female																	Lane 1 Increasing Misspot	
7409	Intention				66 Male																						Past Right Shoulder Increasing Misspot	
7410	Intention	Exceeding Reas. Safe Speed	Distractions Outside Vehicle		20 Male																						Past Right Shoulder Increasing Misspot	
7411	Other Contributing Circ Not Listed				14 Female																						Past Right Shoulder Increasing Misspot	
7412	None				68 Male																						Lane 1 Decreasing Misspot	
7413	Under Influence of Alcohol	Apparently Fatigued			37 Male																						Past Right Shoulder Increasing Misspot	
7414	Driver Not Distracted				37 Male																						Lane 1 Decreasing Misspot	
7415	Operating Defective Equipment				36 Male																						Past Right Shoulder Increasing Misspot	
7416	Under Influence of Alcohol				36 Male																						Past Right Shoulder Increasing Misspot	
7417	Under Influence of Alcohol	Apparently Fatigued			21 Male																						Past Right Shoulder Increasing Misspot	
7418	None				36 Male																						Lane 1 Decreasing Misspot	
7419	Apparently Asleep or Fatigued				36 Female																						Past Right Shoulder Increasing Misspot	
7420	Apparently Asleep or Fatigued	Intention			64 Male																						Past Right Shoulder Increasing Misspot	
7421	None				64 Female																						Lane 1 Increasing Misspot	
7422	None				66 Female																						Past Right Shoulder Increasing Misspot	
7423	Other Contributing Circ Not Listed	Distractions Outside Vehicle			64 Female	None																						Past Right Shoulder Increasing Misspot
7424	Under Influence of Drugs	Over Center Line			70 Male	None				22 Male																		Intersecting Road Increasing Misspot
7425	Apparently Asleep or Fatigued				64 Male																						Past Right Shoulder Increasing Misspot	
7426	None				64 Male																						Past Right Shoulder Increasing Misspot	
7427	Under Influence of Alcohol				22 Female																						Past Right Shoulder Increasing Misspot	
7428	Exceeding Reas. Safe Speed				24 Male	None				56 Female																	Lane 1 Increasing Misspot	
7429	Follow Too Closely				67 Female	None				67 Male																	Lane 1 Decreasing Misspot	
7430	None				67 Female																						Lane 1 Decreasing Misspot	
7431	None				67 Female																						Lane 1 Decreasing Misspot	
7432	Intention	Follow Too Closely			47 Male	None				76 Male	None																	Lane 1 Increasing Misspot
7433	Under Influence of Alcohol				47 Male	None				63 Male	None																Lane 1 Decreasing Misspot	
7434	Other Contributing Circ Not Listed				47 Male	None				74 Female																	Past Right Shoulder Increasing Misspot	
7435	Operating Defective Equipment	Intention			42 Male	None				42 Male																	Lane 1 Decreasing Misspot	
7436	Intention	Distractions Outside Vehicle			42 Male	None				42 Male																	Lane 1 Increasing Misspot	
7437	Follow Too Closely				24 Male	None				63 Female																	Lane 1 Increasing Misspot	
7438	Intention	Exceeding Reas. Safe Speed	Over Center Line		67 Male	None				31 Female																	Lane 1 Decreasing Misspot	
7439	Exceeding Reas. Safe Speed	Follow Too Closely			63 Male	None																					Lane 1 Increasing Misspot	
7440	Follow Too Closely				69 Male	None				67 Male																	Lane 1 Increasing Misspot	
7441	Exceeding Reas. Safe Speed				52 Female	None				39 Female																	Lane 1 Increasing Misspot	
7442	Apparently Asleep or Fatigued				64 Male																						Past Right Shoulder Increasing Misspot	
7443	Other Contributing Circ Not Listed	Over Center Line			64 Male	None				49 Male																	Past Right Shoulder Increasing Misspot	
7444	Exceeding Reas. Safe Speed				64 Male																						Lane 1 Decreasing Misspot	
7445	None				64 Male																						Past Right Shoulder Increasing Misspot	
7446	Other Contributing Circ Not Listed	Exceeding Reas. Safe Speed			67 Female																						Lane 1 Decreasing Misspot	
7447	Intention	Apparently Fatigued			23 Male																						Past Right Shoulder Decreasing Misspot	
7448	Under Influence of Drugs				23 Male																						Past Right Shoulder Increasing Misspot	
7449	Exceeding Reas. Safe Speed	Intention			26 Female																						Right Shoulder Decreasing Misspot	
7450	Apparently Asleep or Fatigued				16 Male																						Past Right Shoulder Decreasing Misspot	
7451	Improper Turn/Merge				67 Female																						Past Right Shoulder Increasing Misspot	
7452	Intention				64 Male																						Past Right Shoulder Increasing Misspot	
7453	Apparently Asleep or Fatigued				29 Male																						Past Right Shoulder Increasing Misspot	
7454	Other Contributing Circ Not Listed				64 Male	None																						Past Right Shoulder Increasing Misspot
7455	Intention	Follow Too Closely			64 Male	None				72 Male																	Lane 1 Decreasing Misspot	
7456	Exceeding Reas. Safe Speed				23 Male																						Lane 1 Decreasing Misspot	
7457	None				61 Male																						Past Right Shoulder Increasing Misspot	
7458	Intention				61 Male																						Past Right Shoulder Increasing Misspot	
7459	None				67 Female	None																						Lane 1 Increasing Misspot
7460	Unknown Distraction				67 Female					39 Male																		Lane 1 Decreasing Misspot
7461	Exceeding Reas. Safe Speed				46 Male																						Past Right Shoulder Decreasing Misspot	
7462	Exceeding Reas. Safe Speed	Improper Passing			64 Male																						Past Right Shoulder Increasing Misspot	
7463	Apparently Asleep or Fatigued																											

[illegible]

OBJECTID	JURISDICTION	COUNTY	CITY	PRIMARY THRUWAY	BLOCK NUMBER	INTERSECTING THRUWAY	DET FROM REF POINT	ML OF PT	COMP DIR FROM REF POINT	REFERENCE POINT NAME	MILEPOST	A	B	SR ONLY HISTORY SUSPENSE	REPORT NUMBER	DATE	TIME	MOST SEVERE INJURY TYPE	NUM INV	NUM FATAL	NUM NON FATAL	NUM SUSP	NUM SUSP	NUM SUSP	VEH 1 TYPE	VEH 2 TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD SURFACE CONDITION	ROADWAY CHARACTERISTIC	LIGHTING CONDITION	PRST COLLISION TYPE OBJECT STRUCK	VEH 1 ACTION	VEH 2 ACTION	VEH 1 COMPASS DIRECTION FROM	VEH 1 COMPASS DIRECTION TO	VEH 2 COMPASS DIRECTION FROM	VEH 2 COMPASS DIRECTION TO
7559	State Route	Jefferson		020							0.32	No			E707952	2017-09-07 18:58	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Curve & Grade	Daylight	Roadway Ditch	Going Straight Ahead	Going Straight Ahead	South	North			
7560	State Route	Jefferson		020							0.33	No			E417281	2017-07-07 12:40	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Curve & Grade	Daylight	Quadrant - Face	Going Straight Ahead	Going Straight Ahead	South	North			
7561	State Route	Jefferson		020							0.34	No			E672763	2017-01-26 04:55	Suspected Serious Injury	2	0	3	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Other	Wet	Curve & Grade	Dark No Street Lights	Quadrant - Face	Going Straight Ahead	Going Straight Ahead	West	East	East	West	
7562	State Route	Jefferson		020							0.37	No			E672763	2017-01-26 04:55	Suspected Serious Injury	2	0	3	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Curve & Grade	Daylight	Quadrant - Face	Going Straight Ahead	Going Straight Ahead	West	East	East	West	
7563	State Route	Jefferson		020							0.37	No			E672763	2017-01-26 04:55	Suspected Serious Injury	2	0	3	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Curve & Grade	Daylight	Quadrant - Face	Going Straight Ahead	Going Straight Ahead	West	East	East	West	
7564	State Route	Jefferson		020							0.38	No			E684999	2018-10-29 12:50	No Apparent Injury	0	0	1	0	0	0	Truck Tractor & Semi-Trailer	Truck Tractor & Semi-Trailer	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Curve & Grade	Daylight	Roadway Ditch	Going Straight Ahead	Going Straight Ahead	West	East			
7565	State Route	Jefferson		020							0.43	No			E684999	2018-10-29 12:50	No Apparent Injury	0	0	1	0	0	0	Truck Tractor & Semi-Trailer	Truck Tractor & Semi-Trailer	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Curve & Grade	Daylight	Roadway Ditch	Going Straight Ahead	Going Straight Ahead	West	East			
7566	State Route	Jefferson		020							0.46	No			E582816	2016-10-13 21:29	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Other	Wet	Curve & Grade	Daylight	Roadway Ditch	Going Straight Ahead	Going Straight Ahead	West	East			
7567	State Route	Jefferson		020							0.47	No			E683407	2017-03-04 14:14	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Curve & Grade	Daylight	Roadway Ditch	Going Straight Ahead	Going Straight Ahead	West	East			
7568	State Route	Jefferson		020							0.48	No			E706340	2018-06-21 08:48	No Apparent Injury	0	0	1	0	0	0	Truck Tractor & Semi-Trailer	Truck Tractor & Semi-Trailer	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Curve & Grade	Daylight	Roadway Ditch	Going Straight Ahead	Going Straight Ahead	West	East			
7569	State Route	Jefferson		020							0.50	No			E706340	2018-06-21 08:48	No Apparent Injury	0	0	1	0	0	0	Truck Tractor & Semi-Trailer	Truck Tractor & Semi-Trailer	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Curve & Grade	Daylight	Roadway Ditch	Going Straight Ahead	Going Straight Ahead	West	East	East	West	
7570	State Route	Jefferson		020							0.53	No			E671533	2016-12-30 07:03	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Curve & Grade	Daylight	Roadway Ditch	Going Straight Ahead	Going Straight Ahead	West	East			
7571	State Route	Jefferson		020							0.60	No			E673338	2018-12-17 07:08	Suspected Minor Injury	1	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Curve & Grade	Dark No Street Lights	Vehicle overturned	Going Straight Ahead	Going Straight Ahead	West	East			
7572	State Route	Jefferson		020							0.62	No			E594045	2019-03-08 08:50	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Curve & Grade	Daylight	Roadway Ditch	Going Straight Ahead	Going Straight Ahead	West	East			
7573	State Route	Jefferson		020							0.62	No			E594045	2019-03-08 08:50	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Curve & Grade	Daylight	Roadway Ditch	Going Straight Ahead	Going Straight Ahead	West	East			
7574	State Route	Jefferson		020							0.78	No			E706355	2017-09-03 03:55	Possible Injury	1	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Dark No Street Lights	Tree or Shrub (stationary)	Going Straight Ahead	Going Straight Ahead	East	West			
7575	State Route	Jefferson		020							0.81	No			E683407	2017-03-04 14:14	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Curve & Grade	Daylight	Roadway Ditch	Going Straight Ahead	Going Straight Ahead	East	West			
7576	State Route	Jefferson		020							0.82	No			E468620	2015-12-14 20:45	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Dark No Street Lights	Roadway Ditch	Swerving	Going Straight Ahead	Going Straight Ahead	West	East		
7577	State Route	Jefferson		020							0.82	No			E684762	2017-07-27 12:05	Possible Injury	2	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	Quadrant - Face	Going Straight Ahead	Going Straight Ahead	West	East	West	East	
7578	State Route	Jefferson		020							0.82	No			E671533	2016-12-30 07:03	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Curve & Grade	Dark No Street Lights	Quadrant - Face	Going Straight Ahead	Going Straight Ahead	West	East			
7579	State Route	Jefferson		020							0.85	No			E684568	2018-07-26 22:18	No Apparent Injury	0	0	3	0	0	0	Truck Tractor & Semi-Trailer	Truck Tractor & Semi-Trailer	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Dark No Street Lights	From same direction - both going straight - one stopped - near-end	Going Straight Ahead	Going Straight Ahead	West	East	Vehicle Stopped	Vehicle Stopped	
7580	State Route	Jefferson		020							0.86	No			E671533	2016-12-30 07:03	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Curve & Grade	Dark No Street Lights	Quadrant - Face	Going Straight Ahead	Going Straight Ahead	West	East			
7581	State Route	Jefferson		020							0.88	No			E451773	2015-12-28 20:35	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Curve & Grade	Dark No Street Lights	Roadway Ditch	Going Straight Ahead	Going Straight Ahead	West	East			
7582	State Route	Jefferson		020							1.00	No			E379721	2014-11-28 15:01	Suspected Minor Injury	1	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Dark No Street Lights	Roadway Ditch	Going Straight Ahead	Going Straight Ahead	North	South			
7583	State Route	Jefferson		020							1.00	No			E384048	2015-01-27 01:25	Dead at Scene	1	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Curve & Grade	Dark No Street Lights	Over Embankment - No Quadrant Present	Going Straight Ahead	Going Straight Ahead	North	South			
7584	State Route	Jefferson		020							1.00	No			E584800	2016-06-11 06:32	Suspected Minor Injury	1	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Curve & Grade	Dark No Street Lights	Roadway Ditch	Going Straight Ahead	Going Straight Ahead	East	West			
7585	State Route	Jefferson		020							1.00	No			E684300	2017-01-01 02:40	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Curve & Grade	Dark No Street Lights	Roadway Ditch	Going Straight Ahead	Going Straight Ahead	East	West			
7586	State Route	Jefferson		020							1.00	No			E672427	2017-10-22 00:22	Possible Injury	1	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Curve & Grade	Dark No Street Lights	Roadway Ditch	Going Straight Ahead	Going Straight Ahead	South	North			
7587	State Route	Jefferson		020							1.02	No			E745453	2017-12-21 18:54	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Curve & Grade	Dark No Street Lights	Roadway Ditch	Going Straight Ahead	Going Straight Ahead	East	West			
7588	State Route	Jefferson		020							1.06	No			E684152	2018-03-04 12:10	Possible Injury	1	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Dark No Street Lights	Vehicle Struck Deer	Going Straight Ahead	Going Straight Ahead	East	West			
7589	State Route	Jefferson		020							1.07	No			E663634	2016-12-12 08:10	No Apparent Injury	0	0	1	0	0	0	Truck & Trailer	Truck & Trailer	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	Roadway Ditch	Going Straight Ahead	Going Straight Ahead	West	East			
7590	State Route	Jefferson		020							1.10	No			E395557	2014-02-07 18:40	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	Vehicle started in vehicle	Going Straight Ahead	Going Straight Ahead	West	East			
7591	State Route	Jefferson		020							1.24	No			E691129	2018-11-21 00:24	Suspected Serious Injury	1	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Curve & Grade	Dark No Street Lights	Vehicle Struck Deer	Going Straight Ahead	Going Straight Ahead	West	East	East	West	
7592	State Route	Jefferson		020							1.34	No			E675738	2016-01-01 01:00	Suspected Minor Injury	1	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Curve & Grade	Dark No Street Lights	Earth Bank or Edge	Going Straight Ahead	Going Straight Ahead	East	West			
7593	State Route	Jefferson		020							1.36	No			E675738	2016-01-01 01:00	Suspected Minor Injury	1	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Curve & Grade	Dark No Street Lights	Vehicle Struck Deer	Going Straight Ahead	Going Straight Ahead	East	West			
7594	State Route	Jefferson		020							1.40	No			E571101	2016-08-03 12:56	Possible Injury	1	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Curve & Grade	Daylight	Quadrant - Face	Going Straight Ahead	Going Straight Ahead	East	West			
7595	State Route	Jefferson		020							1.40	No			E718484	2017-09-09 23:25	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Curve & Grade	Dark No Street Lights	Roadway Ditch	Going Straight Ahead	Going Straight Ahead	East	West			
7596	State Route	Jefferson		020							1.42	No			E739201	2018-02-28 14:07	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Straight & Level	Daylight	Roadway Ditch	Going Straight Ahead	Going Straight Ahead	East	West			
7597	State Route	Jefferson		020							1.50	No			E706953	2018-05-04 19:30	No Apparent Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Level	Daylight	From same direction - both going straight - both moving - sideswipe	Going Straight Ahead	Going Straight Ahead	West	East	West	East	
7598	State Route	Jefferson		020							1.50	No			E671533	2016-12-30 07:03	No Apparent Injury	0	0	1	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet	Curve & Grade	Dark No Street Lights	Roadway Ditch	Going Straight Ahead	Going Straight Ahead	West	East			
7599	State Route	Jefferson		020							1.62	No			E601165	2019-05-26 17:50	Possible Injury	0	0	2	0	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear											

[illegible]

[illegible]

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	JURISDICTION	COUNTY	CITY	PRIMARY TRAFFICWAY	BLOCK NUMBER	INTERSECTING TRAFFICWAY	DIST FROM REF. POINT	MI. OF RT.	COMP DIR FROM PT. POINT	REFERENCE POINT NAME	MILEPOST A B	IS ONLY TRUCKS/ BUS/ TRAILER	REPORT NUMBER	DATE	TIME	MOST SEVERE INJURY TYPE	NUM INV.	NUM FATAL	NUM INJ.	NUM PERS.	NUM UNK.	VEH. 1 TYPE	VEH. 2 TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD SURFACE CONDITION	ROADWAY CHARACTERISTIC	LIGHTING CONDITION	FIRST COLLISION TYPE OBJECT/STRUCK	VEH. 1 ACTION	VEH. 2 ACTION	VEH. 1 COMPASS DIRECTION FROM	VEH. 1 COMPASS DIRECTION TO	VEH. 2 COMPASS DIRECTION FROM	VEH. 2 COMPASS DIRECTION TO
7919	State Route	Jefferson	104								1.68	No	6847810	2019-11-28 11:16		No Apparent Injury	0	0	1	0	0	Truck Tractor & Semi-Trailer		Not at Intersection and Not Related		Wet	Straight & Grade	Daylight	Over Roadway Power Lines			East	West		
7920	State Route	Jefferson	104								1.71	No	6820214	2019-10-29 10:16		No Apparent Injury	0	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.		Clear or Partly Cloudy	Wet	Straight & Grade	Daylight	Dark No Street Lights	Vehicle Strikes Deer	Going Straight Ahead	East	West			
7921	State Route	Jefferson	104								2.00	No	6829819	2019-10-02 19:44		No Apparent Injury	0	0	1	0	0	Passenger Car		Clear or Partly Cloudy	Dry	Straight & Grade	Dark No Street Lights	Vehicle Strikes Deer	Going Straight Ahead	East	West				
7922	State Route	Jefferson	104								2.06	No	6816616	2019-06-09 10:49		No Apparent Injury	0	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.		Clear or Partly Cloudy	Dry	Straight & Grade	Dark No Street Lights	Vehicle Strikes Deer	Going Straight Ahead	East	West				
7923	State Route	Jefferson	104								2.10	No	6837078	2019-10-10 18:16		No Apparent Injury	0	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.		Clear or Partly Cloudy	Dry	Straight & Grade	Dark No Street Lights	Vehicle Strikes Deer	Going Straight Ahead	East	West				
7924	State Route	Jefferson	104								2.88	No	6762564	2019-01-25 06:40		No Apparent Injury	0	0	1	0	0	Passenger Car		Clear or Partly Cloudy	Wet	Straight & Grade	Dark No Street Lights	Domestic animal other (dog, dog, etc)	Going Straight Ahead	East	West				
7925	State Route	Jefferson	104								3.04	No	6812603	2019-03-29 10:29		No Apparent Injury	0	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.		Clear or Partly Cloudy	Dry	Straight & Grade	Dark No Street Lights	Vehicle Strikes Deer	Going Straight Ahead	East	West				
7926	State Route	Jefferson	104								3.00	No	6889990	2019-12-04 11:24		No Apparent Injury	0	0	2	0	0	Passenger Car	Passenger Car	Not at Intersection and Not Related	Overcast	Wet	Straight & Grade	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	East	West	East	West	
7927	State Route	Jefferson	104								3.00	No	6845372	2019-06-04 10:00		No Apparent Injury	0	0	1	0	0	Passenger Car		Clear or Partly Cloudy	Wet	Straight & Grade	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	East	West	East	West		
7928	State Route	Jefferson	104								3.03	No	6889339	2017-06-26 1:41		No Apparent Injury	0	0	2	0	0	Passenger Car	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	Vehicle Strikes Deer	Going Straight Ahead	East	West	East	West	
7929	State Route	Jefferson	104								3.05	No	2748300	2019-10-08 20:40		No Apparent Injury	0	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.		Clear or Partly Cloudy	Wet	Straight & Grade	Dark No Street Lights	Vehicle Strikes Deer	Going Straight Ahead	East	West	East	West		
7930	State Route	Jefferson	104								3.04	No	6804380	2019-02-16 16:00		No Apparent Injury	0	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.		Clear or Partly Cloudy	Dry	Straight & Grade	Dark No Street Lights	Vehicle Strikes Deer	Going Straight Ahead	East	West	East	West		
7931	State Route	Jefferson	104								3.18	No	6444082	2019-07-18 17:06		No Apparent Injury	0	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.		Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	Fire started in vehicle	Going Straight Ahead	East	West	East	West		
7932	State Route	Jefferson	104								3.38	No	6487306	2019-11-19 09:36		No Apparent Injury	0	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.		Clear or Partly Cloudy	Wet	Straight & Grade	Dark Street Lights On	Roadway Ditch	Going Straight Ahead	East	West	East	West		
7933	State Route	Jefferson	104								3.38	No	6837224	2019-09-09 10:22		No Apparent Injury	0	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.		Clear or Partly Cloudy	Dry	Straight & Grade	Dark No Street Lights	Vehicle Strikes Deer	Going Straight Ahead	East	West	East	West		
7934	State Route	Jefferson	104								3.46	No	6820913	2019-03-31 04:40		No Apparent Injury	0	0	1	0	0	Truck Tractor & Semi-Trailer		Clear or Partly Cloudy	Dry	Straight & Grade	Dark No Street Lights	Guardrail - Face	Going Straight Ahead	East	West	East	West		
7935	State Route	Jefferson	104								3.63	No	6836003	2019-08-28 10:38		No Apparent Injury	0	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.	Truck & Van/Mini Van/Under 10,000 lb.	Not at Intersection and Not Related	Overcast	Wet	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	East	West	East	West			
7936	State Route	Jefferson	104								3.67	No	6866376	2017-05-03 16:20		No Apparent Injury	0	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.		Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	Fire started in vehicle	Going Straight Ahead	East	West	East	West		
7937	State Route	Jefferson	104								3.68	No	6820712	2019-06-11 18:27		No Apparent Injury	0	0	1	0	0	Passenger Car		Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	Roadway Ditch	Going Straight Ahead	East	West	East	West		
7938	State Route	Jefferson	104								3.68	No	6820712	2019-11-28 11:24		No Apparent Injury	0	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.	Truck & Van/Mini Van/Under 10,000 lb.	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	East	West	East	West			
7939	State Route	Jefferson	104								4.20	No	6860801	2019-09-05 17:41		Possible Injury	2	0	2	0	0	Truck & Van/Mini Van/Under 10,000 lb.	Truck & Van/Mini Van/Under 10,000 lb.	Not at Intersection and Not Related	Overcast	Wet	Straight & Grade	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	East	West	East	West	
7940	State Route	Jefferson	104								4.28	No	6820643	2019-10-16 16:00		No Apparent Injury	0	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.		Clear or Partly Cloudy	Wet	Straight & Grade	Dark Street Lights On	Guardrail - Face	Going Straight Ahead	East	West	East	West		
7941	State Route	Jefferson	104								4.31	No	6454372	2019-11-14 19:20		No Apparent Injury	0	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.		Clear or Partly Cloudy	Wet	Straight & Grade	Daylight	Guardrail - Face	Going Straight Ahead	East	West	East	West		
7942	State Route	Jefferson	104								4.34	No	6871903	2019-08-07 11:13		Possible Injury	4	0	2	0	0	Passenger Car	Truck & Van/Mini Van/Under 10,000 lb.	At Intersection and Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	East	West	East	West	
7943	State Route	Jefferson	104								4.34	No	6871903	2019-08-07 11:13		Possible Injury	4	0	2	0	0	Passenger Car	Truck & Van/Mini Van/Under 10,000 lb.	At Intersection and Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	East	West	East	West	
7944	State Route	Jefferson	104								4.34	No	6784659	2017-11-17 12:50		Possible Injury	1	0	1	0	0	Passenger Car		Clear or Partly Cloudy	Overcast	Wet	Straight & Grade	Daylight	Over Embankment - No Guardrail Present	Going Straight Ahead	East	West	East	West	
7945	State Route	Jefferson	104								4.34	No	6873370	2017-12-04 00:11		No Apparent Injury	0	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.		Clear or Partly Cloudy	Overcast	Wet	Straight & Grade	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	East	West	East	West	
7946	State Route	Jefferson	104								4.34	No	6825757	2019-05-13 14:40		Suspected Serious Injury	3	0	2	0	0	Passenger Car	Truck & Van/Mini Van/Under 10,000 lb.	At Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	From opposite direction - both moving - head-on	Going Straight Ahead	East	West	East	West	
7947	State Route	Jefferson	104								4.34	No	6878739	2019-11-22 04:40		Dead at Scene	0	1	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.		Clear or Partly Cloudy	Dry	Straight & Grade	Dark Street Lights On	Guardrail - Through Over or Under	Going Straight Ahead	East	West	East	West		
7948	State Route	Jefferson	104								4.34	No	6820672	2019-12-04 10:38		Suspected Minor Injury	1	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.		Clear or Partly Cloudy	Dry	Straight & Grade	Dark Street Lights On	Guardrail - Through Over or Under	Going Straight Ahead	East	West	East	West		
7949	State Route	Jefferson	104								4.36	No	6727027	2017-10-16 06:45		No Apparent Injury	1	0	2	0	0	Passenger Car	Truck & Van/Mini Van/Under 10,000 lb.	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Straight & Grade	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	East	West	East	West	
7950	State Route	Jefferson	104								4.43	No	6845376	2019-03-28 10:58		No Apparent Injury	0	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.		Clear or Partly Cloudy	Dry	Straight & Grade	Dark No Street Lights	Vehicle Strikes Deer	Going Straight Ahead	East	West	East	West		
7951	State Route	Jefferson	104								4.45	No	6850518	2019-11-28 02:54		No Apparent Injury	0	0	1	0	0	Passenger Car		Clear or Partly Cloudy	Wet	Straight & Grade	Dark No Street Lights	Vehicle Strikes Deer	Going Straight Ahead	East	West	East	West		
7952	State Route	Jefferson	104								4.46	No	6753742	2017-12-24 18:54		No Apparent Injury	0	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.		Clear or Partly Cloudy	Wet	Straight & Grade	Dark No Street Lights	Roadway Ditch	Going Straight Ahead	East	West	East	West		
7953	State Route	Jefferson	104								4.47	No	6818482	2019-07-15 13:44		No Apparent Injury	0	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.		Clear or Partly Cloudy	Wet	Straight & Grade	Dark No Street Lights	Vehicle Strikes Deer	Going Straight Ahead	East	West	East	West		
7954	State Route	Jefferson	104								4.81	No	6717797	2019-02-27 19:12		No Apparent Injury	0	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.		Clear or Partly Cloudy	Wet	Straight & Grade	Dark No Street Lights	Roadway Ditch	Going Straight Ahead	East	West	East	West		
7955	State Route	Jefferson	104								4.81	No	6845376	2019-03-28 10:58		No Apparent Injury	0	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.		Clear or Partly Cloudy	Wet	Straight & Grade	Dark No Street Lights	Vehicle Strikes Deer	Going Straight Ahead	East	West	East	West		
7956	State Route	Jefferson	104								5.00	No	6841297	2019-05-01 15:34		Suspected Serious Injury	1	0	2	0	0	Motorcycle		Clear or Partly Cloudy	Wet	Straight & Grade	Daylight	Vehicle overturned	Going Straight Ahead	East	West	East	West		
7957	State Route	Jefferson	104								5.00	No	6753740	2017-12-24 18:15		Possible Injury	1	0	2	0	0	Passenger Car		Clear or Partly Cloudy	Wet	Straight & Grade	Dark No Street Lights	Roadway Ditch	Going Straight Ahead	East	West	East	West		
7958	State Route	Jefferson	104								5.00	No	6869920	2019-10-12 15:41		No Apparent Injury	0	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.		Clear or Partly Cloudy	Overcast	Wet	Straight & Grade	Dark No Street Lights	Vehicle Strikes Deer	Going Straight Ahead	East	West	East	West	
7959	State Route	Jefferson	104								5.25	No	6859917	2019-10-18 14:45		No Apparent Injury	0	0	2	0	0	Truck (Flatbed Van, etc)		Clear or Partly Cloudy	Wet	Straight & Grade	Daylight	From opposite direction - both going straight - one stopped - rear-end	Going Straight Ahead	East	West	East	West		
7960	State Route	Jefferson	104								5.25	No	6818600	2019-12-04 10:38		Dead at Scene	0	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.		Clear or Partly Cloudy	Wet	Straight & Grade	Dark No Street Lights	Roadway Ditch	Going Straight Ahead	East	West	East	West		
7961	State Route	Jefferson	104								5.25	No	6818600	2019-12-04 10:38		Dead at Scene	0	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.		Clear or Partly Cloudy	Wet	Straight & Grade	Dark No Street Lights	Roadway Ditch	Going Straight Ahead	East	West	East	West		
7962	State Route	Jefferson	104								5.50	No	6475769	2019-10-24 19:00		No Apparent Injury	0	0	1	0	0	Passenger Car		Clear or Partly Cloudy	Dry	Straight & Grade	Dark No Street Lights	Vehicle Strikes Deer	Going Straight Ahead	East	West	East	West		
7963	State Route	Jefferson	104								5.50	No	6475769	2019-10-24 19:00		No Apparent Injury	0	0	1	0	0	Truck & Van/Mini Van/Under 10,000 lb.		Clear or Partly Cloudy	Dry	Straight & Grade	Dark No Street Lights	Vehicle Strikes Deer	Going Straight Ahead	East	West	East	West		
7964	State Route	Jefferson	104								5.50	No	6888294	2019-11-17 14:38		No Apparent Injury	0	0	1	0	0	Passenger Car		Clear or Partly Cloudy	Overcast	Wet	Straight & Grade	Daylight	Guardrail - Face	Going Straight Ahead	East	West	East	West	
7965	State Route	Jefferson	104								5.50	No	6888294	2019-11-17 14:38		No Apparent Injury	0	0	1	0	0	Passenger Car		Clear											

OBJECTID	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_1_UNIT_1	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_2_UNIT_1	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_3_UNIT_1	MV_DRIVER AGE_UNIT_1	MV_DRIVER GENDER_UNIT_1	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_1_UNIT_2	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_2_UNIT_2	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_3_UNIT_2	MV_DRIVER AGE_UNIT_2	MV_DRIVER GENDER_UNIT_2	BICYCLIST_CONTRIBUTING CIRCUMSTANCE_1_UNIT_1	BICYCLIST_CONTRIBUTING CIRCUMSTANCE_2_UNIT_1	BICYCLIST_CONTRIBUTING CIRCUMSTANCE_3_UNIT_1	BICYCLIST AGE_UNIT_1	BICYCLIST GENDER_UNIT_1	BICYCLIST_CONTRIBUTING CIRCUMSTANCE_1_UNIT_2	BICYCLIST_CONTRIBUTING CIRCUMSTANCE_2_UNIT_2	BICYCLIST_CONTRIBUTING CIRCUMSTANCE_3_UNIT_2	BICYCLIST AGE_UNIT_2	BICYCLIST GENDER_UNIT_2	PEDESTRIAN_CONTRIBUTING CIRCUMSTANCE_1_UNIT_2	PEDESTRIAN_CONTRIBUTING CIRCUMSTANCE_2_UNIT_2	PEDESTRIAN_CONTRIBUTING CIRCUMSTANCE_3_UNIT_2	PEDESTRIAN AGE_UNIT_2	PEDESTRIAN GENDER_UNIT_2	PEDESTRIAN GENDER_UNIT_2	FIRST_IMPACT_LOCATION City, St, County, Rd, S, Misc, 2010, Forward, St	
7919	None				48 Male																					Lane 1 Decreasing Misspot		
7920	None				48 Male																					Lane 1 Decreasing Misspot		
7921	None				68 Female																					Lane 1 Decreasing Misspot		
7922	None				38 Female																					Lane 1 Increasing Misspot		
7923	None				38 Female																					Lane 1 Increasing Misspot		
7924	None				44 Male																					Lane 1 Decreasing Misspot		
7925	None				62 Female																					Lane 1 Increasing Misspot		
7926	Under Influence of Alcohol				20 Male	None				78 Female																Lane 1 Decreasing Misspot		
7927	None				10 Male																					Lane 1 Increasing Misspot		
7928	Operating Defective Equipment				60 Female	None				66 Female																Lane 1 Decreasing Misspot		
7929	None				60 Female																					Lane 1 Decreasing Misspot		
7930	Exceeding Reas. Safe Speed				64 Male																					Past Right Shoulder Increasing Misspot		
7931	Operating Defective Equipment	Other Contributing Circ Not Listed			62 Male																					Lane 1 Increasing Misspot		
7932	Exceeding Reas. Safe Speed				60 Male																					Past Right Shoulder Increasing Misspot		
7933	None				64 Male																					Lane 1 Decreasing Misspot		
7934	Other Contributing Circ Not Listed				74 Male																					Right Shoulder Increasing Misspot		
7935	Under Influence of Alcohol	Intention	Did Not Grant RW to Vehicle		60 Female	None				38 Male																	Lane 1 Decreasing Misspot	
7936	Other Contributing Circ Not Listed				60 Male																					Right Shoulder Decreasing Misspot		
7937	Apparently Asleep or Fatigued				60 Male	None				49 Male																	Past Right Shoulder Increasing Misspot	
7938	Did Not Grant RW to Vehicle				62 Female					38 Male																	Lane 1 Increasing Misspot	
7939	Intention	Follow Too Closely			60 Male	None				38 Male																	Lane 1 Decreasing Misspot	
7940	Other Contributing Circ Not Listed	Exceeding Reas. Safe Speed			60 Male																					Right Shoulder Increasing Misspot		
7941	Operating Defective Equipment	Exceeding Reas. Safe Speed	Over Center Line		60 Male																						Right Shoulder Increasing Misspot	
7942	Intention				17 Female	None				33 Female																	Lane 1 Increasing Misspot	
7943	Improper Turn/Merge		Intention		60 Male	None				68 Male																	Right Shoulder Increasing Misspot	
7944	Improper Turn/Merge		Intention		92 Female	None				92 Female																	Past Right Shoulder Decreasing Misspot	
7945	Exceeding Reas. Safe Speed				70 Male																						Past Right Shoulder Decreasing Misspot	
7946	Under Influence of Drugs				70 Male	None				77 Male																	Lane 1 Increasing Misspot	
7947	Other Contributing Circ Not Listed	Exceeding Reas. Safe Speed			26 Male																						Right Shoulder Decreasing Misspot	
7948	Other Contributing Circ Not Listed				74 Male																						Intersecting Road Decreasing Misspot	
7949	Improper U-Turn				63 Male	None				48 Male																	Lane 1 Decreasing Misspot	
7950	Improper Passing				48 Male																						Right Shoulder Increasing Misspot	
7951	None				64 Female																						Lane 1 Decreasing Misspot	
7952	Exceeding Reas. Safe Speed				38 Female																						Past Right Shoulder Decreasing Misspot	
7953	Improper Passing				60 Male																						Right Shoulder Increasing Misspot	
7954	Exceeding Reas. Safe Speed				16 Male																						Past Right Shoulder Decreasing Misspot	
7955	Other Contributing Circ Not Listed				40 Male	None				26 Female																	Lane 1 Increasing Misspot	
7956	Exceeding Reas. Safe Speed				40 Male																						Past Right Shoulder Decreasing Misspot	
7957	Follow Too Closely				63 Male	None				43 Male																	Lane 1 Increasing Misspot	
7958	None				38 Female																						Lane 1 Decreasing Misspot	
7959	Operating Handheld Cell Phone				38 Female	None				27 Female																	Lane 1 Decreasing Misspot	
7960	Under Influence of Alcohol	Exceeding Reas. Safe Speed			60 Male																						Past Right Shoulder Decreasing Misspot	
7961	Intention				60 Female	None				60 Female																	Right Shoulder Increasing Misspot	
7962	Other Contributing Circ Not Listed				60 Male																						Lane 1 Decreasing Misspot	
7963	Other Contributing Circ Not Listed				10 Female																						Past Right Shoulder Decreasing Misspot	
7964	Other Contributing Circ Not Listed				60 Male																						Right Shoulder Increasing Misspot	
7965	Intention	Exceeding Reas. Safe Speed	Follow Too Closely		20 Male	None				60 Male																	Lane 1 Decreasing Misspot	
7966	Exceeding Stated Speed Limit				30 Male																						Past Right Shoulder Increasing Misspot	
7967	Other Contributing Circ Not Listed				30 Male																						Lane 1 Decreasing Misspot	
7968	Driver Not Distracted				44 Male																						Lane 1 Decreasing Misspot	
7969	None				64 Male																						Past Right Shoulder Decreasing Misspot	
7970	Under Influence of Alcohol				30 Male	None				18 Male																	Lane 1 Increasing Misspot	
7971	None				78 Female																						Lane 1 Increasing Misspot	
7972	None				78 Female																						Lane 1 Increasing Misspot	
7973	Other Contributing Circ Not Listed				64 Female																						Past Right Shoulder Increasing Misspot	
7974	Other Contributing Circ Not Listed				60 Female					22 Male																		Lane 1 Decreasing Misspot
7975	None				22 Male																						Past Right Shoulder Increasing Misspot	
7976	Other Contributing Circ Not Listed	Exceeding Stated Speed Limit			60 Male																						Past Right Shoulder Increasing Misspot	
7977	Operating Defective Equipment	Other Contributing Circ Not Listed			40 Male																						Past Right Shoulder Decreasing Misspot	
7978	Apparently Asleep or Fatigued				74 Male	None				63 Female																	Lane 1 Increasing Misspot	
7979	Intention	Other Driver Distractions Inside Vehicle	Over Center Line		60 Male					60 Male																	Lane 1 Decreasing Misspot	
7980	Exceeding Reas. Safe Speed				38 Male																						Past Right Shoulder Increasing Misspot	
7981	Intention	Exceeding Reas. Safe Speed			20 Female	None				24 Female																	Lane 1 Increasing Misspot	
7982	Operating Defective Equipment				10 Male																						Lane 1 Decreasing Misspot	
7983	None				60 Male																						Intersecting Road Decreasing Misspot	
7984	Exceeding Stated Speed Limit	Non Motorist on Wrong Side of Road	Improper Passing		60 Male	None				32 Male																	Lane 1 Increasing Misspot	
7985	None				63 Female	Exceeding Reas. Safe Speed				63 Female																	Lane 1 Decreasing Misspot	
7986	Intention	Improper Passing			18 Female	Other Contributing Circ Not Listed																					Lane 1 Decreasing Misspot	
7987	Other Contributing Circ Not Listed	Did Not Grant RW to Vehicle	Improper Passing		60 Female					50 Male																	Lane 1 Decreasing Misspot	
7988	None				60 Female																						Right Shoulder Decreasing Misspot	
7989	Operating Defective Equipment				30 Female																						Right Shoulder Decreasing Misspot	
7990	Improper U-Turn	Intention	Did Not Grant RW to Vehicle		72 Male	None				65 Male																	Lane 1 Decreasing Misspot	
7991	Intention	Follow Too Closely			50 Male	None				79 Male																	Intersecting Road Decreasing Misspot	
7992	Improper Turn/Merge				72 Female	None																					Lane 1 Increasing Misspot	
7993	Did Not Grant RW to Vehicle				18 Male	None				74 Male																	Lane 1 Decreasing Misspot	
7994	Other Contributing Circ Not Listed				60 Female	None																					Intersecting Road Decreasing Misspot	
7995	Did Not Grant RW to Vehicle				60 Female	None				57 Male																	Lane 1 Decreasing Misspot	
7996	Exceeding Reas. Safe Speed				20 Female	None				32 Male																	Left Turn Lane Decreasing Misspot	
7997	Operating Other Electronic Devices (ompo	Did Not Grant RW to Vehicle			60 Female					60 Female																	Lane 1 Decreasing Misspot	
7998	Did Not Grant RW to Vehicle				70 Male	None				81 Male																	Lane 1 Increasing Misspot	
7999	Did Not Grant RW to Vehicle				18 Female	None				72 Female																	Left Turn Lane Decreasing Misspot	
8000	Follow Too Closely				30 Male					29 Male																	Intersecting Road Decreasing Misspot	
8001	Under Influence of Alcohol	Improper Turn/Merge	Intention		20 Male	None				29 Male																	Lane 1 Decreasing Misspot	
8002	Did Not Grant RW to Vehicle				38																							

OBJECT ID	JURISDICTION	COUNTY	CITY	PRIMARY TRAFFICWAY	BLOCK NUMBER	INTERSECTING TRAFFICWAY	DIST FROM REF. POINT	MI. OF FT.	COMP DIR FROM REF. POINT	REFERENCE POINT NAME	MI/POST A B	SP ONLY HISTORY SUSPENSE LN	REPORT NUMBER	DATE	TIME	MOST SEVERE INJURY TYPE	NUM INJ	NUM FATAL	NUM VEH	NUM BIKES	NUM PEDES	VEH 1 TYPE	VEH 2 TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD SURFACE CONDITION	ROADWAY CHARACTERISTIC	LIGHTING CONDITION	FIRST COLLISION TYPE OBJECT/STRUCK	VEH 1 ACTION	VEH 2 ACTION	VEH 1 COMPASS DIRECTION FROM	VEH 1 COMPASS DIRECTION TO	VEH 2 COMPASS DIRECTION FROM	VEH 2 COMPASS DIRECTION TO
8099	State Route	Jefferson	104								12.80	No	2746827	2014-09-20	22:00	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	West	East
8100	State Route	Jefferson	104								12.81	No	2746827	2014-09-20	22:00	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	West	East
8101	State Route	Jefferson	104								12.81	No	E344429	2014-07-17	17:58	Possible Injury	1	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
8102	State Route	Jefferson	104								12.81	No	E344447	2015-02-10	15:13	Possible Injury	1	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	West	East	Vehicle Stopped	Vehicle Stopped
8103	State Route	Jefferson	104								12.81	No	E344447	2015-02-10	15:13	Possible Injury	1	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped in Roadway	West	East	Vehicle Stopped	Vehicle Stopped
8104	State Route	Jefferson	104								12.81	No	E344447	2015-02-10	15:13	Possible Injury	1	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped in Roadway	West	East	Vehicle Stopped	Vehicle Stopped
8105	State Route	Jefferson	104								12.81	No	E344447	2015-02-10	15:13	Possible Injury	1	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped in Roadway	West	East	Vehicle Stopped	Vehicle Stopped
8106	State Route	Jefferson	104								12.88	No	E756758	2018-01-09	08:59	Possible Injury	1	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8107	State Route	Jefferson	104								12.88	No	E756758	2018-01-09	08:59	Possible Injury	1	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8108	State Route	Jefferson	104								12.88	No	E756758	2018-01-09	08:59	Possible Injury	1	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8109	State Route	Jefferson	104								13.00	No	E756758	2018-01-09	08:59	Possible Injury	1	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8110	State Route	Jefferson	104								13.00	No	E756758	2018-01-09	08:59	Possible Injury	1	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8111	State Route	Jefferson	104								13.00	No	E499498	2015-12-22	05:25	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8112	State Route	Jefferson	104								13.00	No	E625496	2016-03-14	04:05	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8113	State Route	Jefferson	104								13.00	No	E625496	2016-03-14	04:05	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8114	State Route	Jefferson	104								13.01	No	E653408	2017-03-19	16:00	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8115	State Route	Jefferson	104								13.02	No	E653408	2017-03-19	16:00	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8116	State Route	Jefferson	104								13.07	No	E653408	2017-03-19	16:00	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8117	State Route	Jefferson	104								13.07	No	E653408	2017-03-19	16:00	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8118	State Route	Jefferson	104								13.07	No	E653408	2017-03-19	16:00	No Apparent Injury	0	0	2	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8119	State Route	Jefferson	104								13.14	No	E656884	2018-02-18	14:38	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8120	State Route	Jefferson	104								13.16	No	E656884	2018-02-18	14:38	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8121	State Route	Jefferson	104								13.18	No	E656884	2018-02-18	14:38	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8122	State Route	Jefferson	104								13.20	No	E656884	2018-02-18	14:38	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8123	State Route	Jefferson	104								13.20	No	E656884	2018-02-18	14:38	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8124	State Route	Jefferson	104								13.20	No	E656884	2018-02-18	14:38	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8125	State Route	Jefferson	104								13.21	No	E716397	2017-09-27	18:55	Possible Injury	1	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8126	State Route	Jefferson	104								13.21	No	E716397	2017-09-27	18:55	Possible Injury	1	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8127	State Route	Jefferson	104								13.21	No	E656884	2018-02-18	14:38	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8128	State Route	Jefferson	104								13.21	No	E656884	2018-02-18	14:38	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8129	State Route	Jefferson	104								13.26	No	E656884	2018-02-18	14:38	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8130	State Route	Jefferson	104								13.26	No	E656884	2018-02-18	14:38	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8131	State Route	Jefferson	104								13.26	No	E656884	2018-02-18	14:38	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8132	State Route	Jefferson	104								13.30	No	E656884	2018-02-18	14:38	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8133	State Route	Jefferson	104								13.36	No	E656884	2018-02-18	14:38	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8134	State Route	Jefferson	104								13.46	No	E656884	2018-02-18	14:38	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8135	State Route	Jefferson	104								13.46	No	E656884	2018-02-18	14:38	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8136	State Route	Jefferson	104								13.46	No	E656884	2018-02-18	14:38	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8137	State Route	Jefferson	104								13.51	No	E630177	2016-12-18	18:05	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8138	State Route	Jefferson	104								13.51	No	E747603	2017-12-12	17:21	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8139	State Route	Jefferson	104								13.56	No	E747603	2017-12-12	17:21	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8140	State Route	Jefferson	104								13.60	No	E818063	2016-08-31	12:50	No Apparent Injury	0	0	1	0	0	Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	A Intersection and Not Related	Overcast	Wet	Straight & Level	Dark No Street Lights	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West	East	Vehicle Stopped	Vehicle Stopped
8141	State Route	Jefferson	104																																

OBJECTID	MV_DRIVER_CONTRIBUTING_CIRCUMSTANCE_1_UNIT_1	MV_DRIVER_CONTRIBUTING_CIRCUMSTANCE_2_UNIT_1	MV_DRIVER_CONTRIBUTING_CIRCUMSTANCE_3_UNIT_1	MV_DRIVER_AGE_UNIT_1	MV_DRIVER_GENDER_UNIT_1	MV_DRIVER_CONTRIBUTING_CIRCUMSTANCE_1_UNIT_2	MV_DRIVER_CONTRIBUTING_CIRCUMSTANCE_2_UNIT_2	MV_DRIVER_CONTRIBUTING_CIRCUMSTANCE_3_UNIT_2	MV_DRIVER_AGE_UNIT_2	MV_DRIVER_GENDER_UNIT_2	BICYCLIST_CONTRIBUTING_CIRCUMSTANCE_1_UNIT_1	BICYCLIST_CONTRIBUTING_CIRCUMSTANCE_2_UNIT_1	BICYCLIST_CONTRIBUTING_CIRCUMSTANCE_3_UNIT_1	BICYCLIST_AGE_UNIT_1	BICYCLIST_GENDER_UNIT_1	BICYCLIST_CONTRIBUTING_CIRCUMSTANCE_1_UNIT_2	BICYCLIST_CONTRIBUTING_CIRCUMSTANCE_2_UNIT_2	BICYCLIST_CONTRIBUTING_CIRCUMSTANCE_3_UNIT_2	BICYCLIST_AGE_UNIT_2	BICYCLIST_GENDER_UNIT_2	PEDESTRIAN_CONTRIBUTING_CIRCUMSTANCE_1_UNIT_2	PEDESTRIAN_CONTRIBUTING_CIRCUMSTANCE_2_UNIT_2	PEDESTRIAN_CONTRIBUTING_CIRCUMSTANCE_3_UNIT_2	PEDESTRIAN_AGE_UNIT_2	PEDESTRIAN_GENDER_UNIT_2	FIRST_IMPACT_LOCATION
																										City, St, County, Rd, #, Misc, 2019, Forward, St
8099	Follow Too Closely			19	Male	None			19	Female																Lane 1 Increasing Misspot
8100	Follow Too Closely			37	Female	None			37	Female																Lane 1 Increasing Misspot
8101	Exceeding Reas. Safe Speed	Exceeding Reas. Safe Speed		67	Female	None			18	Male																Lane 1 Increasing Misspot
8102	Exceeding Reas. Safe Speed			27	Male	Other Contributing Circ Not Listed			27	Male																Lane 1 Increasing Misspot
8103	Operation Defective Equipment	Intention	Exceeding Reas. Safe Speed	29	Male	Other Contributing Circ Not Listed			29	Male																Lane 1 Increasing Misspot
8104	Intention	Exceeding Reas. Safe Speed		27	Female	None			65	Female																Lane 1 Increasing Misspot
8105	Intention	Follow Too Closely		63	Male	None			63	Male																Lane 1 Increasing Misspot
8106	Exceeding Reas. Safe Speed			79	Male	None			79	Male																Lane 1 Increasing Misspot
8107	None			23	Male	None			23	Male																Lane 1 Increasing Misspot
8108	Intention	Other Driver Distractions Inside Vehicle	Follow Too Closely	31	Female	None			31	Female																Lane 1 Increasing Misspot
8109	Intention	Distractions Outside Vehicle		58	Female	None			58	Female																Lane 1 Increasing Misspot
8110	Other Driver Distractions Inside Vehicle	Improper Passing	Follow Too Closely	26	Female	None			26	Male																Lane 1 Increasing Misspot
8111	None			23	Male	None			23	Male																Right Shoulder Increasing Misspot
8112	Follow Too Closely			72	Male	None			60	Female																Lane 1 Increasing Misspot
8113	Under Influence of Alcohol			13	Female	None			13	Female																Past Right Shoulder Increasing Misspot
8114	Intention	Follow Too Closely		19	Male	None			42	Male																Lane 1 Increasing Misspot
8115	None			23	Male	None			23	Female																Lane 1 Decreasing Misspot
8116	Exceeding Reas. Safe Speed			23	Female	None			28	Female																Lane 1 Decreasing Misspot
8117	None			49	Male	None			49	Male																Lane 1 Decreasing Misspot
8118	Other Contributing Circ Not Listed			31	Male	None			81	Male																Lane 1 Decreasing Misspot
8119	Exceeding Reas. Safe Speed			49	Male	None			49	Male																Right Shoulder Decreasing Misspot
8120	None			27	Female	None			27	Female																Lane 1 Increasing Misspot
8121	Exceeding Reas. Safe Speed			23	Male	None			23	Male																Past Right Shoulder Decreasing Misspot
8122	Distractions Outside Vehicle			28	Female	None			28	Female																Lane 1 Decreasing Misspot
8123	None			71	Male	None			40	Male																Lane 1 Decreasing Misspot
8124	None			43	Male	None			43	Male																Lane 1 Decreasing Misspot
8125	None			23	Male	None			23	Male																Lane 1 Increasing Misspot
8126	None			23	Male	None			23	Male																Lane 1 Decreasing Misspot
8127	None			76	Male	None			76	Male																Lane 1 Decreasing Misspot
8128	None			66	Male	None			66	Male																Lane 1 Decreasing Misspot
8129	Unknown Distraction			66	Male	None			66	Male																Past Right Shoulder Increasing Misspot
8130	None			63	Male	None			41	Female																Lane 1 Decreasing Misspot
8131	Intention	Follow Too Closely		11	Male	None			60	Male																Lane 1 Decreasing Misspot
8132	None			62	Female	None			62	Female																Lane 1 Decreasing Misspot
8133	None			66	Male	None			66	Male																Lane 1 Decreasing Misspot
8134	Follow Too Closely			66	Male	None			16	Male																Lane 1 Increasing Misspot
8135	None			36	Male	None			36	Male																Lane 1 Decreasing Misspot
8136	Intention	Follow Too Closely		26	Female	None			68	Male																Lane 1 Increasing Misspot
8137	Intention	Exceeding Reas. Safe Speed		49	Male	None			49	Male																Lane 1 Increasing Misspot
8138	Exceeding Reas. Safe Speed			27	Male	None			27	Male																Past Right Shoulder Decreasing Misspot
8139	None			27	Male	None			27	Male																Lane 1 Decreasing Misspot
8140	None			26	Female	None			76	Male																Lane 1 Increasing Misspot
8141	Other Contributing Circ Not Listed	Did Not Grant RW to Vehicle		34	Male	None			73	Male																Lane 2 Decreasing Misspot
8142	Other Contributing Circ Not Listed	Intention		68	Female	None			23	Female																Lane 1 Decreasing Misspot
8143	Improper U Turn			68	Female	None			68	Female																Lane 1 Decreasing Misspot
8144	Follow Too Closely			71	Female	None			71	Female																Lane 2 Decreasing Misspot
8145	Exceeding Reas. Safe Speed			24	Male	None			48	Male																Lane 1 Increasing Misspot
8146	Intention			66	Male	None			17	Female																Lane 1 Increasing Misspot
8147	Other Contributing Circ Not Listed	Distractions Outside Vehicle	Improper Passing	68	Male	Other Contributing Circ Not Listed	Distractions Outside Vehicle		44	Male																Lane 2 Decreasing Misspot
8148	Intention			36	Female	None			36	Female																Past Right Shoulder Decreasing Misspot
8149	None			26	Male	None			26	Male																Lane 1 Decreasing Misspot
8150	Intention	Did Not Grant RW to Vehicle		73	Female	None			67	Male																Lane 1 Decreasing Misspot
8151	Did Not Grant RW to Vehicle			44	Female	None			44	Female																Left Turn Lane Decreasing Misspot
8152	Did Not Grant RW to Vehicle			18	Male	None			67	Female																Lane 2 Decreasing Misspot
8153	Other Driver Distractions Inside Vehicle			71	Female	None			62	Female																Intersecting Road Decreasing Misspot
8154	Did Not Grant RW to Vehicle			46	Male	None			46	Male																Lane 1 Decreasing Misspot
8155	Intention	Did Not Grant RW to Vehicle		67	Female	None			68	Male																Lane 1 Decreasing Misspot
8156	Did Not Grant RW to Vehicle			68	Male	None			24	Male																Lane 2 Decreasing Misspot
8157	Intention	Did Not Grant RW to Vehicle		68	Male	None			24	Male																Left Turn Lane Decreasing Misspot
8158	Intention	Did Not Grant RW to Vehicle		32	Male	None			62	Male																Lane 1 Decreasing Misspot
8159	Other Contributing Circ Not Listed			27	Female	None			27	Female																Lane 1 Increasing Misspot
8160	Improper Turn/Merge	Other Contributing Circ Not Listed	Intention	62	Female	None			62	Female																Lane 1 Increasing Misspot
8161	Intention	Did Not Grant RW to Vehicle		26	Female	None			73	Male																Left Turn Lane Increasing Misspot
8162	Intention	Did Not Grant RW to Vehicle		71	Male	None			71	Male																Lane 1 Decreasing Misspot
8163	None			66	Male	Other Contributing Circ Not Listed			66	Male																Lane 1 Decreasing Misspot
8164	Intention	Did Not Grant RW to Vehicle		62	Male	Increasing Speed Speed Limit			62	Male																Lane 1 Decreasing Misspot
8165	Intention	Did Not Grant RW to Vehicle		46	Female	None			74	Male																Lane 1 Decreasing Misspot
8166	Did Not Grant RW to Vehicle			19	Male	None			44	Male																Lane 1 Decreasing Misspot
8167	Other Contributing Circ Not Listed			26	Male	None			26	Male																Past Right Shoulder Increasing Misspot
8168	Intention	Did Not Grant RW to Vehicle		81	Male	None			28	Male																Lane 1 Decreasing Misspot
8169	Intention	Did Not Grant RW to Vehicle		24	Male	None			24	Male																Left Turn Lane Decreasing Misspot
8170	Other Contributing Circ Not Listed	Intention		26	Male	None			63	Male																Lane 1 Decreasing Misspot
8171	Other Contributing Circ Not Listed	Intention		31	Male	None			67	Male																Intersecting Road Decreasing Misspot
8172	Non Motorist on Wrong Side of Road			63	Female	None			63	Female																Lane 1 Decreasing Misspot
8173	Did Not Grant RW to Vehicle			68	Male	None			68	Male																Lane 2 Decreasing Misspot
8174	Under Influence of Alcohol			19	Male	None			38	Male																Intersecting Road Decreasing Misspot
8175	Exceeding Reas. Safe Speed			19	Male	None			38	Male																Intersecting Road Decreasing Misspot
8176	Intention	Did Not Grant RW to Vehicle		22	Male	None			78	Female																Lane 1 Decreasing Misspot
8177	Operation Defective Equipment			27	Male	None			77	Male																Lane 1 Increasing Misspot
8178	Intention	Did Not Grant RW to Vehicle		31	Male	None			78	Female																Lane 1 Decreasing Misspot
8179	Intention	Did Not Grant RW to Vehicle		61	Male	None			71	Female																Lane 1 Decreasing Misspot
8180	Under Influence of Alcohol	Intention	Did Not Grant RW to Vehicle	34	Male	None			24	Male																

[illegible]

*APPENDIX D – CLALLAM COUNTY 2021-2026 TRANSPORTATION IMPROVEMENT PROGRAM (TIP)
(DIGITAL)*



CR RESOLUTION 9, 2020

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11/24/20

ADOPTING THE SIX YEAR TRANSPORTATION IMPROVEMENT PROGRAM (2021-2026)

THE BOARD OF CLALLAM COUNTY COMMISSIONERS finds as follows:

1. RCW 36.81.121 and WAC 136-15-010 requires the Board of County Commissioners to annually adopt a Six Year Transportation Improvement Program.
2. Pursuant to WAC 136-20-060 the County Engineer prepared a 2020 Bridge Inspection Report to inform bridge priorities in preparing the Six Year Transportation Improvement Program.
3. Six Year Transportation Improvement Program projects consider traffic volumes, roadway conditions, geometrics, safety and accident history; bridge inspection report, local significance, and available funding.
4. W.A.C. 136-20-060 and W.A.C. 136-14-050 requires that the Board has the Engineer's Bridge Report and the Priority Array available to consider at the time of determining the program.
5. Clallam County Planning Commission held a work session on the Six Year Transportation Improvement Program (2021-2026) on October 21, 2020.
6. The Clallam County Department of Community Development Issued a determination of non-significance (DNS) on the Draft Six-Year TIP (2021-2026) following review of the environmental checklist under the State Environmental Policy Act (SEPA).
7. The Clallam County Planning Commissioner held a public hearing on the Six Year Transportation Improvement Program (2021-2026) on November 4, 2020, and recommended approval of the Six-Year Transportation Improvement Program.
8. Pursuant to RCW 36.81.121, a public hearing before the Board is required to be held so all taxpayers have a chance to comment on the proposed program.

NOW, THEREFORE, BE IT RESOLVED by the Board of Clallam County Commissioners, in consideration of the above findings of fact:

1. A public hearing was held on November 24, 2020 on the Six Year Transportation Improvement Program for 2021-2026.
2. That the attached Six Year Transportation Improvement Program is hereby adopted from 2021 through 2026.

PASSED AND ADOPTED this 24 day of November 2020



ATTEST:

L. Gores
Loni Gores, CMC, Clerk of the Board

BOARD OF CLALLAM COUNTY COMMISSIONERS

Mark Ozias
Mark Ozias, Chair

Randy Johnson
Randy Johnson

Bill Peach
Bill Peach

Clallam County Six-Year (2021-2026) Transportation Improvement Program (TIP)

Guide to Codes and Acronyms

Road Functional Class

06=Rural Minor Arterial
07=Rural Major Collector
08=Rural Minor Collector
09=Rural Local Access

16=Urban Minor Arterial
17=Urban Collector Arterial
19=Urban Local Access

Improvement Types

01=New construction
02=Relocation Project
03=Reconstruction
04=Major Widening
05=Minor Widening
06=Other Enhancements
07=Resurfacing

08=New Bridge Construction
09=Bridge or Culvert Replacement (e.g., fish passage project)
10=Bridge Rehabilitation
11=Minor Bridge Rehabilitation
12=Safety/Traffic Operation
23=Transit
32=Non-Motor Vehicle Project (e.g., Olympic Discovery Trail)

Funding Status

S=Project selected for funding. Funding has been secured for all or some project phases.
P=Planned Project. Funding has not been fully determined & may be dependent on grant or other agency.
P/U=Planned project (Unfunded)

Project Phase

PE=Preliminary Engineering, R/W=Right-of-Way; Const=Construction

Utility Codes

C=Cable; P=Power; T=Telephone; W=Water; S=Sewer; G=Gas; O=Other

Federal Fund Codes

FEMA=Federal Emergency Management Agency Hazard Mitigation Program
FLAP=Federal Lands Access Program
HSIP=Highway Safety Improvement Program
STBG=Surface Transportation Block Program
TA=Transportation Alternatives

State Fund Codes

CAPP=County Arterial Preservation Program
RAP=Rural Arterial Program
TIA=Transportation Improvement Account
UATA=Urban Arterial Trust Account
RCO-FBRB=Recreation Conservation Office-Fish Barrier Removal Board
RCO-SRFB=Recreation Conservation Office-Salmon Recovery Funding Board
RCO=Recreation Conservation Office

Other Fund Codes (local funding)

REET=Real Estate Excise Tax (County)
LTAC=Lodging Tax Advisory Committee Grant (County)
LEKT=Lower Elwha S'Klallam Tribe
PVT=Private Donation

*Local Funding

This column in the table shows the amount of local funds which are used on a project. These funds come primarily from the property tax road levy, and the County's share of the State Motor Vehicle Fuel Tax. They can also come from other local funding sources such as the local Real Estate Excise Tax, lodging tax grants, Tribes, and other sources.

SIX YEAR TRANSPORTATION IMPROVEMENT PROGRAM 2021 TO 2026

Hearing Date :
Adoption Date:
Resolution No.
Amendment Date:

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U N C I L A D S I	I T E M N U M B E R	PROJECT IDENTIFICATION					PROJECT COSTS IN THOUSANDS OF DOLLARS														
		A. PROJECT / ROAD NAME (ROAD LOG / BRIDGE NUMBER); PROJECT NUMBER					PROJECT PHASE	MONTH/ YEAR PHASE STARTS	FUNDING SOURCE INFORMATION						EXPENDITURE SCHEDULE (LOCAL AGENCY)				FEDERALLY FUNDED PROJECTS ONLY		
		B. BEGINNING AND END MILEPOST (M.P.)							FEDERAL FUND CODE	FEDERAL COST BY PHASE	STATE OR OTHER FUND CODE	STATE FUNDS	LOCAL FUNDS	PROJECT PHASE TOTALS (See note 1)	1st 2021	2nd 2022	3rd 2023	4th thru 6th 2024-2026	ENVIR. TYPE	FEDERALLY FUNDED PROJECTS ONLY	
		C. DESCRIPTION OF WORK																			
1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	
5	1	Black Diamond Road (#31800) M.P. 2.66-M.P. 3.40 (Horse/Alice to Wellman Rd) Widen shoulders, improve recoverable slopes, & potential separated path.	05 12 22	S	0.48	CPTW	PE RAW CONST TOTAL	6/15 MSEP 7/22 7/22					30 50 460 540	30 50 728 828	20 50 718 70	17	18	19	EIS CE EA NA	Y/N DATE	
7	2	Carlsborg Road (#64270) M.P. 1.01 to M.P. 1.75 Shoulder widening and resurfacing funded by Rural Arterial Program (RAP) with County match.	03 07	S	0.75	C.P.T.W.S	PE RAW CONST TOTAL	8/19 5/21			RAP 472K		30 50 741	30 50 741	30 50 701	10			EIS CE EA NA	Y/N DATE	
8	3	Clallam Slough Bridge Replacement (#19400 BR1) On Frontier Street (#19400) M.P. 0.07 to M.P. 0.44	04 09	S	0.37	CPWTS	PE RAW CONST TOTAL	8/20 STP(BR) 9/193		1,258 87 3,808 5,193				1,298 87 3,808 5,193	423 87 2,856 423	885 87 2,856 952			EIS CE EA NA	Y/N DATE	
	4	Countywide Overlay Projects Overlay improvements at various intersections or outside areas throughout the county.	07	S	0.57	CPTW	PE RAW CONST TOTAL	1/21 8/21					144 0 1,066 1,290	144 0 1,058 1,200	24 0 176 200	24 0 176 200	24 0 176 200	72 0 528 500	EIS CE EA NA	Y/N DATE	
7	5	Dry Creek Road (#31870) M.P. 0.00 to M.P. 0.88 Reconstruct Road to Major Collector Standards, Widen, Realign, Regrade, and Resurface. Combination of state, tribal, and local funding.	03 05 07	S	0.88	CPTW	PE RAW CONST TOTAL	1/17 1/18 5/20			RAP \$1,035K REET \$500K LEKT \$125K	90 180 765	110 120 270	200 300 1,035	200 300 1,035	200 300 1,015	200 300 10		EIS CE EA NA	Y/N DATE	
7	6	Edgewood Drive, Rife to Reddick Road (#93110) M.P. 0.67 to M.P. 2.38 Reconstruct Road to Major Collector Standards Widen, Realign, Regrade, Resurface	03 05 07	P	1.51	CPTW	PE RAW CONST TOTAL	1/22 1/22 5/23			RAP \$2.4 mil Pending		119 227 2,378	119 227 2,378	119 227 2,378	119 227 2,378	119 227 2,378	119 227 2,378	EIS CE EA NA	Y/N DATE	
8	7	Happy Valley Road (#50380) M.P. 0.74 to M.P. 1.44 Reconstruct road north of McFarland Drive to west of 3rd Avenue Widen, Regrade, Drainage	03 05 07	P	0.70	CPTW	PE RAW CONST TOTAL	1/24 1/24 1/25					50 0 500 550	50 0 500 550	50 0 500 550	50 0 500 550	50 0 500 550	50 0 500 550	EIS CE EA NA	Y/N DATE	
9	8	Heath Road (#43560) M.P. 0.00 to M.P. 1.50 Shoulder widening	05	S	1.50	CPTW	PE RAW CONST TOTAL	1/22 1/22 5/22					15 15 85 85	15 15 85 85	15 15 85 85	15 15 85 85	15 15 85 85	15 15 85 85	EIS CE EA NA	Y/N DATE	
7	9	Hoko Falls Bridge Repair (#91450 BR2) Hoko Road M.P. 0.88 to M.P. 0.91 Repair Expansion Joints, clean bridge, prepare and paint bearings, seal deck	11	P	0.03	PTW	PE RAW CONST TOTAL	1/24 8/25					25 175 200	25 175 200	25 175 200	25 175 200	25 175 200	25 175 200	EIS CE EA NA	Y/N DATE	
7	10	Hoko-Ozette Road-Lower Hoko Wetland Complex Culvert Replacement Hoko - Ozette Road M.P. 0.97 Design for Replacement of Existing Undersized Culvert with a Fish Passable Structure	09	S	0.01	CPTW	PE RAW CONST TOTAL	1/21			RQ-FSR3 \$200K	200	200	200	200	200	200	200	200	EIS CE EA NA	Y/N DATE

SIX YEAR TRANSPORTATION IMPROVEMENT PROGRAM 2021 TO 2026

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U N C T I O N S	I T E M N U M B E R	PROJECT IDENTIFICATION A. PROJECT / ROAD NAME (ROAD LOG / BRIDGE NUMBER); PROJECT NUMBER B. BEGINNING AND END MILEPOST (M/P) C. DESCRIPTION OF WORK	P R O J E C T P H A S E	S T A T U S	T O T A L L E N G T H	U T I L I T Y C O D E	PROJECT COSTS IN THOUSANDS OF DOLLARS																
							P R O J E C T P H A S E	M O N T H /	Y E A R	FUNDING SOURCE INFORMATION						EXPENDITURE SCHEDULE (LOCAL AGENCY)					FEDERALLY FUNDED PROJECTS ONLY		
										FEDERAL FUND CODE	FEDERAL COST BY PHASE	STATE OR OTHER FUND CODE	STATE FUNDS	LOCAL FUNDS	PROJECT PHASE TOTALS (See RMA 1)	1st	2nd	3rd	4th thru 5th	ENVIR TYPE	R/W REQ Y/N DATE MONTH/YR		
																2021	2022	2023	2024-2026				
1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21			
7	11	Hoko - Ozette Road Johnson Creek Triple Culvert Replacement Hoko - Ozette Road M.P. 9.4 Replace Existing Undersized Culvert with a Larger Culvert or Replace with a Bridge	09	S	0.01	CPTW	PE RAW CONST TOTAL	1/21			RCO FBRB 199K	189		199	189				EA	X	Y/N N DATE		
7	12	Hoko - Ozette Road - Tailbot Creek Culvert Replacement Hoko - Ozette Road M.P. 0.27 Design for Replacement of Existing Undersized Culvert with a Fish Passable Structure	09	S	0.01	CPTW	PE RAW CONST TOTAL	1/21			RCO-FBRB \$197K	187		197	187				EA	X	Y/N N DATE		
7	13	Joyce Piedmont Road (#02210) M.P. 0.00 to 4.3 (SR112 to East Beach Road) Rennshurst Road, Replace Culverts and Repave to an Average width of 26 feet Project will be built by Western Federal Lands Highway Division.	02 03 04 07	S S P	4.00	CPTW	PE RAW CONST TOTAL	1/19 1/19 5/23	FLAP \$487		564 Tot. Credits for Local Match		90 90		250		90		EA		Y/N DATE		
9	14	Kugel Creek Culvert Replacement Cooper Ranch Road M.P. 0.24 Replace Existing Undersized Culvert with a Bridge. Fish passage improvement.	09	S	0.01	CPTW	PE RAW CONST TOTAL	10/18 5/21			RCO 780K	80 700 780	20 75 95	100 776 876					EA	X	N DATE		
7	15	Laird Road (#03110) M.P. 0.23 to 0.70 Resurface and re-align funded by Rural Arterial Program (RAP) with County match. Project construction completed in 2020.	05 07	S	0.47	CPTW	PE RAW CONST TOTAL	1/20 5/20			RAP \$495K RAP \$770K		13 495	10 710					EA		Y/N N DATE		
8	16	Lutzgessell Road (#45308) and Voice of America Road (#45309) Lutzgessell M.P. 0.05 to M.P. 0.26, Voice of America M.P. 0.30 to 0.57 Relocate & widen 1440' of Voice of America Rd. that provides access in Dungeness Recreation Area & National Wildlife Refuge away from bluff. Replace abrupt 90 degree turn near entrance to safer, curved transition from Kitchen-Dick Rd. to Lutzgessell Rd. add left turn lane off of Lutzgessell at Park entrance; and widen shoulders.	02 05 06 07	P	0.21 0.27	CPTW	PE RAW CONST TOTAL		FLAP 1,129	145			21	189			166		EA		Y/N DATE		
Trail	17	Olympic Discovery Trail, Blyn to East County Line Clear grade and construct 800-foot segment of bicycle, pedestrian & equestrian multi-use trail from Diamond Pt. Rd to Old Gardiner Rd. This will be a separated path within the existing Right of Way of US 101, and complete the trail to eastern County line. Includes coordination with Jefferson Co. for short trail to link to Old Gardiner Rd.	32	S	0.20	PTW	PE RAW CONST TOTAL	10/18 9/20					10 50 60	10 50 60					EA	X	Y/N Y DATE		
Trail	18	Olympic Discovery Trail, Elwha River to Freshwater Bay Rd. Gap Continue to plan route, buy Right of Way and construct a bicycle/pedestrian/equestrian multi-use trail for the gap between the Elwha River Bridge to the new (2019) segment between Freshwater Bay Rd. and Thompson Rd.	32	P P P	4.50	PTW	PE RAW CONST TOTAL	1/20				100 300 1,100	100 300 1,100			10 10	280	1,100	EA	X	Y/N Y DATE		
Trail	19	Olympic Discovery Trail, Thompson/Onelia Rd to Gossard Rd Gap Continue to plan route, buy Right of Way & construct a bicycle/pedestrian/equestrian trail for the gap between the Thompson Rd. & Gossard Rd. trail segments.	32	P P P	7.00	PTW	PE RAW CONST TOTAL					100 500 2,800 3,400	100 500 2,800 3,400	10 10		50 470	80	2,800 3,320	EA	X	Y/N Y DATE		

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FUNCTIONAL CLASSIFICATION	ITEM NUMBER	PROJECT IDENTIFICATION					PROJECT COSTS IN THOUSANDS OF DOLLARS																
		PROJECT / ROAD NAME (ROAD LOG / BRIDGE NUMBER); PROJECT NUMBER	BEGINNING AND END MILEPOST (M.P.)	DESCRIPTION OF WORK	ELEMENT TYPE	STATUS	TOTAL LENGTH	UTILITY CODE	PROJECT PHASE	MONTH-YEAR PHASE STARTS	FUNDING SOURCE INFORMATION						EXPENDITURE SCHEDULE (LOCAL AGENCY)				FEDERALLY FUNDED PROJECTS ONLY		
											FEDERAL FUND CODE	FEDERAL COST BY PHASE	STATE OR OTHER FUND CODE	STATE FUNDS	LOCAL FUNDS	PROJECT PHASE TOTALS (See note 1)	1st 2021	2nd 2022	3rd 2023	4th thru 6th 2024-2026	ENVIR. TYPE	RAW REQ Y/N	DATE MONTH/YR
1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21			
Trail	20	Olympic Discovery Trail, Waterline Road Section Complete survey, secure remaining Right-of-Way, widen & pave existing gravel logging access road through the Lyre River Canyon provide paved trail connection between the Gossett/Waterline Rd. and U.S. Crescent Service Railroad trail segments	32	S	3.50	PTW	PE RAW CONST TOTAL	10/14 12/14 6/24					50 80 780 830	50 80 780 830			50		EIS CE EA NA		Y/N Y DATE		
Trail	21	Olympic Discovery Trail, Mary Clark Rd. to Forks "A" Rd. Gap Continue to plan, issue, buy Right of Way & construct bicycle/pedestrian/equestrian multi-user trail	32	P F P	11.00	PTW	PE RAW CONST TOTAL						50 500 2,450 3,000	50 500 2,450 3,000			50	780	EIS CE EA NA	X	Y/N Y DATE		
Trail	22	Olympic Discovery Trail, Forks "A" Rd. to Calawah River Boat Launch Park Plan, Purchase Right of Way and Construct Paved Bicycle, Pedestrian, and Equestrian Multi-User Trail	32	S S P	1.00	PTW	PE RAW CONST TOTAL	10/18 10/18 6/22	TA \$450K	85	PVT \$330K		10 145 1,115 1,270	75 145 1,500 1,720	75	75	10	10	30	EIS CE EA NA		Y/N Y DATE	
Trail	23	Olympic Discovery Trail, Forks to LaPush Plan, Purchase Right of Way and Construct Paved Bicycle, Pedestrian, and Equestrian Multi-User Trail. Parking lot improvements at Olympic National Park (ONP). 2nd and 3rd beach trailheads. Bogachiel bridge resurfacing	06 11 32	S S S	12.00	PTW	PE RAW CONST TOTAL	1/23 7/21 6/24	FLAP 3,153 million ONP \$200K	498 5,661 6,157	RCO \$905K PVT \$200K		498 1,207 5,661	498 725 5,661			498		EIS CE EA NA	X	Y/N Y DATE		
7	24	Sequim Dungeness and Woodcock Road Intersection Improvement M.P. 2.42 to M.P. 2.62 Roundabout intersection improvement	3 5 6 7	S	0.20	CPTW	PE RAW CONST TOTAL	2/20 7/20 4/21	STIP \$490K	45 45 800			5 5 77	30 50 827			30 50 617	10	EIS CE EA NA		Y/N DATE		
8	25	Sot Duc Bridge Repair (#98090 BR1) Quilayute Road M.P. 5.47 to M.P. 5.52 Rehab Portals and Parbs Recant	11	P	0.05	CPTW	PE RAW CONST TOTAL	8/24 8/25					50 250 300					10 250 260	EIS CE EA NA	X	Y/N N DATE		
7	26	Taylor - Cutoff Left Turn Lane M.P. 0.00 to M.P. 0.10 Widen Road and Stripe a center left turn lane	5 6 7	S	0.10	CPTW	PE RAW CONST TOTAL	1/24 6/25					20 130 150	20 130 150				20 130 150	EIS CE EA NA		Y/N DATE		
9	27	Towne Road (#62270) M.P. 0.76 to M.P. 1.99 Reconstruct road between Woodcock Rd. & future alignment on top of relocated levee. Widen, Regrade, Drainage	3 5 6 7	S	1.23	CPTW	PE RAW CONST TOTAL	1/18 1/18 6/22			REET		70 80 460 590	70 80 460 590	10 40				EIS CE EA NA		Y/N DATE		
9	28	Towne Road, No. (#2270, #2270A, #2600) M.P. 1.89 to Now Intersection with Sequim-Dungeness Way Construct road segment on top of Dungeness River relocated levee and construct roundabout at intersection with Sequim-Dungeness Way.	2, 3, 5, 6 & 7	S	0.75	CPTW	PE RAW CONST TOTAL	1/18 1/18 6/22			REET			810 810			500 800	10	EIS CE EA NA		Y/N DATE		

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FUNCTIONAL CLASSIFICATION	ITEM NUMBER	PROJECT IDENTIFICATION A. PROJECT / ROAD NAME (ROAD LOG / BRIDGE NUMBER); PROJECT NUMBER B. BEGINNING AND END MILEPOST (MP) C. DESCRIPTION OF WORK	PROJECT TYPES	STATUS	TOTAL LENGTH	UTILITY	PROJECT COSTS IN THOUSANDS OF DOLLARS														
							PROJECT PHASE	MONTH/YEAR PHASE STARTS	FUNDING SOURCE INFORMATION						EXPENDITURE SCHEDULE (LOCAL AGENCY)				FEDERALLY FUNDED PROJECTS ONLY		
									FEDERAL FUND CODE	FEDERAL COST BY PHASE	STATE OR OTHER FUND CODE	STATE FUNDS	LOCAL FUNDS	PROJECT PHASE TOTALS (See note 1)	1st	2nd	3rd	4th thru 6th	ENVIR TYPE	RAW REQ Y/N	
															2021	2022	2023	2024-2026			DATE MONTH/YR
1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	
9	29	Widen Creek Road - Culvert Replacement Widen Creek Road M.P. 0.13 and M.P. 0.38 and at Swede Road M.P. 0.00 Design for Replacement of Three Existing Undersized Culverts with a Fish Passable Structures	09	P	0.01	CPTW	PE RAW CONST TOTAL	1/20			RCC-SRPB	137	25	162	118				PS CE EA NA	Y/N h DATE	
N/A	30	Countywide Various Improvements Traffic control devices, spot safety, shoulder widening, drainage, surfacing upgrades or other improvements.	08	P	0.01	CPTW	PE RAW CONST TOTAL					137	25	162	118	5	5	5	15	PS CE EA NA	Y/N n DATE
		GRAND TOTALS <i>(Note: Totals includes the Federal Lands Access Program (FLAP) funded projects managed by Western Federal Lands Highway Division.)</i>												45,560	5,848	5,713	8,707	20,201			

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F L N C C P L A S N B I	I R E M U M B E R	PROJECT IDENTIFICATION					PROJECT COSTS IN THOUSANDS OF DOLLARS													
		A. PROJECT / ROAD NAME (ROAD LOG / BRIDGE NUMBER), PROJECT NUMBER					PROJECT PHASE	MONTH/ YEAR PHASE STARTS	FUNDING SOURCE INFORMATION						EXPENDITURE SCHEDULE (LOCAL AGENCY)				FEDERALLY FUNDED PROJECTS ONLY	
		B. BEGINNING AND END MILEPOST (M.P.)							FEDERAL FUND CODE	FEDERAL COST BY PHASE	STATE OR OTHER FUND CODE	STATE FUNDS	LOCAL FUNDS	PROJECT TOTAL \$ (See note 1)	1st	2nd	3rd	4th thru 6th	FMR TYPE	R/W REQ Y/N DATE MONTH/YR
		C. DESCRIPTION OF WORK													2021	2022	2023	2024-2026		
1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21
		PLANNED (UNFUNDED PROJECTS) - PROJECT NUMBERS U1 - U36																		
5	U1	Anderson Road (East) (#47590) M.P. 0.00 to M.P. 0.45 (Cays Rd to Thorton Drive - east crossing) Drainage, Widening, Resurfacing, Potential Rural Arterial Program Project.	03 05 07	P/U	0.45	CPTO	PE R/W CONST TOTAL						40 50 400 490	460					ES CE EA NA	Y/N DATE
8	U2	Adairberry Road (#44300) M.P. 0.00 to M.P. 2.78 Reconstruct road between Sherburne Road & Hooker Road. Widening, Drainage - 3 Phases	02 03 05 07	P/U	2.78	CPTW	PE R/W CONST TOTAL						100 300 2380 2780	2780					ES CE EA NA	Y/N DATE
8	U3	Barr Road (North) (#43110) M.P. 0.15 to M.P. 0.82 Reconstruct road north of SR 101 to west 90 degree corner. Widening, Regrade, Drainage - 2 Phases	02 03 05 07	P/U	0.64	CPTW	PE R/W CONST TOTAL						50 50 300 400	400					ES CE EA NA	Y/N DATE
8	U4	Brown Road (#53810) M.P. 0.57 to M.P. 1.47 Sequim City Limits to Port Williams Road 3R upgrade plus walkways/bikeway	05 06 32	P/U	0.50	CPSTWO	PE R/W CONST TOTAL						60 75 330 435	435					ES CE EA NA	Y/N DATE
17	U5	Edgewood Drive, Reddick to City Limits (#93110) M.P. 2.38 to M.P. 2.99 Reconstruct Road to Major Collector Standards Widening, Resurfacing, Regrade, Resurface	03 04 07	P/U	0.61	CPTW	PE R/W CONST TOTAL						50 50 850 950	950					ES CE EA NA	Y/N DATE
12	U6	Emmis Creek Road (#35200) M.P. 0.00 to M.P. 0.27 Baker Street to Front Street 3R upgrade plus walkways/bikeway	05 06 32	P/U	0.27	CPTW	PE R/W CONST TOTAL						60 60 400 520	520					ES CE EA NA	Y/N DATE
9	U7	Heath Road Connector to Gilbert Road (#48650) Heath Road to Gilbert Road, No. 45850 Construct Connector Road between Heath and Gilbert Carlsborg Transportation Study #13	02 03 05 07	P/U	0.08	CPTW	PE R/W CONST TOTAL						300 300 1300 1900	1900					ES CE EA NA	Y/N DATE
7	U8	Hoko-Ozette Road (#91460) M.P. 19.5 Relocate road segment (~ 400 feet or less) away from slide area, reconstruct and resurface. May need 40 to 50 feet of additional right-of-way.	02 03 07	P/U	0.08	PT	PE R/W CONST TOTAL						10 10 150 170	170						
7	U9	Hoko-Ozette Road, No. 91480 Various segments M.P. 0.00 to M.P. 21.31 Reconstruct road away from Big River bank in 4 phases M.P. 0.79-1.0, 2.2-2.4, 2.7-3.0, 4.1-4.4	02 03 05 07	P/U	1.01	PTW	PE R/W CONST TOTAL						120 100 750 970	970					ES CE EA NA	Y/N DATE
7	U10	Hoko-Ozette Road, No. 91460 M.P. 5.54 to M.P. 5.84 "Big Fat" in "Borrow Site" Rebuild to City/County Standards	03 05 07	P/U	0.30	CPT	PE R/W CONST TOTAL						24 18 235 277	277					ES CE EA NA	Y/N DATE

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U N C C T I A O S I	T E M N U M B E R	PROJECT IDENTIFICATION A. PROJECT / ROAD NAME (ROAD LOG / BRIDGE NUMBER), PROJECT NUMBER B. BEGINNING AND END MILEPOST (M.P.) C. DESCRIPTION OF WORK	1 2 3 4 5	6 7 8 9 10	11 12 13 14 15	PROJECT COSTS IN THOUSANDS OF DOLLARS														FEDERALLY FUNDED PROJECTS ONLY	
						PROJECT PHASE	MONTH/ YEAR PHASE STARTS	FUNDING SOURCE INFORMATION						EXPENDITURE SCHEDULE (LOCAL AGENCY)				ENVIRONMENTAL TYPE	ROW REQ Y/N (DATE MONTH/YR)		
								FEDERAL FUND CODE	FEDERAL COST BY PHASE	STATE OR OTHER FUND CODE	STATE FUND	LOCAL FUND	PROJECT PHASE TOTALS (See note 1)	2021	2022	2023	2024-2025				
1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	
9	U11	Kirk Road Extension M.P. 0.28 to M.P. 0.50 Upgrade Existing Road and Extend to Altember Road Carlsborg Transportation Study #10	03 04 07	PAU	0.22	CPT	PE RAW CONST TOTAL						150 160 880 890	890					EA CE EA NA	Y DATE	
9	U12	Lower Elwha Road (#31310) M.P. 0.78 to M.P. 2.78 Widen, Drainage, Sidewalk, Reseal	03 04 06 07	PAU	1.99	CPTW	PE RAW CONST TOTAL						100 100 1,558 1,758	1,758					EA CE EA NA	Y DATE	
9	U13	Marine Drive (#47530) M.P. 0.60 to M.P. 0.72 Cuts Road to Clinic Sph Road Bluff Drainage Enhancements	05 06 07	PAU	0.72	CPTWO	PE RAW CONST TOTAL						58 40 533 631	631					EA CE EA NA	Y DATE	
9	U14	Mary Clark Road (#20100) M.P. 2.32 to M.P. 2.60 Realign road at Bear Springs Hatchery Corner	02 05 07	PAU	0.28	PT	PE RAW CONST TOTAL						10 25 150 185	185					EA CE EA NA	Y DATE	
9	U15	Mill Road Traffic Island (#44930) M.P. 0.00 Install Raised Traffic Island to make Mill Road Right in-Right Out Intersection Carlsborg Transportation Study # 11D	05 06 07	PAU	0.1	CPTWO	PE RAW CONST TOTAL						50 30 110 190	190					EA CE EA NA	Y DATE	
9	U16	Mill Road Extension to Business Park Loop (#44930) M.P. 0.57 to 0.73 Extend Mill Road to tie into the Industrial Business Park Loop Carlsborg Transportation Study # 6	05 06 07	PAU	0.1	CPTWO	PE RAW CONST TOTAL						50 50 250 350	350					EA CE EA NA	Y DATE	
8	U17	Mt. Angeles Road M.P. 0.47 to M.P. 1.12 Proposed Rural Arterial Program Project Widen, Realign, Regrade, Reseal	02 03 04 07	PAU	0.65	CPTW	PE RAW CONST TOTAL						75 65 600 740	740					EA CE EA NA	Y DATE	
7	U18	Old Olympic Highway (#34120) M.P. 4.00 to 5.67 Widen, Regrade, Pulverize, Reseal & Drainage from McDonald Creek Bridge to Cays Road	03 05 07	PAU	2.67	CPTW	PE RAW CONST TOTAL						100 920 1,020 1,020	1,020					EA CE EA NA		
9	U19	Parkwood Boulevard Traffic Island (#44940) M.P. 0.00 Install Raised Traffic Island to make Parkwood Boulevard a Right in- Right Out Intersection. Carlsborg Transportation Study #11D.	05 06 07	PAU	0.1	CPTWO	PE RAW CONST TOTAL						30 50 110 190	190					EA CE EA NA	Y DATE	

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J N C C L A O S I	I T E M N U M B E R	PROJECT IDENTIFICATION A. PROJECT / ROAD NAME (ROAD LOG / BRIDGE NUMBER), PROJECT NUMBER B. BEGINNING AND END MILEPOST (M.P.) C. DESCRIPTION OF WORK	P R O J E C T T Y P E S	S T A T U S	T O T A L L E N G T H	U T I L I T Y C O D E S	PROJECT COSTS IN THOUSANDS OF DOLLARS														
							PROJECT PHASE	MONTH/ YEAR PHASE STARTS	FUNDING SOURCE INFORMATION						EXPENDITURE SCHEDULE (LOCAL AGENCY)				FEDERALLY FUNDED PROJECTS ONLY		
									FEDERAL FUND CODE	FEDERAL COST BY PHASE	STATE OR OTHER FUND CODE	STATE FUNDS	LOCAL FUNDS	PROJECT PHASE TOTALS (See note 1)	1st 2021	2nd 2022	3rd 2023	4th thru 6th 2024-2026	ENVIR. TYPE	R/W REQ. Y/N	DATE MONTH/YR
1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	
19	U20	Pioneer Road (#35820) M.P. 0.00 to M.P. 0.35 Reconstruct road between SR 101 & Baker Street Widen, Regrade, Drainage - 2 Phases	02 03 05 07	PAU	0.35	CPTW	PE RAW CONST TOTAL						50 100 400 550	550					EIS CE EA NA	Y/N DATE	
8	U21	Quillayute Road (#11206) M.P. 1.72 to 2.67 Quillayute Weather Station to Mine Smith Road Regrade, Drain, Widen, Resurface	03 04 07	PAU	1.15	CPTW	PE RAW CONST TOTAL						100 100 875 1,075	1,075					EIS CE EA NA	Y/N DATE	
8	U22	Rumple Road, East (#44730) M.P. 0.66 to M.P. 1.14 Carlsborg Road to Mill Road Regrade, Drain, Widen, Resurface	03 04 07	PAU	0.25	CPTW	PE RAW CONST TOTAL						20 25 105 150	150					EIS CE EA NA	Y/N DATE	
8	U23	Rumple Road Roundabout (#44730) M.P. 0.66 (At intersection with Carlsborg Road and realigned Speth Road) Construct a four-legged roundabout with realigned Speth Road Carlsborg Transportation Study #1	03 04 07	PAU	0.25	CPTW	PE RAW CONST TOTAL						78 100 560 738	738					EIS CE EA NA	Y/N DATE	
7	U24	Sequim-Dungeness Way (#99350) M.P. 1.00 to 4.62 Sequim City Limits to Marine Drive, except Woodcock Road vicinity Regrade	06 07	PAU	3.62	N/A	PE RAW CONST TOTAL						29 43 652 718	718					EIS CE EA NA	Y/N DATE	
9	U25	Speth Road (#45420) M.P. 0.00 to M.P. 0.80 Kitchen-Dick Road to Carlsborg Road Regrade, Drain, Widen, Resurface	03 04 07	PAU	1.25	CPTW	PE RAW CONST TOTAL						52 109 800 961	961					EIS CE EA NA	Y/N DATE	
9	U26	Speth Road to West Rumple Road Connector Route (#45420) M.P. 0.00 to M.P. 0.80 (West Rumple Road) Construct New Connector Segment between Speth and West Rumple Roads Carlsborg Transportation Study #5	03 04 07	PAU	1.25	CPTW	PE RAW CONST TOTAL						100 90 800 990	990					EIS CE EA NA	Y/N DATE	
9	U27	Spencer Road Connector to Mill Road (#44659) M.P. 1.50 to Mill Road Construct New Connector Segment between Spencer Road and Mill Road Carlsborg Transportation Study # 14	03 04 07	PAU	0.75	CPTW	PE RAW CONST TOTAL						300 300 1,884 2,484	2,484					EIS CE EA NA	Y/N DATE	
N/A	U28	Sunland Non-motorized Trail Trail beside Taylor Blvd in Sunland for pedestrian access - Sequim Way to Holland Road	02	PAU	2.00	PTW	PE RAW CONST TOTAL						40 40 200 300	300					EIS CE EA NA	Y/N Y DATE	

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F U N D I N G S T R U C T U R E	I T E M N U M B E R	PROJECT IDENTIFICATION A. PROJECT / ROAD NAME (ROAD LOG / BRIDGE NUMBER), PROJECT NUMBER B. BEGINNING AND END MILEPOST (M.P.) C. DESCRIPTION OF WORK	I M P R O V E M E N T T Y P E	S T A T U S	T O T A L J E N S T H	U T I L I T Y C O D E	PROJECT COSTS IN THOUSANDS OF DOLLARS														
							PROJECT PHASE	MONTH/ YEAR PHASE STARTS	FUNDING SOURCE INFORMATION						EXPENDITURE SCHEDULE (LOCAL AGENCY)				FEDERALLY FUNDED PROJECTS ONLY		
									FEDERAL FUND CODE	FEDERAL COST BY PHASE	STATE OR OTHER FUND CODE	STATE FUNDS	LOCAL FUNDS	PROJECT PHASE TOTALS (See note 1)	1st	2nd	3rd	4th thru 6th	ENVIR TYPE	R/W REQ. Y/N DATE MONTH/YR	
															2021	2022	2023	2024-2026			
1	2	3	4	5	6	7	8	9	10	11	12	13	14		16	17	18	19	20	21	
7	U29	Trout Creek Bridge Replacement (#91488 BR4) M.P. 17.18 to M.P. 17.29 Hoko-Ozette Road	04 09	P/U	0.10	PT	PE RW CONST TOTAL						50 10 475 535	535					CE CE EA NA	Y/N DATE	
7	U50	US 101 Underpasses of McDonald Creek Bridge (#94120 BR1) Reconnect South Barn Road and Shemurus Road to North Side of US 101 by extending those County Roads under the New McDonald Creek US 101 Bridges	04 09 07	P/U	0.09	CPT	PE RW CONST TOTAL						200 1,948 2,148	2,148					EIS CE EA NA	Y/N DATE	
9	U31	West Bridge (#18758 BR1) West Road, No. 18760 M.P. 0.32 to M.P. 0.38 (West Road, No. 18760) Resurface and lengthen bridge	06	P/U	0.06	CPTW	PE RW CONST TOTAL						348 30 1,113 1,491	1,491					EIS CE EA NA	Y/N DATE	
9	U32	Wintahawk to West Runnon Connector (#44738) M.P. 0.60 - Intersection with West Runnon Road Wintahawk Street to West Runnon Road Construct new Connector Segment between Wintahawk St. & W. Runnon Rd. Carlson's Transportation Study # 12	03 04 07	P/U	1.25	CPTWO	PE RW CONST TOTAL						50 20 90 160	160					EIS CE EA NA	DATE	
		TOTAL UNFUNDED PROJECTS												27,011							

1-Total project costs represents the total estimated cost of the project. The funding represented in the Six Year Transportation Improvement Program (TIP) may be less than the total project funding for two reasons. One, the project is underway and has expended money during the years prior to the first year of the TIP. Two, the project is planned for the later years of the TIP and will not be completed by the final year of the TIP.

*APPENDIX E – JEFFERSON COUNTY 2021-2026 TRANSPORTATION IMPROVEMENT PROGRAM (TIP)
(DIGITAL)*

Jefferson County Public Works Adopted 2021 - 2026 Six-Year Transportation Improvement Program



Jefferson County Public Works
623 Sheridan Street
Port Townsend, WA 98368
Phone: 360-385-9160
www.co.jefferson.wa.us/publicworks



ADOPTED 2021 - 2026

Six-Year Transportation Improvement Program

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Bridge Inventory List for East & West County	
Reference Materials & Definitions	
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Department of Public Works Jefferson County, Washington

623 Sheridan Street
Port Townsend, WA 98368
360-385-9160

Welcome to the adopted 2021-2026 Six-Year Transportation Improvement Program (TIP) and its supporting materials. The Six-Year TIP is a rolling plan of proposed transportation projects that shows anticipated, partial, and/or total secured funding. The plan is annually updated to provide direction to transportation investments on Jefferson County's public roads and non-motorized inventory. The TIP development is a process that identifies and prioritizes future project phases to balance needs with financial resources, and schedules a six year planning period for reasonably viable projects.

A variety of capital preservation and improvement projects are included in the TIP but are reliant on receiving grants or federal investments such as Surface Transportation Program (STP) and state grants from CRAB, RCO, WSDOT and/or WDFW. The TIP prioritized list is highly influenced by the availability of these funds. The majority of the listed projects do not have secured funding as of TIP adoption. Historically, projects on Jefferson County's TIP have averaged over 80% funding from State and Federal sources; however, non-local transportation revenue sources usually fund specific types of improvements only on certain collector roads and trails. Federal and state grant program criteria can strongly influence which projects and features are accomplished. Often in smaller agencies, local match fund availability limits the number and size of grant applications. Local funds shown for this proposed Six-Year TIP average just \$197,000 per year and primarily come from the local Road Levy and Motor Vehicle Fuel Tax. Jefferson County has a limited tax base for its 400 road miles. Our transportation revenues are among the lowest per road mile in western Washington. The county road fund has seen a 25% overall reduction in annual operating revenue due to loss of federal land timber revenue in recent years. As a result, road fund reserves are projected to decline progressively each year.

The Six-Year TIP, like any other capital improvement plan, is only a plan for what should be done under the provision of adequate funding. It demonstrates typical or likely funding and/or a distribution of limited revenue sources. Decisions about advancing projects into design, acquisition, and construction are made through the county budgeting process. Since the Six-Year TIP is updated annually, it can be changed to reflect new needs, priorities, revenues, and revised budget assumptions.

If you would like more information, feel free to contact the Jefferson County Public Works office: 623 Sheridan Street, Port Townsend, WA (360)385-9160.

Monte Reinders, P.E.
Public Works Director/County Engineer

cc: PW } 10-20-20
fund }

**COUNTY OF JEFFERSON
STATE OF WASHINGTON**

In the Matter of Adoption of the }
2021-2026 Six-Year Transportation }
Improvement Program }

RESOLUTION NO. 67 20

WHEREAS, State law obligates the legislative authority of each county to adopt on an annual basis a six-year transportation improvement program, holding a public hearing prior to adoption, the purpose of which is to "assure that each county shall perpetually have available advanced plans looking to the future for not less than six years as a guide in carrying out a coordinated transportation program" (RCW 36.81.121(1)); and,

WHEREAS, a multi-year analysis of the Road Fund was prepared covering the six-year program period using the County's best estimate of future revenues and expenditures in accordance with WAC 136-15-030; and,

WHEREAS, the Board of County Commissioners, Jefferson County, in the State of Washington, has reviewed the transportation project priority array, the road fund forecast and bridge condition report as prepared by the Department of Public Works, and has conducted a public hearing on the corresponding proposed six-year transportation improvement program;

IT IS HEREBY RESOLVED that the Board of County Commissioners does approve and adopt the Six-Year Transportation Improvement Program for the years 2021 through 2026 inclusive in accordance with WAC 136-15.

ADOPTED and signed this 26th day of October 2020.



JEFFERSON COUNTY
BOARD OF COMMISSIONERS



Greg Brotherton, Chair

Approved telephonically
David Sullivan, Member

ATTEST:



Carolyn Gallaway, Deputy Clerk



Kate Dean, Member

NOTICE OF PUBLIC HEARING:
PROPOSED SIX-YEAR TRANSPORTATION IMPROVEMENT PROGRAM

Notice is hereby given that a public hearing will be held by the Jefferson County Board of Commissioners on **Monday, October 26, 2020, at 10:30 AM** in the Commissioners' Chambers at the County Courthouse, 1820 Jefferson Street, Port Townsend, Washington, for the purpose of receiving written and verbal testimony about the adoption of the draft Jefferson County 2021-2026 Six-Year Transportation Improvement Program (TIP). "Each county shall perpetually have available advanced plans looking to the future for not less than six years as a guide in carrying out a coordinated transportation program, RCW 36.81.121(1)." Written public comment on the program will be received up and until the public hearing mailed to Jefferson County Commissioners' Office, PO Box 1220, Port Townsend, WA 98368 or emailed to jeffbocc@co.jefferson.wa.us.

The printed program is available for viewing at the Jefferson County Public Works office, 623 Sheridan Street, Port Townsend, open 8:00AM to 5:00PM, the Commissioners' office, 1820 Jefferson Street, Port Townsend, open 8:30AM to 4:30PM, and on the county website at www.co.jefferson.wa.us/444/6-Yr-TIP

Jefferson County ensures full compliance with Title VI of the Civil Rights Act of 1964 by prohibiting discrimination against any person on the basis of race, color, sex or national origin in the provision of benefits & services resulting from its federally assisted programs & activities. The meeting site is ADA accessible. Accommodations for people with disabilities can be arranged with advance notice by calling (360)385-9100.

NOTE: Due to Covid-19, it is unknown if there will be restrictions on the Open Public Meeting Act on the date of this Hearing that will preclude public in-person attendance. Check the Jefferson County website at www.co.jefferson.wa.us for the current status and follow "Quick Links-Videos of Meetings-Streaming Live." to testify at the public hearing.


Greg Brotherton, Chair
Board of Jefferson County Commissioners

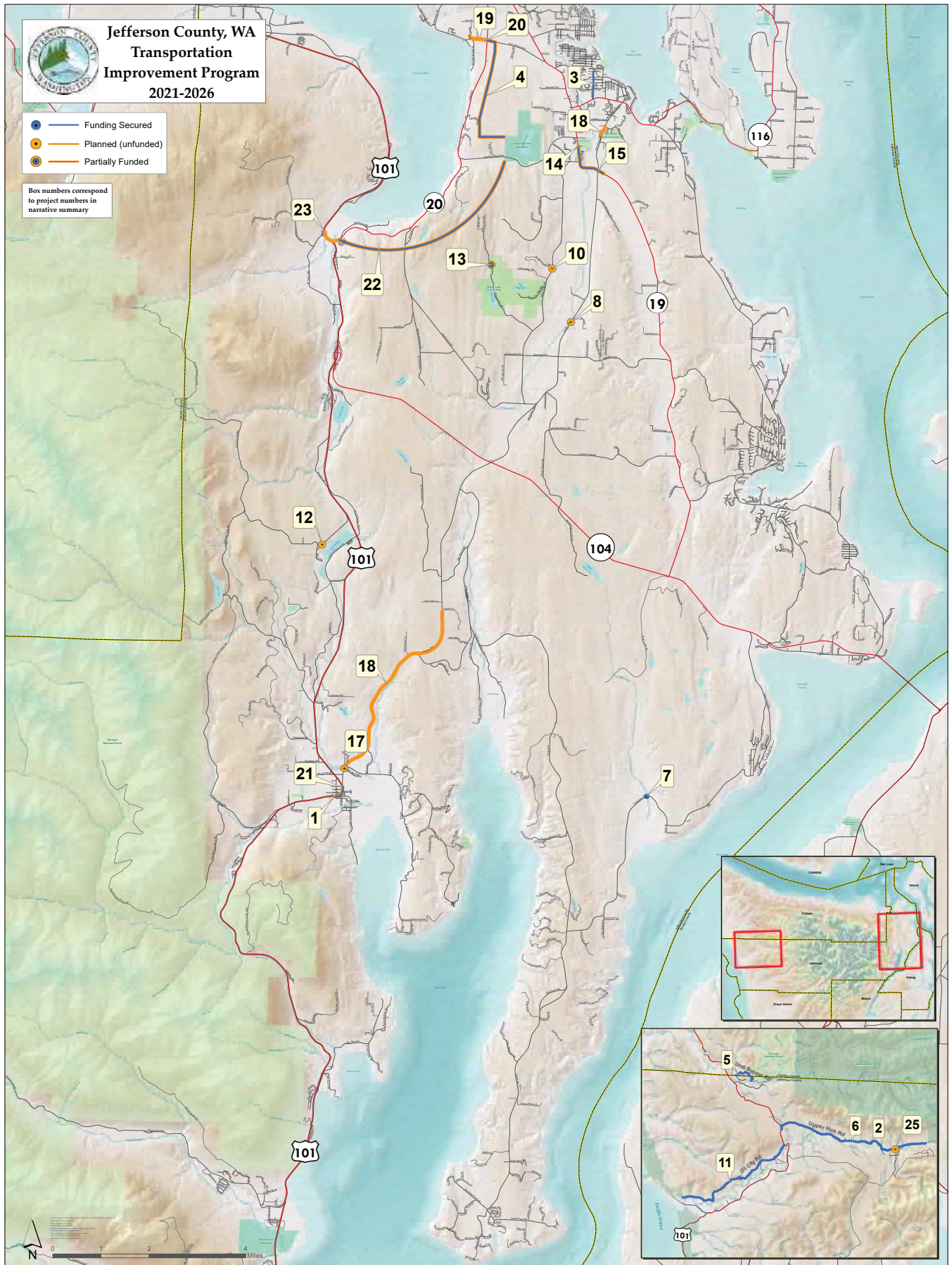
10/5/2020
Date



**Jefferson County, WA
Transportation
Improvement Program
2021-2026**

- Funding Secured
- Planned (unfunded)
- Partially Funded

Box numbers correspond
to project numbers in
narrative summary



**SIX-YEAR
TRANSPORTATION IMPROVEMENT PROGRAM
2021 - 2026**

FUNC. CLASS	TIP PROJECT NO.	PROJECT IDENTIFICATION A. Road Log Number - Bridge Number B. Beginning and End Milepost C. Project (No.) / Road Name D. Description of Work E. Total Cost (incl. prior/future)	IMPROVEMENT TYPE	FUND STATUS	ENVIRONMENTAL ASSESS	TOTAL LENGTH (mi.)	PROJECT COSTS IN DOLLARS										EXPENDITURE SCHEDULE								
							PROJECT PHASE	MONTH / YEAR FUND STARTS	FUNDING SOURCE INFORMATION						OTHER REVENUES	LOCAL FUNDS	TOTAL	YEAR 1 2021	YEAR 2 2022	YEAR 3 2023	YEAR 4 2024	YEAR 5 2025	YEAR 6 2026	TOTAL 2021-2026	
									FEDERAL FUNDS		STATE FUNDS														
									FEDERAL FUND TYPE	FEDERAL COST BY PHASE	STATE FUND TYPE	STATE COST BY PHASE													
00	1	US 101 & connected county roads MP 294.6 to MP 295.0 Quilcene Complete Streets - Phase 1 (1801933) Publicly driven ped/bike improvements involving: US 101, traffic calming, sidewalk, crosswalk & lighting improvements with a focus on connecting to the school campus. Total = \$997,500 (with \$52,416 from PIF)	28	S/P	E	0.50	PE R/W CN Total	1/13 3/20 6/21						10,000 0 0	10,000 0 0	10,000 0 0							10,000 0 0		
07	2	91420 MP 0.00 to MP 12.04 (18019410) Upper Hoh Road Phase 2 - FLAP Project Local funds, total \$63,621,for infrastructure protection as performed by FHWA Western Federal Lands (WFL) division, thru FLAP program. Total FLAP = \$14,029,621 (uses toll credits)	06	S	S	12.04	PE R/W CN Total							0 0										0 0	
09	3	63710 MP 0.00 to MP 0.46 (18020570) Cedar Avenue, Ness' Corner (SR116) to Hayden Road Ped-Bike WSDOT Safe Routes to School funds for ped/bike safety improvements including multi-use path, sidewalk, crosswalks, bike lanes, and RRFB. Total = \$880,300	28	S	E	0.61	PE R/W CN Total	6/19 6/21		SRTS	17,805 803,152			17,805 803,152	17,805 803,152									17,805 803,152	
00	4	850200 4 Corners Road to Anderson Lake State Park Olympic Discovery Trail (ODT) - Connection (1801989) PS&E, ROW (RCO RRG) and Construction (STBG & RCO WWRP) of OD segment between Jefferson Transit P&R and Anderson Lake State Park Total Construction = \$1,189,020 (STBG) + \$2,189,021 (RCO WWRP)	28	S/P	E	2.70	PE R/W CN Total	1/17 1/20 6/22			RCO-RRG RCO-RRG	38,070 231,500			38,070 231,500	38,070 231,500								38,070 231,500	
09	5	15100 MP 0.90 to MP 2.185 (18020460) Undi Road Bypass - FLAP Project Local funds, total \$33,000, to preserve access to ONP & USFS as performed by FHWA Western Federal Lands division, thru FLAP program. Total FLAP = \$2,034,407 (uses toll credits)	06	S	I	1.285	PE R/W CN Total	1/17 6/22	FLAP FLAP					13,336 0	13,336 0	6,668 0	6,668 0							13,336 0	
07	6	91420 MP 0.00 to MP 12.04 (18020560) Upper Hoh Road Pavement Preservation Pavement preservation & safety improvements along Upper Hoh Rd. to Hoh Rain Forest/ONP boundary performed by FHWA, WFL division. Total = \$1,567,600 (FLAP) + \$408,400 (RAP)	06 21	S	E	12.04	PE R/W CN Total	1/17 6/21	FLAP FLAP	0 0	RAP RAP	61,400 347,000			61,400 347,000	6,400 347,000	55,000 347,000							61,400 347,000	
08	7	41870 MP 4.71 (18020580) Thorndyke Road Culvert Replacement - Thorndyke Creek Replace parallel culverts at Thorndyke Creek with fish-passable crossing. RCO will manage permits and design review for lead agency FBRB. Total = \$1,843,723(FBRB) +\$220,366(NFPP) +\$75,000(PIF)	11	S/P	I	N/A	PE R/W CN Total	7/19 7/21						5,000 70,000	13,000 1,935,776	8,000 1,935,776	5,000 1,940,776							13,000 1,935,776	
07	8	93150 MP 3.26 Center Road Culvert Correction - Chimacum Creek Replace non-conforming road culvert with fish passable culvert design for Chimacum Creek salmon. Total grants= \$2,497,500	11	P	I	N/A	PE R/W CN Total	7/21 6/25			FBRB	202,300 1,997,500			35,700 52,500	238,000 2,350,000	28,000 2,350,000	100,000 2,350,000	100,000 2,350,000	10,000 2,350,000				238,000 2,350,000	
00	9	Countywide Bridge Load Re-Rating Federally required re-analysis of 22 county bridge load ratings for accomodation of new specialized hauling vehicles by 12-31-22. Total STBG = \$111,325.50 (No Construction)	18	S	E	N/A	PL R/W CN Total	1/21	STP-R	111,325					17,375	128,700	128,700							128,700	
09	10	51410 MP 2.59 (18020591) West Valley Road Culvert Replacement - Naylors Creek Replace culvert at Naylor Creek with fish-passable crossing and realign creek for 300ft. RCO will manage permits& design review for FBRB. Total Grants = (\$99,425+\$684,980)FBRB Plan&Restore +\$127,854 NFPP	11	S/P	I	N/A	PE R/W CN Total	10/19 6/24	NFPP NFPP	7,425 120,429		FBRB FBRB			6,250 0	13,675 802,859	6,250 3,000	1,225 4,000	6,200 798,859					13,675 802,859	
09	11	13430 MP 8.50 to MP 10.98 Oil City Road Engineering Assessment - FLAP Project Local funds for infrastructure protection, planning project performed by FHWA Western Federal Lands division, thru FLAP program. Total FLAP = \$250,000 (uses toll credits)	18	S	E	2.48	PE R/W CN Total	1/21	FLAP					30,000 0 0	30,000 0 0	15,000 15,000	15,000 15,000							30,000 0 0	

**SIX-YEAR
TRANSPORTATION IMPROVEMENT PROGRAM
2021 - 2026**

FUNC. CLASS	TIP PROJECT NO.	PROJECT IDENTIFICATION A. Road Log Number - Bridge Number B. Beginning and End Milepost C. Project (No.) / Road Name D. Description of Work E. Total Cost (incl. prior/future)	IMPROVEMENT TYPE	FUND STATUS	ENVIRONMENTAL ASSESS	TOTAL LENGTH (mi.)	PROJECT COSTS IN DOLLARS										EXPENDITURE SCHEDULE											
							PROJECT PHASE	MONTH / YEAR FUND STARTS	FUNDING SOURCE INFORMATION																			
									FEDERAL FUNDS		STATE FUNDS		OTHER REVENUES	LOCAL FUNDS	TOTAL													
									FEDERAL FUND TYPE	FEDERAL COST BY PHASE	STATE FUND TYPE	STATE COST BY PHASE																
YEAR 1 2021	YEAR 2 2022	YEAR 3 2023	YEAR 4 2024	YEAR 5 2025	YEAR 6 2026	TOTAL 2021-2026																						
09	12	35290 MP 0.84 (18020310) Snow Creek Road Culvert Replacement Replace and upslope a deteriorated metal culvert above Lake Leland on an unnamed tributary w/o anadromous fish. Total FEMA (Hazard Mitigation Program + Local Funds) = \$1,292,486	11	P	E	N/A	PE R/W CN Total																					
								1/20	FEMA	86,250	FEMAm	14,375		14,375	115,000	110,000	5,000											115,000
								6/21	FEMA	11,250	FEMAm	1,875		1,875	15,000	15,000	0											15,000
								6/22	FEMA	1,010,345	FEMAm	168,391		168,391	1,347,127	0	1,347,127											1,347,127
								Total		1,107,845		184,641	0	184,641	1,477,127	125,000	1,352,127	0	0	0	0	0	0	1,477,127				
09	13	51750 MP 1.82 (18020591) Gibbs Lake Road Culvert Replacement - Naylors Creek Replace culvert at Naylor Creek with fish-passable crossing. RCO will manage permits and design review for lead agency FBRB. Total Grants= \$99,425(FBRB-secure)+\$606,592(FBRB)+\$105,152(NFPP)	11	S/P	I	N/A	PE R/W CN Total																					
								10/19					8,625	8,625					8,625								8,625	
								6/25	NFPP	105,152	FBRB	599,261		0	704,413					4,000	700,413						704,413	
								Total		105,152		599,261	0	8,625	713,038	0	0	0	0	12,625	700,413	0	713,038					
								SR19 MP 9.56 to MP 10.1 (18020040) Rhody Drive Ped-Bike Improvements - North Segment Pedestrian & bicycle facilities along SR19 connecting HJ Carroll Park/Rick Tollefson Trail to finished improvements at Chimacum Schools Campus. Total secure grants = \$109,907 TAP	28	S/P	E	1.0	PE R/W CN Total															
7/21	TAP	86,021				13,425	99,446	78,525						20,921										99,446				
6/22			SRTS	388,947			388,947							388,947										388,947				
Total		86,021		388,947	0	13,425	488,393	78,525						409,868	0	0	0							488,393				
SR19 MP 8.94 to MP 9.56 (18020040) Rhody Drive Ped-Bike Improvements - South Segment Pedestrian & bicycle facilities along SR19 connecting the Chimacum School Campus to the Chimacum Crossroads commercial corridor. Total secured grant = \$152,093	28	S/P	E	1.0	PE R/W CN Total																							
7/21						TAP	165,223				22,226	187,449	69,858	117,591										187,449				
7/22						TAP	37,137				5,796	42,933		42,933										42,933				
6/23								SRTS	979,992			979,992		979,992				979,992						979,992				
Total							202,360		979,992	0	28,022	1,210,374	69,858	160,524	979,992	0	0	0						1,210,374				
09	16	850100 & 527109 (18019560) Rick Tollefson Trail - Ballfield Connection Old Hadlock Road and Chimacum Road intersection realignment with multi-use trail connecting the Rick Tollefson Trail to Bob Bates Fields. Total = \$208,269 (with \$140,000 from PIF)	28	P	E	0.30	PE R/W CN Total																					
								1/19						17,605	17,605		17,605									17,605		
								6/24						0												0		
								Total		0		140,000	32,965	172,965		172,965				172,965						172,965		
										0	0	140,000	50,570	190,570	0	17,605	172,965	0	17,605	0	0						190,570	
07	17	93150 Center Road MP 14.60 Little Quilcene River Bridge Replacement 1955, 64-ft. concrete bridge with structural deficiencies is eligible for replacement through federal bridge program. Total = \$2,949,981	11	P	E	1.0	PE R/W CN Total																					
								1/22	BRR	316,000					316,000		316,000									316,000		
								1/22	BRR	50,000				50,000		50,000										50,000		
								6/23	BRR	2,583,981				2,583,981		2,583,981				2,583,981						2,583,981		
								Total		2,949,981		0	0	0	2,949,981	0	366,000	0	366,000	0	2,583,981	0				2,949,981		
07	18	93150 MP 10.34 to MP 14.58 Center Road 2R Resurface 4.24 miles of Center Road north from the Little Quilcene River. Total grants = \$1,700,700	05	P	E	4.24	PE R/W CN Total																					
								1/23			RAP	53,800		6,086	59,886		59,886									59,886		
								6/24			RAP	1,646,900		183,086	1,829,986		1,829,986				1,829,986					1,829,986		
								Total		0		1,700,700	0	189,172	1,889,872	0	0	59,886	1,829,986	0						1,889,872		
																											0	
00	19	850200 S. Discovery and 4 Corners Roads (18019890) Olympic Discovery Trail (ODT) Link Multi-use path connection from Milo Curry Trailhead of the ODT-Larry Scott Trail to the Jefferson Transit Main Base/Park n Ride. Total = \$535,258	28	P	E	0.62	PE R/W CN Total																					
								7/21			PBP	126,813		10,000	136,813	26,013	50,400	60,400								136,813		
												33,904			33,904		33,904									33,904		
								6/22			PBP	354,541			354,541		354,541				354,541					354,541		
								Total		0		515,258	0	10,000	525,258	26,013	50,400	94,304	354,541	0						525,258		
08	20	60150 MP 4.52 to MP 4.84 South Discovery Road 3R Resurface County Road, widen shoulders, some re-alignment, remove clear zone hazards from Adelma Beach Road to SR20 intersection. Total grant = \$768,600	04, 28	P	E	.32	PE R/W CN Total																					
								4/23			RAP	118,800		8,200	127,000							127,000				127,000		
								9/24			RAP	27,000		3,000	30,000							30,000				30,000		
								6/26			RAP	622,800		69,200	692,000							692,000				692,000		
								Total		0		768,600	0	80,400	849,000	0	0	0	0	157,000						849,000		
00	21	US 101 & multiple county roads MP 294.54 to MP 295.03 Quilcene Complete Streets - Phase 2 Phase 2 project implementing plan to provide sidewalks, crosswalks, & bicycle lanes to connect school, community center & local businesses. Total = \$1,341,750	28	P	E	0.05	PE R/W CN Total																					
								7/25			PBP	181,750			181,750					30,000	151,750					181,750		
												0														0		
								1/26			PBP	1,160,000			1,160,000							1,160,000				1,160,000		
								Total		0		1,341,750	0	0	1,341,750	0	0	0	30,000	151,750						1,341,750		
00	22	850200 Anderson Lake State Park to HWY-101 Olympic Discovery Trail (ODT) - Eaglemount Complete a preferred route report, begin PS&E development, permitting and right-of-way appraisal and acquisition. Total = \$406,491 (Planning and Right-of-Way only)	15, 16, 28	S	E	8.0	PE R/W CN Total																					
								1/20			RCO-RRG	14,000			14,000											14,000		
								1/20			RCO-RRG	60,000			60,000											60,000		
								N/A						0											0			
								Total		0		74,000	0	0	74,000	74,000	0	0	0	0	0					74,000		

**SIX-YEAR
TRANSPORTATION IMPROVEMENT PROGRAM
2021 - 2026**

FUNC. CLASS	TIP PROJECT NO.	PROJECT IDENTIFICATION A. Road Log Number - Bridge Number B. Beginning and End Milepost C. Project (No.) / Road Name D. Description of Work E. Total Cost (incl. prior/future)	IMPROVEMENT TYPE	FUND STATUS	ENVIRONMENTAL ASSESS	TOTAL LENGTH (mi.)	PROJECT COSTS IN DOLLARS										EXPENDITURE SCHEDULE								
							PROJECT PHASE	MONTH / YEAR FUND STARTS	FUNDING SOURCE INFORMATION						OTHER REVENUES	LOCAL FUNDS	TOTAL	YEAR 1 2021	YEAR 2 2022	YEAR 3 2023	YEAR 4 2024	YEAR 5 2025	YEAR 6 2026	TOTAL 2021-2026	
									FEDERAL FUNDS		STATE FUNDS														
									FEDERAL FUND TYPE	FEDERAL COST BY PHASE	STATE FUND TYPE	STATE COST BY PHASE													
00	23	850200 Discovery Bay Village to SR-20/HWY-101 Junction Olympic Discovery Trail (ODT) Discovery Bay - Segment B ODT segment from existing South Discovery Bay ODT terminus (over Salmon & Snow Creeks) to SR 20/US 101 intersection. Total = \$2,520,000	28	P	E	.33	PE																	0	
							R/W	7/23			RCO-RRG	300,000							50,000	250,000				300,000	
							CN	4/24			RCO-RRG	20,000								20,000				20,000	
								6/25			RCO-RRG	2,200,000									2,200,000			2,200,000	
							Total			0		2,520,000		0	0	0	50,000	270,000		2,200,000			2,520,000		
00	24	Countywide Safety Improvements Systemic safety countermeasures applied to county network identified in a local plan and funded thru a biennial WSDOT federal grant program. Total = \$375,000	21	P	E	N/A	PE	1/22	HSIP	56,250					18,750	75,000		25,000		25,000			25,000	75,000	
							R/W									0								0	
							CN	6/23	HSIP	240,000					60,000	300,000			150,000		150,000			300,000	
							Total			296,250		0		78,750	375,000	0	25,000	150,000	25,000	150,000	25,000			375,000	
07	25	91420 MP 9.7 to MP 9.8 (18019880) Upper Hoh Road Spruce Creek Mitigation Environmental mitigation project in response to 2015 emergency repairs. Total = \$161,297	11	P	E	0.1	PE	11/18	ER	9,012					1,407	10,419	10,419							10,419	
							R/W									0								0	
							CN	6/22	ER	105,963					16,537	122,500		122,500						122,500	
							Total			114,975		0	0	17,944	132,919	10,419	122,500	0	0	0	0	0		132,919	
00	26	Countywide Bridge Heavy Maintenance Program Protection of primary bridge components, seismic retrofits, etc. through a biennial WSDOT administered federal grant program. Total = \$350,000	48	P	E	N/A	PE	1/22	BRR	40,000					10,000	50,000			25,000		25,000			50,000	
							R/W									0								0	
							CN	6/23	BRR	240,000					60,000	300,000				150,000		150,000		300,000	
							Total			280,000		0		70,000	350,000	0	0	25,000	150,000	25,000	150,000	150,000		350,000	
00	27	Countywide Various Road Improvements Various projects countywide involving lighting, signage, intersection control, culvert replacement/rehabilitation, drainage, surfacing upgrades, pedestrian and bicycle facilities, or other improvements using local funds. Total = \$255,000		P	E	N/A	PE								30,000	30,000	5,000	5,000	5,000	5,000	5,000	5,000	5,000	30,000	
							R/W								0	0								0	
							CN								225,000	225,000	25,000	100,000	25,000	25,000	25,000	25,000		225,000	
							Total			0		0	0	255,000	255,000	30,000	105,000	30,000	30,000	30,000	30,000	30,000		255,000	
26,525,362											7,091,149		17,984,087	267,416	1,182,710	26,525,362	2,479,126	4,732,693	5,347,388	5,874,992	3,834,163	4,257,000		26,525,362	
2,292,979										PE		877,506		1,129,113	0	286,360	2,292,979	578,708	740,410	256,486	128,625	558,750	30,000	2,292,979	
496,337										R/W		98,387		374,279	5,000	18,671	496,337	319,500	92,933	33,904	0	50,000	0	496,337	
23,736,046										CN		6,115,256		16,480,695	262,416	877,679	23,736,046	1,580,918	3,899,350	5,056,998	5,746,367	3,225,413	4,227,000		23,736,046
26,525,362										Total		7,091,149		17,984,087	267,416	1,182,710	26,525,362	2,479,126	4,732,693	5,347,388	5,874,992	3,834,163	4,257,000		26,525,362

Guide to Codes, Abbreviations and Acronyms

The following is a guide to codes used in the TIP project spreadsheet for Jefferson County recognized by state and federal agencies.

FEDERAL FUNCTIONAL CLASSIFICATION (FFC)

03	Rural Other Principal Arterial	09	Rural Local Access
06	Rural Minor Arterial	17	Urban Collector
07	Rural Major Collector	19	Urban Local Access
08	Rural Minor Collector	00	No Functional Classification, ex. trails

IMPROVEMENT TYPES per the Statewide Transportation Improvement Program (STIP) Categories

01	New construction roadway	18	Planning
03	Reconstruction - added capacity	19	Research
04	Reconstruction - no added capacity	20	Environmental only
05	4R Maintenance - resurfacing	21	Safety
06	4R Maintenance - restoration & rehabilitation	22	Rail/highway crossing
07	4R Maintenance - relocation	23	Transit
08	Bridge - new construction	24	Traffic Management/Engineering - HOV
10	Bridge replacement - added capacity	25	Vehicle Weight Enforcement
11	Bridge replacement - no added capacity	26	Ferry Boats
13	Bridge rehabilitation - added capacity	27	Administration
14	Bridge rehabilitation - no added capacity	28	Facilities for Pedestrians & Bicycles
15	Preliminary Engineering	40	Special Bridge
16	Right-of-Way	48	Bridge Protection
17	Construction Engineering	49	Bridge Inspection

FUNDING STATUS

S = Secured (funded); P = Planned (unfunded); S/P = Partial secure funded (most likely by phases)

PROJECT PHASE (by STIP definition)

PL = Planning not leading to construction; PE = Preliminary Engineering & Design; R/W = Right-of-Way acquisition; an
CN = Construction & Construction Engineering

Guide to Codes, Abbreviations and Acronyms

The following is a guide to abbreviations and acronyms used in the TIP project spreadsheet for Jefferson County.

FEDERAL FUND CODES (competitive funds applicable to Jefferson County)

BRR	Bridge Replacement and Rehabilitation, Program (aka "BRAC")
ER	Emergency Relief
FEMA	Federal Emergency Management Agency, Hazard Mitigation Program
FLAP	Federal Lands Access Program from WFL
HIP-R	Highway Infrastructure Program (2018 only)
HSIP	Highway Safety Improvement Program
NFPP	National Fish Passage Program
SRSII	Secure Rural School-Title II
SRTS	Surface Transportation Program-Safe Routes to Schools from Federal
STBG	Surface Transportation Block Grant Setasides
STP-R	Surface Transportation Program-Rural
TAP-R	Surface Transportation Program-Transportation Alternatives from RTPO
WAFH	Washington Forest Highways, Program

STATE FUND CODES (competitive funds applicable to Jefferson County)

ALEA	Aquatic Lands Enhancement Account
FBRB	Fish Barrier Removal Board
FEMAm	Federal Emergency Management Agency, Hazard Mitigation Program match, from State
NOVA	Non-Highway and Off-Road Vehicle Activities, Program
PBP	Pedestrian and Bicycle Program
PSAR	Puget Sound Acquisition and Restoration, Fund
RAP	Rural Arterial Program from CRAB
RRG	Recreation Resource Grant (managed by Recreation Conservation Office (RCO))
SRFB	Salmon Recovery Funding Board
SRTS	Safe Routes to School, Program from State
TIB	Transportation Improvement Board, Complete Streets Award or Urban Sidewalk
WWRP	Washington Wildlife Recreation Program

OTHER FUND CODES (applicable to Jefferson County)

CAPP	County Arterial Preservation Program
PIF	Public Infrastructure Fund
PILT	Payment in Lieu of Taxes

ENVIRONMENTAL ASSESSMENT

Applicable to CRAB

E	EXEMPT
I	INSIGNIFICANT
S	SIGNIFICANT

Applicable to Federal Funding

Categorically Exempt CE or Documented Categorically Exempt DCE
Environmental Assessment EA
Environmental Impact Statement EIS

PAST ACCOMPLISHMENT

Quilcene Center Road 2R & Complete Street

Resurface and Restore Center Road

MP14.61 to MP15.01

2020-2025 Adopted TIP

TIP Priority Array #4

Remove and replace layers of deteriorating asphalt pavement & replace sidewalks curb and ADA ramps.

Total Funding \$788,000



Contractor:
Nordland Construction NW Inc.

Substantial Completion:
August, 2020



PAST ACCOMPLISHMENT

South Discovery Road, MP 3.18 – 4.58

Pavement Preservation

2019-2024 Adopted TIP

TIP Priority Array #3

Resurface and restore a 1.39 mile section of asphalt road to preserve the roadway structure and improve ride quality and safety. Minor shoulder widening, paving and edge finishing. Contractor: Lakeside Industries, Inc.

Substantial Completion:	July, 2019
Total Funding	\$549,631
State (RAP)	\$450,000
Local	\$99,631



PAST ACCOMPLISHMENT

OLYMPIC DISCOVERY TRAIL

South Discovery Bay – Segment B to Larry Scott Trail

2019 – 2024 Adopted TIP

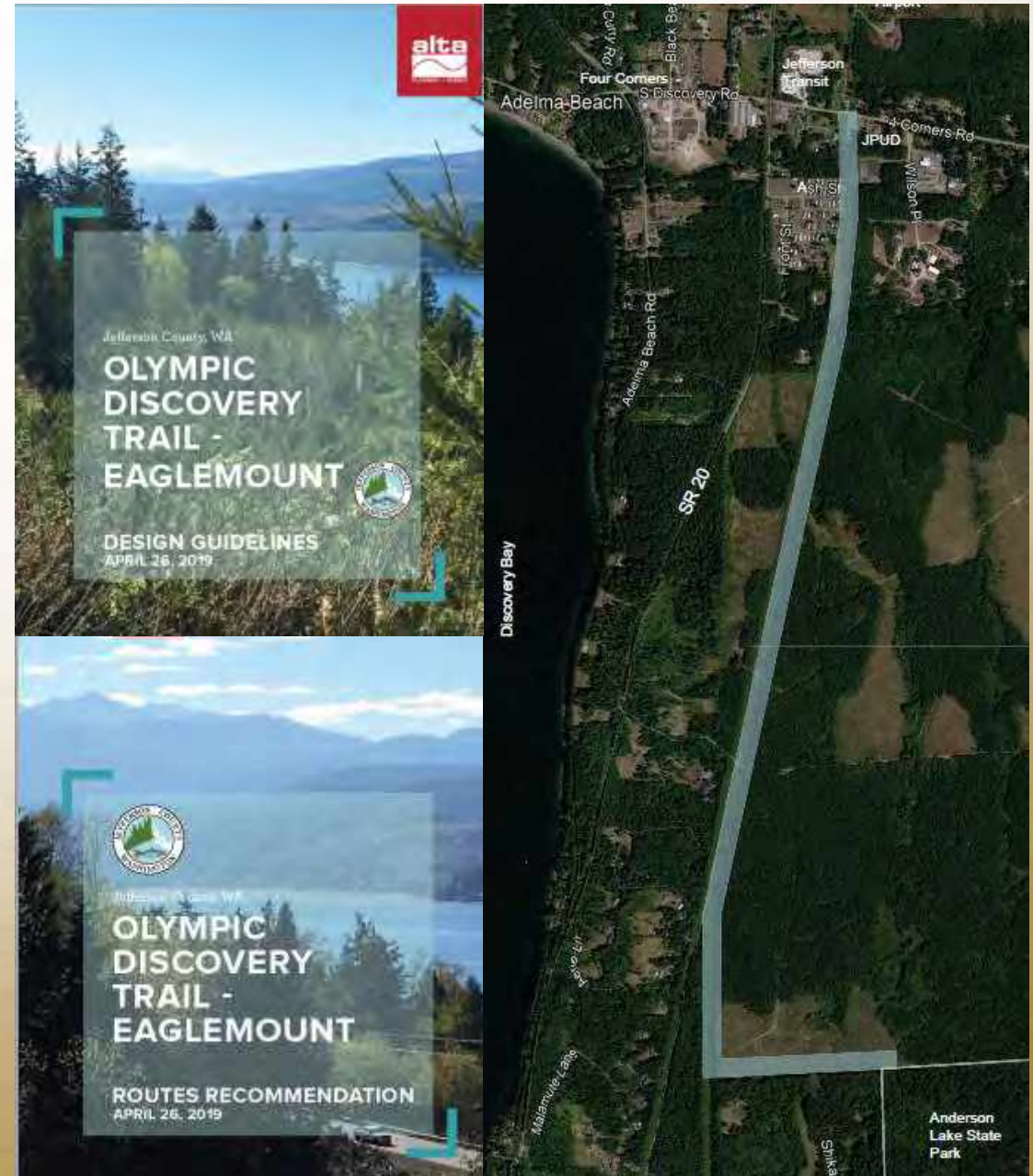
TIP Priority Array #4

Complete a preferred route report, begin PS&E development, SEPA, and right-of-way appraisal and acquisition.

Preferred route report complete, April 2019 with design guidelines.

Preferred route selected from 4 Corners Road to Anderson Lake Park, June 2019
Acquisition Phase continues, near completion

Total RCO Funds	= \$1,000,000
Remaining for ROW	= \$74,000 (2021-2026 TIP)





East Jefferson - 18 projects

TRANSPORTATION IMPROVEMENT PROGRAM 2021 – 2026 27 PROJECTS

- DESIGN (PE)
- RIGHT-OF-WAY (ROW)
- CONSTRUCTION (CN)

West Jefferson – 5 Projects

County wide – 4 Projects



TOP 7 CRITERIA FOR ESTABLISHING TIP PRIORITY

Ranked #1	Criteria 16	GRANT AVAILABILITY:
	96.7%	Transportation infrastructure is eligible for grants, has a) an application filed to grant agencies, b) indication of grant availability, or c) secured grant funding.
Ranked #2	Criteria 5	SCHOOL PEDESTRIAN ROUTE:
	90.0%	Road segment is an authorized school pedestrian route sanctioned by the school transportation coordinator and needs sufficient pedestrian facilities to the school campus.
Ranked #3a	Criteria 14	COMMUNITY PLANNING/COMPREHENSIVE LAND USE CONSISTENCY:
	83.3%	Road segment provides mobility within and accessibility to an area of potential development which is land use consistent with zoning and approved codes. Road segment is identified in an adopted Growth Management Act sub-area or comprehensive plan for correction of deficiencies or improvements.
Ranked #3b	Criteria 17	ECONOMIC DEVELOPMENT:
	83.3%	Road segment, if not improved, will have a measurable and obvious impact on economic development or other rational measures of public necessity.
Ranked #4a	Criteria 2	CRASH HISTORY:
	80.0%	Road segment has significant hazard potential, defects or collisions. The inventory documents one or more serious injuries and fatalities to pedestrian, bicyclist or driver/passenger within past five years.
Ranked #4b	Criteria 1	NON-MOTORIZED NEEDS:
	80.0%	Road or trail segment is on adopted alignment or non-motorized route map, frequented by non-motorized users, and needs sufficient active transportation facilities for the latent demand.
Ranked #4c	Criteria 6	TRANSIT AND SCHOOL BUS ROUTES:
	80.0%	Road segment supports fixed-route transit and/or school routes with numerous stops, and high volumes of users within one-mile of the school "safe-routes-to-school".

TIP PRIORITY ARRAY 2021 - 2026


Top 14 Projects with secured or partially secured funding.



Two Pending Grant Funded Projects
as of October 2020

GRANT FUNDS PENDING FROM FEMA

12	35290 MP 0.84 (18020310) Snow Creek Road Culvert Replacement
----	--



Bottom rusted out with water and debris leaking through failing culvert

TIP PROJECT NO.	PROJECT IDENTIFICATION A. Road Log Number - Bridge Number - Beginning and End Milepost B. Project / Road Name	Total Cost 2021 - 2026	Secured Funds 2021-2026
1	US 101 & connected county roads MP 294.6 to MP 295.0 Quilcene Complete Streets - Phase 1 (1801933)	\$ 757,766	\$ 705,350
2	91420 MP 0.00 to MP 12.04 (18019410) Upper Hoh Road Phase 2 - FLAP Project	\$ 10,000	\$ 10,000
3	63710 MP 0.00 to MP 0.46 (18020570) Cedar Avenue, Ness' Corner (SR116) to Hayden Road Ped-Bike	\$ 820,957	\$ 820,957
4	850200 4 Corners Road to Anderson Lake State Park Olympic Discovery Trail (ODT) - Connection	\$ 3,647,611	\$ 269,570
5	15100 MP 0.90 to MP 2.185 (18020460) Undi Road Bypass - FLAP Project	\$ 13,336	\$ 13,336
6	91420 MP 0.00 to MP 12.04 (18020560) Upper Hoh Road Pavement Preservation	\$ 408,400	\$ 408,400
7	41870 MP 4.71 (18020580) Thorndyke Road Culvert Replacement - Thorndyke Creek	\$ 1,958,776	\$ 1,738,410
9	Countywide Bridge Load Re-Rating	\$ 128,700	\$ 128,700
10	51410 MP 2.59 (18020591) West Valley Road Culvert Replacement - Naylor's Creek	\$ 819,534	\$ 2,550
11	13430 MP 8.50 to MP 10.98 Oil City Road Engineering Assessment - FLAP Project	\$ 30,000	\$ 30,000
13	51750 MP 1.82 (18020591) Gibbs Lake Road Culvert Replacement - Naylor's Creek	\$ 713,038	\$ 8,625
14	SR19 MP 9.56 to MP 10.1 (18020040) Rhody Drive Ped-Bike Improvements - North Segment	\$ 488,393	\$ 99,446
15	SR19 MP 8.94 to MP 9.56 (18020040) Rhody Drive Ped-Bike Improvements - South Segment	\$ 1,210,374	\$ 230,382
22	850200 Anderson Lake State Park to HWY-101 Olympic Discovery Trail (ODT) - Eagle Mount	\$ 74,000	\$ 74,000

TOTAL SECURED

4,539,726

BY PROJECT CATEGORY

EXAMPLE PROJECTS WITH SECURED AND ANTICIPATED FUNDING IN 2021

Road & Intersection Improvements

Culvert & Bridge Replacement /Repair/Heavy Maintenance

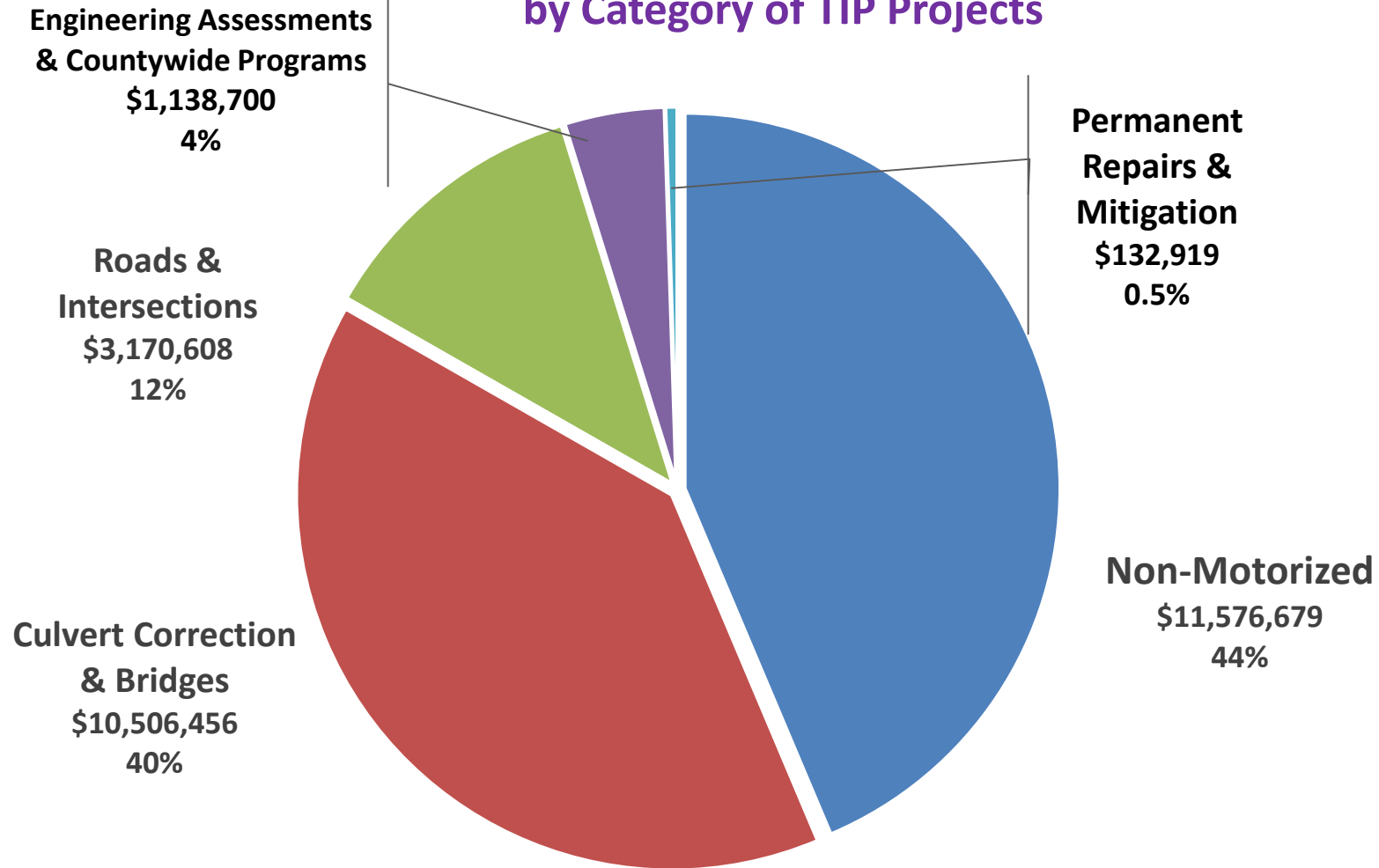
Non-Motorized Transportation

Assessments & Countywide Programs

Mitigation for Emergency Projects

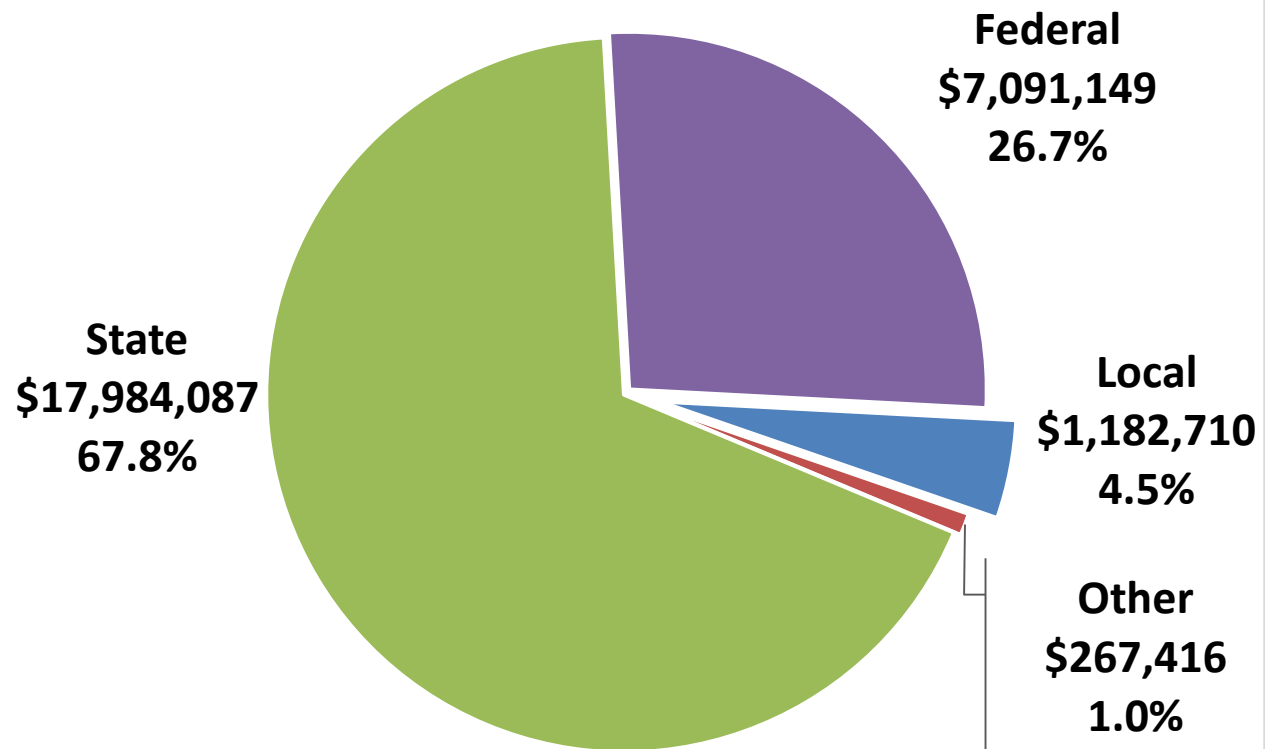


2021-2026* Projected Expenditures by Category of TIP Projects



Total TIP* Project Funding (27 projects) = \$26,525,362

2021-2026* Projected Expenditures by Type of Funding



Total TIP* Project Funding (27 projects) = \$26,525,362

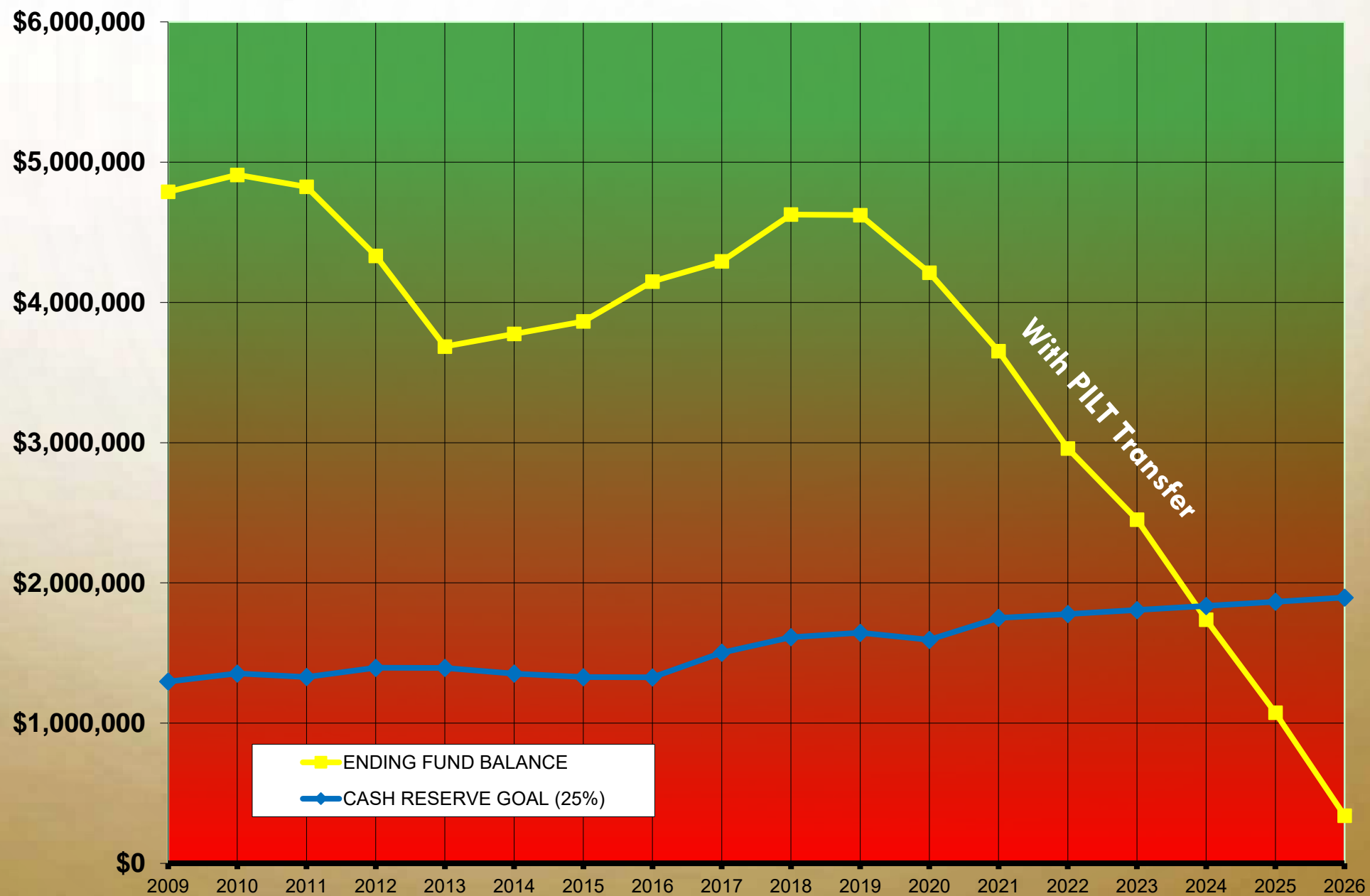
REVENUE ASSUMPTIONS

- 2020 Projected Loss of \$300K MVFT
- In 2021, MVFT Restored to pre-COVID Levels
- Every Year \$450K PILT to Backfill SRS
- Grant Funds Aim for 90% Project Total
- 100% Federal managed bridge program
- Local Funds are \$197K per year to Match Grants
- PIF is excellent Last \$\$ for needed Match
- Road Diversion Capped at \$720K per year
- Maintain a 25% Operating Reserve until 2024

2021-2026 County Road Fund Projections

			ACTUAL	PROJ	BUDGET	TOTAL
	DESCRIPTION	Incr.	2019	2020	2021	2021-2026
Operating Revenues	PROPERTY TAXES (+ NEW CN)	0.5%	4,548,564	4,634,222	4,628,139	28,118,267
	PROPERTY TAXES: 1% (I-747)	1.0%				235,612
	BANKED CAPACITY (Taken)	0.5%	0	0	0	0
	ROADS LEVY SHIFT		0	0	0	0
	DIVERSION (Traffic Enforcement)	1.0%	(720,000)	(720,000)	(720,000)	(4,320,000)
	TIMBER TAX	0.0%	219,200	136,717	200,000	1,200,000
	FEDERAL FOREST (SRS)	0.0%	403,540	363,190	85,000	510,000
	PILT TRANSFER (partial offset)	3.0%	86,172	131,344	425,211	2,750,439
	DNR TIMBER SALES	0.0%	212,927	140,026	125,000	750,000
	FUEL TAX	0.5%	1,478,291	1,253,328	1,582,000	9,611,444
	CAPA, TPA, MVA FUNDS	0.0%	254,688	244,943	267,000	1,602,000
	PERMITS & FEES	1.0%	203,765	184,754	153,000	941,258
	OTHER MISC. REVENUE	0.0%	38,059	29,108	3,500	21,000
	REIMBUSIBLE SERVICES		12,141	11,745	2,930	2,940
	TOTAL OPERATING REVENUES:		6,737,347	6,409,377	6,751,780	41,422,960

2021-2026 Six-Year County Road Fund Projections



FIRST YEAR OF THE TIP IS THE DRAFT ANNUAL CONSTRUCTION PROGRAM

about blank

Jefferson County
2021
Annual Construction Program
WAC 138-16

(A) TOTAL CONSTRUCTION DONE (total sum of column 13 + column 14): 1,779,928.00
(B) COMPLETED COUNTY FORCES LIMIT: 1,262,448.55
(C) TOTAL COUNTY FORCES CONSTRUCTION (total sum of column 14): 517,479.45

Date of Environmental Assessment
Date of Final Adoption
Performance/Reimbursement Numbers

(1)	(2)	(3)	(4)	(5)	(6)	(7)	(8)	(9)	(10)	(11)	(12)	(13)	(14)	(15)				
Annual Program Item No.	5-Year Road Program Item No.	Project Name	Amount	Road Segment Information Road Name	BRP	EAR	FTE	Project Description	Project Type Code	Performance Measurement	County Rank Number	Source of Funds Account	Other Funds Program Number	GRS-CE (95-00)	Repl of Mile (99-00)	Yr. of Construction	County Funds	Grand Total (All 15)
1	1	Uptown Loop on State - Street 1	10502	Road Name: HWY 101 From Clark Road To Abington Street	2046	2048	20	0.00	BR	E	100000	662,450.00	000000	100000	000000	2021	20	1727,560
2	2	Upper E.L. Road from 2nd L&N Truss	91272	Road Name: Upper E.L. Rd From HWY 101 To GHD Boundary	905	1204	17	1.00	BR	S	85000	4000	000000	000000	000000	2021	20	35000
3	3	SR 16 & 16th Avenue Pavement Reconstruction	427,600	Road Name: SR 16 From SR 16 To Highway Road	1000	500	20	0.00	P&T	E	100000	871,890.00	000000	000000	000000	2021	20	1,293,490
4	4	Cheney Damaging Road Construction - Damaging to ALSP	0					0.00	P&T	E	0	000000	000000	000000	000000	2021	20	0
5	5	Upper E.L. Road from 2nd L&N Truss	15000	Road Name: Upper E.L. Rd From HWY 101 To GHD Boundary	905	1204	17	1.00	BR	E	25000	4000	000000	000000	000000	2021	20	35000
6	6	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
7	7	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
8	8	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
9	9	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
10	10	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
11	11	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
12	12	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
13	13	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
14	14	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
15	15	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
16	16	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
17	17	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
18	18	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
19	19	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
20	20	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
21	21	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
22	22	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
23	23	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
24	24	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
25	25	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
26	26	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
27	27	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
28	28	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
29	29	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
30	30	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
31	31	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
32	32	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
33	33	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
34	34	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
35	35	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
36	36	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
37	37	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
38	38	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
39	39	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
40	40	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
41	41	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
42	42	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
43	43	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
44	44	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
45	45	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
46	46	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
47	47	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
48	48	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
49	49	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
50	50	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
51	51	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
52	52	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
53	53	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
54	54	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
55	55	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
56	56	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
57	57	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
58	58	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
59	59	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
60	60	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
61	61	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
62	62	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
63	63	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
64	64	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
65	65	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
66	66	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
67	67	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
68	68	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
69	69	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
70	70	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
71	71	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
72	72	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
73	73	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
74	74	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
75	75	Upper E.L. Road from 2nd L&N Truss	0					0.00	BR	E	0	000000	000000	000000	000000	2021	20	0
76	76	Upper E.L. Road from 2nd L&N Tr																

UPDATED BRIDGE CONDITION REPORT 2020

Jefferson County Master Inventory

Total # of Bridges = 35 West County = 20

East County = 15

Total # of Permanently Closed Bridges = 2

Total # of Active Bridges = 35 incl. pedestrian

Recommended Bridge Maintenance Activity at:

- Hurst Creek – Clearwater Road
Steel Diaphragm Painting
- Contractor's Creek – Old Gardiner Road
Pile Bracing and Stiffening
- Cassel Creek – Oil City Road
Steel Girders Painting

Recommended Bridge Replacement at::

- Little Quilcene River – Center Road
TIP #17



Tower Creek Bridge Scour

Upper Hoh Road

TIP Project #2

Est. Completion 2022

FLAP dependent

2013 FHWA Mandate

Specialized Hauling Vehicle (SHV) class will require new load ratings on 22 active bridges and potential new postings by Dec. 31, 2022

UPPER HOH ROAD – a FLAP Project

TIP Project #2

Regionally Significant

Est. Completion 2022

Total Project Budget = \$14,029,621 Uses Toll Credits

World famous Hoh Rainforest reached via 12 miles

traveling this county road only way in & out

Important to tourism economy for Greater Olympic Peninsula

Nearly 300,000 visitors annually; 85% of road traffic or
315,000 trips per year (2018)

New bank washout every 2 to 3 years

Emergency expenditures average \$400,000/year

Route Jurisdiction Transfer to WSDOT considered

MOU with County, WSDOT, FHWA, NPS

Federal Land Access Program (FLAP) investments

\$13 million allocated for bridges and bank protection, ex.

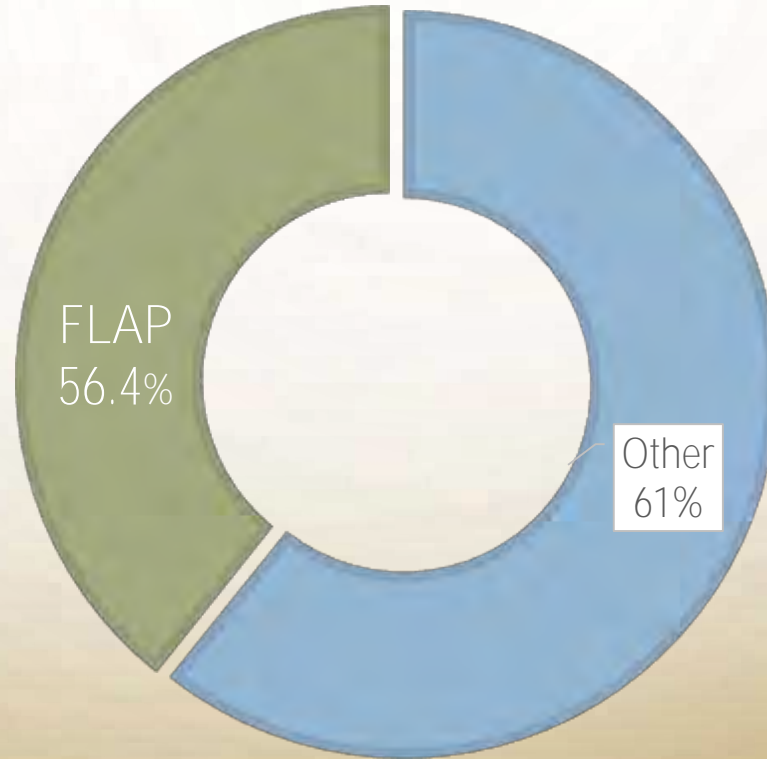
Tower and Canyon Creeks

\$2 million allocated for pavement/guardrail and safety
improvements

**First dolosse(s) permitted for bank protection on Olympic
Peninsula**



**TOTAL FUNDING 27 TIP PROJECTS
SECURE FLAP VS. NON-FLAP (OTHER)
FEDERAL LANDS ACCESS PROGRAM, ADMINISTERED BY FHWA**

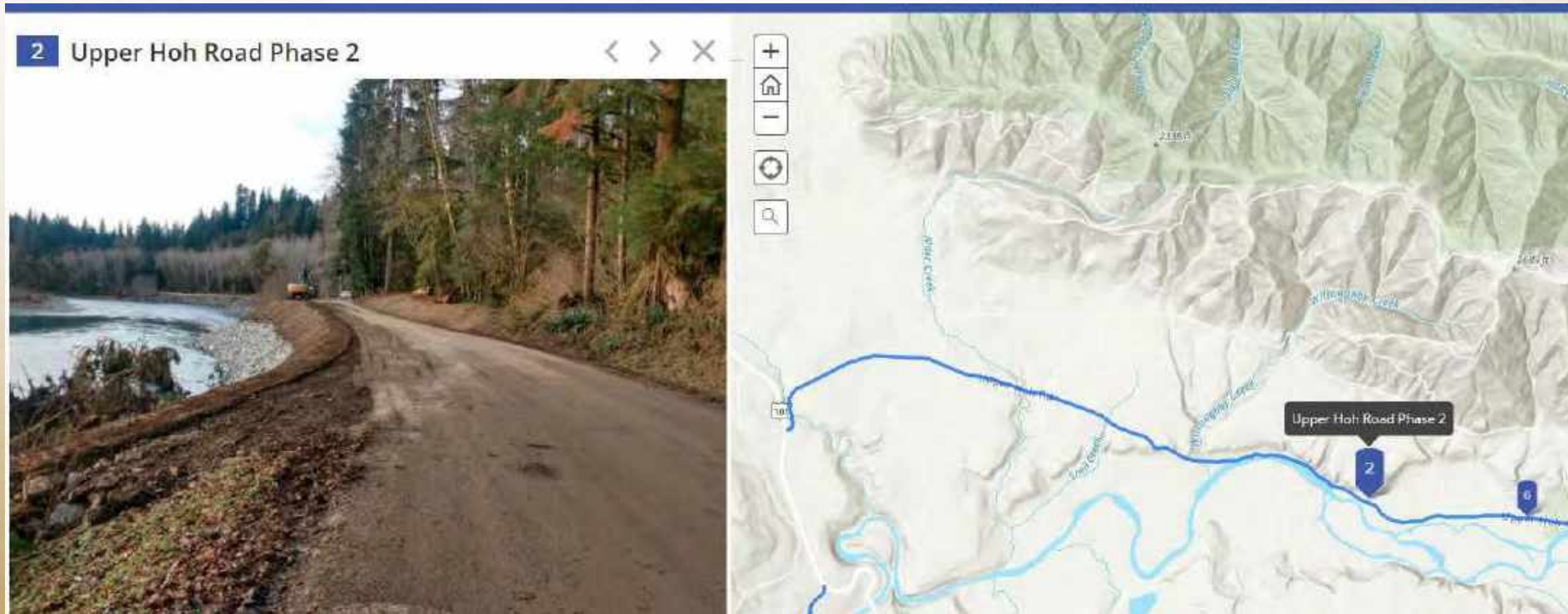


Total Project Funding, 27 projects = \$45,453,144

Interactive STORYMAPPING all 27 TIP projects with site specific pegs.

Go to

<https://arcg.is/1fO5n8>



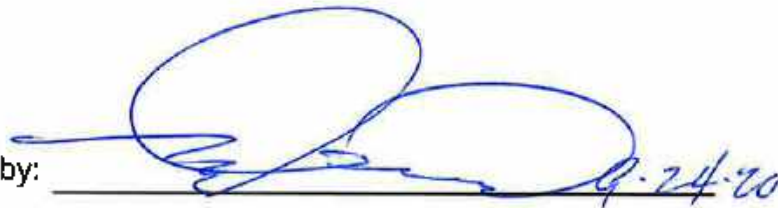
Bridge Condition Report 2020

Jefferson County, Washington



2020 Bridge Condition Report

Approved by:



Monte Reinders, P.E.
Public Works Director/County Engineer

Prepared by:



Mark Thurston, P.E.
Lead Bridge Inspector/Engineer III

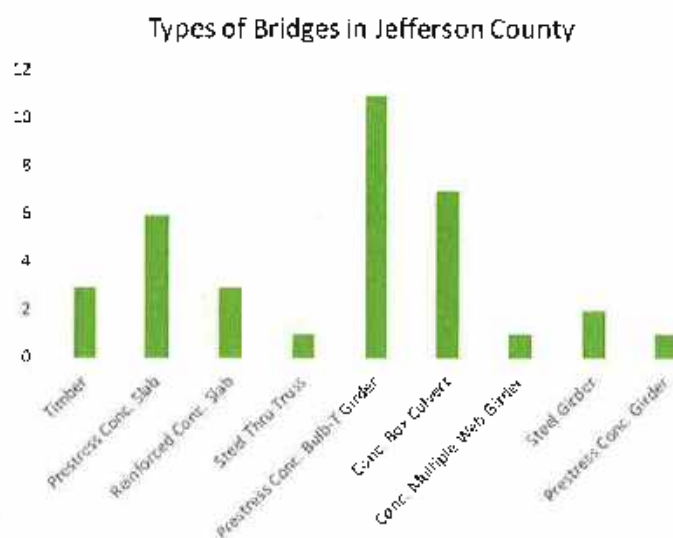


As required by WAC 136-20-060, "Each county engineer shall furnish the county legislative authority with a written report of the findings of the bridge inspection effort. This report shall be made available to said authority and shall be consulted during the preparation of the proposed six-year transportation program revision. The report shall include the county engineer's recommendations as to replacement, repair or load restriction for each deficient bridge. The resolution of adoption of the six-year transportation program shall include assurances to the effect that the county engineer's report with respect to deficient bridges was available to said authority during the preparation of the program. It is highly recommended that deficient short span bridges, drainage structures, and large culverts be included in said report."

INTRODUCTION

Jefferson County Public Works has 35 active bridges—see the attached Master Bridge List (Appendix A) for a listing of the county bridges. Of these, 33 bridges meet the federal definition of a bridge and are subject to the National Bridge Inspections Standards. Two of the bridges do not meet the federal bridge definition, one because it is a pedestrian bridge, and one because it has less than a twenty foot span; nevertheless, these two bridges are on Jefferson County's biannual bridge inspection program. Jefferson County also has two permanently closed

BRIDGE INVENTORY

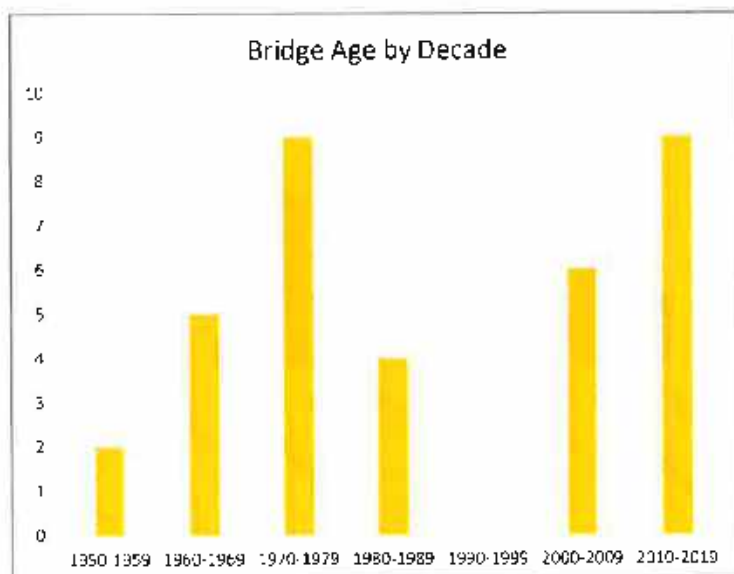


bridges, which are not listed on the Master Bridge List.

BRIDGE INSPECTIONS

The National Bridge Inspection Standards (NBIS) and WAC 136-20-030 mandate that public agencies inspect and report on all bridges at least once every two years (routine inspection). Jefferson County staff performs all routine inspections. Special inspections are required for bridges that cannot be inspected adequately from the ground. For these bridges an Under-Bridge Inspection Truck (UBIT) is required. Steel bridges with fracture critical members may also require special inspections with the UBIT and/or other special equipment. Jefferson County has two bridges which require the UBIT inspection and one bridge which requires both the UBIT and the fracture critical inspection (identified in the attached Master Bridge List). Jefferson County currently has a contract with the Washington State Department of Transportation (WSDOT) to perform these inspections.

Jefferson County Public Works has split up the bridge inspections so that East Jefferson County bridges are inspected in odd-numbered years and West Jefferson County bridges are inspected in even-numbered years. In conformance with that schedule, Jefferson County completed the biennial inspections for all West Jefferson County bridges in 2020. The



completed inspection reports were submitted to WSDOT, which verifies compliance with the NBIS and reports to the Federal Highway Administration (FHWA).

All bridges are assigned a sufficiency rating (SR), which is a calculated score based on numbers assigned to all of the bridge elements reviewed by the bridge inspector. The SR is a number from 0 to 100, with 100 being an entirely sufficient bridge, and 0 being an entirely insufficient bridge. For additional explanation of the sufficiency rating and other ratings see the attached WSDOT publication, Bridges and Structures – Ratings (Appendix B). The sufficiency rating is used to determine if a bridge is eligible for federal rehabilitation or replacement funding. Under the current funding requirements, to be eligible for replacement a bridge must be structurally deficient and have a sufficiency rating of less than 40; to be eligible for rehabilitation a bridge must be structurally deficient and have a sufficiency rating of less than 80. Currently, the county has one bridge that is classified as structurally deficient and has a sufficiency rating less than 40: Little Quilcene Bridge on Center Road. Public Works plans to apply for bridge replacement funding as described under Programmed Projects.

As described in previous Bridge Condition reports, the Tower Creek Bridge is being monitored for potential scour problems

DEFICIENT BRIDGES



Tower Creek Bridge

associated with down-cutting of the streambed. The Tower Creek streambed experienced significant down-cutting in 2007-2008 due to migration of the Hoh River and heavy flows in Tower Creek. However, based on regular monitoring, the streambed elevation has been relatively stable since 2008. Public Works will continue to monitor this situation. The Tower Creek Bridge is scheduled to be replaced in 2021-2022 by the Federal Highway Administration Western Federal Lands Division, and is being funded entirely by a Federal Lands Access Program grant.

POSTED BRIDGES

All bridges are required to have a "Load Rating" calculation. The Load Rating establishes how much weight the bridge can carry for several standard configurations of vehicle axle loads. If the load rating calculation shows that a bridge cannot safely carry one or more of the legal vehicle axle load configurations, then the bridge must be posted with the appropriate load limits. Jefferson County currently has two bridges that are load posted, Meyers Bridge on Belfrage Road and Hemphill Creek Bridge on Dowans

Creek Road. In addition, the load rating calculations for the Yarr Bridge indicate that it should be posted for a number of legal vehicles. However, the bridge has been carrying normal traffic for an appreciable length of time and shows no distress. In accordance with the Manual for Bridge Evaluation (published by



Yarr Bridge

the American Association of State Highway and Transportation Officials), and as recommended by the WSDOT Local Programs Bridge Engineer, Jefferson County is following a plan of increased inspection frequency as an acceptable alternative to load posting. Public Works inspects the Yarr Bridge on a 12 month frequency,

as opposed to the standard 24 month frequency, and intends to continue with this plan, provided that the bridge continues to show no signs of distress.



Underside of Hurst Creek Bridge

Similarly, the load rating calculations for the Little Quilcene Bridge on Center Road indicate that it should be posted for a number of legal loads, but this bridge has also been carrying normal traffic for an appreciable length of time and shows no distress. Based on the Manual for Bridge evaluation, the load rating document states that load posting is optional and should be at the County's discretion. The County has exercised this option and not posted the bridge. However, as of September 2020, FHWA has disagreed with the County's discretionary authority and has informed the County that the bridge must be posted. The County is complying with this request and intends to post the bridge.

A 2013 FHWA mandate requires that all bridges be load rated for a relatively new class of vehicle, the Specialized Hauling Vehicle (SHV). SHV's are closely-spaced multi-axle single unit trucks

introduced by the trucking industry in the last decade. Examples include dump trucks, construction vehicles, solid waste trucks and other hauling trucks. The FHWA has divided bridges into two categories, with the first category requiring updated load ratings by December 31, 2017, and the second category requiring updated load ratings by December 31, 2022. Prior to the 2013 mandate, Jefferson County already had eight bridges with load ratings that met the new FHWA requirements. Jefferson County has completed new load ratings for three bridges for the 2017 deadline. An additional twenty-two bridges will need to be re-load rated prior to the 2022 deadline. It is possible that some Jefferson County bridges will need to be load posted for SHV's after the load rating results are known.

**OVERWEIGHT/
OVERSIZE VEHICLE
PERMITTING PROCESS**

Last year Public Works developed a new form and process for evaluating and issuing overweight/oversize vehicle permits. A policy was written to help ensure that consistent procedures are used in processing permits, and a new, more detailed application form was developed. The new application form, along with associated webpage updates, was deployed last fall and has been used successfully to issue permits.

OBSOLETE BRIDGES

The Old Little Quilcene Bridge and the Maple Creek Bridge have been placed in obsolete status and removed from the Master Bridge List. Both are permanently closed to vehicular traffic. This action removes the requirement for biennial bridge inspection.

**MAINTENANCE
ACTIVITIES**

Routine maintenance of the county's bridges is conducted by the road maintenance department. Typical routine maintenance consists of deck cleaning, brush clearing, and minor miscellaneous repairs.

In April 2019, the County applied for \$90,830 in Bridge Replacement Advisory Committee (BRAC) funds for a preventative maintenance project on the Hurst Creek Bridge, Clearwater Road milepost 2.66. The Hurst Creek Bridge has concrete girders with steel diaphragms between the girders at mid-span. The steel diaphragms are in need of repainting or replacement. In September 2019 we learned the project would not be funded through BRAC, so an alternative funding source will need to be found.

No emergency repairs or inspections were performed in 2020 at the time of this report.

EMERGENCY REPAIRS AND INSPECTIONS

No bridge projects were completed in 2020.

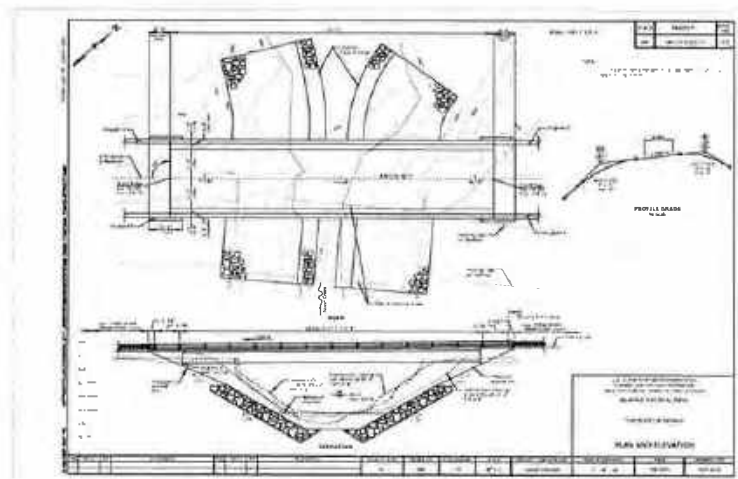
COMPLETED PROJECTS

Current projects include the following:

CURRENT PROJECTS

Upper Hoh Road/Olympic National Park Federal Lands Access Program (FLAP) Project:

Design is complete on a new 120 ft. long concrete bridge to replace the existing bridge at Tower Creek, Upper Hoh Road milepost 7.5. This project is being managed by the Federal Highway Administration Western Federal Lands Division, and is funded almost entirely by a FLAP



Preliminary Tower Creek Bridge drawing



Culvert at Thorndyke Creek

grant and toll credits. Construction is scheduled for 2021-2022.

Design is complete on a new 120 ft. long concrete bridge to replace the culvert at Canyon Creek, Upper Hoh Road milepost 10.0. This project is being managed by the Federal Highway Administration Western Federal Lands Division, and

is funded almost entirely by a Federal Lands Access Program grant and toll credits. Construction is scheduled for 2021-2022. However, federal funding for this project is uncertain and may delay the project.

The fish-barrier culverts at Thorndyke Road milepost 4.71 (Thorndyke Creek) will be replaced with a new bridge. Public Works has secured grant funding from the Fish Barrier Removal Board (FBRB) for project design and has completed the preliminary design process. Public Works applied for construction funds from the FBRB in 2020 and plans to construct the project in 2021-2022.

The fish-barrier culverts at West Valley Road milepost 2.59 (Naylor Creek) and Gibbs Lake Road milepost 1.82 (Naylor Creek) will be replaced with new structures most likely meeting bridge criteria. Public Works has secured grant funding from the FBRB for project design, and has completed the preliminary design process. Public Works applied for construction funds from the FBRB in 2020 and plans to construct the project in 2022-2023.

In addition to Current Projects, the following potential bridge projects are programmed in the 2020-2025 Transportation Improvement Program (TIP):

PROGRAMMED PROJECTS

The culvert at Center Road milepost 3.26 (Chimacum Creek) will be replaced with a fish-passable structure, which will most likely meet the criteria for a bridge, given the stream width.

Re-load rating of 22 county bridges, as described previously in this document.

The Little Quilcene River Bridge on Center Road has been reclassified by the Washington State Department of Transportation as Structurally Deficient and the Sufficiency Rating was lowered from 60 to 22, based on a new load rating performed by the County in 2017. The load rating was required by the federal mandate to load rate bridges for specialized hauling vehicles, and the bridge was found to be deficient for some of these vehicles. Given the revised Sufficiency Rating and the Structurally Deficient classification, the bridge is now eligible for federal bridge replacement funds. Jefferson County intends to apply for bridge replacement funds in the next funding round, which will occur in early 2021.

Two or more heavy maintenance projects are anticipated during the period 2020-2025, and are identified as Countywide Bridge Improvements in the TIP. With 35 bridges in its inventory, and half of them around 50 years old, the county has to plan and budget for ongoing maintenance, in addition to capital projects. Potential maintenance projects include pile bracing/stiffening at Contractor's Creek Bridge, and repainting of the steel girders at Cassel Creek Bridge.

RECOMMENDED PROJECTS

Hurst Creek Bridge steel diaphragm repainting/replacement, as described under maintenance activities.

Appendix A

Jefferson County Public Works

Master Bridge List

LAST UPDATED: 9/4/19

Total number of bridges in inventory:	35
Total number of permanently closed bridges:	2
Total number of active bridges:	35
Total number of West Jefferson County bridges:	19 active bridges 1 short-span (non NBIS) bridge 1 permanently closed bridge
Total number of East Jefferson County Bridges	14 active bridges 1 pedestrian (non NBIS) bridge 1 permanently closed bridge

Jefferson County Public Works
Master Bridge List
East Jefferson County

Last Updated: 9/2/2020

FEDERAL STRUCTURE NUMBER	COUNTY NUMBER	NAME	ROAD NUMBER	ROAD NAME	MILEPOST	LENGTH (FEET)	BRIDGE TYPE		YEAR BUILT	FRACTURE CRITICAL	SCOUR CRITICAL	LOAD POSTED	DATE OF LAST INSPECTION	SUFFICIENCY RATING	FUNCTIONALLY OBSOLETE (FO)	STRUCTURALLY DEFICIENT (SD)	INSPECTION FREQUENCY (MONTHS)	REMARKS
							Superstructure	Substructure										
7996700	15E	Hunts	634509	Hunt Rd.	0.15	27	Reinforced Conc. Slab	Conc. Abutment w/ C.I.P. conc. piles	1971				4/16/19	89.68			24	
8165100	16E	Little Quilecene River	931507	Cancer Rd.	14.6	64	Reinforced Conc. Slab, Conc. Girder	Conc. Pile cap, Prestress. Conc. piles	1955				4/16/19	28.21	SD		24	Eligible for bridge replacement funds.
8464900	17E	Yarr	931507	Center Rd.	3.44	25	Conc. Multiple Web Girder	Conc. Abutment w/ Prestress. Conc. piles	1955				4/9/20	64.97			12	
8320300	23E	Linger Longer	301309	Linger Longer Rd.	0.42	81	Prestress. Conc. Bulb-T Girder	Conc. Abutment w/ Conc. column over C.I.P. conc. piles	1969				4/16/19	63.91			24	
8381400	24E	Rocky Brook	250008	Dosewallips Rd.	3.01	47	Prestress. Conc. Bulb-T Girder	Conc. Abutment w/ Conc. Spread footing	1964				4/16/19	75.92			24	
8229900	25E	Meyers	503409	Belfrage Rd.	0.12	30	Timber	Timber abutment, timber piles	1977			X	6/26/19	74.85			24	
8488200	26E	West Uncas	500209	West Uncas Rd.	1.89	61	Reinf. Conc. Slab	Conc. Abutment, Conc. column over C.I.P. conc. pile	1964				4/16/19	83.26			24	
8136900	28E	Leland Creek	344009	Rice Lake Rd.	0.33	32	Prestress. Conc. Slab	Conc. Abutment, Conc. spread footing	1987				4/16/19	88.10	FO		24	
8660000	29E	Contractors Creek	501409	Old Gardiner Rd.	3.56	294	Timber Deck, Timber Girders	Conc. Pile cap, steel piles	2000				8/4/20	87.42			24	UBIT EVERY 4 YEARS – LAST UBIT 8/4/2020

Jefferson County Public Works
Master Bridge List
East Jefferson County

Last Updated: 9/2/2020

FEDERAL STRUCTURE NUMBER	COUNTY NUMBER	NAME	ROAD NUMBER	ROAD NAME	MILEPOST	LENGTH (FEET)	BRIDGE TYPE		YEAR BUILT	FRACTURE CRITICAL	SCOUR CRITICAL	LOAD POSTED	LAS INSPECTION	SUFFICIENCY RATING	FUNCTIONALLY OBSOLETE (FO) STRUCTURALLY DEFICIENT (SD)	INSPECTION FREQUENCY (MONTHS)	REMARKS
							Superstructure	Substructure									
8754000	31E	Shine Creek	447608	Smith Point Rd.	0.25	70	Prestress. Conc. Slab	Conc. Abutment w/ Conc. Pile	2005				4/16/19	97.60		24	
8753900	32E	Donovan Creek	327508	E. Quilcene Rd.	0.77	70	Prestress. Conc. Slab	Conc. Abutment w/ Conc. Pile	2006				4/16/19	87.36		24	
8753800	33E	Old Tarboo	357509	Old Tarboo Rd.	0.88	42	Prestress. Conc. Slab	Conc. Abutment w/ steel piles	2005				4/16/19	89.98		24	
8753800	38E	Andrews Creek	352909	Snow Creek Rd.	3.78	24	Conc. 3-sided Box Culvert	Conc. Spread Footing	2014				4/16/19	91.14		24	
8898600	41E	RTMT Pedestrian	352909	Rick Tollefson Mem. Trail	0.17	80	Steel Thru Truss	Conc. Spread Footing	2017				4/30/19	N/A		24	
8902400	42E	Wally Bowman	500209	West Uncas Rd.	0.81	80	Prestress Conc. Bulb-T Girder	Conc. Abutment w/ Conc. Pile	2018				4/16/19	98.95		24	

Jefferson County Public Works
Master Bridge List
West Jefferson County

Last Updated: 9/2/2020

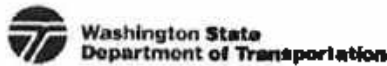
FEDERAL STRUCTURE NUMBER	COUNTY NUMBER	NAME	ROAD NUMBER	ROAD NAME	MILEPOST	LENGTH (FEET)	BRIDGE TYPE		YEAR BUILT	FRACTURE CRITICAL	SCOUR CRITICAL	LOAD POSTED	LAST INSPECTION	SUFFICIENCY RATING	STRUCTURALLY DEFICIENT (SD) FUNCTIONALLY OBSOLETE (FO)	INSPECTION FREQUENCY (MONTHS)	REMARKS
							Superstructure	Substructure									
8451600	01W	Fletcher Creek	911607	Quinault S. Shore Rd.	3.19	78	Prestress. Conc. Bulb-T Girder	Conc. Abutment w/ C.I.P. conc. pile	1974				5/20/20	99.62		24	
8258500	02W	Barlow	134309	Oil City Rd.	9.75	64	Prestress. Conc. Slab	Conc. Abutment w/ C.I.P. Conc. Pile	2005				5/20/20	83.43		24	
8145800	03W	Anderson Creek	134309	Oil City Rd.	4.44	87	Prestress. Conc. Bulb-T Girder	Conc. Abutment w/ C.I.P. conc. pile	1974				5/20/20	91.91		24	
8280100	04W	Hell Roaring Creek	914207	Upper Hoh Rd.	0.16	120	Prestress. Conc. Girder	Conc. Abutment w/ steel H pile	1982				5/21/20	76.65		24	
8298800	05W	Alder Creek	914207	Upper Hoh Rd.	2.07	67	Prestress. Conc. Bulb-T Girder	Conc. Abutment w/ C.I.P. conc. pile	1972				5/21/20	90.21		24	
8383600	06W	Rock Creek	914207	Upper Hoh Rd.	6.49	83	Prestress. Conc. Bulb-T Girder	Conc. Abutment w/ C.I.P. conc. pile	1973				5/21/20	77.41		24	
8384000	07W	Tower Creek	914207	Upper Hoh Rd.	7.5	71	Prestress. Conc. Bulb-T Girder	Conc. Abutment w/ C.I.P. conc. pile	1973				5/21/20	78.81		24	
8391000	08W	Queets River	107509	Clearwater Rd.	0.6	844	Steel Plate Girder, C.I.P. conc. deck	Conc. Piers, Conc. Abutment w/ steel H pile	1988				7/11/18	98.61		24	UBIT EVERY 4 YEARS – LAST UBIT 7/11/2018
8047800	09W	Hemphill Creek	146809	Dowans Creek Rd.	2.3	31	Timber Deck, Timber Girders	Timber abutment, timber piles	1977				5/20/20	76.55		24	
7966600	10W	Dowans Creek	146809	Dowans Creek Rd.	0.52	81	Prestress. Conc. Bulb-T Girder	Conc. Abutment w/ C.I.P. conc. pile	1974				5/20/20	94.63		24	

Jefferson County Public Works
Master Bridge List
West Jefferson County

Last Updated: 9/2/2020

FEDERAL STRUCTURE NUMBER	COUNTY NUMBER	NAME	ROAD NUMBER	ROAD NAME	MILEPOST	LENGTH (FEET)	BRIDGE TYPE		YEAR BUILT	FRACTURE CRITICAL	SCOUR CRITICAL	LOAD POSTED	LAST INSPECTION	SUFFICIENCY RATING	STRUCTURALLY DEFICIENT (SD) FUNCTIONALLY OBSOLETE (FO)	INSPECTION FREQUENCY (MONTHS)	REMARKS
							Superstructure	Substructure									
8475700	11W	Owl Creek	135109	Maple Creek Rd	1.29	69	Prestress Conc Slab	Conc. Abutment w/ steel H piles	1989				5/20/20	83.39	FO	24	
8253600	13W	Hurst Creek	107509	Clearwater Rd.	2.66	67	Prestress Conc. Bulb-T Girder	Conc. Abutment, Conc. column, Conc. footing	1969				5/20/20	98.82		24	Maintenance needed - paint/replace steel diaphragms
8339400	27W	Willoughby Creek	914207	Upper Hoh Rd.	3.52	60	Prestress Conc Bulb-T Girder	Conc. Pile cap w/ Prestress Conc. piles	1962				5/21/20	45.63		24	
8659900	30W	Cassel Creek	134309	Oil City Rd.	3.59	154	Steel Girder	Conc. Pile cap, steel piles	2000	X			7/9/18	80.32	FO	24	Fracture Critical Special Inspection - WSDOT performs inspection with LBCT biennially. Upcoming maintenance - paint steel girders
8829800	34W	Pole Creek	914207	Upper Hoh Rd.	8.8	37	Conc. 3-sided Box Culvert	Conc. Spread Footing	2010				5/21/20	80.20		24	
8845600	35W	Dismal Creek	914207	Upper Hoh Rd.	9.19	22	Conc. 3-sided Box Culvert	Conc. Spread Footing	2011				5/21/20	73.19		24	
8853500	36W	Spruce Creek	914207	Upper Hoh Rd.	9.7	22	Conc. 3-sided Box Culvert	Conc. Spread Footing	2012				5/21/20	77.41		24	
8863300	37W	Alder Creek Tributary	914207	Upper Hoh Rd.	2.15	25	Conc. 3-sided Box Culvert	Conc. Spread Footing	2013				5/21/20	76.65		24	
8886600	39W	Upper Hoh Rd. MP 6.95	914207	Upper Hoh Rd.	6.95	21	Conc. 3-sided Box Culvert	Conc. Spread Footing	2016				5/21/20	82.69		24	
XG030700	41E	Donkey Creek	107509	Clearwater Rd.	1.29	17	Conc. 3-sided Box Culvert	Open-ended steel pipe piling	2015				5/20/20			24	

Appendix B



Bridge Ratings

The safety of bridge structures in Washington State is ensured through a meticulous inspection system. All public bridge owners, such as WSDOT, Counties and Cities, follow the same bridge inspection procedures. The condition rating of all bridge decks, superstructures and substructures and other elements based on these inspections.

USDOT's Federal Highway Administration (FHWA) requires all public bridge owners (state, city, and county) to inspect and their bridges and report information including bridge condition ratings as part of their requirements in the National Bridge Inventory Standards (NBIS).

Important aspects of the NBIS were:

1. All states must perform periodic inspections of bridges greater than 20 feet in span on at least a biennial basis.



SR6 Willapa River bridge



SR20 Deception Pass Bridge

2. Data collection was standardized and must be reported to FHWA.
3. Qualifications for inspection personnel were defined.
4. Training programs were developed and implemented.
5. The Bridge Replacement Program (BRP) was established to provide funding for bridge replacement on the system.

Structurally Deficient

Structurally deficient means that a bridge requires repair or replacement of a certain component. This may include cracked or spalled concrete, the bridge deck, the support structure, or the entire bridge itself. If the condition is such that it no longer is able to carry its intended traffic loads it may be weight restricted. Being structurally deficient does not imply that the bridge is in danger of collapse or unsafe to the traveling public. If a bridge is open then it is considered safe.

A bridge is classified as "Structurally Deficient" when bridge inspectors give either the superstructure, deck, and/or substructure a rating of four or less on a scale of zero to nine. WSDOT's poor condition category uses the same data, criteria, and rating scale.

WSDOT has 137 state owned bridges that are classified as structurally deficient as of Jan. 2015. A list of these bridges is available in pdf and web page and map format.

Functionally Obsolete

Functional obsolescence is assessed by comparing the existing design of each bridge to current standards.

A bridge can be categorized functionally obsolete a number of ways including: substandard bridge widths, low vertical clearance that can lead to repeated damage from over height trucks, load-carrying capacity, or flood potential.

There are 866 WSDOT bridges that are rated "Functionally Obsolete"



Interstate B2 Columbia River bridge near Umatilla

Good, Fair, Poor Condition Rating

Good: A range from no problems to some minor deterioration of structural elements.

Fair: All primary structural elements are sound but may have deficiencies such as minor section loss, deterioration, cracking, spalling, or scour.

Poor: Advanced deficiencies such as section loss, deterioration, cracking, spalling, scour, or seriously affected primary structural components. Bridges rated in poor condition may be posted with truck weight restrictions.

A summary of the WSDOT bridge network conditions is available in the 2014 Bridge Annual Report in the Gray notebook.

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Bridge inspection Definitions

What are "general condition ratings?" According to the National Bridge Inspection Standards (NBIS), condition ratings are used to describe an existing bridge or culvert compared with its condition if it were new. The ratings are based on the materials, physical condition of the deck (riding surface), the superstructure (supports immediately beneath the driving surface) and the substructures (foundation and supporting posts and piers). General condition ratings range from 0 (failed condition) to 9 (excellent). For detailed definitions, click [here](#).

Which bridges are included in the NBI system? NBI structures are bridges or culverts that carry vehicular traffic and have an opening longer than 20 feet measured along the center of the roadway.

What bridges are not considered part of the NBI system? Non-NBI structures include bridges or culverts that carry vehicular traffic and are equal to or less than 20 feet measured along the center of the roadway.

VDOT exceeds the NBI standards by inspecting and documenting in our inventory all bridges regardless of their length and all culverts having an opening greater than 36 square feet.

What is a "structurally deficient" bridge? Bridges are considered structurally deficient if they have been restricted to light vehicles, closed to traffic or require rehabilitation. Structurally deficient means there are elements of the bridge that need to be monitored and/or repaired. The fact that a bridge is "structurally deficient" does not imply that it is likely to collapse or that it is unsafe. It means the bridge must be monitored, inspected and maintained.

How is "structural deficiency" determined? The condition of different parts of a bridge is rated on a scale of 0 to 9 (with 9 being "excellent" and zero being "failed"). A structurally deficient bridge is one for which the deck (riding surface), the superstructure (supports immediately beneath the driving surface) or the substructure (foundation and supporting posts and piers) are rated in condition 4 or less.

What makes a bridge structurally deficient, and are structural deficient bridges unsafe? The fact that a bridge is "structurally deficient" does not imply that it is likely to collapse or that it is unsafe. A "deficient" bridge is one with some maintenance concerns that do not pose a safety risk. A "deficient" bridge typically requires maintenance and repair and eventual rehabilitation or replacement to address deficiencies. To remain open to traffic, structurally deficient bridges are often posted with reduced weight limits that restrict the gross weight of vehicles using the bridges. If unsafe conditions are identified during a physical inspection, the structure must be closed.

What is a "functionally obsolete" bridge? A functionally obsolete bridge is one that was built to standards that are not used today. These bridges are not automatically rated as structurally deficient, nor are they inherently unsafe. Functionally obsolete bridges are those that do not have adequate lane widths, shoulder widths, or vertical clearances to serve current traffic demand, or those that may be occasionally flooded.

A functionally obsolete bridge is similar to an older house. A house built in 1950 might be perfectly acceptable to live in, but it does not meet all of today's building codes. Yet, when it comes time to consider upgrading that house or making improvements, the owner must look at ways to bring the structure up to current standards.

What is a "fracture-critical" bridge? A fracture-critical bridge is one that does not contain redundant supporting elements. This means that if those key supports fail, the bridge would be in danger of collapse. This does not mean the bridge is inherently unsafe, only that there is a lack of redundancy in its design.

What is a bridge's "sufficiency rating?" Sufficiency ratings were developed by the Federal Highway Administration to serve as a prioritization tool to allocate funds. The rating varies from 0 percent (poor) to 100 percent (very good). The formula considers structural adequacy, whether the bridge is functionally obsolete and level of service provided to the public.

History of Federal Bridge Inspection Program The federal bridge inspection program regulations were developed as a result of the Federal-Aid Highway Act of 1968 following the collapse of the Silver Bridge in Point Pleasant, West Virginia. The United States Secretary of Transportation established the National Bridge Inspection Standards (NBIS) to locate and evaluate existing bridge deficiencies to ensure the safety of the traveling public.

The 1968 Federal-Aid Highway Act directed the states to maintain an inventory of federal-aid highway system bridges. This was amended over time to establish criteria for NBIS bridges including:

- Defining the NBIS to bridges to those on the federal-aid highway system
- Requiring inspections of bridges longer than 20 feet on all public roads
- Expanding bridge inspection programs to include special inspection procedures for fracture-critical members and underwater inspection



Bridge Condition Code key

<u>Code</u>	<u>Description</u>
N	NOT APPLICABLE
9	EXCELLENT CONDITION
8	VERY GOOD CONDITION No problems noted.
7	GOOD CONDITION Some minor problems.
6	SATISFACTORY CONDITION Structural elements show some minor deterioration.
5	FAIR CONDITION All primary structural elements are sound but may have some minor section loss (due to corrosion), cracking, spalling (deterioration of concrete surface) or scour (erosion of soil)
4	POOR CONDITION Advanced section loss, deterioration, spalling or scour.
3	SERIOUS CONDITION Loss of section, deterioration, spalling or scour have seriously affected primary structural components. Local failures are possible. Fatigue cracks in steel or shear cracks in concrete may be present.
2	CRITICAL CONDITION Advanced deterioration of primary structural elements. Fatigue cracks in steel or shear cracks in concrete may be present or scour may have removed substructure support. Unless closely monitored it may be necessary to close the bridge until corrective action is taken.
1	"IMMINENT" FAILURE CONDITION Major deterioration or section loss present in critical structural components or obvious vertical or horizontal movement affecting structure stability. Bridge is closed to traffic but corrective action may put back in light service.
0	FAILED CONDITION Out of service - beyond corrective action.

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County Road Administration Board (CRAB)			Secondary	Toni	Cox	Engineering Technician
County Road Administration Board (CRAB)			Secondary	Randy	Hart	Grant Program Manager
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Washington State Department of Transportation	HQ	Local Programs	Main	Huntley	Nancy	Project Prioritization Engineer
Washington State Department of Transportation	Olympic Region		Main	John	Wynands	Regional Administrator
Washington State Department of Transportation	Olympic Region	Traffic Operations	Secondary	Sarah	Ott	Traffic Operations Engineer
Optional Others						
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Jefferson Conservation District			Main	Al	Cairns	Director
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Quilcene School District			Main	Gena	Lont	Director of Business & Finance
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City of Port Townsend	Public Works Department		Secondary	Dave	Peterson	City Engineer
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FHWA	Western Federal Lands		Main	Andrew	Rasmussen	WA FLAP Program Manager
Olympic National Park			Main	Lisa	Turecek	Chief of Maintenance
Peninsula RTPO	Lead Planning Agency	Kitsap Transit	Main	Ed	Coviello	Land Use Planner
Transportation Improvement Board (TIB)			Main	Chris	Langhoff	Project Engineer
Jefferson Transit			Main	Jayme	Brook	Grants & Procurement Coordinator
Port of Port Townsend			Main	Eric	Toews	Deputy Director
Clallam County	Department of Public Works		Main	Steve	Gray	Transportation Program Manager
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Hoh Tribe			Main	Wendy	Largent	Transportation Planner
Quinalt Nation			Main	Michael	Cardwell	Landuse Planner
Quinalt Nation			Secondary	Charles	Warsinske	DCD Director