

JAMESTOWN S'KLALLAM TRIBE 2020

STRATEGIC TRANSPORTATION
SAFETY PLAN UPDATE

Prepared by:



TABLE OF CONTENTS

VISION	1
INTRODUCTION	1
PROCESS	2
Plan Development	
Safety Stakeholders	
EXISTING EFFORTS	3
MOTOR VEHICLE CRASH DATA REVIEW AND ANALYSIS	6
Crash Factors	7
Environment Factors	8
Roadway Factors	9
Driver Factors and Roadway Departure	10
Risky Driving Behaviors	
Vulnerable Road Users	18
EMPHASIS AREAS	21
Emphasis Area 1 – Vulnerable Road Users	22
Description	22
Goals	
Pedestrian and Bicycle Strategies	
Older Driver Age 65+ Strategies	26
Emphasis Area 2 – Risky Driving Behaviors and Roadway Departure	
DescriptionGoals	
Strategies	
Enforcement and Policy Strategies	
Education and Safety Culture Strategies	
Emphasis Area 3- Rear-End Crashes	
Description	33
Goal	
Strategies	
Emphasis Area 4 - Wildlife/Animal Crashes	
Description	
Goal	
Strategies	
Emphasis Area 5– US 101 and Intersections with Local Roads	
DescriptionGoal	
Strategies	
Emphasis Area 6– Blyn Corridor Transportation Improvements	
Description	
Goal	
Strategies	35
IMPLEMENTATION AND EVALUATION	37
The Jamestown S'Klallam Transportation Safety Management Committee	37
Strategy Implementation Champions	37

TABLE OF CONTENTS (CONT.)

APPENDICES

Appendix A – Jamestown S'Klallam SR 101 Road Safety Audit (Digital)

Appendix B – Crash Severity – Sequim and Blyn Map

Appendix C – WSDOT Crash Data 2014-2019 (Digital)

Appendix D – Clallam County 2021-2026 Transportation Improvement Program (TIP) (Digital)

Appendix E – Jefferson County 2021-2026 Transportation Improvement Program (TIP) (Digital)

"Preserving Our Future Generations"

Improve Roadway Safety and Move Toward Zero Deaths and Zero Serious Injuries

INTRODUCTION

For thousands of years, the S'Klallam Tribes (meaning "strong people") lived and prospered in a community based on strength, pride and survival on the lands now known as the Olympic Peninsula in Northwest Washington State. Their culture was rich in art, song, spirituality, traditional knowledge and social structure.

By 1855, S'Klallam Tribal leaders signed the Point No Point Treaty. By 1874, friction with settlers made it clear that the Jamestown S'Klallam community would not be able to remain in their traditional lands near Dungeness in Washington Territory. Under the leadership of Lord James Balch, a group of approximately 100 members pooled their money to purchase 210 acres of land rather than move to the reservation in Skokomish territory. They supported themselves by farming, fishing and working in local pulp mills, and received some aid from the federal government. In 1939, the Port Gamble band, located along the eastern end of S'Klallam territory accepted a reservation and became an officially recognized Tribe. The Jamestown people, not wanting to lose the land they had purchased or the independence they had worked so hard to obtain, continued to refuse to move to a reservation. In 1968, the Lower Elwha band located west of Port Angeles accepted reservation lands and was recognized by the federal government. Though "officially unrecognized", the Jamestown Tribe continued to receive services from the federal government until 1953, when those services were discontinued. Tribal leaders realized that without federal recognition, their rights to fish, hunt and gather in their usual and accustomed places were becoming increasingly limited by policy and by a growing local population. This affected their overall economic conditions and their ability to provide for the basic needs of their citizens, such as healthcare and education. In the mid 1970's, the Tribe began its effort to regain federal recognition and, after a lengthy legal struggle, was finally re-recognized on February 10, 1981. In 1988, the Jamestown S'Klallam Tribe became one of the first 20 tribes to participate in the Self-Governance Demonstration Project, showing that Tribes could manage their own finances, programs and services.

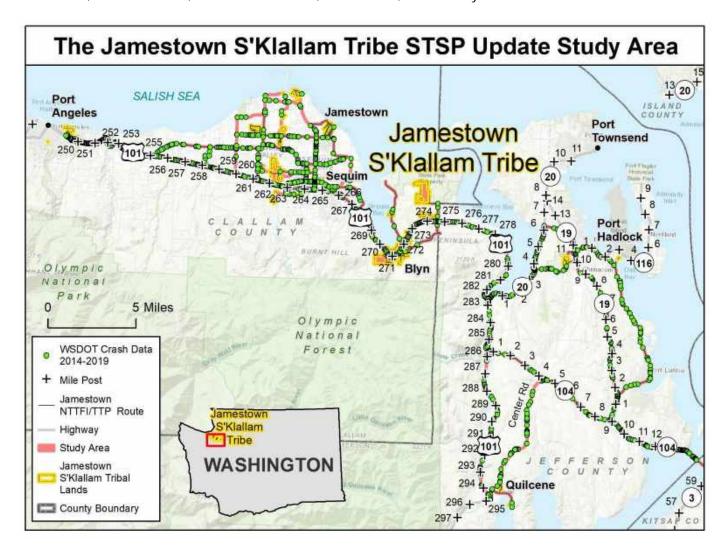
Today, the Jamestown S'Klallam Tribal campus is located in the village of Blyn, in Clallam County, WA, on a naturally beautiful landscape along the southern shores of the Sequim Bay, approximately 70 miles northwest of Seattle. In this area you will find the Tribal government administration, natural resources, social and community services departments, the Tribal Library, Jamestown Family Dental Clinic, 7 Cedars Resort and Hotel, and the Longhouse Market Deli & Fueling Station, and the Jamestown Public Safety and Justice Center. The total Tribal property in both Clallam and Jefferson counties is 1,153.5 acres. Just as it always has been, an abundance of fish, wildlife and vegetation inhabit this region. The importance and ongoing desire of preservation, conservation, and restoration by the Jamestown S'Klallam people will continue to allow this cohesion of people with their surroundings to exist for many, many more years to come.

This document represents the second Strategic Transportation Safety Plan (STSP) for The Jamestown S'Klallam Tribe and tells the story of the needs and strategies to support the health and well-being of their community. The Plan will also set the stage for future funding applications, safety initiatives, and safety campaigns. Implementation of the safety improvements identified in this Plan will work towards reducing the risk of death and serious injuries that may occur to transportation users within and near tribal lands. The Plan's development and future updates are the responsibility of The Jamestown S'Klallam Tribe Department of Administration with input from the Tribe's Safety Partners.

One of the goals of the Jamestown S'Klallam STSP is to raise awareness about transportation safety by providing crash data that supports the prioritization of needed transportation safety improvements. The focus is on the specific transportation network affecting safe ingress and egress to and from Tribal facilities and the community. The STSP will be used as a tool to apply for and justify future Tribal Transportation Plan Safety Funding and other grant funding.

PLAN DEVELOPMENT

The Jamestown S'Klallam Tribe issued a Request for Proposal for a Strategic Transportation Safety Plan Update (STSP) in May 2020. Red Plains Professional was selected for the project; the contract was signed and executed June 2020. Crash data received from Washington State Department of Transportation (WSDOT), analyzed and mapped. The STSP study area includes roadways connecting Jamestown S'Klallam Tribal Lands, Tribal Enterprises, Tribal Government and main community housing areas. The 2016 study area included main arterials: US 101 MP 250-295, SR 19 MP 0-14, SR 20 MP 0-12, SR 116 MP 0-9, SR 104 MP 0-13, Anderson Lake Road, and Oak Bay Road. The 2020 study area was updated to include all Tribal Transportation Program Inventory (TTP) and non-TTP routes connecting to main tribal properties, and main arterials: US 101 MP 250-295, SR 19 MP 0-14, SR 20 MP 0-6.3, SR 116 MP 0-2, SR 104 MP 0-13, Anderson Lake Road, Center Road, and Oak Bay Road.



All project data is viewable with the following Web Mapping Application: https://red-plains.xyz/JamestownSKlallam

SAFETY STAKEHOLDERS

The following entities contributed to this plan. The partnerships developed during this process represent good resources for the Tribe, as the plan is evaluated, implemented and updated.

Stakeholders were contacted, sent the Web Mapping Application with all crash data and a copy of the Emphasis Areas. Both the Clallam and Jefferson County Contacts were appreciative to receive the information and expressed appreciation for the continued cooperation with the Jamestown S'Klallam Tribe. They identified several areas where they might be able to collaborate on some of the suggested improvements and would be contacting Annette Nesse with the Tribe directly. See Appendix D for Clallam County 2021-2026 Transportation Improvement Program (TIP) and Appendix E for Jefferson County 2021-2026 Transportation Improvement Program (TIP).

Jamestown S'Klallam Tribe

- Tribal Leadership
- Tribal Staff:
 - o Transportation Program Manager
 - o Planner

State of Washington

• Washington State Department Of Transportation (WSDOT)

City of Sequim

• Sequim Police Department

Clallam County

- Sheriff
- Planning Department

Jefferson County

- Sheriff
- Planning Department

EXISTING EFFORTS

Jamestown S'Klallam Tribe

The Tribe is dedicated to the betterment of transportation safety. These documents address a variety of safety concerns the Tribe has with their transportation system in and around the Tribal properties, facilities and roads accessing them. The main safety concern is that US 101 divides the North and South Tribal campuses and enterprise properties and there are serious challenges that exist with access to and from US 101. The activities, programs, projects and policies listed below, to address transportation safety, were completed. Additionally, the Jamestown S'Klallam Tribal Council and Administration has adopted high standards and expectations for all development projects, requiring consideration of transportation safety. They dedicate significant effort to enhance the environment and its safety for both Tribal citizens and the traveling public.

- A Roadway Safety Audit (RSA) conducted, in April 2014, on intersections with US 101, MP 267 MP 274.6, reviewed fifteen intersections and developed countermeasures to improve the safety and reduce the crashes at each intersection. As a result, the realignment of the Chicken Coop Road/Zaccardo Road/US 101 intersection was completed in 2018, a partnership between WSDOT and the Jamestown S'Klallam Tribe (See Appendix A Jamestown S'Klallam SR 101 RSA).
- US 101, East Sequim Bay Road Access Point Intersection Justification Report September 2009
- Intersection Justification Report Chicken Coop Road and Zaccardo Road Realignment Traffic Analysis -March 2012
- Traffic Analysis Report for the Jamestown S'Klallam Tribe Long Range Transportation Plan August 2014
- 2014 Tribal Transportation Program Roadway Inventory & Long Range Transportation Plan March 2016
- Olympic Discovery Trail Development and Enhancements
- Old Blyn Highway Traffic Calming Project road and campus improvements (with pedestrian safety focus)
 completed in 2014
- The US 101 Tribal Pedestrian Underpass Project completed in 2004
- Jamestown S'Klallam Long Range Transportation Plan completed in 2016

Washington State Transportation Improvement Projects (STIP) 2020-2023

The 2020-2023 Statewide Transportation Improvement Program (STIP), approved in January 2020, as required by the Federal Transportation Act. Approximately 1,200 statewide transportation improvement projects utilizing \$3.3 billion in federal funds are in the 2020-23 STIP statewide, representing Washington's highest priority projects.

The STIP is a four-year, fiscally constrained prioritized multimodal transportation program of state, local, tribal, and public transportation (transit) projects, which includes highways, streets, roads, railroads, transit-hubs, park-and-ride lots, bridges, sidewalks, bike lanes, ferry terminals, trails and safety projects funded with federal, state, tribal and local sources. The STIP is a calendar year document, developed on an annual basis, with monthly updates from January through October. The STIP comprises local, metropolitan and regional transportation improvement programs (TIPs). Through state, metropolitan, regional, tribal and local planning processes, projects are identified. An environment of community engagement is woven into each stage of the planning process merging public dialogue forums with practical solutions to deliver transportation projects that "fit" into the communities and augment their transportation needs. Projects programmed in the STIP are the highest priority for the available funding, to preserve and improve the state's transportation network and achieving the national goals established in the Moving Ahead for Progress in the 21st Century Act (MAP-21) and continued in the Fixing America's Surface Transportation Act (FAST). Only those projects programmed in the approved STIP are allowable to utilize federal funds by the Federal Highway Administration (FHWA) and the Federal Transit Administration (FTA). https://wsdot.wa.gov/LocalPrograms/ProgramMgmt/STIP.htm

The 2020-23 STIP has 44 projects- with 4 specifically addressing transportation safety were in the Jamestown S'Klallam study area. Project 310101E, US 101 - Morse Creek Vicinity - Safety Improvements is currently in progress.

Washington State Transportation Improvement Projects (STIP) 2020-2023 Jamestown S'Klallam STSP Study Area									
Project #	Project Title	Description	Region	County	Subprogram 1	Subcategory 1	Add Date	Date Complete	
310101E	US 101/Morse Creek Vicinity - Safety Improvements	Through a statewide analysis, two locations within the project limits have been identified as Collision Analysis Locations (CAL).	Olympic	Clallam	Safety	Collision Reduction	2020-09-21	2021-07-15	
310101V	US 101/Vic Deer Park Rd to Dungeness River Br - Install Cable Barrier	Through a statewide analysis, it has been determined that placing median barrier on limited access divided highways is a cost effective method of reducing the severity of cross over collisions.	Olympic	Clallam	Safety	Collision Prevention	2019-10-28	2020-03-12	
310153E	US 101/Golf Course Rd to N of W Uncas Rd - Rumble Strips	Collision data indicates run off the road incidents occur along higher speed highways. By installing shoulder rumble strips or profiled plastic line, the frequency of run off the road incidents may be reduced.	Olympic	Clallam, Jefferson	Safety	Collision Prevention	2021-12-20	2022-09-30	
310401D	SR 104/Paradise Bay-Shine Rd - Intersection Safety Improvement	The intersection of SR 104/Shine Road is an Intersection Analysis Location (IAL) for the year 2016. 90% of the crashes at this intersection were entering at angle type crashes from Paradise Road. This project will provide intersection control.	Olympic	Jefferson	Safety	Collision Reduction	2021-10-04	2022-09-20	

Figure 1 – Washington State Transportation Projects (STIP) 2020-2023 within study area.

'US 101 between Sequim and Port Angeles is a main arterial highway that connects Olympic Peninsula travelers, freight and local businesses. At Morse Creek specifically, the existing roadway alignment has seen several crashes over the years and needs to be addressed to improve the safety for all travelers. In collaboration with local stakeholders and elected officials, this project will improve safety by constructing a raised, landscaped median, implementing a reduced speed limit to 40 mph and other proven traffic calming measures to reduce the probability of median cross-over incidents and severity of crashes along this curved highway.' https://wsdot.wa.gov/projects/us101/morse-creek/home



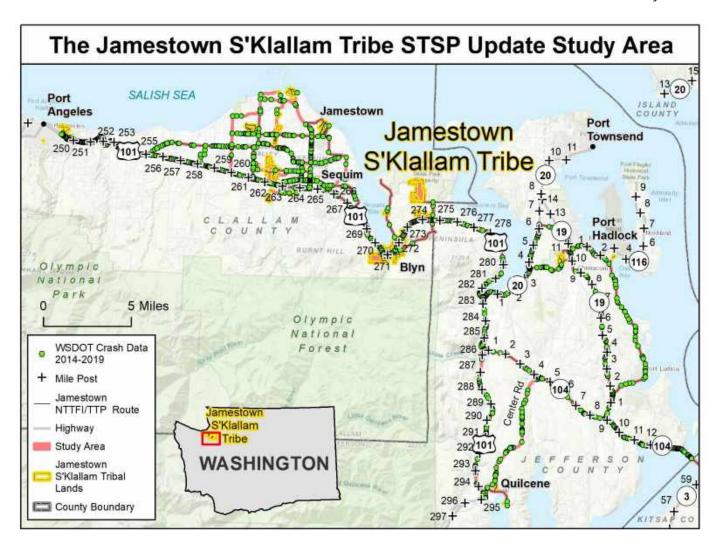
Figure 2 – Design Concept (Source: WSDOT)



Figure 3 – Project Location Map (Source WSDOT).

MOTOR VEHICLE CRASH DATA REVIEW AND ANALYSIS

The STSP update study area includes roadways connecting Jamestown S'Klallam Tribal Lands, Tribal Enterprises, Tribal Administration and main community housing areas. The study area includes all Tribal Transportation Program Inventory (TTP) and non TTP routes connecting to main tribal properties, and main arterials: US 101 MP 250-295, SR 19 MP 0-14, SR 20 MP 0-6.3, SR 116 MP 0-2, SR 104 MP 0-13, Anderson Lake Road, Center Road, and Oak Bay Road.

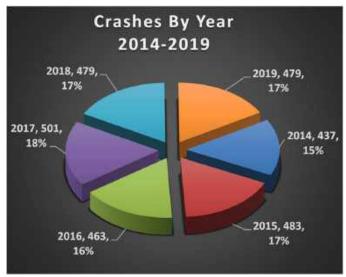


The data source for the following analysis was the WSDOT Crash Data for the timeframe January 1, 2014 to December 31, 2019. All project data is viewable with the following Web Mapping Application: https://red-plains.xyz/JamestownSKlallam.

During the 6-year period from 2014 to 2019 within the study area, a total of 2,842 reported crashes resulted in 25 fatality crashes, 85 major (suspected serious) injury, 801 minor injury crashes, and 1,885 property damage only crashes. In comparison, the 2016 STSP reported a 4-year period from 2010-2013, a total of 1,237 reported crashes resulted in 7 fatality crashes, 202 major injuries, 282 minor injuries and 730 property damage only crashes. See Crash Severity – Sequim and Blyn Map in Appendix B for distribution of crashes in the main tribal community. Traffic volumes increased in Blyn WA from 14,000 in 2013 to 17,000 in 2019, a 21% increase.

The statistics on the following page tell the story of crashes occurring within the Jamestown S'Klallam Transportation System.

- 2017 reported the highest number of all crashes with 501. 2014 reported the lowest number with only 437. Trend is showing increase in crash frequency throughout the timeframe. Same trend as in 2016 plan.
- 40% of all crashes involved 1 vehicle, less than the 47% in 2016 plan.
- 54% involved 2 vehicles, higher than the 47% in 2016 plan.
- 32% of all crashes resulted in either injury or fatality, 25 (1%) fatalities reported. Lower than the 40% in 2016 plan. Percentage of fatalities was the same, although numbers of fatalities were higher 25 vs 7.

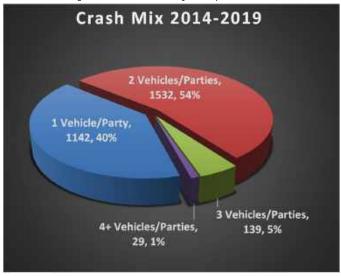


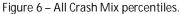
Crashes By Year 2014-2019

500
400
300
200
437
483
483
501
479
479
100
2014
2015
2016
2017
2018
2019

Figure 4 – All crashes by Year percentiles.

Figure 5 – All crashes by Year totals.





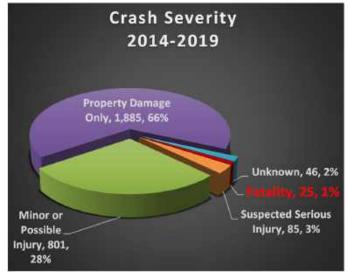


Figure 7 – All Crash Severity percentiles.

- 25% of crashes were Rear-End, the highest reported crash type in both 2014-2019 and 2010-2013 data sets.
- 12% of crashes involved Animal/Wildlife, 5% lower rate than 17% reported in 2016 plan.
- 30 crashes involved Pedestrian, and 27 involved Pedalcyclist, 2% of all crashes. There were 18 pedestrian/pedalcyclist crashes, 1% of all crashes in the 2016 plan.

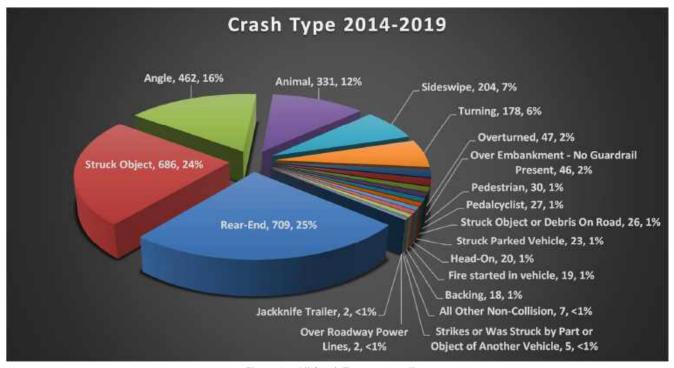


Figure 8 – All Crash Type percentiles.

ENVIRONMENT FACTORS

- 30% of all crashes occurred at night or in low light conditions. The same as in 2016 plan.
- 28% of all crashes occurred in Ice, Snow, Slush or Wet driving conditions. Higher than in 2016 plan at 25%.

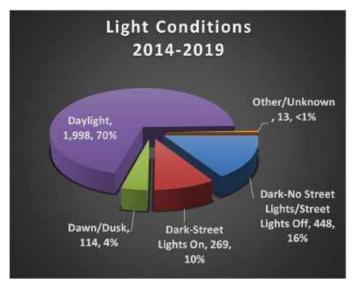


Figure 9 – All Light Conditions percentiles.

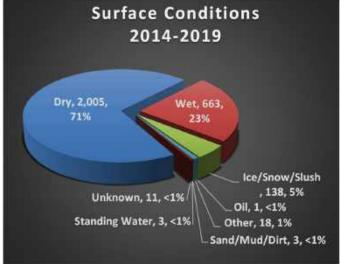


Figure 10 – All Surface Conditions percentiles.

ROADWAY FACTORS

- 48% of all crashes occurred at or were related to Intersections, Driveways or Roundabouts. Same as in 2016 plan.
- 43% of all crashes occurred on roadway geometries of Grade (Hill), Sag, Hillcrest or Curve. Not reported in 2016 plan.

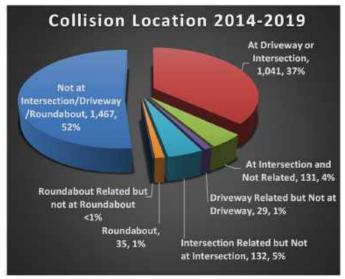


Figure 11 – All Crash Location percentiles.

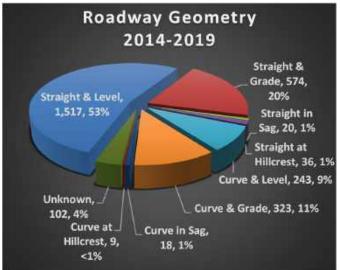


Figure 12 – All Roadway Geometry percentiles.

- 25% of all crashes involved Drivers 65 years of age or older age group at 25%. Higher than the 16% in the 2016 plan.
- 18% of all crashes involved Inattention as the primary human factor preceding crash. Higher than the 6% in the 2016 plan.
- 6% of all crashes, 8% of injury crashes and 28% of fatal crashes involved drivers Under Influence of Alcohol, Drugs or Medication. About the same as in the 2016 plan at 5%.
- 10% of all crashes and 13% of injury crashes.
 This was lower than the 19% in the 2016 plan.

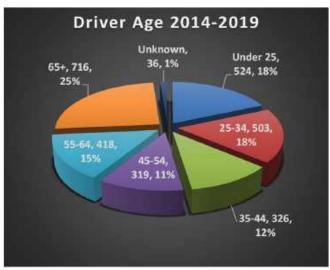


Figure 13 – All Driver Age percentiles.

- 9% of all crashes, 11% of injury crashes and 4% of fatal crashes involved Fail to Yield. Less than the 12% in 2016 plan.
- 8% of all crashes involved Follow Too Closely. 9% in the 2016 plan.
- 6% of all and injury crashes, and 4% of fatal crashes involved Distracted Driving. Not studied in 2016 plan.
- 5% of all crashes, 6% of injury crashes and 16% of all fatal crashes involved drivers Apparently Asleep, Fatigued or III. Not studied in 2016 plan
- 11% of all crashes and 25% of all fatal crashes involved Other Improper Driving, Parking Violation. Not studied in 2016 plan.

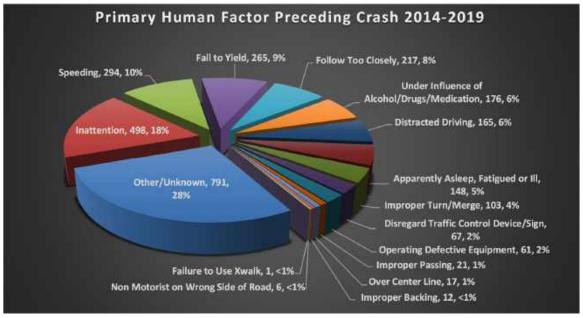


Figure 14 – All Human Factor Preceding Crash percentiles.

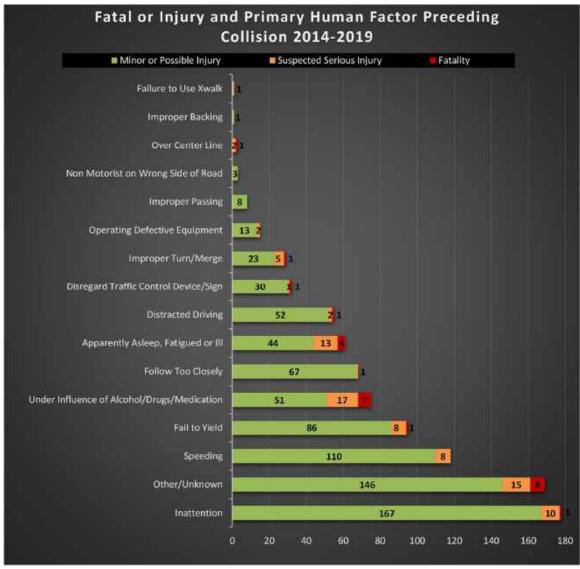


Figure 15 – All Fatal or Injury and Primary Factor Preceding Crash Totals.

The FHWA (Federal Highway Administration) defines Roadway Departure as a crash which a vehicle crosses an edge line, center line or otherwise leaves the traveled way. For this study, Roadway Departure crashes include those identified by WSDOT as Crash Types involving lane departure: Head-On, Over Embankment-No Guardrail Present, Overturned, Sideswipe, Struck Object, Struck Parked Vehicle and Primary Human Factor Preceding Crash of Over Center Line.

- 36% crashes involved Roadway Departure. 33% in 2016 plan.
- 76% of all fatal crashes and 56% of Suspected Serious injury crashes were Roadway Departure. Not studied in 2016 plan.



Figure 16 – All Crash Type percentiles.

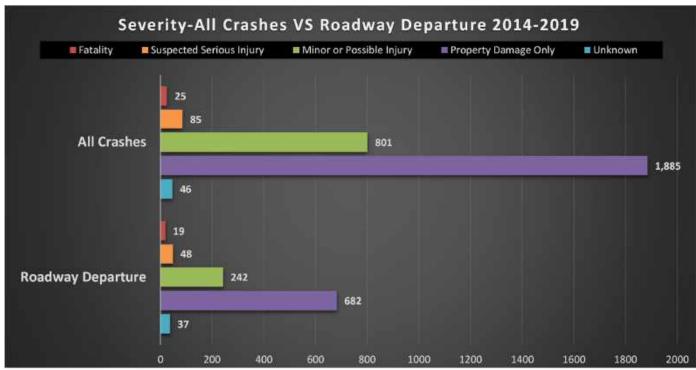
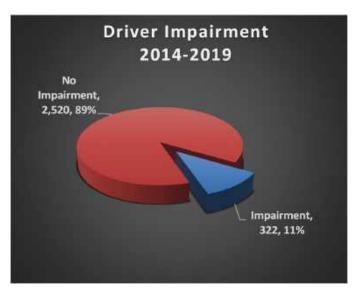


Figure 17 – Severity - All crashes vs Roadway Departure Involved totals.

- 11% of all crashes involved impaired driving. This is double the 5% in 2016 plan.
- 54% of impaired drivers were Under Influence of Alcohol, Drugs or Medication, 46% were Asleep, Fatigued or III. Not studied in 2016 plan
- 20% of impaired drivers were 65+ or 25-34 years of age. This compares to 5% for age 65+ and 22% for 25-34 on 2016 plan.
- 56% of all fatal crashes, 14% of injury crashes and 11% of all crashes involve Driver Impairment (Apparently Asleep, Fatigued, III, or Under Influence of Alcohol, Drugs or Medication). Not studied in 2016 plan.



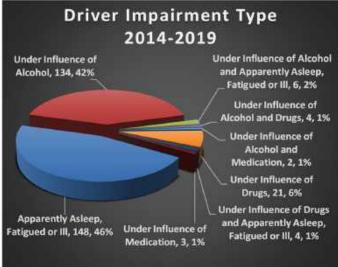


Figure 18 – All Driver Impairment percentiles.

Figure 19 – All Driver Impairment Type percentiles.

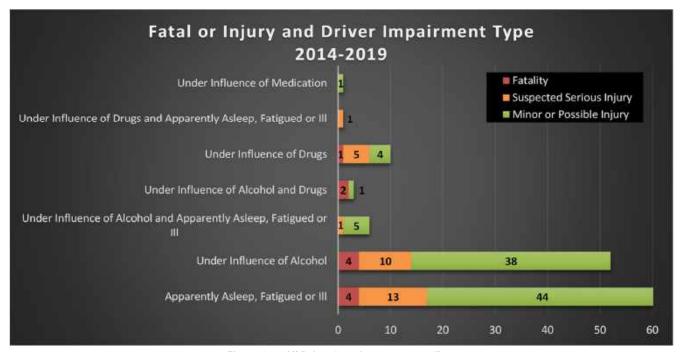
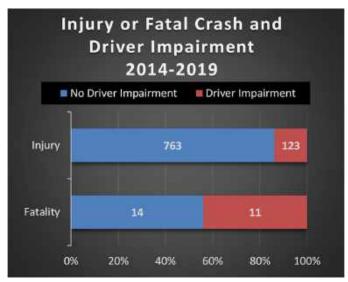


Figure 20 - All Driver Impairment percentiles.





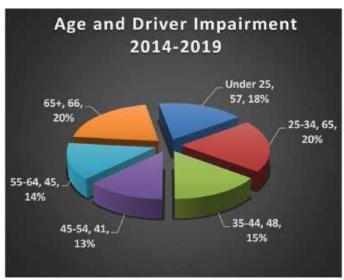


Figure 22 – All Age and Driver Impairment percentiles.

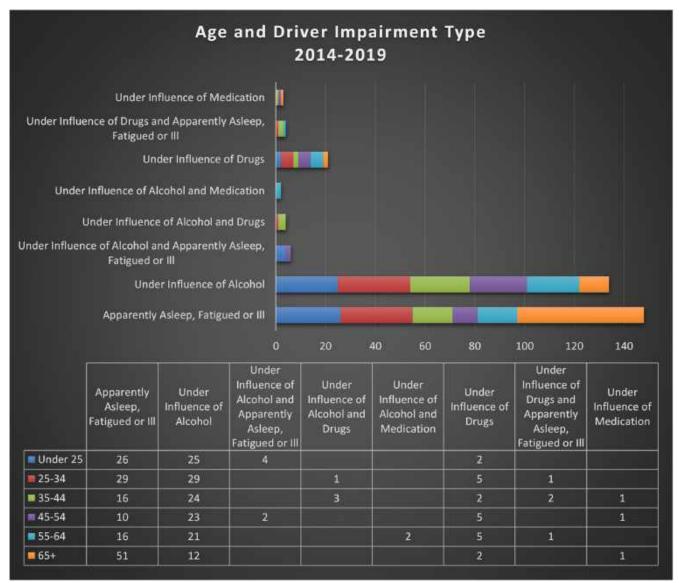


Figure 23 – All Driver Impairment percentiles.

RISKY DRIVING BEHAVIORS

The following factors were used to define Risky Driving Behaviors (RDB), Primary Human Factor Preceding Crash due to the high occurrence of crash fatalities or injuries: Inattention, Driver Impairment (Under Influence of Alcohol, Drugs or Medication or Apparently Asleep, Fatigued or III), Speeding, Fail to Yield, Follow Closely, and Distracted Driving. Further analysis was completed on RDBs to understand these crashes. Not studied in 2016 plan.

- 62% of all crashes were RDB's, including 56% of fatal crashes and 72% of injury crashes.
- 24% of all crashes were Both Roadway Departure and RDB's, including 48% of fatal crashes, and 24% of injury crashes.

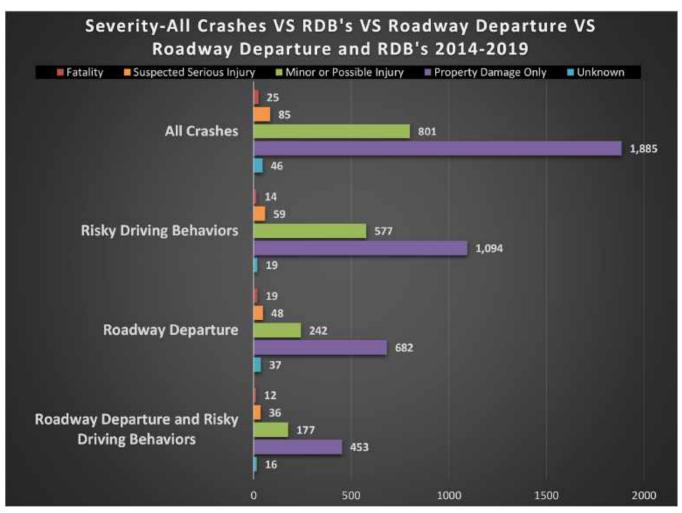


Figure 24 – Severity – All Crashes VS RDB's VS Roadway Departure totals.

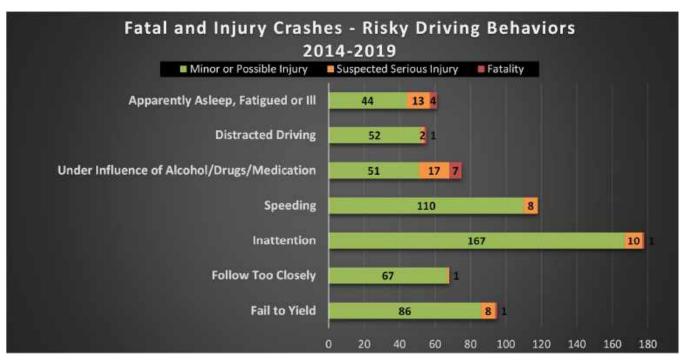


Figure 25 – All Fatal and Injury crashes - Risky Driving Behavior totals.

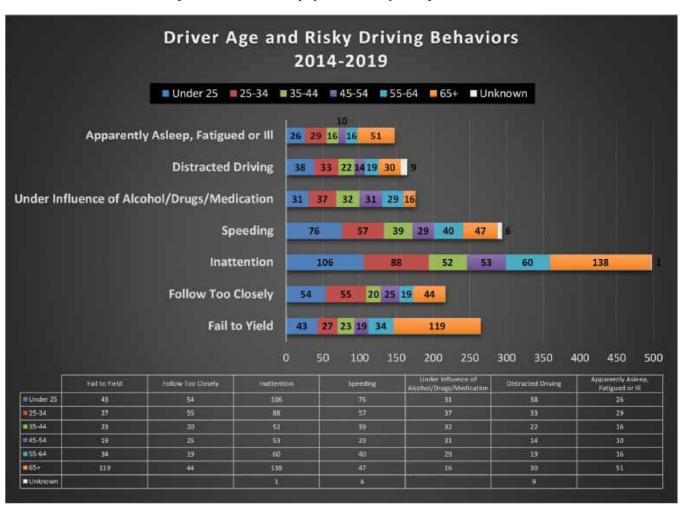


Figure 26 – All Driver Age and Risky Driving Behavior totals.

- Drivers 65 or older had the highest rate of crashes involving Fail to Yield at 45%, Inattention at 28%, and Driver Impairment-Apparently Asleep, Fatigued or III at 34%.
- Drivers under 25 and 25-34 had the highest rate of crashes involving Follow Too Closely at 25% each.
- Drivers under 25 had the highest rate of crashes involving Speeding at 26%, and Distracted Driving at 23%.
- Drivers 25-34 had the highest rate of crashes involving Driver Impairment-Under Influence of Alcohol, Drugs or Medication.

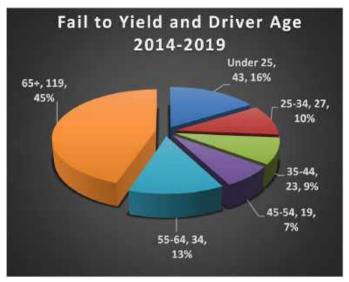


Figure 27 – All Fail to Yield and Driver Age percentiles.

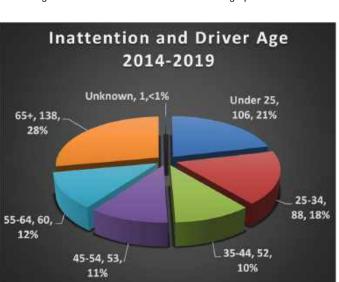


Figure 29 – All Inattention and Driver Age percentiles.

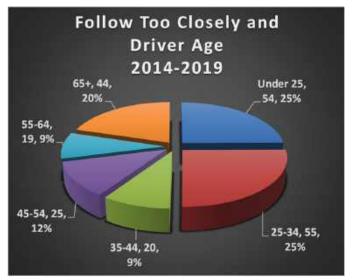


Figure 28 – All Follow Too Closely and Driver Age percentiles.

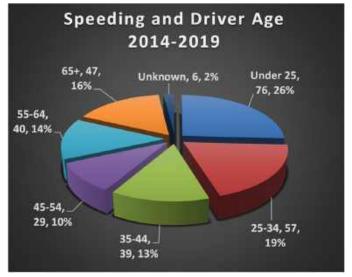


Figure 30 – All Speeding and Driver Age percentiles.

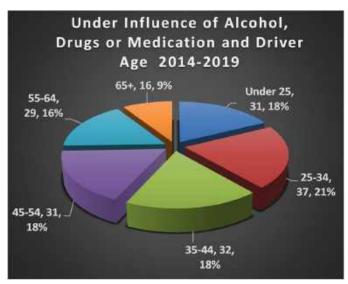


Figure 31 – All Under Influence of Alcohol, Drugs or Medication and Driver Age percentiles.

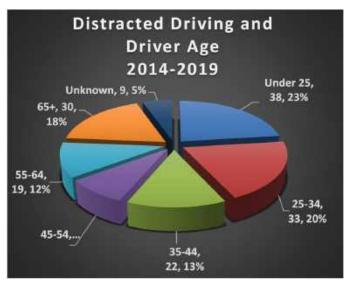


Figure 32 – All Distracted Driving and Driver Age percentiles.

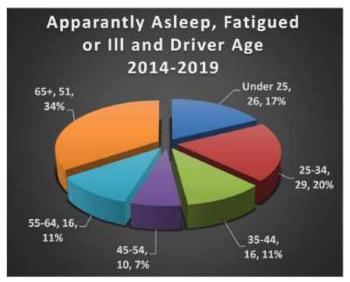


Figure 33 – All Apparently Asleep, Fatigued or III and Driver Age percentiles.

VULNERABLE ROAD USERS

Vulnerable Road Users include pedestrians, pedalcyclists, motorcyclists and road users 65 or older. Vulnerable road users are more exposed than other drivers operating vehicles, making them more susceptible to injury in the event of a crash. Not studied in 2016 plan.

- 28% of all crashes involved Pedestrian, Pedalcyclist, Motorcyclist or 65 or older road users.
- 41% of crashes involving Vulnerable Road Users resulted in injury or death. Only 34% of all crashes resulted in injury or death, the rates are much higher for Vulnerable Road Users.
- There were 13 fatal crashes involving Vulnerable Road Users. 6 (46%) of the fatal crashes involved Drivers 65 or older.
- Drivers 65 or older represent 24% of all drivers and were in 84% of crashes involving Vulnerable Road Users, with 46% of fatal crashes, 65% of suspected serious injury, 78% of minor or possible injury.

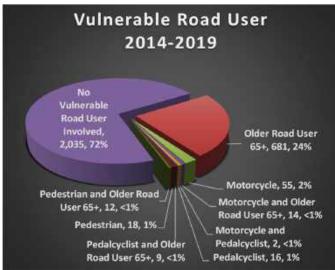


Figure 34 – Crashes involving Vulnerable Road User percentiles.

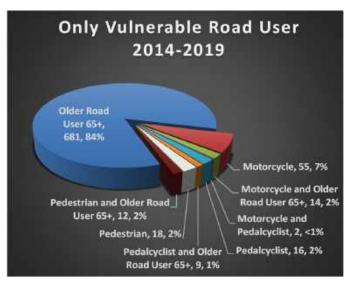


Figure 35 – Crashes only Involving Vulnerable Road User percentiles.



Figure 36 – Crash Severity - Vulnerable Road User percentiles.

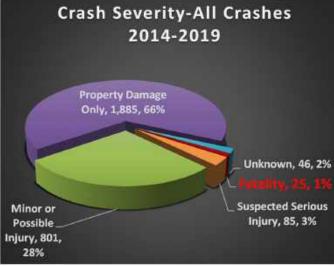


Figure 37 – Crashes Severity - All Crashes percentiles.



Figure 38 – Fatality crash and Vulnerable Road User totals.



Figure 39 – Crashes only Involving Vulnerable Road User percentiles.



Figure 40 – Fatal crashes Involving Vulnerable Road Users and RDBs totals.

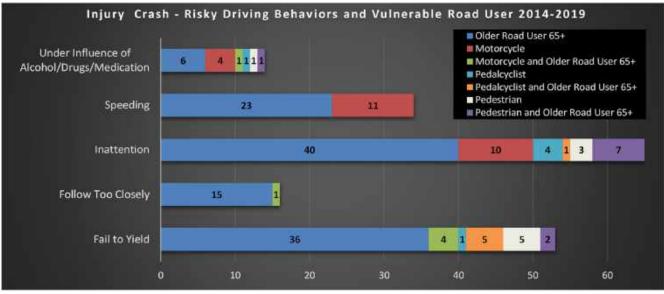


Figure 41 – Fatal crashes Involving Vulnerable Road Users and RDBs totals.

EMPHASIS AREAS

After reviewing the available data, 6 emphasis areas are selected for added attention in the transportation safety efforts of the Jamestown S'Klallam Tribe. These emphasis areas represent the most significant opportunities to accomplish the Tribe's vision:

Emphasis Area 1 - Vulnerable Road Users

Emphasis Area 2 – Risky Driving Behaviors and Roadway Departure

Emphasis Area 3 – Rear-End Crashes

Emphasis Area 4 - Wildlife/Animal Crashes

Emphasis Area 5 – US 101 and Intersections with Local Roads

Emphasis Area 6 – Blyn Corridor Road improvements

Each emphasis area is described below and accompanied by a list of strategies that, if implemented, are expected to reduce the associated crashes and enhance safety. Each strategy is assigned to a department or task force that is responsible for implementation and evaluation.

EMPHASIS AREA 1 – VULNERABLE ROAD USERS

DESCRIPTION

Vulnerable Road Users include pedestrians, pedalcyclists, motorcyclists and drivers 65 or older. They are of concern to the Tribe. There is an increase in pedestrian and pedalcyclist usage on the Olympic Peninsula with the recent enhancements to the Olympic Discovery Trail.

From 2014-2019, 30 crashes involved pedestrians and 27 involved pedalcyclist (2.0% of total crashes). Of the 57 crashes, 3 were fatalities, 4 were serious injuries, 48 were minor injuries and 2 resulted in no injury.

In the 2016 Plan, 8 crashes involved pedestrians and 10 involved pedalcyclists (1.4% of total crashes). Of the 18 crashes, 4 were serious injuries, 13 were minor injuries and 1 resulted in no injury.

- 28% of all crashes involved Pedestrian, Pedalcyclist, Motorcyclist or 65 or older road users.
- 41% of crashes involving Vulnerable Road Users resulted in injury or death. Only 34% of all crashes resulted in injury or death, the rates are much higher for Vulnerable Road Users.
- There were 13 fatal crashes involving Vulnerable Road Users. 6 (46%) of the fatal crashes involved Drivers 65 or older.
- Drivers 65 or older represent 24% of all drivers and were in 84% of crashes involving Vulnerable Road Users, with 46% of fatal crashes, 65% of suspected serious injury, 78% of minor or possible injury.

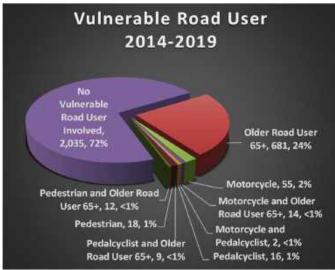


Figure 42 – Crashes involving Vulnerable Road User percentiles.

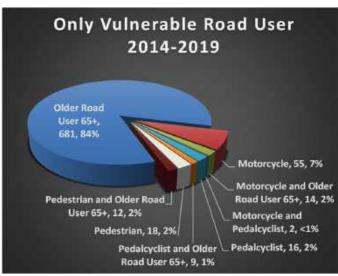


Figure 43 – Crashes only Involving Vulnerable Road User percentiles.

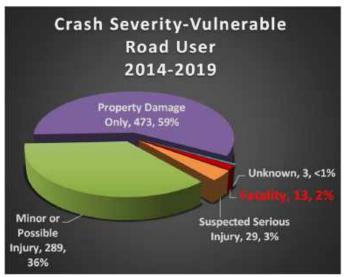


Figure 44 – Crash Severity - Vulnerable Road User percentiles.

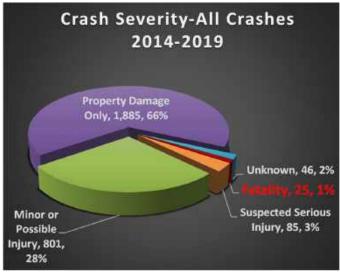


Figure 45 – Crashes Severity - All Crashes percentiles.

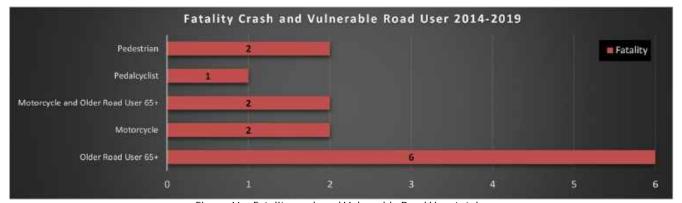


Figure 46 – Fatality crash and Vulnerable Road User totals.

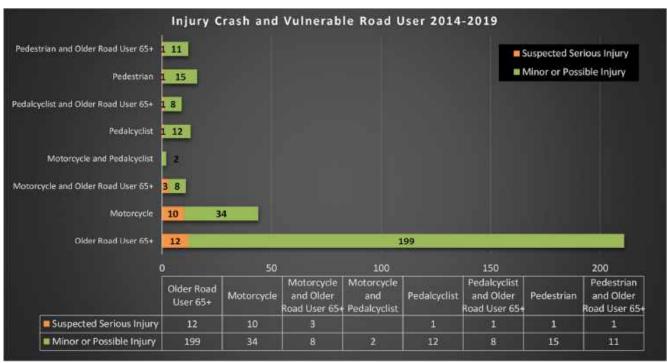


Figure 47 – Crashes only Involving Vulnerable Road User percentiles.



Figure 48 – Fatal crashes Involving Vulnerable Road Users and RDBs totals.

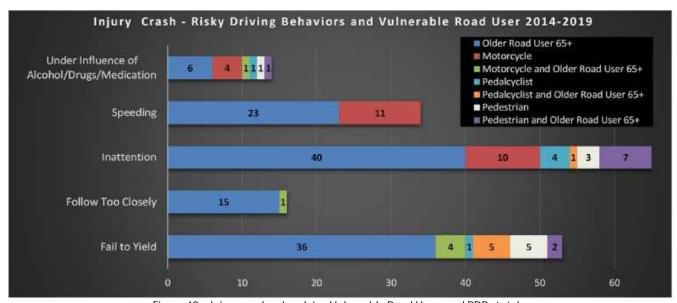


Figure 49 – Injury crashes Involving Vulnerable Road Users and RDBs totals.

Additional data analysis will be necessary to determine trends in the pedestrian and bicycle crashes. The following common factors need to be identified:

- Location Factors. Were there any hazards that need to be addressed in the crash locations?
- Human Factors. What were the human factors that contributed to the crash?
- Environmental Factors. What were the environmental factors in play?

This emphasis area identified in the Tribe's 2016 Long Range Transportation Plan as Prioritized Project Number 8:

8. Comprehensive Pedestrian and Bicycle Plan (System Wide): With the many-programmed Tribal and area development plans, it is apparent that the connection of the tribal community members to the local goods and services (as supported by foot or bicycle travel) requires significant planning, expansion, and coordination. The pedestrian plan must focus on the condition rating of the existing trails and pathways utilized by the tribal citizens (youth to elders) to access services by foot. Additional study and community involvement will be required to also identify the desired connections that currently do not exist. In this plan, recreational trails should be strongly considered not only for the health and benefit of the Tribal and non-Tribal local communities but also for potential enhancement of commercial developments for the visiting public. Walking, hiking and biking trails provide a great opportunity for Tribal community enhancement by incorporating cultural education and preservation through interpretive signage, planned bench seating location with educational placards and interactive stations, the display of traditional tribal art, and environmental enhancement and education of plant and animal species. For extended hours of operation and use, path lighting should certainly be considered particularly here in the Pacific NW where we experience short days and overcast low light conditions seasonally. The plan should strongly consider connectivity to other internal and external paths and trails. There are many planned phases, for instance, of the Olympic Discovery Trail, which are identified as separate projects. In instances where the potential exists for vehicle-pedestrian conflicts, well-designed safe crossings and walkways should be implemented to discourage accidents. A significant challenge that will be faced by the Tribe through the study will be to gain project support and buy-in from those local, state, and regional agencies responsible for many of the roads traveling through the pedestrian trails and pathways transportation system. The LRTP identified significant challenges to pedestrian safety such as the Tribal community, administration, Tribal services, and economic develop areas bisection on either side of US101. Several local and county roads will likely require pedestrian facility upgrades required to improved driver education and warning of the presence of pedestrians and pedestrian facilities. Traffic calming measures must be implemented to warn the unexpecting vehicle driver of the proximity to the tribal community and increased likeliness of pedestrian traffic. Where possible and practical, designs must be implemented to reduce the potential for conflicts even if that includes the development of pathways and trails that do not share the same alignments of existing roads. This project is one of the highest priorities of the Tribal Public and Administration as supported by the recent pedestrian improvements near the tribal administration area, tunnel under US101, and extensions of the ODT. The effort must continue as new area developments and expansion occurs. The Tribe may utilize TTP funding to support this continued pedestrian transportation safety effort which significantly enhances the community and area.

GOALS

Reduce the number of pedestrian and bicycle involved crashes by 10% by 2025.

PEDESTRIAN AND BICYCLE STRATEGIES

- Conduct a multidisciplinary Road Safety Audit (RSA) to analyze the locations identified as high pedestrian crash areas or areas with increased risk of crashes due to high pedestrian use and congestion. (Strategy Champion: Jamestown S'Klallam Transportation and Planning staff (JST Staff)
- Coordinate with the State DOT Safe Routes to School Program to develop a school walking plan. (Strategy Champions: JST Staff)
- Develop a Pedestrian and Bicycle Plan. (Strategy Champion: JST Staff)
- Continue to implement Pedestrian safety improvements to the transportation network as the Tribe has been through the last 10 years. (Strategy Champion: JST Staff)
- Seek funding to design pedestrian facility enhancements. (Strategy Champion: JST Staff).
- Apply for a Federal Highway Tribal Transportation Safety Grant in 2022 to secure funding for design and construction of improvements. (Strategy Champion: JST Staff)
- Work with Clallam County on the Olympic Discovery Trail (ODT) to develop a connector trail through Tribal Property on the East side of Diamond Point Road at the intersection with US 101 to tie in with a section, recently completed by the county, of the ODT that ties into Old Gardiner Road. This would tie into the existing ODT at the trailhead on the Westside of Diamond Point Road. (Strategy Champion: JST Staff and Clallam County Transportation Program Manager)

OLDER DRIVER AGE 65+ STRATEGIES

 Implement education program for the community encouraging enrollment in driving safety classes such as AAA's Roadwise Driver Course. These classes focus on tips and techniques for addressing factors typical among aging drivers including changing vision, reduced response times, and effects of prescription medications. (Strategy Champions: JST Staff, Sequim City Police, WSP)

EMPHASIS AREA 2 – RISKY DRIVING BEHAVIORS AND ROADWAY DEPARTURE

DESCRIPTION

Unsafe behaviors - Risky Driving Behaviors are becoming an accepted part of the culture on many tribal roads (based on an increase in crashes) and often cause serious injury or death. When analyzing the data, the Jamestown S'Klallam Tribe is, unfortunately, no exception.

Reducing fatal and serious injury crashes can be accomplished through identifying and deterring unsafe or risky behaviors by drivers and other transportation users, and preventing roadway departure.

The FHWA (Federal Highway Administration) defines Roadway Departure as a crash which a vehicle crosses an edge line, center line or otherwise leaves the traveled way. For this study, Roadway Departure crashes include those identified by WSDOT as Crash Types involving lane departure: Head-On, Over Embankment-No Guardrail Present, Overturned, Sideswipe, Struck Object, Struck Parked Vehicle and Primary Human Factor Preceding Crash of Over Center Line.

- 36% crashes involved Roadway Departure. Compared to 33% in 2016 plan.
- 76% of all fatal crashes and 56% of Suspected Serious injury crashes were Roadway Departure.

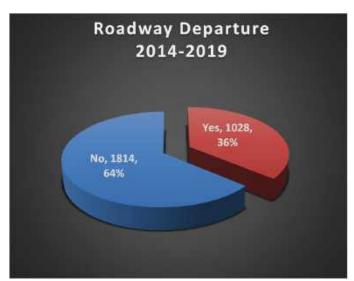


Figure 50 – All Crash Type percentiles.

The following Primary Human Factor Preceding Crash factors were used to define Risky Driving Behaviors (RDB) due to the high occurrence of crash fatalities or injuries: Inattention, Driver Impairment (Under Influence of Alcohol, Drugs or Medication or Apparently Asleep, Fatigued or III), Speeding, Fail to Yield, Follow Closely, and Distracted Driving. RDB's can have devastating consequences as a result of these behaviors are evident in the Crash data and result in severe consequences. WSDOT Crash data showed that from 2014-2019, 1,763 or 62% of all crashes involved RDB's, including 56% of fatal crashes and 72% of Injury crashes. Inattention and distracted driving was the highest RDB reported by far, involving 24% of All Primary Human Factors Preceding.

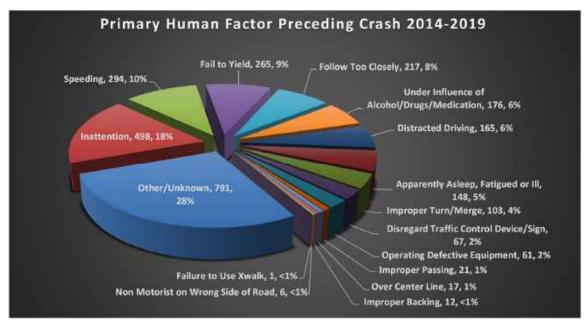


Figure 51 – All Human Factor Preceding Crash percentiles.

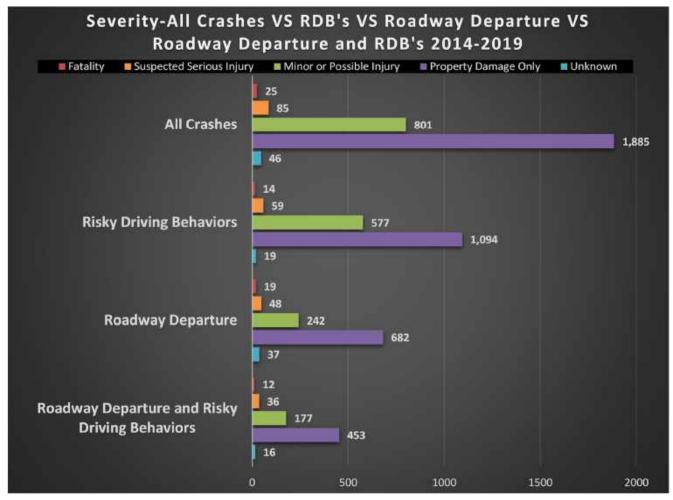


Figure 52 – Severity – All Crashes VS RDB's VS Roadway Departure totals.

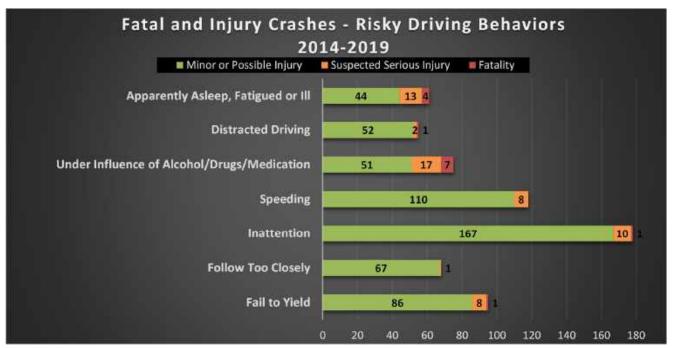


Figure 53 – All Fatal and Injury crashes - Risky Driving Behavior totals.

- Drivers 65 or older had the highest rate of crashes involving Fail to Yield at 45%, Inattention at 28%, and Driver Impairment-Apparently Asleep, Fatigued or III at 34%.
- Drivers under 25 and 25-34 had the highest rate of crashes involving Follow Too Closely at 25% each.
- Drivers under 25 had the highest rate of crashes involving Speeding at 26%, and Distracted Driving at 23%.
- Drivers 25-34 had the highest rate of crashes involving Driver Impairment-Under Influence of Alcohol, Drugs or Medication.

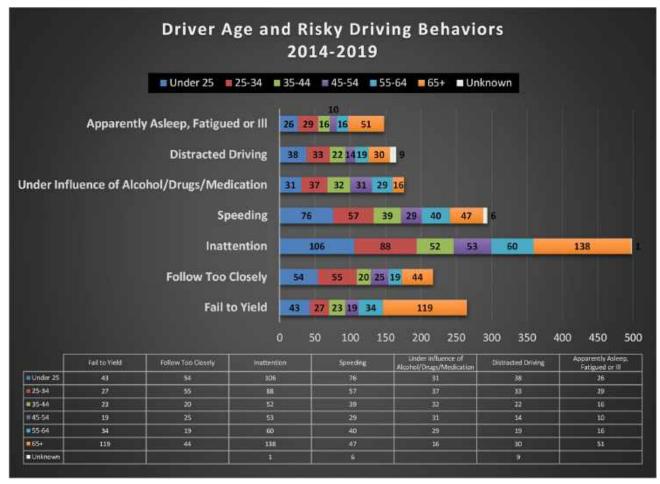


Figure 54 – All Driver Age and Risky Driving Behavior totals.

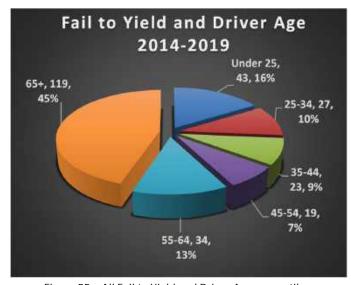


Figure 55 – All Fail to Yield and Driver Age percentiles.

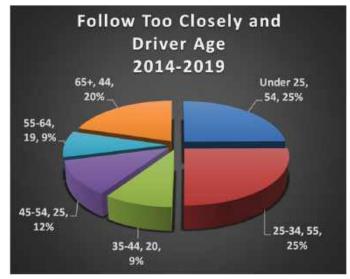


Figure 56 – All Follow Too Closely and Driver Age percentiles.

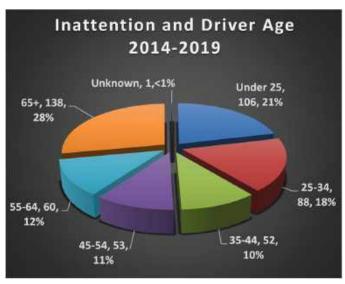


Figure 57 – All Inattention and Driver Age percentiles.

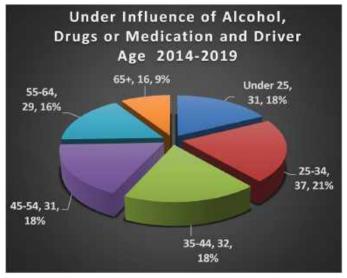


Figure 59 – All Under Influence of Alcohol, Drugs or Medication and Driver Age percentiles.

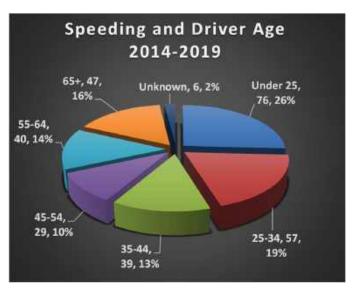


Figure 58 – All Speeding and Driver Age percentiles.

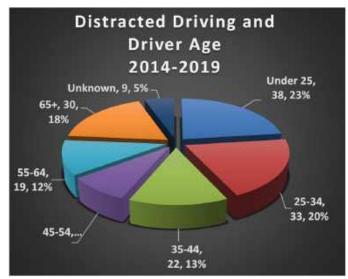


Figure 60 – All Distracted Driving and Driver Age percentiles.

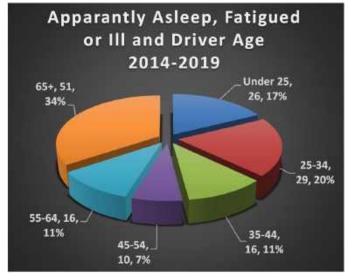


Figure 61 – All Apparently Asleep, Fatigued or III and Driver Age percentiles.

GOALS

Reduce Fatalities and Serious Injuries that involve Roadway Departure and Risky Driving Behaviors by at least 10% by 2025.

STRATEGIES

Conduct a Systemic RSA that evaluates Roadway Departure risks including roadway condition, visibility, striping
and signage in the community. The RSA should specifically evaluate locations with reported crashes involving
Roadway Departure including Crash types: Head-On, Over Embankment-No Guardrail Present, Overturned,
Sideswipe, Struck Object, and Struck Parked Vehicle. (Strategy Champions: JST Staff, City of Sequim, Clallam
and Jefferson Counties, Sequim City Police, WSP)

ENFORCEMENT AND POLICY STRATEGIES

- Enforce Laws related to Inattention, Driver Impairment (Under Influence of Alcohol, Drugs or Medication or Apparently Asleep, Fatigued or III), Speeding, Fail to Yield, Follow Closely, and Distracted Driving. Renewed police patrols and presence at community concern locations for driving enforcement. (Strategy Champions: City of Sequim Police, WSP)
- Create a task force to evaluate, refine, and improve laws and policies for traffic enforcement in the community.
 The task force should specifically evaluate laws and policies concerning the following topics: Risky Driving
 Behaviors and Roadway Departure. Risky Driving Behaviors include Inattention, Driver Impairment (Under
 Influence of Alcohol, Drugs or Medication or Apparently Asleep, Fatigued or III), Speeding, Fail to Yield, Follow
 Closely, and Distracted Driving. (Strategy Champions: JST Staff, Clallam and Jefferson Counties, Sequim City
 Police, WSP)
- Intersection and Speed Management. Develop a policy for conducting studies to monitor intersection-driving behaviors and set speed limits. (Strategy Champions: JST Staff, Clallam and Jefferson Counties, Sequim City Police, WSP)

EDUCATION AND SAFETY CULTURE STRATEGIES

- Conduct additional data analysis to determine target audience for educational efforts. The data in this plan identifies that a significant percentage of crashes occurring are caused by drivers under 25 years of age, 25-34 and 65 years of age or older. This would be the initial target audience for a safety campaign marketed to the community to raise awareness that these behaviors are unacceptable. (Strategy Champions: JST Staff, City of Sequim, Clallam and Jefferson Counties, Sequim City Police, WSP)
- Create a media campaign using billboards and displays at the Tribe's Casino and Gas Station to encourage drivers to adopt a culture of safe driving. (Strategy Champions: JST Staff, Casino Public Relations)
- Implement an education program for the community that involves safety signage and a mock crash like the "Every 15-Minutes" program. (Strategy Champions: Sequim City Police, WSP)



EMPHASIS AREA 3- REAR-END CRASHES

DESCRIPTION

Approximately 25%, or 709, of all crashes were rear-end and were primarily caused by: speeding, following too close, and/or driver inattention. Of the 709 crashes, 270 serious/minor injuries (38%) occurred.

GOAL

Reduce rear end crashes by 10% by 2025.

STRATEGIES

- Develop a media campaign to emphasize: Slowing Down, Backing Off, and Paying Attention (Strategy Champions: JST Staff, WSP)
- Work with the WSP to increase enforcement patrols to reduce speeding. (Strategy Champions: JST Staff, WSP)
- Work with WSDOT to highlight signage needs on curves and areas within the Jamestown S'Klallam Study area. (Strategy Champions: JST Staff WSDOT)

EMPHASIS AREA 4 – WILDLIFE/ANIMAL CRASHES

DESCRIPTION

In the six years analyzed from January 2014 to December 2019, 331 reported crashes (12%) occurred involving animals and vehicles. While this percentage is lower than in 2016 plan at 233 crashes (19%) the actual number of crashes is higher by 98 crashes due to an increase in the ADT. Vehicle wildlife crashes nationwide cause approximately 200 deaths per year and the estimated cost to society is \$1.2 billion per year. Research completed by the Western Transportation Institute at Montana State University calculated the average total costs associated with an animal/vehicle crash for two species: \$7,890 for deer and \$17,100 for elk. Based on these numbers, the combined cost of these crashes is approximately \$4.1 million per year. The focus of this emphasis area is to identify how to reduce animal/vehicle crashes and to minimize the consequences when they occur. Two specific sections of US 101 were identified within 6.0 miles either side of Blyn, WA; approximately MP 266 to 270 (25 crashes) and MP 270.5 to 276.5 (20 crashes).

GOAL

Reduce crashes involving animals within reservation service area by 10% by 2023.

STRATEGIES

- Conduct a Road Safety Audit (RSA) on those areas with a higher incidence of animal/vehicle crashes (Strategy Champion: JST Staff)
- Work with the WSDOT and Washington Department of Fish and Wildlife (WDFW) to delineate and sign these
 areas to warn drivers of the hazards (Strategy Champion: JST Staff)

DESCRIPTION

A Road Safety Audit (RSA) was performed in April 2014 on intersections with US 101, from MP 267 to MP 274.6. The RSA team reviewed fifteen intersections and developed countermeasures to improve the safety and reduce the crashes at each intersection. The realignment of the Chicken Coop Road/Zaccardo Road/US 101 intersection, completed in 2018, through a partnership between WSDOT and the Jamestown S'Klallam Tribe, was one of the projects. (See Appendix A - Jamestown S'Klallam SR 101 RSA).

Current crash data (2014-2019) was analyzed and compared with the crash data set (2009-2013) used in the 2014 RSA study for eight (8) of those intersections. The data sets included intersection, driveway or related crashes. Overall, the trend shows on increase in the total number of crashes, from thirty-two (32) from 2009-2013 to sixty-four (64) from 2014-2019. This is an annual average increase of 7 to 10.5 crashes per year from 2009-2019. Fatal/Injury crashes changed from 1 fatality and nineteen (19) injuries from 2009-2013, to no fatalities and twenty-nine (29) injuries from 2014-2019. Five of the intersections showed significantly higher crash rates: Palo Alto Rd, Louella Rd, Sophus Rd, Knapp Guiles Rd, and Chicken Coop-Diamond Point Rd. These intersections remain a major concern to the Tribe. The increase in injury crashes at these intersections further reinforces the need for improvements to reduce crashes.

US 101 Roadway Safety Audit Intersection Crash History (2009-2013) vs Current Trends (2014-2019)																						
	Crash History 2009-2013 (5 years)										Crash History 2014-2019 (6 years)											
		S	eve	rity			Туре	:			Severity Type											
Cross road with US 101	# Crashes	Fatal	Injury	Property Damage Only	Angle	Head-On	Rear End	Animal	Struck Object	# Crashes	Fatal	Injury	Property Damage Only	Angle	Head-On	Rear End	Animal	Struck Object	Overturned	Turning	Sideswipe	Backing
Palo Alto Rd	3	1	2		2		1			8		2	6	2		2	1	1	1	1		
Louella Rd	4		3	1	2		2			13		7	6	2	1	5		2		3		
Schoolhouse Point Ln	3		2	1			2	1		5		3	2			4		1				
7 Cedars Casino Driveway	4		1	3	4					4		4		2		1				1		
Sophus/Silva Rd	8		6	2	5	1		1	1	11		2	9	6		1		1		1	2	
Pierce Rd	3		2	1			3			3		2	1			1		1			1	
Knapp/Guiles Rd	1			1			1			9		6	3	2		4	1	2				
Chicken Coop-Diamond Point Rd	6		3	3	2	1	1		2	10		3	7	5		1	1			2		1
Total # Crashes	32	1	19	12	15	2	10	2	3	63	0	29	34	19	1	19	3	8	1	8	3	1

Figure 62 – US 101 Intersection Crash Data comparison 2009-2019.

GOAL

Reduce left turn crashes by 10% by 2025.

STRATEGIES

- Work with WSDOT and Clallam County to implement the countermeasures identified in the Jamestown S'Klallam SR 101 RSA. (Strategy Champions: JST Staff, WSDOT, Clallam County Transportation Program Manager)
- Apply for TTPSF funding to assist in the implementation of the improvements. (Strategy Champion: JST Staff)

• Work with WSDOT to add additional signs on curves and areas within the Jamestown S'Klallam Study area. (Strategy Champions: JST Staff, WSDOT)

EMPHASIS AREA 6- BLYN CORRIDOR TRANSPORTATION IMPROVEMENTS

DESCRIPTION

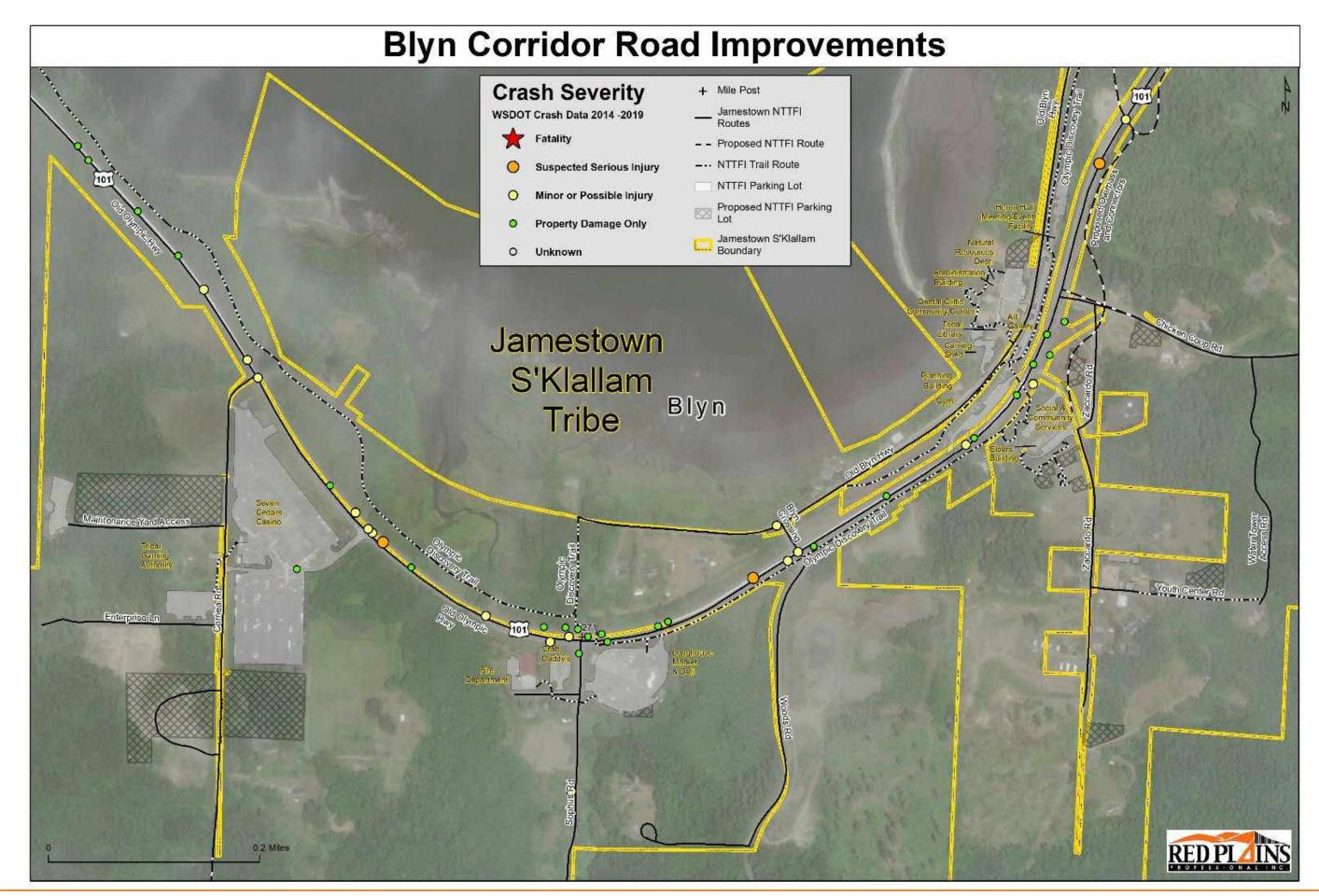
Old Blyn Road, MP 272.26, to the west side of the Casino/Resort at MP 270.6 where the Old Blyn Road ties back into US 101 is a very crowded corridor, with very high vehicle and pedestrian use. The area includes the Tribal government campus, Longhouse Market, and Casino/Hotel complex and many other driver distractions. There is heavy foot traffic within this area as Tribal members and visitors access the many Tribal facilities, creating a potential conflict between pedestrians, pedalcyclists and vehicles. There have been several near misses in the corridor. The realignment of the Chicken Coop Rd and Zaccardo Rd intersection reduced the congestion some, however more work needs to be done.

GOAL

Improve safe access for pedestrians, pedalcyclists, and vehicles within this corridor by 2023.

STRATEGIES

 Perform a traffic analysis through comparative analysis of the physical roadway conditions and the traffic, pedestrian and pedalcyclists behavior and movement conditions. The traffic analysis should include intersection level of service during peak hours, traffic volumes, traffic speeds, and general modeled traffic patterns. (Strategy Champions: JST Staff, WSDOT)



IMPLEMENTATION AND EVALUATION

THE JAMESTOWN S'KLALLAM TRANSPORTATION SAFETY MANAGEMENT COMMITTEE

For this plan to be successful, it must be implemented and monitored, revisions to this plan will be necessary, as success will mandate change. The Safety Stakeholders will be interviewed on a yearly basis to discuss the Safety Plan and get their concerns/input. The JST Staff will review the STSP annually to evaluate progress toward each goal, discuss the progress of strategies that are being implemented, and consider any needed revisions/updates to this plan.

STRATEGY IMPLEMENTATION CHAMPIONS

The strategies listed above designate a champion for each strategy and this champion has the lead on implementation of that particular strategy. Many of the strategies may require an implementation plan be created that is separate from this document. As needed, the strategy champions will build an action plan for their strategy that outlines the implementation steps, schedule, and needed resources. The strategy champions will report to JST Staff on their strategy when updates are available or as requested.



Jamestown S'Klallam SR 101 Road Safety Audit

Prepared for Jamestown S'Klallam Tribe

April 2014

Prepared by **Parametrix**

Jamestown S'Klallam SR 101 Road Safety Audit

Prepared for

Jamestown S'Klallam 1033 Old Blyn Highway Sequim, WA 98382

Prepared by

Parametrix

411 108th Avenue NE, Suite 1800 Bellevue, WA 98004-5571 T. 425.458.6200 F. 425.458.6363 www.parametrix.com

CITATION

Parametrix. 2014. Jamestown S'Klallam SR 101 Road Safety Audit. Prepared by Parametrix, Bellevue, Washington. April 2014.

TABLE OF CONTENTS

	1. INTRODUCTION	1
	2. OVERVIEW OF THE STUDY AREA	1
	3. ROAD SAFETY AUDIT PROCESS AND TEAM	2
	4. FINDINGS AND RECOMMENDATION	2
-	NEWDIOEG	

APPENDICES

- A Road Safety Audit Team
- B Maps

April 2014 | 554-1761-014 i

ACRONYMS

FHWA Federal Highway Adminstration

HSM Highway Safety Manual

MEV million entering vehicles

MVM million vehicle miles
RSA road safety audit

WSDOT Washington State Department of Transportation

April 2014 | 554-1761-014 iii

1. INTRODUCTION

Jamestown S'Klallam Tribe (Tribe) initiated a road safety audit (RSA) along US 101 between Happy Valley Road (mile post 267) and Chicken Coop/Diamond Point Road (mile post 274.6). The Tribal governmental facilities and main economic enterprises are located adjacent to US 101. The Tribe is interested in improving the safety and mobility of this particular intersection for our Tribal Citizens accessing services, for residents living in the vicinity and for visitors to the Olympic Peninsula.

The RSA is a study that supplements WSDOT past safety improvements along mainline US 101, with the RSA focusing on County roads and driveways intersecting with US 101. The intent of the RSA is to understand the deficiencies and driver behaviors that contribute to collisions, and to identify the best safety improvements to reduce the frequency and severity of collisions in the future.

The Washington State Department of Transportation (WSDOT) has completed safety improvements along US 101 including this stretch of roadway over the past 10 years. These safety improvements were completed in an effort to reduce the collision rate. The eight mile section of US 101 corridor between Happy Valley Road and Diamond Point Road has a rate of 0.71 collisions per million vehicle miles (MVM); however some sections have much higher rates. The mile stretch of roadway for example approaching and departing Chicken Coop Road/Zaccardo Road intersection area has a collision rate of 1.34 per MVM. The state average for a similar roadway is 1.11 collisions per MVM. WSDOT recently reduced the posted speed limit for a portion of the study area from 55 mph to 50 mph to address concerns raised by the public and the Tribe related to near miss collisions, and to address major collisions that resulted in loss of property or injury.

2. OVERVIEW OF THE STUDY AREA

The US 101 corridor loops around the Olympic Peninsula and Olympic National Park, providing access to dozens of cities, communities, parks and recreational land uses. The corridor runs through the Clallam, Jefferson, Grays Harbor, and Mason counties. Tourism is one of the Tribe's and the North Olympic Peninsula's primary economic drivers. US 101 supports over 5 million vehicles annually, mainly visitors to Olympic National Park, and to the North Olympic Peninsula's other increasingly popular attractions such as river and saltwater fishing, local community festivals (e.g. Sequim's Lavender Festival, Port Angeles' Crab Festival, etc.), as well as to the nine tribal nations located on the Olympic Peninsula.

Within the nearly eight mile study area, the US 101 corridor land use includes single family residences, commercial, and community center land uses including Jamestown S'Klallam 7 Cedars resort, gas station and deli, and visitor centers.

The study area included the following intersections with US 101:

- Happy Valley Road
- Palo Alto Road
- W Sequim Bay Road
- Barker Road
- Louella Road
- Schoolhouse Point Lane
- Corriea Road

- 7 Cedars Casino Driveway
- Sophus Road
- Chicken Coop-Zaccardo Road
- Blyn Road
- Old Blyn Highway
- Pierce Road
- Knapp/Guiles Road
- Chicken Coop-Diamond Point Road

3. ROAD SAFETY AUDIT PROCESS AND TEAM

As described by the Federal Highway Administration (FHWA), an RSA is the formal safety performance examination of an existing or future road or intersection by an independent, multidisciplinary team. The RSA identifies potential existing safety issues and opportunities for improvement.

An RSA differs from a standard safety analysis in that it is performed by an independent team of experts representing multiple disciplines, considers human factors (such as driving at excessive speeds or nighttime visibility), and findings are documented in a formal report.

The RSA process includes the following steps and is described in FHWA Road Safety Audit Guidelines (FHWA publication FHWA-SA-06-06).

- Identify project
- Select RSA team
- Conduct start-up meeting
- Collect historical data for collisions and traffic volumes
- Perform field reviews
- Conduct analysis and prepare report
- Present findings to Project Owner
- Prepare formal response
- Incorporate findings

The RSA study team for the Jamestown S'Klallam US 101 project includes a Tribe representative, a WSDOT traffic safety and operations engineer, and three consultant engineers. The engineers completing the audit each have over fifteen years of experience in roadway design or traffic operations analysis.

Clallam County was invited to participate but were unavailable for the field work. Traffic volumes, collision history, and local operational issues were provided by the County to supplement data collected.

4. FINDINGS AND RECOMMENDATIONS

The following summarizes for each intersection, the existing conditions and the countermeasures identified by the RSA team. The countermeasures are rated as low/medium/high effort (or cost) and benefit. The level of benefit for some improvements was determined using the Highway Safety Manual (HSM).

The following summary table (Exhibit 1) summarizes the existing issues and opportunities for safety improvements. Exhibits 2 through 16 describe the collision

history (rates, type, and severity), volumes, and countermeasures for the study intersections. The most typical collision types are vehicles rear-ending another vehicle turning from US 101 onto a side road (where a left or right turn pocket did not exist) and collisions as vehicles enter US 101 from side roads. Countermeasures identified include installing left or right turn lanes, and adding illumination. There are a few locations where all capacity improvements are already installed. The countermeasures for these locations could include signalization, closing access, or constructing a roundabout to reduce conflict points and speeds.

Deer and elk regularly cross the US 101 corridor within the study area. Reviewing the collision trends along US 101, collisions with wildlife is a frequent occurrence both at intersections and along US 101 between intersections. However this RSA focuses on vehicular collisions with other vehicles or persons rather than minimizing wildlife collisions.

4

Exhibit 1. Summary of Existing Conditions and Opportunities

	Existing Conditions (X) and Opportunities (O)												
				L	JS 101	Cross Road		Intersection					
	Cross road with US 101	Improve sight distance	Illumination	Left turn lane	Left turn receiving lane (refuge)	Right turn lane	Right turn receiving	Shoulder widening	Modify approach skew	Modify approach grade	Signal	Roundabout	Close access
1	Happy Valley Rd	Х	0	0	0				0				
2	Palo Alto Rd	х	0	0	0				0				
3	W Sequim Bay Rd	0	0						0				
4	Barker Rd	х	0	0	0					0			
5	Louella Rd	х	0	0	0			0					
6	Schoolhouse Point Ln	х	0	0	0								
7	Corriea Rd	х	0	0	0	0							0
8	7 Cedars Casino Driveway	х	Х	Χ	Х	Х						0	
9	Sophus Rd	х	Х	Х	Х	Х						0	
10	Chicken Coop-Zaccardo Rd	х	0	0	0	0		0	0				0
11	Blyn Rd	х											0
12	Old Blyn Hwy	х		0	0								
13	Pierce Rd	х		0	0								
14	Knapp/Guiles Rd	х		0									
15	Chicken Coop-Diamond Point Rd	х	0									0	

Exhibit 2. Happy Valley Road Detail

Happy Valley Road / US 101

Collision History (2009 – 2013)

Rate 0.13 collisions per million entering vehicles

Severity 4 collisions, 1 resulted in injury

Type 3 rear end (left turns from main road to side road)

1 entering at angle

Volume on major road: 17,000 ADT Volume on minor road: 350 ADT

Existing Conditions

Adequate sight distance is available.

Collisions typically occur as vehicles exceed safe speeds.

There is no left turn pocket provided from westbound US 101 onto Happy Valley Road.

Happy Valley Road skewed (70 degrees) at intersection with US 101. This is within the guidelines of 60 to 120 degrees.

No illumination at intersection.

Countermeasures

 Widen section to provide a left turn pocket from westbound US 101 onto Happy Valley Road and receiving refuge lane for turns onto US 101.

Level of Effort: Medium

Potential Safety Benefit: High (44% reduction

in collisions)

2. Install illumination

Level of Effort: Medium Potential Safety Benefit: Low (5% reduction in collisions)

Reconstruct intersection approach to minimize skew.

Level of Effort: Medium

Potential Safety Benefit: Medium (10%

reduction in collisions)

View North onto US 101



View South onto US 101



Exhibit 3. Palo Alto Road Detail

Palo Alto Road / US 101

Collision History (2009 - 2013)

Rate 0.10 collisions per million entering vehicles

Severity 3 collisions, 2 resulted in injury and 1 resulted in fatality

Type 2 entering at angle

1 rear end (left turn from main road to side road)

Volume on major road: 16,000 ADT Volume on minor road: 800 ADT

Existing Conditions

Adequate sight distance is available.

Collisions typically occur as vehicles exceed safe speeds.

There is no left turn pocket provided from westbound US 101 onto Palo Alto Road.

Palo Alto Road intersects US 101 with a skew of 30 degrees.

No illumination at intersection.

Countermeasures

 Widen section to provide a left turn pocket from westbound US 101 onto Palo Alto Road and receiving refuge lane for turns onto US 101.

Level of Effort: Medium Potential Safety Benefit: High (44% reduction in collisions)

2. Install illumination

Level of Effort: Medium Potential Safety Benefit: Low (no reduction, all collisions during the day)

3. Reconstruct intersection approach to minimize skew.

Level of Effort: Medium Potential Safety Benefit: Medium (28% reduction in collisions)

View North onto US 101

6



View South onto US 101



Exhibit 4. W Sequim Bay Road Detail

W Sequim Bay Road / US 101

Collision History (2009 – 2013)

Rate 0.03 collisions per million entering vehicles

Severity 1 collision resulted in fatality

Type 1 vehicle strikes object

Volume on major road: 16,000 ADT Volume on minor road: 2,000 ADT

Existing Conditions

Adequate sight distance is available. However, the guardrail on the north side of the roadway, east of W Sequim Bay Road, potentially obstructing sight distance for southbound vehicles turning onto US 101.

W Sequim Bay road intersects US 101 with a skew (40 degrees).

Stop bar located 8-9 feet back from roadway.

Steep grade on W Sequim Bay road approaching US 101.

No illumination at intersection.

Countermeasures

1. Remove barrier to improve sight distance.

Level of Effort: Low

Potential Safety Benefit: Medium

2. Install illumination

Level of Effort: Medium Potential Safety Benefit: Medium (15%

reduction in collisions)

Reconstruct intersection approach to minimize skew.

Level of Effort: Medium

Potential Safety Benefit: Medium (24%

reduction in collisions)

View North onto US 101



View South onto US 101



Exhibit 5. Barker Road Detail

Barker Road / US 101

Collision History (2009 – 2013)

Rate 0.07 collisions per million entering vehicles

Severity 2 collisions with no injury

Type 1 rear end (left turn from main road to side road)

1 vehicle strikes deer

Volume on major road: 16,000 ADT Volume on minor road: < 50 ADT

Existing Conditions

Two-way left turn lane tapers out at the intersection. (Merging movements potentially occurring at intersection).

Steep grade on Barker Road approaching US 101.

Adequate sight distance is available.

Collisions typically occur as vehicles exceed safe speeds.

speeus.

No illumination at intersection.

Countermeasures

 Widen roadway to allow left turn lane to extend south of intersection.

Level of Effort: High

Potential Safety Benefit: High (56% reduction

in collisions)

Restripe so left turn lane fully merges north of the intersection.

Level of Effort: Low

Potential Safety Benefit: Medium

3. Install illumination

Level of Effort: Medium

Potential Safety Benefit: Low (all collisions

occurred during the day)

4. Reduce grade approaching US 101

Level of Effort: Medium

Potential Safety Benefit: Medium (14%

reduction in collisions)

View North onto US 101



View South onto US 101



Exhibit 6. Louella Road Detail

Louella Road / US 101

Collision History (2009 – 2013)

Rate 0.14 collisions per million entering vehicles

Severity 4 collisions with 3 resulted in injury

Type 2 entering at angle (entering from side road to main road)

2 rear end (left turns from main road to side road)

Volume on major road: 16,000 ADT Volume on minor road: 450 ADT

Existing Conditions

No left turn pocket for northbound vehicles turning from US 101 to Louella Road. Guardrail exists along the east side of roadway. No refuge for northbound vehicles approaching a northbound left-turn vehicle.

Stream crossing exists south of intersection.

Adequate sight distance is available.

Collisions typically occur as vehicles exceed safe speeds.

No illumination at intersection.

Countermeasures

 Widen roadway to construct a northbound left turn lane and receiving refuge lane.

Level of Effort: High

Potential Safety Benefit: High (44% reduction

in collisions)

2. Remove guardrail and widen shoulder to allow for refuge or recovery area.

Level of Effort: Medium

Potential Safety Benefit: Medium (13%

reduction in collisions)

3. Install illumination

Level of Effort: Medium

Potential Safety Benefit: Low (6% reduction in

collisions)

View North onto US 101



View South onto US 101



Exhibit 7. Schoolhouse Point Lane

Schoolhouse Point Lane / US 101

Collision History (2009 – 2013)

Rate 0.10 collisions per million entering vehicles

Severity 3 collisions with 2 resulted in injury

Type 2 rear end (left turns from main road to side road)

1 vehicle strikes deer

Volume on major road: 16,000 ADT Volume on minor road: 400 ADT

Existing Conditions

Wide shoulders exist on the north side of US 101 through the Schoolhouse Point Lane intersection.

No left turn lane is provided for vehicles turning off US 101 onto Schoolhouse Point Lane.

Adequate sight distance is available.

Collisions typically occur as vehicles exceed safe speeds.

No illumination at intersection.

Countermeasures

 Widen roadway to construct a southeastbound left turn lane and receiving refuge lane.

Level of Effort: Medium

Potential Safety Benefit: High (44% reduction

in collisions)

2. Install illumination

Level of Effort: Medium

Potential Safety Benefit: Low (collisions

occurred during the day)

View West onto US 101



View East onto US 101



Exhibit 8. Corriea Road Detail

Corriea Road / US 101

Collision History (2009 - 2013)

Rate 0.03 collisions per million entering vehicles

Severity 2 collisions with 1 resulted in injury

Type 1 entering at angle (entering from side road to main road)

1 rear end (right turn from main road to side road)

Volume on major road: 16,000 ADT Volume on minor road: 200 ADT

Existing Conditions

Corriea Road west of US 101 is a low volume road providing a second access point to and from the 7 Cedars Casino and two residences.

A solid white stripe exists along US 101 as it intersects with Corriea Road. This indicates only right in/right out movements are allowed although left turn movements are observed.

Corriea Road east of US 101 provides access to a single family residence and a commercial lot.

Adequate sight distance is available.

Collisions typically occur as vehicles exceed safe speeds.

No illumination at intersection.

Countermeasures

1. Close access to US 101.

Level of Effort: Low

Potential Safety Benefit: High

2. Widen roadway to construct an eastbound right turn pocket.

Level of Effort: Medium

Potential Safety Benefit: Medium (14%

reduction in collisions)

Widen roadway to construct an eastbound left turn lane and receiving refuge lane.

Level of Effort: Medium

Potential Safety Benefit: High (44% reduction

in collisions)

3. Install illumination

Level of Effort: Medium

Potential Safety Benefit: Medium (15%

reduction in collisions)

View West onto US 101



View East onto US 101



Exhibit 9. 7 Cedars Casino Driveway

7 Cedars Casino Driveway / US 101

Collision History (2009 - 2013)

Rate 0.13 collisions per million entering vehicles

Severity 4 collisions with 1 resulted in injury

Type 4 entering at angle (entering from side road to main road)

Volume on major road: 16,000 ADT Volume on minor road: 1,000 ADT

Existing Conditions

Left and right turn lanes are provided at the driveway and along US 101. There is a refuge or receiving lane on US 101 for vehicles exiting the driveway northbound and turning westbound onto US 101.

Adequate sight distance is available.

Collisions typically occur as vehicles exceed safe speeds.

Countermeasures

1. Install a roundabout.

Level of Effort: High Potential Safety Benefit: High (71% reduction in collisions)

View West onto US 101

View East onto US 101





View North from Driveway



Exhibit 10. Sophus Road Detail

Sophus Road / US 101

Collision History (2009 – 2013)

Rate 0.29 collisions per million entering vehicles

Severity 8 collisions with 6 resulted in injury

Type 5 entering at angle (entering from side road to main road)

1 WB L hit by EB vehicle1 vehicle strikes deer1 vehicle strikes object

Volume on major road: 14,000 ADT

Volume on minor road: 2,000 ADT (estimated)

Existing Conditions

Left and right turn lanes are provided on Sophus Road and along US 101. There is a refuge or receiving lane on US 101 for vehicles entering westbound US 101 from Sophus Road.

Adequate sight distance is available.

Collisions typically occur as vehicles exceed safe speeds.

Countermeasures

1. Install a roundabout.

Level of Effort: High

Potential Safety Benefit: High (71% reduction

in collisions)

View West onto US 101



View East onto US 101



Exhibit 11. Chicken Coop Road/Zaccardo Road Detail

Chicken Coop Road / Zaccardo Road / US 101

Collision History (2009 – 2013)

Rate 0.16 collisions per million entering vehicles (0.15 for 2004 to 2013)

Severity 4 collisions with 1 resulted in injury

Type 2 rear ends (turn from main road to side roads)

1 entering at angle (entering from side road to main road)

1 vehicle strikes object

Volume on major road: 14,000 ADT

Volume on minor road: 300 ADT (Chicken Coop Road), 100 ADT (Zaccardo)

Volume on minor road. 300 ADT (Chicken Co								
Existing Conditions	Countermeasures							
Within a few hundred feet there are four access points to US 101. This includes	 Realign Zaccardo Road to T with Chicken Co Road. Reduces access points. 	ор						
Zaccardo Road, the driveway to the Tribal center, and Chicken Coop Road. The Chicken Coop Road intersection includes a	Level of Effort: Medium / Potential Safety Ber High	nefit:						
"spur" or two locations to turn onto and off of	2. Install illumination							
US 101. Chicken Coop Road intersects US 101 with	Level of Effort: Medium / Potential Safety Ber Low (8% reduction in collisions)	nefit:						
a skew (angle). Adequate sight distance is available.	 Add left turn lane from US 101 onto Chicken (Road 	Соор						
Collisions typically occur as vehicles exceed safe speeds.	Level of Effort: Medium / Potential Safety Ber High (44% reduction in collisions)	nefit:						
No illumination at intersection.	 Add right turn lane from US 101 onto Chicken Road 	Соор						
	Level of Effort: Medium / Potential Safety Ber Medium (14% reduction in collisions)	nefit:						
	5. Minor shoulder widening							
	Level of Effort: Medium / Potential Safety Ber Medium (13% reduction in collisions)	nefit:						
	6. Fix roadway skew							
	Level of Effort: Medium / Potential Safety Ber Medium (29% reduction in collisions)	nefit:						

Chicken Coop Road / Zaccardo Road / US 101 (continued)

View West onto US 101

View East onto US 101





Exhibit 12. Blyn Road Detail

Blyn Road / US 101

Collision History (2009 – 2013)

Rate 0.08 collisions per million entering vehicles

Severity 2 collisions with 1 resulted in injury

Type 1 rear end (left turn from main road to side road)

1 vehicle strikes pedestrian

Volume on major road: 14,000 ADT Volume on minor road: 100 ADT

Existing Conditions

Turn pockets do not exist at this location. (The low volume accessing Blyn Road would not warrant additional turn lanes).

Adequate sight distance is available.

Collisions typically occur as vehicles exceed safe speeds.

No illumination at intersection.

Countermeasures

1. Close access.

Level of Effort: Low

Potential Safety Benefit: High

View Southwest onto US 101



View Northeast onto US 101



Exhibit 13. Old Blyn Highway

Old Blyn Highway / US 101

Collision History (2009 - 2013)

Rate 0.12 collisions per million entering vehicles

Severity 3 collisions with 1 resulted in injury

Type 2 rear end (turns from main road to side road)

1 entering at angle (entering from side road to main road)

Volume on major road: 14,000 ADT Volume on minor road: 400 ADT

Existing Conditions	Countermeasures
Turn lanes are not provided at the intersection. Adequate sight distance is available.	Widen roadway to construct a north- eastbound left turn lane and receiving refuge
Collisions typically occur as vehicles exceed safe speeds.	lane. Level of Effort: Medium Potential Safety Benefit: High (44% reduction
No illumination at intersection.	in collisions)

View Southwest onto US 101



View Northeast onto US 101



Exhibit 14. Pierce Road Detail

Pierce Road / US 101

Collision History (2009 – 2013)

Rate 0.12 collisions per million entering vehicles

Severity 3 collisions with 2 resulted in injury

Type 3 rear end (left turns from main road to side road)

Volume on major road: 14,000 ADT Volume on minor road: 600 ADT

Existing Conditions

Turn lanes are not provided at the intersection.

Adequate sight distance is available.

Collisions typically occur as vehicles exceed safe speeds.

No illumination at intersection.

Countermeasures

 Widen roadway to construct a northeastbound left turn lane and receiving refuge lane.

Level of Effort: Medium

Potential Safety Benefit: High (44% reduction

in collisions)

View Southwest onto US 101

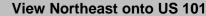






Exhibit 15. Knapp/Guiles Road Detail

Knapp / Guiles Road / US 101

Collision History (2009 – 2013)

Rate 0.04 collisions per million entering vehicles

Severity 1 collision with no injury

Type 1 rear end (turn from main road to side road)

Volume on major road: 14,000 ADT Volume on minor road: 300 ADT

Existing Conditions

Turn lanes are not provided at the intersection.

Adequate sight distance is available.

Collisions typically occur as vehicles exceed safe speeds.

No illumination at intersection.

Countermeasures

 Widen roadway to construct west and eastbound left turn lanes.

Level of Effort: Medium

Potential Safety Reposit: High

Potential Safety Benefit: High (44% reduction

in collisions)

From Guiles Road (south side of US 101)

View West onto US 101

View East onto US 101





Exhibit 16. Chicken Coop/Diamond Point Road

Chicken Coop Road / Diamond Point Road / US 101

Collision History (2009 – 2013)

Rate 0.22 collisions per million entering vehicles

Severity 6 collisions with 3 resulted in injury

Type 2 entering at angle (entering from side road to main road)

1 rear end (turn from main road to side road)

2 roadway/ditch1 head on collision

Volume on major road: 14,000 ADT Volume on minor road: 2,000 ADT

Existing Conditions	Countermeasures
East and westbound left turn lanes are provided along US 101.	Install illumination Level of Effort: Medium
Adequate sight distance is available.	Potential Safety Benefit: Low (9% reduction in
Collisions typically occur as vehicles exceed safe	collisions)
speeds.	Install a roundabout
No illumination.	Level of Effort: High Potential Safety Benefit: High (71% reduction in collisions)

From Diamond Point Road (north side of US 101)

View West onto US 101 View East onto US 101





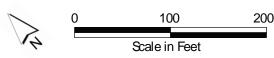
Appendix A Road Safety Audit Team

A-1 – Road Safety Audit

Name	Organization	Participation in
Annette Nesse, Chief Operations Officer	Jamestown S'Klallam	Workshop
Steve Bennett, P.E.	WSDOT	Workshop, Field visit
Happy Longfellow, P.E.	Parametrix, Roadway Design	Workshop, Field visit
Tresia Hammonds	Parametrix, Traffic Operations and Planning	Workshop, Field visit
Greg Stidham, P.E.	Parametrix, Roadway Design	Workshop, Field visit

Appendix B Maps





#	Description	Severity
1	Entering at angle	No Injury
2	NWB vehicle rear-ended	No Injury
3	NWB vehicle rear-ended	Injury
4	NWB vehicle rear-ended	No Injury

Represents general collision location

1 Happy Valley Road





#	Description	Severity
1	Entering at angle	Fatality
2	Entering at angle	No Injury
3	NB rear-ended	No Injury

Represents general collision location

2 Palo Alto Road





#	Description	Severity
1	Vehicle Strikes Object	Fatality



0 100 200 Scale in Feet

#	Description	Severity
1	SB R rear-ended	No Injury
2	Vehicle strikes deer	No Injury

Barker Road



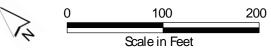


#	Description	Severity
1	Entering at angle	No Injury
2	Entering at angle	Injury
3	NB L rear-ended	Injury
4	NB L rear-ended	Injury

[#] Represents general collision location

5 Louella Road



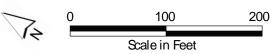


#	Description	Severity
1	SBL rear-ended	Injury
2	SBL rear-ended	Injury
3	Vehicle strikes deer	No Injury

[#] Represents general collision location

Schoolhouse Point Lane

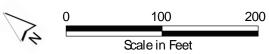




#	Description	Severity
1	Entering at angle	Injury
2	SEB rear-ended	No Injury

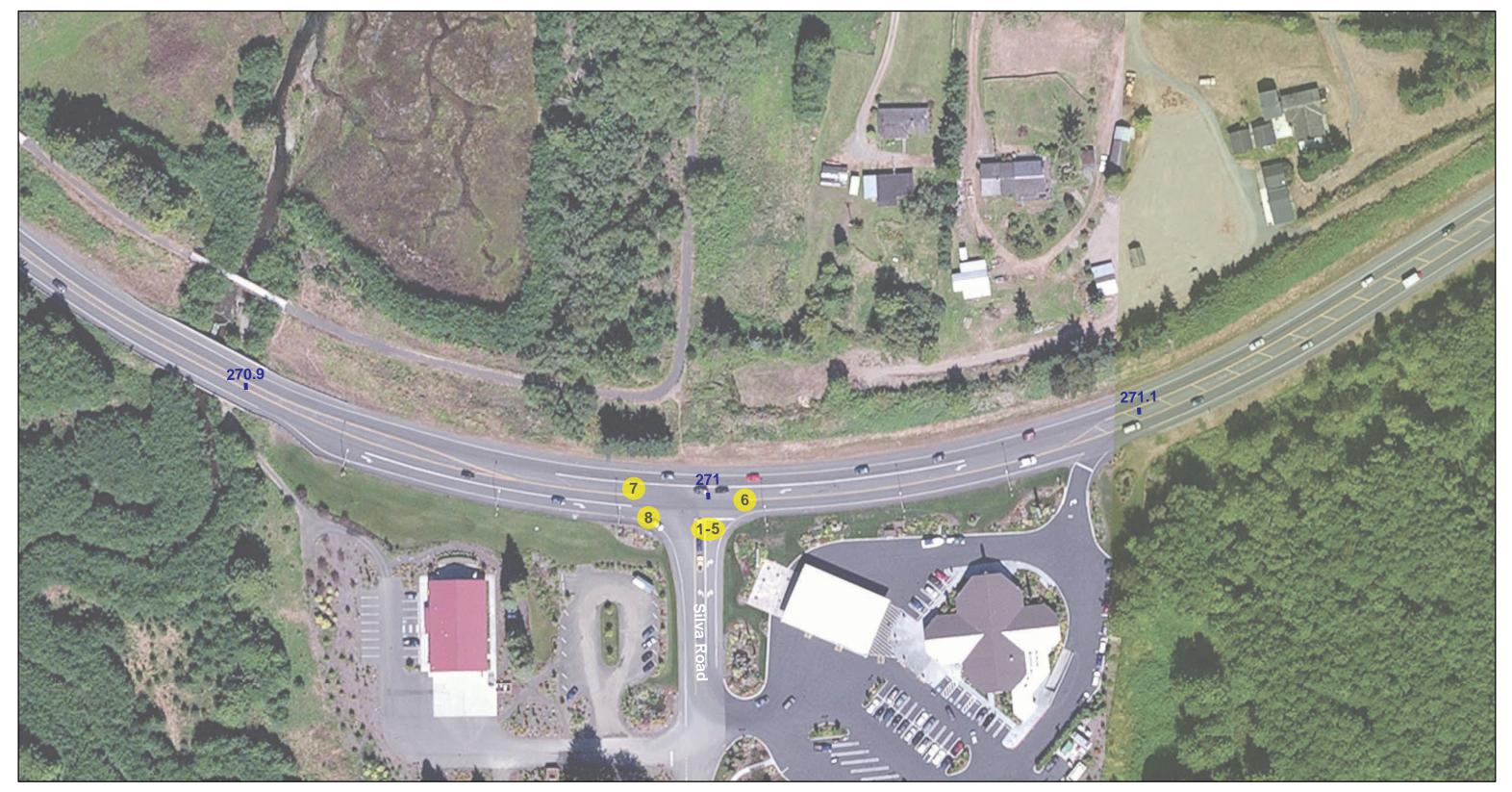
7 Corriea Road





#	Description	Severity
1	Entering at angle	No Injury
2	Entering at angle	No Injury
3	Entering at angle	No Injury
4	Entering at angle	Injury

8 Casino Driveway



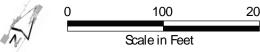
\wedge	0	100	200
$\stackrel{N}{\longrightarrow}$		Scale in Feet	

#	Description	Severity
1	Entering at angle	Injury
2	Entering at angle	Injury
3	Entering at angle	Injury
4	Entering at angle	Injury

#	Description	Severity
5	Entering at angle	No Injury
6	WB L hit by EB vehicle	Injury
7	Vehicle strikes deer	Injury
8	Vehicle strikes object	No Injury

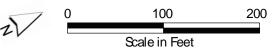
9 Silva Road





10 Snowcreek Road

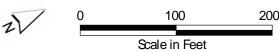




#	Description	Severity
1	SB R rear-ended	Injury
2	Vehicle strikes object	No Injury
3	Entering at angle	No Injury
4	SB L rear-ended	No Injury

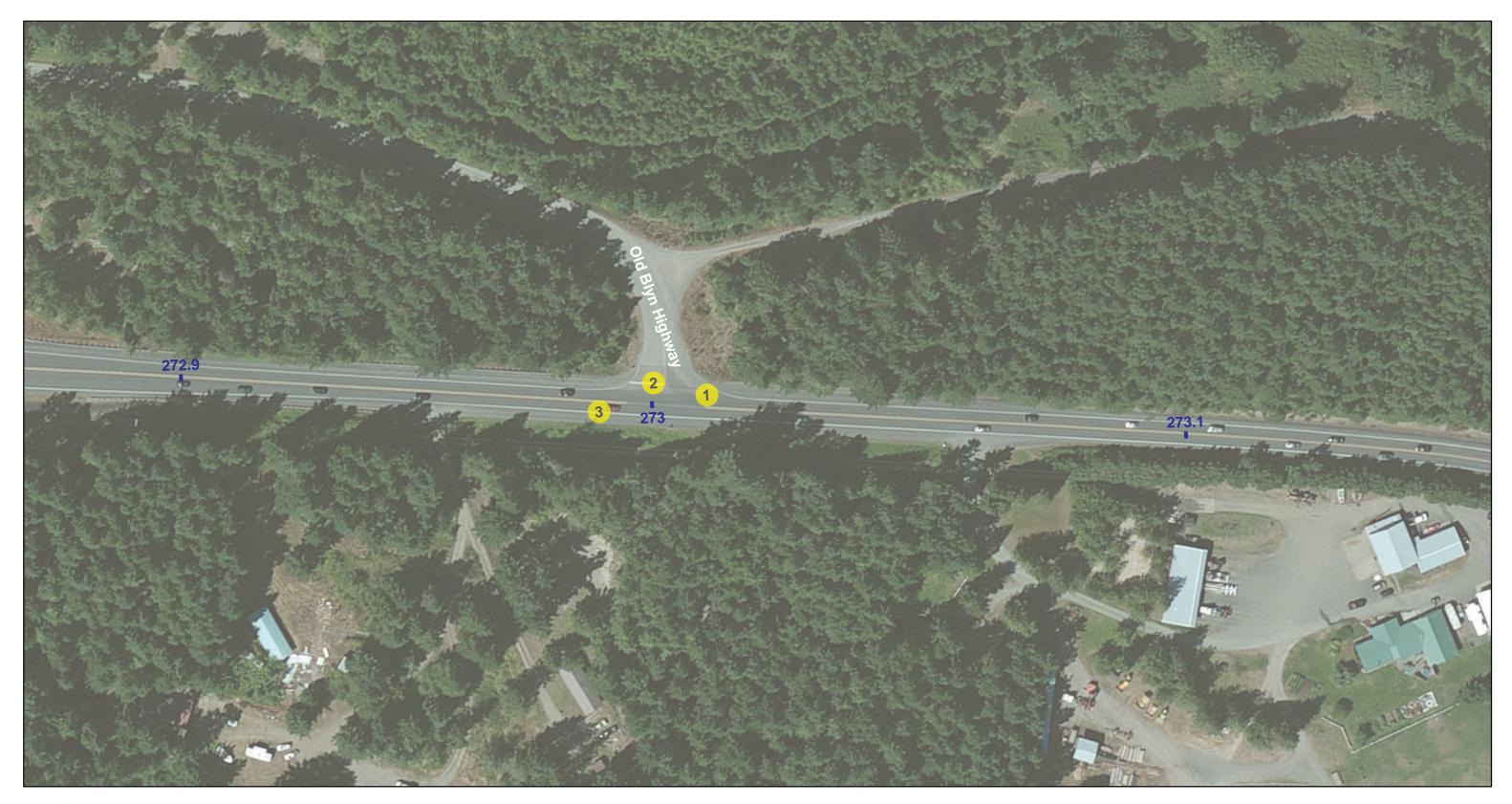
Chicken Coop Road





#	Description	Severity
1	NEB rear-ended	No Injury
2	Vehicle strikes pedestrian	Injury

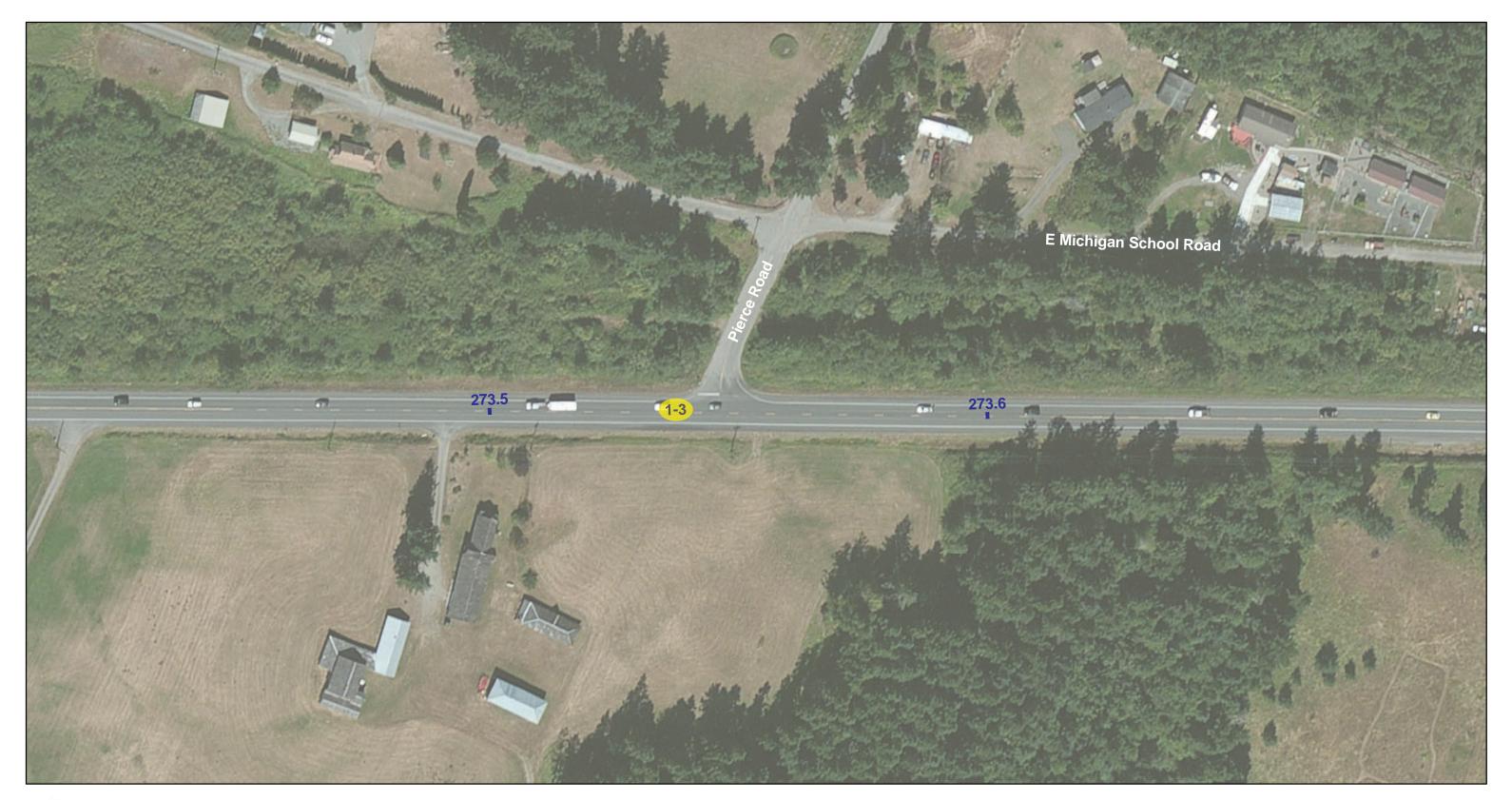
12 Blyn Road





#	Description	Severity
1	SWB R rear-ended	Injury
2	Entering at angle	No Injury
3	NEB L rear-ended	No Injury

13 Old Blyn Highway

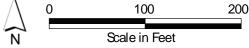


1	0	100	200
N		Scale in Feet	

#	Description	Severity
1	NEB L rear-ended	No Injury
2	NEB L rear-ended	Injury
3	NEB L rear-ended	Injury

14 Pierce Road





#	Description	Severity
1	WB rear-ended	No Injury

[#] Represents general collision location

15 Knapp/Guiles Road



$/ \setminus$	0	100	200
\overrightarrow{N}		Scale in Feet	

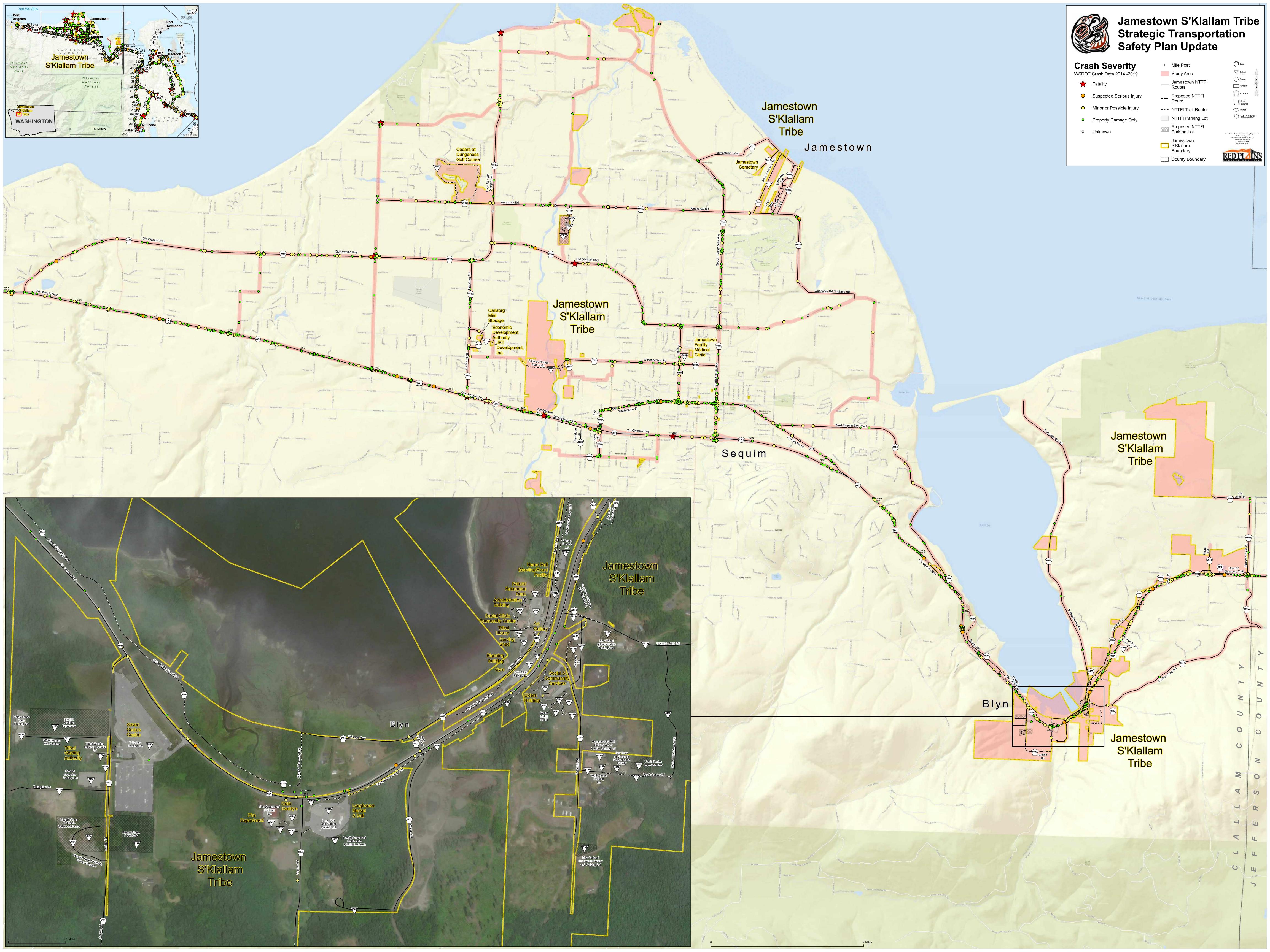
#	Description	Severity
1	WB rear-ended	No Injury
2	Entering at angle	Injury
3	Entering at angle	Injury

#	Description	Severity
4	Roadway ditch	Injury
5	Roadway ditch	No Injury
6	Head on collision	No Injury

Represents general collision location

Chicken Coop/Diamond Point







OBJECTID JURISDICTION	COUNTY CITY	TRAFFICWAY	BLOCK INTERSECTING F REF	DIST ML OF COMP_DIR REFERENCE ROOM PORT_NAME MLEPOST A SR_ONLY REPORT PORT_NAME MLEPOST B SUSPENSE_ROOM NAME.	BER DATE TIME INJURY_TYPE	NUM NUM NUM NUM NUM BIR	ES VEH_1_TTPE	VEH.2_TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD_SURFACE ROADWAY CHARACTERISTIC		RRST_COLLISION TYPE_OBJECT_STRUCK	VEH_1 ACTION	VEH_2 ACTION	VEH.1.COMPASS UREL1.COMPASS UPEL2.COMPASS URECTION, FROM DIRECTION, TO
5400 City Street 5401 City Street	Clallam Port Angeles Clallam Sequim Clallam Sequim	5TH ST E BELL ST 1	OLD OLYMPIC HWY	62 F E S SEQUIMAVE No E570	337 2015-03-13 20:08 No Apparent Injury 218 2016-08-02 15:15 No Apparent Injury	0 0 2 0	Not Stated Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Dark-No Street Lights Daylight	One parked-one moving From same direction - one left turn - one straight One parked-one moving	Making Left Turn Going Straight Ahead	Legally Parked, Unoccupied Going Straight Ahead Legally Parked, Unoccupied	South West South North West East
5401 City Street 5402 City Street 5403 City Street 5404 City Street 5405 City Street 5406 City Street 5406 City Street 5407 City Street	Clallam Sequim Clallam Sequim	E FIR ST 0 E MAPLE ST 1:	N SEQUIMAVE 00 S SEQUIMAVE	No 3759 No E495	171 2017-08-12 12-01 Suspected Minor Injury 1769 2015-12-19 13-10 No Apparent Injury 1769 2015-12-19 13-10 No Apparent Injury 1769	0 0 2 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Dickup, Panel Truck or Vanette under 10,000 lb	Page anger Car	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Paining	Dry Straight & Level Wet Straight & Level Wat Straight & Level	Daylight Daylight	Vehicle - Pedalo;clist From same direction - all others From opposite direction - one left turn - one straight	Making Right Turn	Stopped at Signal or Stop Sign	East North Vehicle Backing Vehicle Backing Vehicle Stopped Vehicle Stopped East Wart East
5405 City Street 5406 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	E SILBERHORN RD 0 E SILBERHORN RD	S SEQUIMAVE RIVER RD SILBER LN	No 3202 No 3615	64 2019-02-10 18:45 No Apparent Injury 126 2014-07-04 09:30 No Apparent Injury	0 0 2 0	O Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car	At Intersection and Related At Intersection and Related	Snowing Clear or Partly Cloudy	Wet Straight & Level Snow/Slush Straight & Level Dry Straight & Level	Dark-Street Lights On Daylight	Entering at unique and the state of the stat	Making Left Turn Starting in Traffic Lane Going Straight Ahead	Starting in Traffic Lane Going Straight Ahead Going Straight Ahead	West East South North West East West East
5407 City Street 5408 City Street 5409 City Street 5410 City Street 6410 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	E SPRUCE ST 0 E WASHINGTON ST 9	N SEQUIM AVE 00 LEE CHATFIELD WAY N BROWN RD	No 2727 No E499	84 2016-12-19 12:34 Possible Injury 84 2015-12-29 10:00 No Apparent Injury	1 0 1 1 0 0 2 0	Pickup, Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Wet Straight & Level Dry Straight & Level	Davlight	Vehicle going straight hits pedestrian From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped at Signal or Stop Sign	East West Vehicle Stopped Vehicle Stopped
5409 City Street 5410 City Street	Clallam Sequim	E WASHINGTON ST 0 E WASHINGTON ST 7	00 N BROWN RD	No 2728 No E335	139 2017-03-27 09:00 No Apparent Injury 1394 2014-06-14 13:55 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Passenger Car	At Intersection and Related At Intersection and Related	Overcast	Dry Straight & Level	Daylight	Entering at angle Entering at angle	Going Straight Ahead Going Straight Ahead Making Left Turn	Going Straight Ahead Going Straight Ahead	North South West East North East West East
5411 City Street 5412 City Street 5413 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	E WASHINGTON ST 7: E WASHINGTON ST 0: F WASHINGTON ST 3:	00 N BROWN RD N BROWN RD 10 N GOVAN AVE	No E340 No E818 No 2767	2014-2014 (1.3.25) Munipulariti sipury 2014-2014 (1.3.25) Munipulariti sipury 2014-2016 (1.3.25) (1.3.25) Munipulariti sipury 2014-2016-10-061 (1.3.25) Munipulariti sipury 2017-06-11 (1.3.25) Munipulariti sipury 2017-06-11 (1.3.25) Munipulariti sipury 2017-06-11 (1.3.25) Munipulariti sipury 2017-06-11 (1.3.25) Munipulariti sipury 2017-07-07-07-07-07-07-07-07-07-07-07-07-07	0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car	At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Going Straight Ahead Stopped for Traffic Stopped for Traffic Going Straight Ahead	West East West East East West Vehicle Stopped Vehicle Stopped East West Vehicle Stopped
5414 City Street 5415 City Street	Clallam Sequim Clallam Sequim	E WASHINGTON ST 0 E WASHINGTON ST 0	N RHODEFER RD N RHODEFER RD	No 3759	179 2017-09-11 15:40 Suspected Minor Injury 291 2017-06-01 12:46 Suspected Serious Injury	3 0 3 0	Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Other	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Grade Dry Straight & Grade	Daylight Daylight	Entering at angle Entering at angle	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead Going Straight Ahead	North South East West North South East West
5416 City Street 5417 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	E WASHINGTON ST 0 E WASHINGTON ST 1	N RHODEFER RD 00 N SEQUIMAVE	No E756 No 3046	256 2017-12-31 12:31 Suspected Minor Injury 164 2014-07-14 11:41 Possible Injury	4 0 2 0 2 0 2 0	0 Passenger Car 0 Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight Daylight	Entering at angle From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead	Going Straight Ahead Going Straight Ahead Legally Parked, Unoccupied	North South East West East West East West
5418 City Street 5419 City Street	Clallam Sequim	E WASHINGTON ST 11 E WASHINGTON ST 11	00 N SEQUIMAVE 00 N SEQUIMAVE	No 3046 No E378	166 2014-07-18 09:51 No Apparent Injury	1 0 2 0	Bus or Motor Stage Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Wet Straight & Level		One parked-one moving From same direction - both going straight - one stopped - rear-end	Changing Lanes Slowing	Legally Parked, Unoccupied Stopped at Signal or Stop Sign	East Northwest East West Vehicle Stopped Vehicle Stopped White Stopped Vehicle Stopped
5421 City Street 5422 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	E WASHINGTON ST 1: E WASHINGTON ST 3:	00 N SEQUIMAVE 00 N SUNNYSIDE AVE	No E527	509 2014-11-04 13:00 Possible Injury	1 0 2 0	0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level Dry Dry Straight & Level	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Stopped for Traffic Going Straight Ahead Going Straight Ahead	Stopped for Traffic Stopped for Traffic	Wehicle Stopped Vehide Stopped East West East West Vehide Stopped Vehide Stopped East West East Vehide Stopped
5423 City Street 5424 City Street 5425 City Street 5426 City Street	Clallam Sequim Clallam Sequim	E WASHINGTON ST 10 E WASHINGTON ST 0	000 S BLAKE AVE S BROWN RD S RHODEFER RD	No 3615 No 3759	131 2015-12-17 13:40 Suspected Minor Injury 124 2018-09-04 10:19 No Apparent Injury	2 0 2 0 0 0 2 0	Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb Truck (Flatbad Van,etc) Passenger Car Passenger Car Passenger Car Passenger Car Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Not Related At Intersection and Related	Overcast	Wet Straight & Level Dry Straight & Level	Daylight	From opposite direction - one stopped - head-on Entering at angle		Stopped for Traffic Going Straight Ahead Going Straight Ahead	East West Vehicle Stopped North South West East
5425 City Street 5426 City Street	Clallam Sequim Clallam Sequim		300 S RHODEFER RD	No 2728 No E550	001 2017-01-15 13:11 Possible Injury 066 2016-05-31 13:50 Possible Injury	1 0 2 0	0 Truck (Flatbad, Van, etc) 0 Passenger Car	Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade	Daylight Daylight	Entering at angle Building	Going Straight Ahead Making Right Turn Making Right Turn		South East West East West Southeast
5427 City Street 5428 City Street 5429 City Street 5430 City Street 5431 City Street	Clallam Sequim	E WASHINGTON ST 1: E WASHINGTON ST 0: F WASHINGTON ST 1:	S SEQUIMAVE S SEQUIMAVE S SEQUIMAVE	NO 2456 NO 3759 No E367	108 2018-02-01 16:33 No Apparent Injury 108 2018-02-01 16:33 No Apparent Injury 108 2014-09-15 12:43 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car Passenger Car Pickup Panel Truck or Vanette under 10 000 lb	At Intersection and Related At Intersection and Related Entering Roundahout	Clear or Partly Cloudy Raining Clear or Partly Cloudy	Dry Straight & Level Wet Straight & Level Dry Gurse & Grade	Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end Entering at annie	Going Straight Ahead Stopped at Signal or Stop Sign Going Straight Ahead	Stopped for Traffic Starting in Traffic Lane Going Straight Abead	East West Vehicle Stopped Vehicle Backing Vehicle Stopped Vehicle Backing Vehicle Stopped West Fast North South
5430 City Street 5431 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	E WASHINGTON ST 1: E WASHINGTON ST 0	000 S STILL RD S SUNNYSIDE AVE	No 2727 No 3759 No 3759	175 2015-04-20 11:40 Possible Injury 107 2018-01-25 15:42 No Apparent Injury	1 0 2 0	O Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Truck (Flatbad, Van,etc) O Parsenger Car. O Parsenger Car.	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade Dry Straight & Grade Wet Straight & Level	Daylight Daylight	Entering at angle From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead Stopped for Traffic Stopped for Traffic	West Wehicle Stopped Vehicle Stopped West East West East
5432 City Street 5433 City Street 5434 City Street 5435 City Street	Clallam Sequim Clallam Sequim	E WASHINGTON ST 3 E WASHINGTON ST 11	00 S SUNNYSIDE AVE 000 W SEQUIM BAY RD 000 W SEQUIM BAY RD	No E401 No 3046	771 2015-02-18 12:56 Possible Injury 167 2014-06-24 12:45 Possible Injury	1 0 2 0 1 0 2 0 0 0 1 0	0 Passenger Car 0 Pickup,Panel Truck or Vanette under 10,000 lb		At Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end Metal Sign Post	Slowing Going Straight Ahead	Slowing Going Straight Ahead	East West East West West East West East
5434 City Street 5435 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	E WASHINGTON ST 1:	100 W SEQUIMBAY RD	450 F NW BOGGS PL No E299			O Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Danasas Car	At Intersection and Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Grade Dry Straight & Grade Dry Straight & Grade	Dark-Street Lights On	Guardrail - Leading End	Making Left Turn Going Straight Ahead	Ctons and face Tradition	West North East West Month East West
5437 City Street 5438 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	E WASHINGTON ST 6 E WASHINGTON ST 8	00	131 F E CENTER PARK WAY No E587 364 F NW LEE CHATFIELD WAY No 3046	158 2016-09-22 13:20 No Apparent Injury	0 0 2 0	U Pickup,Panel Truck or vanette under 10,000 lb	Passenger Car Passenger Car	Driveway Related but Not at Driveway Driveway Related but Not at Driveway At Driveway	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Dry Straight & Level	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end From opposite direction - all others	Going Straight Ahead Going Straight Ahead Making Left Turn	Stopped for Traffic Slowing Making Right Turn Legally Parked, Unoccupied	West East West East Southeast West West West Southeast West Northeast West
5439 City Street 5440 City Street	Cialiam Sequim	E WASHINGTON ST 1: E WASHINGTON ST 1:	00	25 F W N SUNNYSIDE ANE No E954 30 F E S SEQUIM AVE No 3046 75 F E S SEQUIM AVE No E943	69 2014-11-15 19:13 No Apparent Injury	0 0 2 0	0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level		One parkedone moving From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead	Other*	East West East West East
5441 City Street 5442 City Street	Clallam Sequim Clallam Sequim	E WASHINGTON ST 1: E WASHINGTON ST 2:	00	75 F E S SEQUIMAVE No E943 207 F E S SUNNYSIDE AVE No E862 75 F NW SEQUIMBAY RD No E311	572 2019-07-25 17:17 No Apparent Injury 588 2018-11-08 16:01 No Apparent Injury 592 2014-02-16 06:08 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Net Stand	Passenger Car Truck (Flatbad, Van, etc)	Intersection Related but Not at Intersection At Driveway Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight Daylight	From same direction - both going straight - both moving - sideswipe From same direction - both going straight - both moving - sideswipe	Changing Lanes Changing Lanes Going Straight Ahead	Going Straight Ahead Going Straight Ahead	East West East West East West East West
5444 City Street 5445 City Street	Clallam Sequim Clallam Sequim	E WASHINGTON ST 1: E WASHINGTON ST 1:	200	O.S.M. NIW SHADES DD No. 2787	59 2016-04-23 12:35 Unknown 307 2017-07-24 13:16 No Apparent Injury	0 0 1 0	Not Stated Pickup Panel Truck or Vanette under 10 000 lb.	Passenger Car	Not at Intersection and Not Related	Unknown Clear or Partly Cloudy	Wet Straight & Level Unknown Curve in Sag Dry Straight & Grade	Unknown	Tree or Stump (stationary) Fence Force annue	Other*	Stopped at Signal or Stop Sign	West North Vehicle Stopped Vehicle Stopped
5444 City Street 5445 City Street 5446 City Street 5447 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	E WASHINGTON ST 1: GRANT RD 0	000 RIVER RD	451 F SE W SEQUIM BAY RD No E759 120 F NW W SEQUIM BAY RD No E920 No E920 No E920	93 2019-03-21 18:46 No Apparent Injury 900 2019-05-14 17:19 Possible Injury	0 0 2 0 2 0	O Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car School Bus	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Level Dry	Daylight Daylight	Entering at angle From same direction - both going straight - both moving - rear-end Entering at angle	Making Left Turn Going Straight Ahead Making Left Turn Making Left Turn	Slowing Going Straight Ahead	West East West East West North North South
5448 City Street 5449 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	N 3RD AVE 11 N 5TH AVE 0	00 W WASHINGTON ST N LITTLEJOHN WAY W CEDAR ST	No E407 No E947	335 2015-03-12 07:42 No Apparent Injury 344 2019-08-03 15:10 No Apparent Injury 358 2016-31-26 13:24 No Apparent Injury	0 0 2 0	O Passenger Car School Bus Passenger Car O Pickup Panel Truck or Vanette under 10,000 lb O Pickup Panel Truck or Vanette under 10,000 lb O Pickup Panel Truck or Vanette under 10,000 lb O Pickup Panel Truck or Vanette under 10,000 lb O Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Not Related At Driveway within Major Intersection	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level		Entering at angle From same direction - both going straight - both moving - rear-end	Making Left Turn Going Straight Ahead	Stopped at Signal or Stop Sign Going Straight Ahead Making Left Turn	West North Vehicle Stopped Vehicle Stopped South North South North North
5451 City Street 5452 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	N 5TH AVE 0	W FIR ST W FIR ST	No E613 No 2727 No 1200	100 2016-11-26 12:34 No Apparent Injury 100 2016-11-16 12:23 No Apparent Injury 146 2019-12-03 13:33 Possible Injury	0 0 2 0	O Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb O Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car Passenger Car	At Intersection and Related At Intersection and Related	Raining Clear or Partly Cloudy Overcast	Dry Straight & Level	Daylight Daylight	Entering at angle Entering at angle Entering at angle	Going Straight Ahead Making Left Turn Going Straight Ahead Going Straight Ahead	Going Straight Ahead	POPULA NORTH NORTH East East West North South East West North South
5452 City Street 5453 City Street 5454 City Street 5455 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	N 5TH AVE 0 N 5TH AVE 6	W FIR ST 00 W HENDRICKSON RD	No E740 No E462	46 2019-12-03 13:33 Possible Injury 192 2017-11-20 14:55 No Apparent Injury 343 2015-09-16 17:00 Suspected Minor Injury	0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight	Entering at angle Vehicle turning left hits pedestrian	Going Straight Ahead Starting in Traffic Lane Making Left Turn	Going Straight Ahead Going Straight Ahead	West East North South West North
5456 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	N 5TH AVE 0	00 W HENDRICKSON RD 00 W HENDRICKSON RD W SPRUCE ST	No E502 No 2728	125 2016-01-06 11:15 Possible Injury 115 2017-12-01 12:29 No Apparent Injury	1 0 3 0 0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Raining	Dry Straight & Level Dry Straight & Level Wet Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end Entering at angle	Making Left Turn Going Straight Ahead Making Left Turn	Slowing Going Straight Ahead	North South North South West North South North
5457 City Street 5458 City Street 5459 City Street 5460 City Street	Clallam Sequim Clallam Sequim	N 5TH AVE 3 N 5TH AVE 3	00 W SPRUCE ST 00 W SPRUCE ST 10 W WASHINGTON ST	No 3046 No E477	891 2015-10-29 14:37 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Wet Straight & Level	Daylight Daylight	Entering at angle Entering at angle Street Light Pole or Base	Starting in Traffic Lane Starting in Traffic Lane Making Right Turn Going Straight Ahead	Going Straight Ahead Going Straight Ahead	East West North South East West North South North West North North
5460 City Street 5461 City Street	Clallam Sequim Clallam Sequim	N 5TH AVE N 5TH AVE	W WASHINGTON ST W WASHINGTON ST	No 3202 No E538	172 2014-02-19 16:37 No Apparent Injury 129 2014-02-26 09:15 No Apparent Injury 185 2016-04-19 15:35 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry	Daylight Daylight	Seert Light Follow base Entering at angle From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Stopped at Signal or Stop Sign	Stopped at Signal or Stop Sign Going Straight Ahead	Vesici Vesici South North Vehicle Stopped Vehicle Stopped North South
5461 City Street 5462 City Street 5463 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	N 5TH AVE 0 N 5TH AVE 5	W WASHINGTON ST	No E955 20 F S EUREKAWAY No E690	 2016-04-19 15:35 No Apparent Injury 334 2019-08-31 13:56 Suspected Minor Injury 2017-07-11 15:11 Possible Injury 2016-05-11 20:42 Possible Injury 	1 0 1 0	0 Truck Tractor & Semi-Trailer 0 Pickup, Panel Truck or Vanette under 10,000 lb 0 Passenger Car 1 Passenger Car 1 Plassenger Totar 0 Pickup, Panel Truck or Vanette under 10,000 lb 0 Pickup, Panel Truck or Vanette under 10,000 lb 0 Passenger Car 0 Pickup, Panel Truck or Vanette under 10,000 lb 0 Passenger Car 0 Pickup, Panel Truck or Vanette under 10,000 lb 0 Passenger Car 0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Driveway	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight Daylight	rioni same directivo i con gong staggir - one supped - rearrend Entering at angle Entering at angle	Making Left Turn Going Straight Ahead	Going Straight Ahead	North West South North
5464 City Street 5465 City Street	Clallam Sequim Clallam Sequim	N 5TH AVE 8 N 5TH AVE 1	000		66 2016-05-11 20:42 Possible Injury 151 2019-05-28 09:19 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		At Driveway Not at Intersection and Not Related Intersection Related but Not at Intersection	Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight	Unity Pole Fire Hydrant	Making Right Lurn		South North West South
5466 City Street 5467 City Street 5468 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	N 5TH AVE 6	00	477 F N W HENDRICKSON RD No 2767 107 F N W HENDRICKSON RD No 3800 113 F N W HENDRICKSON RD No E528	35 2016-09-09 13:49 No Apparent Injury 162 2019-08-01 12:01 Possible Injury 194 2016-03-25 15:55 Possible Injury	1 0 2 0	O Passenger Car O Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry		From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic Stopped for Traffic Stopped for Traffic	South North South West North South North South North South North South North South
5469 City Street 5470 City Street	Clallam Seguim Clallam Seguim	N 5TH AVE 6	00	50 F N WHENDRICKSON RD No E723	144 2017-10-16 08:56 Possible Injury	1 0 2 0	0 Pickup Panel Truck or Vanette under 10 000 lb	Pickup Panel Truck or Vanette under 10 000 lb	Intersection Related but Not at Intersection	Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - both moving - rear-end		Slowing	North South North South North South Vehicle Stopped Vehicle Stopped
5471 City Street 5472 City Street	Clallam Sequim Clallam Sequim	N 5TH AVE 6 N 5TH AVE 7	00	200 F N W HENDRICKSON RD No E918 95 F N W HENDRICKSON RD No E902 43 F S W MINSTREL RD No 2767	33 2016-09-07 12:19 No Apparent Injury	2 0 3 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Driveway	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level		From same direction - both going straight - one stopped - sideswipe From same direction - both going straight - both mowing - rear-end Entering at angle	Going Straight Ahead Going Straight Ahead Making Right Turn	Stopped for Traffic Going Straight Ahead Stopped for Traffic	North South North South East North South North
5473 City Street 5474 City Street 5475 City Street 5476 City Street 6477 City Street	Clallam Sequim Clallam Sequim	N 5TH AVE 6 N 5TH AVE 1	00	314 F N W OAK ST No 2978* 250 F N W WASHINGTON ST No 3800 50 F N W WASHINGTON ST No 3803	765 2018-06-23 12:36 No Apparent Injury 168 2019-09-11 14:17 No Apparent Injury	0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Truck (Flatbad,Van.etc)	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Driveway	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight	Entering at angle Entering at angle	Making Left Turn	Going Straight Ahead Going Straight Ahead	East North North North East South North South
5475 City Street 5476 City Street	Clallam Sequim Clallam Sequim	N 7TH AVE	W HENDRICKSON RD W WASHINGTON ST	200 F N WASHINGTON ST NO 3803 50 F N WWASHINGTON ST NO 3202 No 3202	31 2014-03-12 07:51 No Apparent Injury 74 2019-01-13 09:50 Possible Injury	0 0 2 0	O Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Truck (Flatbad,Van,etc)	Passenger Car Passenger Car	Not at Intersection and Not Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - both moving - rear-end Entering at angle Vehicle turning left hits pedestrian	Going Straight Ahead Making Left Turn Making Left Turn	Going Straight Ahead Going Straight Ahead Making Left Turn	North South North South South West East South North East
5478 City Street 5479 City Street	Clallam Sequim Clallam Sequim	N KENDALL RD 0	W HENDRICKSON RD W HENDRICKSON RD	No 2727 No 2727 No E302	174 2019-01-13-09-30 Possible III of 199 2016-11-11 14:48 No Apparent Injury 1741 2014-01-26 17:40 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Related At Intersection and Related	Clear or Parity Cloudy Fog or Smog or Smoke Clear or Parity Cloudy	Dry Straight & Level Dry Straight & Level Wet Straight & Level	Daylight Dark-No Street Lights	From same direction - all others Fence	Backing Going Straight Ahead	Going Straight Ahead	North South North South North South
5480 City Street 5481 City Street	Clallam Sequim Clallam Sequim	N KENDALL RD 7/ N KENDALL RD 0	00 W HENDRICKSON RD W HENDRICKSON RD	No E368 No E677	199 2016-11-11 14:48 No Apparent Injury 199 2016-11-11 14:48 No Apparent Injury 199 2014-10-22 11:04 No Apparent Injury 199 2014-10-22 11:04 No Apparent Injury 199 2017-06-01 19:00 No Apparent Injury 199 2017-06-01 19:00 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Related At Driveway within Major Intersection	Clear or Partly Cloudy Overcast	Dry Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - one stopped - rear-end Fence	Stopped at Signal or Stop Sign Making Right Turn Making Left Turn	Going Straight Ahead	Vehicle Stopped Vehicle Stopped North South East North
5482 City Street 5483 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	N KENDALL RD 7: N SEQUIM AVE N SEQUIM AVE 4	E FIR ST	50 F N W HENDRICKSON RD No 2728 No 2727	105 2017-04-18 12:10 No Apparent Injury 183 2016-12-12 08:58 No Apparent Injury 193 2015-10-05 09:00 No Apparent Injury 195 2016-10-05 09:30 No Apparent Injury 195 2016-10-05 09:30 No Apparent Injury 195	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection At Intersection and Related	Clear or Partly Cloudy Overcast	Dry Straight & Level Wet Straight & Level Dry Straight & Level	Daylight Daylight	Entering at angle Entering at angle	Making Left Turn Going Straight Ahead	Going Straight Ahead Going Straight Ahead	West North North South West East South North
5485 City Street	Clallam Sequim		IO E FIR ST	No E409 No E506 No E617	895 2016-01-16 09:33 No Apparent Injury 896 2016-01-16 09:68 Poperible Injury	0 0 2 0	0 Passenger Car 1 Passenger Car	Passenger Car	At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Grade	Daylight	Entering at angle Entering at angle Vehicle Strikes Pedaloyclist	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead Going Straight Ahead Going Straight Ahead	West East South North East West South North South North
5487 City Street 5488 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	N SEQUIM AVE 0 N SEQUIM AVE 0	E FIR ST E SPRUCE ST E WASHINGTON ST	No E649 No 3202	136 2017-03-04 10:20 No Apparent Injury 172 2019-05-04 09:18 Possible Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level		Entering at angle Vehicle turning right hits pedestrian Street Light Poor Base	Going Straight Ahead Making Right Turn	Starting in Traffic Lane	North South West East East North
5489 City Street 5490 City Street	Clallam Sequim Clallam Sequim	N SEQUIM AVE 1: N SEQUIM AVE 1:	00 E WASHINGTON ST 00 E WASHINGTON ST	No 3615 No E397	183 2016-05-25 16:07 No Apparent Injury 191 2015-02-02 16:01 No Apparent Injury	0 0 1 0	Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Passenger Car Passenger Car	Passenger Car	At Intersection and Related At Driveway within Major Intersection	Overcast	Dry Straight & Level Dry Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Making Right Turn	North South North West
5490 City Street 5491 City Street 5492 City Street 5493 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	N SEQUIMAVE 11 N SEQUIMAVE 0	OLD OLYMPIC HWY OLD OLYMPIC HWY	No E459 No 2260	137 2015-08-28 15:21 No Apparent Injury 153 2015-01-09 07:00 No Apparent Injury 147 2017-12-17 19-28 No Apparent Injury	0 0 2 0	O Passenger Car O Polypare Truck or Vanette under 10,000 lb	Passenger Car Passenger Car	At Intersection and Related Entering Roundabout Entering Roundabout	Clear or Partly Cloudy Fog or Smog or Smoke	Wet Straight & Level Wet Straight & Level Wet Curp & Level	Daylight Dark-Street Lights On Dark-Street Lights On	From same direction - both going straight - one stopped - rear-end Entering at angle Traffic Island	Going Straight Ahead Going Straight Ahead Going Straight Ahead Making Right Turn Going Straight Ahead	Stopped at Signal or Stop Sign Making Left Turn	North South Vehicle Stopped Vehicle Stopped South North West North South North
5494 City Street 5495 City Street	Clallam Sequim Clallam Sequim	N SEQUIM AVE 9 N SEQUIM AVE	300 OLD OLYMPIC HWY	No E412	139 2015-03-28 14:35 No Apparent Injury 179 2016-01-24 23:06 Possible Injury	0 0 2 0	0 Passenger Car 0 Passenger Car	Passenger Car	Entering Roundabout Entering Roundabout	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Level	Daylight	Entering at angle	Making Right Turn Going Straight Ahead	Other*	North West East North South North
5496 City Street 5497 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	N SEQUIM AVE 9 N SEQUIM AVE 9	OLD OLYMPIC HWY OLD OLYMPIC HWY OLD OLYMPIC HWY	No E636 No E641	101 2017-01-27 08:58 Suspected Minor Injury 336 2017-02-05 19:40 No Apparent Injury	1 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Entering Roundabout Entering Roundabout Entering Roundabout	Clear or Partly Cloudy Clear or Partly Cloudy Blowing Sand or Dirt or Snow	Dry Straight & Level Snow/Slush Curve & Level	Dark-Street Lights On Daylight Dark-No Street Lights	Linear Curb Entering at angle	Going Straight Ahead Going Straight Ahead Merging (Entering Traffic)	Other*	North South West East North South
5498 City Street 5499 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	N SEQUIM AVE N SEQUIM AVE	PORT WILLIAMS RD	No E367	549 2014-10-22 10:15 Possible Injury 800 2016-02-13 18:17 Possible Injury	1 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	Entering Roundabout Entering Roundabout At Intersection and Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy			Traffic Island	Merging (Entering Traffic) Going Straight Ahead Going Straight Ahead	Going Straight Ahead	South North West East South North
5500 City Street 5501 City Street	Clallam Sequim	N SEQUIM AVE 0 N SEQUIM AVE 0	PORT WILLIAMS RD PORT WILLIAMS RD PORT WILLIAMS RD W CEDAR ST	No E657 No E889 No 2770	961 2017-03-28 17:40 No Apparent Injury 902 2019-02-04 12:15 No Apparent Injury 973 2019-01-04 18:29 Surported Minor Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related Entering Roundabout	Overcast	Snow/Slush Straight & Level	Dark-Street Lights On Daylight Daylight	Entering at angle From same direction - both going straight - one stopped - rear-end Walkide poing straight bits nodes trian	Going Straight Ahead Slowing Going Straight Ahead	Stopped for Traffic Stopped at Signal or Stop Sign	South North Vehicle Stopped Vehicle Stopped South North Vehicle Stopped Vehicle Stopped Marth South
5502 City Street 5503 City Street 5504 City Street 5505 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	N SEQUIM AVE 0 N SEQUIM AVE 0	W CEDAR ST W CEDAR ST	No 3759 No 3800	113 2017-07-21 09:29 No Apparent Injury 167 2019-09-01 12:28 No Apparent Injury	0 0 2 0	Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Driveway within Major Intersection	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight	Vehicle going straight hits pedestrian From same direction - both going straight - one stopped - rear-end From same direction - one left sun - one straight	Going Straight Ahead Going Straight Ahead Making Left Turn	Stopped for Traffic Making Left Turn	South North South North North South North East
	Clallam Sequim Clallam Sequim	N SEQUIM AVE 2 N SEQUIM AVE 0	00 W CEDAR ST W DEYTONA ST	NO E680	233 2017-06-04 11:50 INO Apparent injury	0 0 1 0	U Prokup,Paner Truck or vanette under 10,000 ib		At Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight	Linear Curb Tree or Stump (stationary)	Going Straight Ahead		South West South North
5507 City Street 5508 City Street 5509 City Street	Clallam Sequim Clallam Sequim	N SEQUIM AVE 0 N SEQUIM AVE 0	W FIR ST W SPRUCE ST	No E674 No 2728	38 2017-05-25 07:48 No Apparent Injury	0 0 2 0	0 Passenger Car	Passenger Car	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight	Entering at angle Entering at angle	Going Straight Ahead Going Straight Ahead Starting in Traffic Lane	Going Straight Ahead Going Straight Ahead	West East South North West East South North East South North South North
5510 City Street 5511 City Street	Clallam Sequim Clallam Sequim	N SEQUIMAVE 0 N SEQUIMAVE 1:	W SPRUCE ST 00 W WASHINGTON ST	No E988	197 2019-11-17 16:59 Suspected Minor Injury 125 2015-12-01 08:09 No Apparent Injury	1 0 1 1	O Pickup Panel Truck or Vaneete under 10,000 is Passenger Car Passenger Car Passenger Car Plassenger Car Plass	Passenger Car	At Intersection and Related At Intersection and Not Related	Overcast Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level	Daylight Dark-No Street Lights Daylight		Going Straight Ahead Going Straight Ahead	Going Straight Ahead Stopped for Traffic	North South South Vehicle Stopped
5512 City Street 5513 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	N SEQUIM AVE 1: N SEQUIM AVE 5:	00 W WASHINGTON ST	422 F N E FIR ST No 3759	342 2015-09-14 16:39 No Apparent Injury 116 2018-04-10 14:53 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related Driveway Related but Not at Driveway	Clear or Partly Cloudy Overcast	Dry Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - one stopped - sideswipe From same direction - both going straight - both moving - sideswipe From same direction - both going straight - one stopped - rear-end	Changing Lanes Changing Lanes Going Straight Ahead	Stopped at Signal or Stop Sign Going Straight Ahead Illegally Parked, Occupied	South North Vehicle Stopped Vehicle Stopped North South North South
5514 City Street 5515 City Street	Clallam Sequim	N SEQUIMAVE 5 N SEQUIMAVE 2	00	444 F N E FIR ST NO E321 100 F N W CEDAR ST NO 2729 80 E N W CEDAR ST NO E986	10 2014-04-10 22:35 No Apparent Injury 125 2015-06-08 09:16 Suspected Minor Injury 132 2019-08-11 20-13 No Apparent Injury	1 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Intersection Related but Not at Intersection	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Met Straight & Level	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end Matel Sing Port	Going Straight Ahead	Going Straight Ahead	North South North South North South South North South
5509	Clallam Sequim Clallam Sequim Clallam Sequim	N SEQUIM AVE 5 N SEQUIM AVE 6	00	100 F N W CEDAR ST No 2729 50 F N W CEDAR ST No E954 492 F S W HENDRICKSON RD No 32020 490 F S W HENDRICKSON RD No E913	90 2015-10-26 08:56 Suspected Minor Injury 082 2019-04-19 15:05 No Apparent Injury	1 0 1 0	O Pickup Danel Track or Vannetse under 10,000 lb Passanger Car 1 Passanger Car 1 Pickup Panel Track or Vannetse under 10,000 lb O Pickup Panel Track or Vannetse under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10.000 lb	At Driveway At Driveway	Clear or Partly Cloudy Raining	Dry Straight & Level Wet Straight & Level	Daylight Dark-Street Lights On Daylight Daylight	Metal Sign Post Vehicle - Pedalcyclist Entering at angle	Going Straight Ahead Making Right Turn	Going Wrong Way on Divided Hwy Going Straight Ahead	
5618 City Steet 5519 City Steet 5510 City Steet 5511 City Steet 5521 City Steet 5521 City Steet 5522 City Steet 5523 City Steet 5525 City Steet 5525 City Steet 5526 City Steet 5526 City Steet 5527 City Steet 5528 City Steet 5529 City Steet 5520 City Steet 5521 City Steet 5523 City Steet 5524 City Steet 5525 City Steet 5526 City Steet	Clallam Sequim	N SEQUIM AVE 3	00	76 F N W SPRUCE ST No 3759 30 F N W WASHINGTON ST NO E853	04 2017-05-13 17:48 No Apparent Injury 509 2018-10-26 13:58 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Intersection Related but Not at Intersection	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry	Daylight Daylight	One parked-one moving. From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - side-swipe	Going Straight Ahead Going Straight Ahead	Legally Parked, Unoccupied Stopped for Traffic	North South North Vehicle Stopped North South Vehicle Stopped Vehicle Stopped
5521 City Street 5522 City Street			N SEQUIM AVE SEQUIM DUNGENESS WAY	No E652 No E874	2017-03-10 15:06 No Apparent Injury 167 2018-12-19 17:00 No Apparent Injury	0 0 2 0	0 Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car Passenger Car	M. Intersection and Not Related Entering Roundabout	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Curve & Level	Dark-Street Lights On	From same direction - all others	Overtaking and Passing Backing Making Right Turn	Other* Stopped for Traffic	West East West East Vehicle Backing Vehicle Stopped Vehicle Stopped
5524 City Street 5525 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	PORT WILLIAMS RD 0 PORT WILLIAMS RD 0 PORT WILLIAMS RD 2	00	71 F SE SEQUIMDUNGENESS WAY No E383 93 F W STONE FARMED No E941	904 2014-12-14 15:15 Possible Injury 172 2019-06-24 14:19 No Apparent Injury	1 0 2 0	Pickup Panel Truck or Variette under 10,000 Ib Pickup Panel Truck o	Pickup,Panel Truck or Vanette under 10,000 lb	Entering Roundabout Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level	Daylight	Entering at angle From same direction - both going straight - both moving - rear-end Mailbox	Making Right Turn Going Straight Ahead Going Straight Ahead Making Left Turn	Going Straight Ahead Going Straight Ahead	East North South North East West East West Wost East West
5526 City Street 5527 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	RIVER RD 6 RIVER RD 0	00 E SILBERHORN RD E SILBERHORN RD	No 2457 No 3769	111 2015-08-04 12:19 No Apparent Injury 180 2017-09-11 17:04 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight	Entering at angle Entering at angle		Going Straight Ahead Going Straight Ahead	North East Vehicle Backing Vehicle Backing East West South North
5528 City Street 5529 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	RIVER RD 0 RIVER RD 0	E SILBERHORN RD W WASHINGTON ST W WASHINGTON ST	No 3803 No E352	343 2019-11-20 15:42 No Apparent Injury 389 2014-08-30 16:52 No Apparent Injury 387 2019-07-04 17:49 Separent Injury	0 0 2 0	0 Pickup Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related Entering Roundabout	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Curve & Level Dry Curve & Level	Daylight Daylight	Entering at angle Entering at angle Entering at angle	Starting in Traffic Lane	Going Straight Ahead Going Straight Ahead	East West North South East North South North East West North North
5531 City Street 5532 City Street	Clallam Sequim	RIVER RD 0		No E938 162 F N GRANTRD No 2727 230 F N GRANTRD No 3046	188 2016-07-14 13:00 No Apparent Injury 189 2014-07-07 08:07 No Apparent Injury	0 0 2 0	O Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb O Pickup, Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway At Driveway	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Overcast	Dry Curve & Level Dry Straight & Level Dry Straight & Level	Daylight	Entering at angle Entering at angle From same direction - one left turn - one straight	Making Right Turn Going Straight Ahead Making Right Turn Making Left Turn	Making Left Turn Stopped at Signal or Stop Sign Going Straight Ahead	East West North West South East Vehicle Stopped South West South North
5533 City Street 5534 City Street	Clallam Sequim Clallam Sequim	RIVER RD 11	00	102 F N SRAVERO NO 2222 230 F N SRAVERO NO 3046 161 F S GRAVERO No 3056 161 F S GRAVERO No 3075 155 F N GRAVERO No 3075 155 F N GRAVERO No 3080 141 F S GRAVERO NO	145 2016-03-03 09:02 No Apparent Injury 109 2018-02-06 13:54 No Apparent Injury	0 0 2 0	O Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb Oliver, Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Driveway	Clear or Partly Cloudy Raining	Wet Straight & Level Wet Straight & Grade	Daylight Daylight	From same direction - both going straight - both moving - rear-end Entering at angle	Making Left Ium Going Straight Ahead Going Straight Ahead Making Left Turn	Slowing Going Straight Ahead	South North South North
5535 City Street 5536 City Street	Clallam Seguim	RIVER RD 1	00	155 F N GRANTRD No 3800 143 F S GRANTRD No E414 164 F N GRANTRD No E901						Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight	Entering at angle	Making Lett Turn	Going Straight Ahead Going Straight Ahead Going Straight Ahead	East South North South West North South North
5537 City Street 5538 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	RIVER RD 1: RIVER RD 1: S 2ND AVE	00 00 W WASHINGTON ST	No E406	909 2015-03-11 14:27 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	At Driveway	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Raining	Dry Straight & Level Dry Straight & Level Wet	Daylight Dark-Street Lights On Daylight Daylight	Entering at angle Entering at angle Following at angle	Slowing Making Left Turn Going Straight Ahead	Going Straight Ahead Going Straight Ahead Going Straight Ahead	East West South North
5540 City Street 5541 City Street	Cialiam Sequim Cialiam Sequim Cialiam Sequim Cialiam Sequim Cialiam Sequim	S 3RD AVE 0 S 3RD AVE 1: S 3RD AVE 1:	00 W WASHINGTON ST	No E941 No 3048 29 F S W WASHINGTON ST No E872					At Intersection and Related At Intersection and Related Intersection Related but Not at Intersection	Clear or Partly Cloudy	Dry Straight & Level	Daylight	Entering at angle From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - sideswipe	Going Straight Ahead	Stopped at Signal or Stop Sign Changing Lanes	South North East West South North South Vehicle Stopped South North South North
5549 Lify Street 5540 City Street 5541 City Street 5542 City Street 5543 City Street 5543 City Street 5544 City Street 5545 City Street 5545 City Street 5546 City Street 5547 City Street 5547 City Street	Cialiani Seguini	3 DIH AVE	W WASHINGTON ST 00 W WASHINGTON ST	No 2728 No 2726	138 2017-03-24 10:58 No Apparent Injury 155 2016-04-15 16:02 Suspected Minor Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	rassinger our	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight	Other Objects Vehicle turning right hits pedestrian	Going Straight Ahead Making Right Turn Making Right Turn Making Left Turn		South East South East
5544 City Street 5545 City Street	Cialiam Sequim	S 5TH AVE 0	W WASHINGTON ST E SILBERHORN RD W WASHINGTON ST	No 3759 No 2728	05 2017-07-06 07:30 Suspected Minor Injury 102 2017-04-08 14:44 No Apparent Injury	0 0 1 1	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Passenger Car		At Intersection and Related At Intersection and Related At Driveway within Major Intersection	Clear or Partly Cloudy	Dry Straight & Level	Daylight	Vehicle turning left hits pedestrian	Making Left Turn Making Left Turn		South West West North
5547 City Street	Clallam Sequim	S 7TH AVE 0 S 7TH AVE 1: S 7TH AVE 0 S 7TH AVE 0	00 W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	No 2727 No 3202 No F980	61 2018-05-25 21:23 Suspected Minor Injury	1 0 1 1	O Passenger Car O Pickup Panel Truck or Vanette under 10,000 ib O Pickup Panel Truck or Vanette under 10,000 ib O Pickup Panel Truck or Vanette under 10,000 ib O Passenger Car O Passenger Car O Passenger Car O Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb		Overcast Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Dark-Street Lights On	Uslihy Box Vehicle turning left hits pedestrian From opposite direction - one left turn - one straight From opposite direction - one left turn - one straight	Making Left Turn Making Left Turn Making Left Turn Making Left Turn	Going Straight Ahead Going Straight Ahead	E351 South
5549 City Street 5550 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	S 7TH AVE 1: S PRIEST RD 1:	273	132 F N E SILBERHORN RD NO E541 No E568 No E568	294 2016-05-08 08:24 No Apparent Injury 327 2016-07-29 14:15 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Truck (Flatbad, Van, etc)	Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Level Dry Curve & Level	Daylight Daylight	Hetal Spg8 Text Streech - one let sur he straight. Hetal Spg8 Text Streech - one let sur he straight. From same direction - both going straight - both moving - sides wipe From opposite direction - one left sur - one right turn.	Making Left Turn Making Left Turn Starting in Traffic Lane Making Right Turn		Southwest North South South
5551 City Street 5552 City Street	Clallam Sequim	S RHODEFER RD 0	W WASHINGTON ST E WASHINGTON ST	No E823 No 2727	152 2018-07-31 13:19 No Apparent Injury 150 2015-12-22 15:20 Suspected Minor Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Overcast	Wet Straight & Grade	Daylight	Vehicle - Pedalcyclist	Making Right Turn Making Left Turn	Starting in Traffic Lane Making Left Turn	North West South West South West
5553 City Street 5554 City Street	Clallam Sequim	S KHODEFER RD S SEQUIMAVE 9	E WASHINGTON ST 300 ALLEY S OF W WASHINGTON ST E ETTA ST E ETTA ST	No E400 No 2727	127 2015-02-09 08:01 Possible Injury 192 2016-10-06 20:21 No Apparent Injury	0 0 2 0	Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Related	Overcast Blowing Sand or Dirt or Snow Clear or Partly Cloudy	Wet Straight & Grade	Daylight	From same direction - all others	Backing Going Straight Ahead	Stopped for Traffic Making Right Turn Stopped for Traffic	North Vehicle Backing Vehicle Stopped Vehicle Stopped North South North West South North Morth South North South North
5556 City Street 5557 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	S SEQUIM AVE 0 S SEQUIM AVE	E ETTA ST E MAPLE ST	No 3800 No E789 No E502	806 2018-04-17 12:08 Suspected Minor Injury 253 2016-01-05 14:32 No Apparent Injury	1 0 1 0	1 Passenger Car 0 Passenger Car	Passenger Car	At Intersection and Not Related At Intersection and Related At Intersection and Related	Raining Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Wet Straight & Level Wet Straight & Level	Daylight Daylight	From same direction - one right turn - one straight From same direction - both going straight - one stopped - rear-end Vehicle Strikes Pedalcyclist Entering at angle	Going Straight Ahead Making Right Turn Going Straight Ahead	Stopped for Traffic Going Straight Ahead	South North South North South East East West North South
5558 City Street 5559 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	S SEQUIMAVE 0 S SEQUIMAVE 9	E MAPLE ST E PRAIRIE ST 1000 E PRAIRIE ST E PRAIRIE ST E PRAIRIE ST E PRAIRIE ST	No 3803 No E378	36 2019-10-09 15:38 No Apparent Injury 306 2014-11-24 09:10 Possible Injury	0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level		Entering at angle From same direction - both going straight - one stopped - rear-end	Making Left Turn	Going Straight Ahead Stopped for Traffic	East South South North South North Vehicle Stopped Vehicle Stopped
5560 City Street 5561 City Street		S SEQUIMAVE 0	E PRAIRIE ST E PRAIRIE ST	No E494 No E723	570 2015-12-16 11:15 Possible Injury 141 2017-10-08 20:40 Possible Injury	1 0 2 0	Passenger Car Passenger Car Passenger Car Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Dark-Street Lights On	Entering at angle Entering at angle	Going Straight Ahead Merging (Entering Traffic) Going Straight Ahead	Going Straight Ahead Going Straight Ahead	East North South North West East South North
5562 City Street 5563 City Street 5564 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	S SEQUIM AVE 9 S SEQUIM AVE 0 S SEQUIM AVE 11	900 E PRAIRIE ST E WASHINGTON ST 00 E WASHINGTON ST	No E774 No 3202	2017-07-24 12:37 No Apparent Injury 167 2019-03-14 15:10 No Apparent Injury 108 2014-11-04 12:28 Document	0 0 2 0	Truck Tractor & Semi-Trailer Pickup Panel Track or Viscette and a 40 000 Track	Pickup Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Dry Straight & Level	Daylight Daylight	Entering at angle Street Light Pole or Base From same direction - both going straight - one stopped - rear-end	Making Left Turn Making Right Turn Going Straight Ahead	Going Straight Ahead Stopped for Traffic	East South North South South East South Vehicle Stopped South North Vehicle Stopped Vehicle Stopped
5565 City Street 5566 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	S SEQUIMAVE 0 S SEQUIMAVE 11	E WASHINGTON ST	NO E371 No E733 No 3046	14 2017-11-25 17:50 Suspected Minor Injury 173 2015-02-19 18:06 Possible Injury	1 0 1 0	1 Passenger Car 1 Passenger Car	app and the service of the service o	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy	Wet Straight & Level Wet Straight & Level	Dark-Street Lights On Dark-Street Lights On	Vehicle - Pedalcyclist Vehicle - Pedalcyclist	Going Straight Ahead		South North North South
5951 City Street 5852 City Street 5854 City Street 5854 City Street 5855 City Street 5855 City Street 5855 City Street 5856 City Street 5856 City Street 5857 City Street	Clallam Sequim	S SEQUIMAVE 0 S SEQUIMAVE 0	W HAMMOND ST W MAPLE ST	No 3759 No E760	167 2017-08-09 11:32 No Apparent Injury 1684 2018-01-14 20:03 No Apparent Injury	0 0 2 0	O Passenger LaT O Truck Tractor & Semi-Trailer O Truck Tractor & Semi-Trailer O Truck Tractor & Truck or Vanette under 10,000 lb Passenger Car Passenger Car O Passenger Car O Passenger Car O Passenger Car O Passenger Car	Passenger Car	At Intersection and Related At Intersection and Related	Overcast Clear or Partly Cloudy Overcast	Dry Straight & Level Dry Straight & Level	Daylight Dark-Street Lights On	Vehicle - Pedalcyclist Vehicle - Pedalcyclist Entering at angle Street Light Pole or Base	Going Straight Ahead Making Left Turn Making Left Turn	Going Straight Ahead	West North South North East South
5569 City Street 5570 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	S SEQUIMAVE 0 S SEQUIMAVE 0	W MAPLE ST W PRAIRIE ST						At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight	Entering at angle Entering at angle	Starting in Traffic Lane	Going Straight Ahead Going Straight Ahead Stranged for Traffic	East West North South West East South North South North Vehicle Stopped Vehicle Stopped
\$569 City Street \$569 City Street \$570 City Street \$571 City Street \$572 City Street \$572 City Street \$573 City Street \$574 City Street \$574 City Street \$575 City Street \$576 City Street \$576 City Street \$576 City Street \$577 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	S SEQUIM AVE 2 S SEQUIM AVE 2 S SEQUIM AVE 2	100	82 F S E BELL ST No 2772 26 F N E BELL ST No E559 26 F N E BELL ST No E827 30 F NW E ETTAST No E349	227 2018-08-11 10:45 No Apparent Injury 2019 2014-08-12 18:48 No Apparent Injury	0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car Truck (Flatbad.Van.etc)	Not at Intersection and Not Related Intersection Related but Not at Intersection Intersection Related but Not at Intersection	Clear or Partly Cloudy	Dry Straight & Level		From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Making Right Turn Going Straight Ahead	Going Straight Ahead Stopped for Traffic Going Straight Ahead Stopped for Traffic	South North Vehicle Stopped Vehicle Stopped East North South North North North South Vehicle Stopped Vehicle Stopped
5574 City Street 5575 City Street	Clallam Sequim Clallam Sequim	S SEQUIMAVE 5 S SEQUIMAVE 6	00	124 F S WHAMMOND ST No 3046 243 F S WHEMLOCK ST No E568 100 E S WHEMLOCK ST No E568	153 2017-10-03 16:35 No Apparent Injury 171 2016-07-26 12:50 Suspected Minor Injury	0 0 2 0	0 Passenger Car 0 Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Daylight	From opposite direction - both going straight - sideswipe	Making Left Turn Going Straight Ahead Starting in Traffic Lane	Going Straight Ahead Going Straight Ahead Stopped for Traffic Stopped for Traffic	South North Vehide Stopped Vehicle Stopped North South North South North South North South North South North South North North South North South South North South South North South North South
5576 City Street 5577 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	S SEQUIMAVE 1: S SEQUIMAVE 0	00	100 F	889 2018-02-26 11:55 Possible Injury 886 2019-09-30 12:00 No Apparent Injury	0 0 2 0	Passenger Car Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Daylight Dark-Street Lights On	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Anead	Stopped for Traffic Stopped for Traffic	South North Vehicle Stopped Vehicle Stopped North South Vehicle Stopped Vehicle Stopped Vehicle Stopped
5575 City Street	Cialiam Sequim	DEQUIMIDUNGENESS WAY 0	JESSLIN EN	No E622	2016-11-28 19:25 Suspected Minor Injury	1 0 1 0	и гижир, Panei Truck or Vanette under 10,000 lb	+	PK intersection and Not Related	overcast	Dry Straight & Level	Jank-Street Lights On	weiai aga rost	Overtaking and Passing		NOTE: SOUTH

OBJECTID MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_1_UNIT_1	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_2_UNIT_1	MV_DRIVER_CONTRIBUTING AGE UNIT_1	MV_DRIVER GENDER UNIT_1	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_1_UNIT_2	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_2_UNIT_2	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_3_UNIT_2 MV_DRIVER GENDER UNIT_2	BICYCLIST_CONTRIBUTING GIRCUMSTANCE_1_UNT_1 GIRCUMSTANCE_2_UNT_1	BICYCLIST_CONTRIBUTING CIRCUMSTANCE_3_UNIT_1 BICYCLIST AGE UNIT_1	BICYCLIST BICYCLIST_CONTRIBUTING CIRCUMSTANCE_1_UNT_2	BICYCLIST_CONTRIBUTING CIRCUMSTANCE, 2_UNIT_2	BICYCLIST_CONTRIBUTING BICYCLIST CIRCUMSTANCE_3_UNIT_2 BICYCLIST GENC	CYCLIST PEDESTRIAN CONTRIBUTING PEDESTRIAN CONTRIBUTE CIRCUMSTANCE 1_UNIT_2 CIRCUMSTANCE_2_UNIT.	ng PEDESTRIAN_CONTRIBUTING PEDESTRIAN AGE_UNIT_2 (PRST_MPACT_LOCATION City_St_County_Rd_8_Mise_2010_forward_St
5399 None 5400 None 5401 Enting or Drinking 6402 Instention		16 18	Male Male	Had Taken Medication None		66 Male			None.		72 Eemal			Lane of P	Shoulder of Primary Trafficway Primary Trafficway Shoulder of Primary Trafficway Shoulder of Primary Trafficway
S401 basing or university S402 basing or university S403 Distractions Custisids Whitele S404 hashestedon S404 basiness of the S404 basiness S404 basiness of the S404 basiness S405 basiness of the S404 basiness S406 basiness of the S404 basiness S408 basiness S409 basi			Male Male Female	Driver Not Distracted Inattention		67 Female 31 Female			None		73 Femal	0		Lane of P	Primary Traficway Primary Traficway Primary Traficway
5405 Did Not Grant RW to Vehicle 5406 None 5407 Distractions Outside Vehicle	Inattention Inattention			Inattention None Operating Defective Equipment		67 Male 19 Female						Inattention	15 M	le Lane of P	Primary Trafficway Primary Trafficway Primary Trafficway Primary Trafficway
5408 Inattention 5409 Inattention 5410 Inattention	Follow Too Closely	74 74	Female Female	None None Driver Not Distracted		72 Female 80 Female 85 Male								Lane of P Lane of P	Primary Trafforway Primary Trafforway Primary Trafforway
5411 Follow Too Closely 5412 Distractions Outside Vehicle 5413 Follow Too Closely 5414 Did Not Grant RW to Vehicle				None Driver Not Distracted		60 Female 61 Female 40 Male 80 Maie								Lane of P	Times y Tatkoway Primary Tatkoway Primary Tatkoway Primary Tatkoway Primary Tatkoway
5413 Follow Too Closely 5414 Did Not Grant RW to Vehicle 5415 Disregard Stop Sign - Flashing Red	Inattention Disregard Stop Sign - Flashing Red Inattention D	24 58 iid Not Grant RW to Vehicle 55	Male Female Male Male	None None None		40 Maie 80 Maie 66 Maie									
S419. Did not crisin K W to Venico S 5419. Director Style Sign - Hashing Red 5416. Did Not Grant R W to Vehicle 5417. Uniform Distration 5418. Instention 5419. Other Conflicting Circ Not Listed 5420. None 5421. Listention 5422. Distractions Cutside Vehicle 5422. Distractions Cutside Vehicle 5423. Accountly Asless or Fatisued		68 64	Female Female	None None		88 Male 41 Female								Lane of P Lane of P	Primary Trafficway Primary Trafficway Primary Trafficway Primary Trafficway Primary Trafficway
5419 Other Contributing Circ Not Listed 5420 None		78 51	Male Male	None Other Contributing Circ Not Listed	Inattention	64 Female 49 Male 79 Male								Lane of P Lane of P	Joulder of Pimary Trafficway Primary Trafficway
5421 Inattention 5422 Distractions Outside Vehicle 5423 Apparently & legen or Estimated	Over Center Line	90 18	Female	None		79 Male 62 Male 60 Female 23 Male								Lane of P Lane of P	Primary Trafficway Primary Trafficway Primary Trafficway Primary Trafficway
5423 Apparently Asleep or Fatigued 5424 Did Not Grant RW to Vehicle 1425 Inattention 5426 Under Influence of Drugs 5427 Other Driver Distractions Inside Vehicle 5429 Driver Not Visit Partent	Over Center Line	85 45	Female Male Male	None		33 Male 68 Female 51 Male								Lane of P	**Innary Tralicway **Innary Tralicway **Innary Tralicway **Innary Tralicway **Unitary Tralicway **Unitary Tralicway **Unitary Tralicway **Unitary Tralicway **Unitary Tralicway
5426 Under Influence of Drugs 5427 Other Driver Distractions Inside Vehicle 5428 Driver Not Distracted			Male Female Male	None	Unknown Distraction	28 Male								Lane of P	Outside Shoulder of Primary Trafficway Primary Trafficway Primary Trafficway
5427 Other Direct Distractions Inside Vehicle 5428 Direct Word Distraction 5429 Apparently III 5430 Follow Too Closely 5431 None 5432 Follow Too Closely 5433 Unknown Distraction 5434 Follow Too Closely 5435 Apparently Allego or Fatigued 5436 Follow Too Closely 5437 Follow Too Closely	Did Not Grant RW to Vehicle Driver Interacting with Passengers, Anim	84 26	Male Female	None None		41 Male 67 Female 54 Male								Lane of P	Hinary Italicusy Hinary Italicusy Hinary Italicusy Hinary Italicusy
5431 None 5432 Follow Too Closely 5433 Unknown Distraction			Male Female Male	Driver Not Distracted Driver Not Distracted	Other Driver Distractions Inside Vehicle	72 Female 68 Female 66 Male									
5434 Under Influence of Alcohol 5435 Apparently Asleep or Fatigued		82 30	Male Male	Driver Not Distracted Driver Not Distracted										Past the C Outside S	Primary Trafficway Outside Shoulder of Primary Trafficway Shoulder of Primary Trafficway
5437 Operating Defective Equipment 5438 Inattention	Follow Too Closely	59 71	Male Male Female	None None		70 Female 33 Male 72 Male								Lane of P Lane of P	Primary Trafficway Primary Trafficway Shoulder of Primary Trafficway
5439 Under Influence of Drugs 5440 Follow Too Closely 5441 Inattention	Unknown Distraction	16	Male	Driver Not Distracted		64 Male									
5442 Did Not Grant RW to Vehicle 5443 Inattention		80 22	Female Female Male	None		25 Female 28 Male								Lane of P Past the 0	Primary Trafficway Primary Trafficway Outside Shoulder of Primary Trafficway
5444 Unknown Distraction 5445 Inattention 5446 Distractions Outside Vehicle		21 37	Female Female	None Driver Not Distracted		71 Female 51 Maie								Past the 0	Outside Shoulder of Primary Trafficway ng Trafficway Primary Trafficway Primary Trafficway
5455 Apperently Meles or Faligned 5456 Cylor Tac Case dy 5456 Cylor Tac Cylor Tac Cylor Tac 5456 Cylor Tac Cylor Tac 5456 Cylor Tac Cylor Tac 5456 Cylor Tac Cylor Tac 5457 Cylor		51 67	Female Female	None Driver Not Distracted None None None		71 Female 51 Male 33 Female 58 Male									
5450 Inattention 5451 Did Not Grant RW to Vehicle	Inattention D	46 73 distractions Outside Vehicle	remale Male Male	Driver Not Distracted None None		57 Female 54 Female 78 Female								Lane of P Lane of P Lane of P	Primary Trafforway Primary Trafforway Primary Trafforway
5452 Did Not Grant RW to Vehicle 5453 Inattention		59	Male	None None		65 Female 70 Male						Inattention		Lane of P	Primary Trafficway Primary Trafficway
5455 Follow Too Closely 5456 Did Not Grant RW to Vehicle		71 21 83	Male Male	None None		59 Male 93 Female						Inattention	59 Fe	Lane of P	ing Traficway **imary Traficway **imary Traficway **imary Traficway
5457 Unknown Distraction 5458 Did Not Grant RW to Vehicle 5459 None		75 67	Female Male Male	None Driver Not Distracted None		49 Female 32 Male								Lane of P	Primary Trafficway Primary Trafficway Oustide Shoulder of Primary Trafficway Primary Trafficway Primary Trafficway
5-55 John Committee of the Vehicle 5-55 John Committee of Vehicle 6-55 John Committee of Vehi			Female Female			93 Female 87 Maie								Lane of P	rimary Trafficway Vimary Trafficway
5462 Did Not Grant R/W to Non Motorist 5463 Other Contributing Circ Not Listed 5464 Under Influence of Alcohol	Inattention		Female Male			69 Male			Inattention		12 Male			Intersection Lane of P Past the C	Pinnary Tatlioway ng Tratlioway Pinnary Tatlioway Outside Shoulder of Primary Tratlioway
9464 Unknown Distraction 5465 Unknown Distraction 5466 Driver Not Distracted 5467 Follow Too Closely 5468 Inattention		86	Female			57 Female								Past the 0	Outside Shoulder of Primary Trafficway
5467 Follow Too Closely 5468 Inattention 5469 Driver Interacting with Passengers, Anim		19	remale	None None Driver Not Distracted		68 Female 64 Female 70 Female								Lane of P Lane of P Lane of P	Primary Trafficway Primary Trafficway Primary Trafficway Primary Trafficway
3468 Internet of the state of t			Male Female			38 Female 72 Female								Lane of P Lane of P	Primary Traficovay Primary Traficovay Primary Traficovay Primary Traficovay
5473 Did Not Grant RW to Vehicle 5474 Did Not Grant RW to Vehicle	Inattention	80 80 85	Male Female	None None		51 Male 49 66 Female									
5473 Did Not Grant RW to Vehicle 5474 Did Not Grant RW to Vehicle 5475 Follow Too Closely 5476 Inattention	Unknown Distraction	49	Male	None None		66 Female 24 Female 38 Female									Primary Trafloway Primary Trafloway Primary Trafloway
5478 Driver Not Distracted 5479 Disregard Stop Sign - Flashing Red	Exceeding Reas. Safe Speed	28 59	Male Female Male	Driver Not Distracted Inattention		53 Male						None	65114	Lane of P	ng Trafficway **Immay Trafficway **Quairide Shoulder of Primary Trafficway **Primary Trafficway **Quairide Shoulder of Primary Trafficway **Quairide Shoulder of Primary Trafficway
S477 Other Contribution Circ Not Listed	Operating Defective Equipment	40	Female Male Male	Inattention	Follow Too Closely	19 Male								Lane of P Past the C	Primary Trafficway Outside Shoulder of Primary Trafficway ng Trafficway
5483 Inattention 5484 Did Not Grant RW to Vehicle	Operating Describe Equipment	81	Female	None Driver Not Distracted		32 Female 40 Female 29 Female								Lane of P	high remove framework of the control
5485 Disregard Stop Sign - Flashing Red 5486 None 5487 None	Inattention D	lid Not Grant RW to Vehicle 24	Female	None Did Not Grant RW to Vehicle		29 Female 33 Female			Did Not Grant RW to Vehicle		61 Male			Lane of P	imary Trafficway
3485 None		81 46	Male Female									None	13 Fe	nale Intersection Outside S	Primary Trafficway Primary Trafficway Intrady Trafficway Grafficway Shoulder of Primary Trafficway
5490 Unknown Distraction 5491 Under Influence of Alcohol 5492 Did Not Grant RW to Vehicle	Follow Too Closely Inattention Fi Inattention	0llow Too Closely 37 17	Male Female Male Female	None None		65 Male 78 Female 19 Male									Primary Trafficway Primary Trafficway Primary Trafficway cation (City/CountyMiss. Trafficway)
5493 Inattention 5494 Disregard Yield Sign - Flashing Yellow		67 81	Male	None		68 Female									
5493 Inationbon 5494 Disregard Yield Sign - Flashing Yellow 5495 Unknown Distraction 5496 None 5497 Did Not Grant RW to Vehicle	Distractions Outside Vehicle	81 65	Male Male	Driver Not Distracted		60 Female								Other Loc Lane of P	cation (ChyCountyMsc. Trafficway) cation (ChyCountyMsc. Trafficway) rimary Trafficway
5498 Distractions Outside Vehicle 5499 Other Contributing Circ Not Listed 5500 Driver Not Distracted		70	Female	None Did Not Grant RW to Vehicle		45 Female 48 Female									Primary Trafficway casion (ChyCountyMsc. Trafficway) Primary Trafficway Primary Trafficway
3497 Dut vot crast in KY VV v sensore \$499 Other Contributing Clin Not Listed \$499 Other Contributing Clin Not Listed \$499 Other Contributing Clin Not Listed \$490 Other Contributing Clin Not		17	Male Female	None		43 Male						None	24 Fe	Lane of P	Primary Trafficway Primary Trafficway
5503 Unknown Distraction 5504 Did Not Grant RW to Vehicle 5505 Inattention	Improper Passing	55 94	Female Male Male	None Driver Not Distracted		50 Female 62 Male								Lane of P Cutside S	Primary Trafficway Primary Trafficway Primary Trafficway Shoulder of Primary Trafficway Outside Shoulder of Primary Trafficway
S506		26 41	Female Male Male	None		18 Maie									
5509 Did Not Grant RW to Vehicle 5510 Did Not Grant RW to Non Motorist		79	Female	None		85 Female						None	81 M	le Lane of P	Primary Traffoway Primary Traffoway Primary Traffoway
5511 Inattention 5512 Did Not Grant RW to Vehicle 5513 Did Not Grant RW to Vehicle		17 87 17	Female Female Male	None Driver Not Distracted None Improper Parking Location		35 Female 66 Female 38 Female 57 Male								Lane of P Lane of P Lane of P	Primary Trafficway Primary Trafficway Primary Trafficway Primary Trafficway Primary Trafficway
565 Did not Calari K V V v enema 555 Did not Calari K V V v enema 555 Did not Calari K V V v enema 555 Did not Calari K V V v Note Notice 1555 Did not K V V v Notice 1554 Did not Calari K V V v Notice 1554 Did not Calari K V V v Notice 1554 Did not Calari K V V Notice 1554 Did not Calari K V V Notice 1555 Did not Calari K V Notice Notic	Eating or Drinking Driver Interacting with Passengers, Anim	16 34	Male Male	Improper Parking Location Unknown Distraction		57 Male 40 Female								Lane of P	imary Trafficway cimary Trafficway Outside Shoulder of Primary Trafficway
5517 Did Not Grant RW to Vehicle		17	Female	None			Non Motorist on Wrong Side of Road	13	Male					Past the C Lane of P Lane of P	Pinnary Trafficway Outside Shoulder of Primary Trafficway Pinnary Trafficway Pinnary Trafficway
5019 Unit vet Craffir W to Venicle 5519 automotion 5520 Instension 5520 Instension 5521 Instension 5522 Instension 5522 Instension 5523 Under Influence of Alcohol 5524 Under Influence of Alcohol 5525 Ottachon Cuttide Vehicle 5526 Uniknown Distaction 5527 Instension	Operating Handheld Cell Phone	65	Male	None None None		48 Maie								Outside S	Shoulder of Primary Trafficway
5522 Inattention 5523 Distractions Outside Vehicle			Male Male Female Female			48 Male 71 Female 56 Male 43 Female									Pinnary Trafficway Pinnary Trafficway Pinnary Trafficway Pinnary Trafficway
5525 Other Contributing Circ Not Listed 5526 Unknown Distraction		68 68 23	Male Male	Driver Not Distracted Unknown Distraction		59 Female 62 Male								Lane of P Past the C Lane of P	Primary Trafficway Outside Shoulder of Primary Trafficway Primary Trafficway Yrafficway
5527 Inattention 5528 Did Not Grant RW to Vehicle	Inattention	53	Female	None		36 Female 39 Female								Lane of P Lane of P	Primary Traffioway Primary Traffioway
Sozia Unitrodes or numberous SSES Posteriories SS		58 79 30	Female Male	None Did Not Grant RW to Vehicle None None		18 Male 34 Male 63 Male								Lane of P Lane of P Intersection	Pilmary Trafficway Primary Trafficway ng Trafficway
5532 Inattention 5533 Follow Too Closely 5534 Did Not Grant RW to Mahicla			Male Female Female Female			71 Female 31 Maie 34 Female								Lane of P	Primary Trafficway Primary Trafficway Primary Trafficway Primary Trafficway
5535 Did Not Grant RW to Vehicle 5536 Did Not Grant RW to Vehicle	Inattention	73 67	Female Female	None Driver Not Distracted		60 Female								Lane of P	dmary Trafficway Vimary Trafficway
5537 Did Not Grant RW to Vehicle 5538 Inattention 5539 Did Not Grant RW to Vehicle		46 58 19	Female Male Male	Driver Not Distracted Driver Not Distracted None None		54 Female 60 Male 92 Male 65 Male								Lane of P Lane of P Lane of P	Primary Trafficway Primary Trafficway Primary Trafficway Primary Trafficway Primary Trafficway
5540 Inattention 5541 Other Contributing Circ Not Listed						54 Male 77 Male									
5543 Did Not Grant R/W to Non Motorist 5544 Did Not Grant R/W to Non Motorist	Inattention	58 54 42	Male Male Female Female									None None	64 Fe 82 M	Past the C male Lane of P le Intersection	Pilinary I Shouler of Punsay Trafficaey Vinary Tarkouler of Punsay Trafficaey Vinary Tarkouler of Punsay Trafficaey Shoulder Shouler of Punsay Trafficaey Shoulder of Punsay Trafficaey Trafficaey Trafficaey Traffic
5545 Exceeding Stated Speed Limit 5546 Did Not Grant RW to Vehicle 5547 Did Not Grant RW to Vehicle	Exceeding Reas. Safe Speed	28	Male Male Male	None		60 Female						None	58 Fe	Past the Courside S	utside Shoulder of Primary Trafficway ihoulder of Primary Trafficway 'iman' Trafficway
5548 Did Not Grant RW to Vehicle 5549 Inattention	Inattention Exceeding Stated Speed Limit	57 53	Male Male	None		37 Maie								Lane of P Lane of P Past the C	Primary Traficway Primary Traficway Outside Shoulder of Primary Trafficway
5550 Inattention 5551 Improper Turn/Merge 5552 Inattention	Did Not Grant RW to Vehicle	86 58	Female	Driver Not Distracted None		55 Male 47 Male			None		EA F	0		Lane of P	Times I Yakusensy United Shoulder of Primary Trafficway Primary Traffoway Ing Traffoway Primary Traffoway Primary Traffoway
5553 Improper Backing 5554 Inattention	Unknown Distraction	35 33 16	Male Male Male Male Female	None None		23 Male 58 Male 50 Female			THATE		04 Femal			Lane of P	Primary Trafficway
5556 Did Not Grant R/W to Non Motorist 5557 Disregard Stop Sign - Flashing Red	Inattention Driver Not Distracted Inattention D	86 68 68 68 Not Grant RW to Vehicle	Female Male	None					Non Motorist on Wrong Side of Road		37 Femal	0			Primary Trafficway Primary Trafficway Ing Trafficway Primary Trafficway Primary Trafficway
5558 Did Not Grant RW to Vehicle 5559 Follow Too Closely	Inattention	89 68	Male Male Male Female	None None		42 Female 51 Male 59 Male 74 Male 41 Gemale								Lane of P Lane of P	Hinnay Trafficoway Hinnay Trafficoway Hinnay Trafficoway Hinnay Trafficoway
5561 Inattention 5562 Inattention						74 Male 43 Female 30 Male									
553.5 On the Clarat RVI or Verbride 553.5 On the Clarat RVI or Verbride 553.5 On the Clarat RVI or Verbride 553.6 On the Clarat RVI or Verbride 554.6 On the Clarat RVI or Verbride 554.7 On the Clarat RVI or Non Motional 554.2 On the Clarat RVI or Non Motional 554.2 On the Clarat RVI or Non Motional 554.2 On the Clarat RVI or Non Motional 554.3 On the Clarat RVI or Non Motional 554.5 On the Clarat RVI or Non Motional 554.5 On the Clarat RVI or Non Motional 555.6 On the Clarat RVI or Non Motional 556.5 On the Clarat RVI or Non Motional 557.5 On the Clarat RVI or Non Motional 558.5 On the Clarat RVI or Non Motional 559.6 On the Clarat RVI or Non Motional 559.6 On the Clarat RVI or Non-Motional	Follow Too Closely	56 30	Male Female	None		65 Male			Did Not Grant DW to Valida		69			Past the C Lane of P	Prince of the Control
5566 None 5567 Unknown Distraction		22	Female Male Female Male						Did Not Grant RW to Vehicle Light Violation: No Lights/Fail to Dim		23 Male 13 Male			Lane of P Lane of P	Primary Trafficway Primary Trafficway Outside Shoulder of Primary Trafficway
5568 Under Influence of Alcohol 5569 Did Not Grant RW to Vehicle		81	Female	None		22 Male 64 Female								Lane of P	Primary Trafficway
SSE8 Under Influencia of Alcohol SSE9 Under Influencia of Alcohol SSE9 Under Colorative Whether SST1 Instruction SST1 Instruction SST2 Under Influencia SST3 Instruction SST3 Instruction SST3 Instruction Description SST4 Under Influencia of Drugs SST5 Under Influencia of Drugs SST6 Under Influencia Of Drugs SST6 Under Influencia Of Drugs SST7 Druke National Value SST7 Druke National Size National SST7 Other National Size National		23 80	Male Male	None Driver Not Distracted None		64 Female 66 Female 85 Male								Lane of P	Pilmary Trafficway Pilmary Trafficway Pilmary Trafficway
5573 Inattention	Follow Too Closely Over Center Line	20	Female Male	None None		55 Male 69 Female 70 Male 75 Male 75 Male 76 Male 76 Male 77 Male 77 78 Male 78 Male									Primary Trafficway Primary Trafficway Primary Trafficway Primary Trafficway
5576 Distractions Outside Vehicle 5577 Driver Not Distracted		52 52	Male	Driver Not Distracted Driver Not Distracted		76 Female								Lane of P	Primary Trafficway
5578 Other Contributing Circ Not Listed	Operating Handheld Cell Phone In	nproper Passing 20	Female	ļ	L			+		ļ				Past the 0	Outside Shoulder of Primary Trafficway

OBJECTID WA_STATE_PLANE SOUTH_Y 2010_FORWARD CO.			L COL COLAGE SURF	IONS	COL LOCATION	COL STU GEOMETRY ARE	DY COL COL TYPE SEVER TEXT TEXT	COL_HUMAN C.F. FACTOR_Decoded MN	CONDITIONS CON Decoded De	SURFACE COL_LOCATION DECOded Decoded		Intersection Emphasis Roa Dept
5400 1001756.82 1017514 2018 5401 1004285.64 1011698.17 2014	4 2 Vehicles/Paries 5 2 Vehicles/Paries 5 2 Vehicles/Paries 7 2 Vehicles/Paries 7 2 Vehicles/Paries 9 2 Ve	Property Damage Only Distracted Driving Under	vm No Dr 25 No Dr 28 No Dr No Dr No Dr No We		Not at Intersection/Driveway/Roundabout At Driveway or Intersection Not at Intersection/Driveway/Roundabout At Driveway or Intersection At Driveway or Intersection	Straight & Level Straight & Level Straight & Level Straight & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Pedalcyclist Older Road User 65+	None None Eating or Drinking Inattention Distractions Outside Vehicle	Dark-No Street Lights/Street Lights Off Daylight Daylight Daylight	Dry Not at Intersection/Driveway/Roundabout Dry At Driveway or Intersection Dry Not at Intersection/Driveway/Roundabout Dry At Driveway or Intersection Wet At Driveway or Intersection	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Distracted Driving Inattention Distracted Driving	Yes No Yes No No
5404 1004200.97 1011140.72 2018 5405 996119.79 1008438.33 2019 5406 996790.03 1008416.13 2014	3 2 Vehicles/Parties Turning 9 2 Vehicles/Parties Angle 4 2 Vehicles/Parties Turning 5 2 Vehicles/Parties Parties 7 Vehicles/Parties Pedestrian	Property Damage Only Inattention 45-54 Property Damage Only Fail to Yeld 25-34 Property Damage Only Other/Unknown 25-34 Mnor or Possible Injury Inattention 65+	No Web No Ioe/Snow No Dr	t Daylight /Slush Dark-Street Lights On	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Straight & Level Straight & Level Straight & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Pedestrian and Older Road User 65+	Inattention Fail to Yield None Distractions Outside Vehicle	Daylight Dark-Street Lights On Ice/Sn Daylight	Wet At Driveway or Intersection own/Slush At Driveway or Intersection Dry At Driveway or Intersection Dry At Driveway or Intersection	Inattention Fail to Yield No Risky Driving Behaviors Involved Inattention	No No No No
5407 1004248.24 1012785.12.2016 5408 1007534.16 1011090.84.201 5409 1006857.27 1011687.201 5410 1006857.27 1011697.201 5411 1006857.27 1011697.201	4 2 Vehicles/Parties Angle 4 2 Vehicles/Parties Rear-End		No W No Dr No Dr 25 No	/ Daylight / Daylight	At Driveway or Intersection	Straight & Level Straight & Level Straight & Level Straight & Level	Older Road User 65+ Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Inattention Inattention Inattention Inattention Follow Too Closely	Daylight Daylight	Wet At Driveway or Intersection Dry At Driveway or Intersection	Inattention Inattention Inattention Follow Too Closely	No No No
5412 1006857.27 1011587 2016 5413 1005377.1 1012077.47 2016 5414 1009465.75 1009903.82 2017 5415 1009465.75 1009903.82 2017	3 2 Vehicles/Parties Rear-End 3 2 Vehicles/Parties Rear-End 7 3 Vehicles/Parties Angle 7 2 Vehicles/Parties Angle	Property Damage Only Distracted Driving 35-44 Property Damage Only Follow Too Closely Under Mnor or Possible Injury Mnor or Possible Injury Fail to Yeld 55-64 Suspected Serious Injury Disregard Traffic Control Device/Sign 35-64		/ Daylight / Daylight / Daylight	At Driveway or Intersection At Intersection and Not Related At Driveway or Intersection At Driveway or Intersection	Straight & Level Straight & Level Straight & Grade Straight & Grade	No Vulnerable Road User Involved	Distractions Outside Vehicle Follow Too Closely Fail to Yield Disregard Traffic Control Device/Sign	Daylight Daylight Daylight	Dry At Driveway or Intersection Dry At Intersection and Not Related Dry At Driveway or Intersection Dry At Driveway or Intersection	Distracted Driving Follow Too Closely Fail to Yield No Risky Driving Behaviors Involved	No No No No
5416 1009465.75 1009903.82.2011 5417 1004221.82 1012121.41 2014 5448 1004221.82 1012121.41 2014	7 2 Vehioles/Pariies Angle 2 Vehioles/Pariies Rear-End 4 2 Vehioles/Pariies Struck Parked Vehiole 4 2 Vehioles/Pariies Rear-End 4 2 Vehioles/Pariies 4 2 Vehi	Minor or Possible Injury Fail to Yield 65+ Minor or Possible Injury Distracted Driving 55-64 Property Damage Only Inattention Under Minor or Possible Injury Other/Unknown 65+	No Dr No Dr 25 No Dr No We	/ Daylight t Daylight	At Driveway or Intersection At Driveway or Intersection At Intersection and Not Related At Driveway or Intersection	Unknown Straight & Level Straight & Level Straight & Level	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	Fail to Yield Unknown Distraction Inattention Other Contributing Circ Not Listed	Daylight Daylight Daylight Daylight	Dry At Driveway or Intersection Dry At Driveway or Intersection Dry At Intersection and Not Related Wet At Driveway or Intersection	Fail to Yield Distracted Driving Inattention No Risky Driving Behaviors Involved	No No Yes No
	5 2 Vehicles/Parties Rear-End 3 3 Vehicles/Parties Rear-End 4 2 Vehicles/Parties Rear-End 5 2 Vehicles/Parties Rear-End 5 2 Vehicles/Parties Head-On	Property Damage Only Other/Unknown 45-54 Minor or Possible Injury Instension 65+ Minor or Possible Injury Distracted Driving Undet Minor or Possible Injury Apparently Asleep, Fatigued or III 65+	No Wr No Dr 25 No Dr Apparently Asleep, Fatigued or III 65+ Wr		At Driveway or Intersection At Driveway or Intersection At Intersection and Not Related At Intersection and Not Related	Straight & Level Unknown Straight & Level Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+	Apparently Asleep or Fatigued	Daylight Daylight	Wet At Driveway or Intersection Dry At Driveway or Intersection Dry At Intersection and Not Related Wet At Intersection and Not Related	No Risky Driving Behaviors Involved Inattention Distracted Driving Apparently Asleep, Fatigued or III	No No No Yes
5426 1009465.74 1009903.81 2016 5427 1004221.82 1012121.41 2014	3 2 Vehicles/Parties Angle 2 Vehicles/Parties Angle 3 2 Vehicles/Parties Principle 4 3 Vehicles/Parties Rear-End 3 Vehicles/Parties Rear-End	Property Damage Only Distracted Driving 55-64	No Dr No Dr Under influence of Drugs 45-54 Dr No Dr Ms Ms		At Driveway or Intersection	Straight & Level Unknown Curve & Grade Straight & Level	Older Road User 65+ No Vulnerable Road User Involved	Fail to Yield Inattention Under Influence of Alcohol/Drugs/Medication Distracted Driving Others Influence of Alcohol/Drugs/Medication	Daylight Daylight Daylight Daylight Daylight Daylight	Dry At Driveway or Intersection	Fail to Yield Inattention Under Influence of Alcohol/Drugs/Medication Distracted Driving	No No Yes No
5428 1004221.83 1012121.41 2015 5429 1004221.82 1012121.41 2015 5430 1008108.28 1010790.93 2015 5431 1004725.43 1012102.24 2016 5432 1004725.44 1012102.25 2018	3 2 Vehides/Paries Rear-End 2 Vehides/Paries Angle 5 2 Vehides/Paries 3 2 Vehides/Paries 6 2 Vehides/Paries 8 2 Vehides/Paries 6 2 Vehides/Paries 7 2 Vehides/P	Property Damage Only Apparently Asleep, Fatigued or III 65+ Minor or Possible Injury Follow Too Closely 25-34 Property Damage Only OheeUuknown 65+	NO	/ Daylight / Daylight ht Daylight	At Driveway or Intersection Roundabout At Driveway or Intersection At Driveway or Intersection At Intersection and Not Related	Straight & Level Curve & Grade Straight & Grade Straight & Level Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User involved Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Other/Unknown Driver Physical Condition Follow Too Closely None Follow Too Closely	Daylight Daylight Daylight	Wet At Driveway or Intersection Dry Roundabout Dry At Driveway or Intersection Wet At Driveway or Intersection Dry At Intersection and Not Related	No Risky Driving Behaviors Involved Apparently Asleep, Fatigued or III Follow Too Closely No Risky Driving Behaviors Involved Follow Too Closely	No No No
5433 1008491.21 1010545.79 2014 5434 1008491.21 1010545.79 2014 5435 1011807.2 1008388.11 2017	4 2 Vehicles:Parties Rear-End 4 1 Vehicle:Party Struck Object 7 1 Vehicle:Party Struck Object 3 2 Vehicles:Parties Rear-End	Mnor or Possible Injury Distracted Driving 45-54 Property Damage Only Under Influence of Alcohol/Drugs/Medication 65+ Property Damage Only Apparently Aeleep, Fatigued or III 25-34 Property Damage Only Follow Too Closely 25-34	No Dr. Under Influence of Acohol 65+ W Apparently Asleep, Fatigued or III 25-34 Dr No 0r 0r	/ Daylight tt Dark-Street Lights On / Dark-Street Lights On	At Intersection and Not Related At Driveway or Intersection Not at Intersection/Driveway/Roundabout Driveway/Related but Not at Driveway	Straight & Level Straight & Grade Straight & Grade Straight & Grade	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Unknown Distraction Under Influence of Alcohol/Drugs/Medication Apparently Asleep or Fatigued Follow Too Closely	Daylight Dark-Street Lights On Dark-Street Lights On	Dry At Intersection and Not Related Wet At Driveway or Intersection Dry Not at Intersection/Driveway/Roundabout Dry Driveway/Related but Not at Driveway	Distracted Driving Under Influence of Alcohol/Drugs/Medication Apparently Asleep, Fatigued or III Follow Too Closely	No Yes Yes No
5437 1006225.98 1011943.9 2016 5438 1007337.71 1011302.04 2017 5439 1004649.6 1012107.67[2015	2 Vehides/Parties Rear-End	Property Damage Only Operating Defective Equipment 55-64 Property Damage Only Inattention 65+ Property Damage Only Under Influence of Alcohol/Drugs/Medication 55-64	No Dr No Dr Under Influence of Drugs 55-64 Dr 25 No Dr		Driveway Related but Not at Driveway At Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Unknown Straight & Level Straight & Level Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Operating Defective Equipment Inattention Under Influence of Alcohol/Drugs/Medication Follow Too Closely	Daylight Daylight Daylight	Dry Driveway Related but Not at Driveway Dry At Driveway or Intersection Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Inattention Under Influence of Alcohol/Drugs/Medication Follow Too Closely	No Yes Yes No
5441 1004296.79 1012119.11 2015 5442 1004930.9 1012094.44 2016 5442 1008423.7 1012094.44 2016	3 2 Vehicles/Parties Sideswipe 3 2 Vehicles/Parties Sideswipe 4 1 Vehicle/Party Struck Object 5 1 Vehicle/Party Struck Object	Property Damage Only Inattention Under Property Damage Only Fail to Yield 65+ Property Damage Only Inattention Under Unknown Distracted Driving Unknown	25 No		Intersection Related but Not at Intersection At Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Unknown Straight & Level Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Inattention Fail to Yield Inattention Unknown Distraction	Daylight Daylight Dark-Street Lights On Other/Linknown Other/Linknown Other/Linknown	Dry Intersection Related but Not at Intersection Dry At Driveway or Intersection Wet Not at Intersection/Driveway/Roundabout Unknown Not at Intersection/Driveway/Roundabout	Inattention Fail to Yield Inattention Distracted Driving	Yes Yes Yes Yes
5445 1008861.98 1010288.64 2017 5446 1008389.24 1010609.5 2015 5447 996215.98 101109.1 2015	7 (2 Vehicles/Parties Angle 3 2 Vehicles/Parties Rear-End 3 2 Vehicles/Parties Angle 5 2 Vehicles/Parties Angle 5 2 Vehicles/Parties Angle	Property Damage Only Inattention Under Property Damage Colly Distracted Driving 35-44 Minor or Possible Injury Fall to Yfeld 45-54 Property Damage Only Improper Turn/Merge 65+	25 No	/ Daylight / Daylight / Daylight / Daylight / Daylight	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Straight & Grade Straight & Level Unknown Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	Inattention Distractions Outside Vehicle Fail to Yield Improper Turn/Merge	Daylight Daylight Daylight	Dry At Driveway or Intersection Dry Not at Intersection/Driveway/Roundabout Dry At Driveway or Intersection Dry At Driveway or Intersection	Inattention Distracted Driving Fail to Yield No Risky Driving Behaviors Involved	No No No No
5451 1001625.1 1013539.51 2016 5452 1001625.1 1013539.53 2016	3 2 Vehicles/Parties Rear-End 5 2 Vehicles/Parties / regle 5 2 Vehicles/Parties / regle 9 2 Vehicles/Parties / regle 9 2 Vehicles/Parties / regle	Property/Damage Only Inattention 65+ Property/Damage Only Fail to Yield Undet Minor or Possible Injury Fail to Yield 55-64-6	Under Influence of Alcohol	t Daylight / Daylight / Daylight	At Intersection and Not Related At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Straight & Level Unknown Straight & Level Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Under Influence of Alcohol/Drugs.Medication Inattention Fail to Yield Fail to Yield	Daylight	Dry	Under Influence of Alcohol/Drugs/Medication Inattention Fail to Yield Fail to Yield	No No No No
5453 1001625.1 1013539.51 2017 5454 1001667.82 1014858.31 2016 5455 1001667.82 1014858.31 2016 5456 1001602.97 1012871.77 2017	7 2 Vehicles/Parties Angle 2 Vehicles/Parties Pedestrian 3 3 Vehicles/Parties Rear-End 7 2 Vehicles/Parties Angle	Property Damage Only Inattention 45-54 Minor or Possible Injury Inattention 65+ Minor or Possible Injury Follow Too Closely Undet Property Damage Only Fail to Yield 65+	No Dr No Dr 25 No Dr No WW	/ Daylight / Daylight tt Daylight	At Driveway or Intersection	Straight & Level Straight & Level Straight & Level Straight & Level	No Vulnerable Road User Involved Pedestrian and Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+	Inattention Inattention Follow Too Closely Fail to Yield	Daylight Daylight Daylight Daylight	Dry At Driveway or Intersection Dry At Driveway or Intersection Dry At Driveway or Intersection Wet At Driveway or Intersection	Inattention Inattention Follow Too Closely Fail to Yield	No No No No
5457 1001602.98 1012871.76 (2016) 5458 1001602.97 1012871.77 (2016) 5459 1001580.87 1012219.69 (2016) 5460 1001580.87 1012219.69 (2016)	5 2 Vehicles/Parties Angle 2 Vehicles/Parties Angle 4 1 Vehicle/Party Struck Object 4 2 Vehicles/Parties Angle 4 2 Vehicles/Parties Angle	Property Damage Only Distracted Driving 65+ Property Damage Only Fail to Yield 65+ Property Damage Only Other/Unknown 45-54 Property Damage Only Inattention 65+	No Dr No WW No Dr No Dr No Dr	t Daylight / Daylight / Daylight	At Driveway or Intersection	Straight & Level Straight & Level Straight & Level Straight & Level	Older Road User 65+ Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+	Unknown Distraction Fail to Yield None Inattention	Daylight		Distracted Driving Fail to Yield No Risky Driving Behaviors Involved Inattention	No No Yes No
5462 1001581.1 1012226.24 2015 5463 1001642.47 1014057.43 2017 5464 1001702.47 1015993.03 2016	5 2 Vehicles/Parties Rear-End 2 2 Vehicles/Parties Pedalcyclist 2 Vehicles/Parties Pedalcyclist 5 1 Vehicles/Parties Pedalcyclist 5 1 Vehicles/Parties Pedalcyclist 5 1 Vehicles/Parties Struck Object 5 1 Vehicles Deathy Struck Chieset	Property Jamage Univ Other Unknown 50-04	No Or No Dr No Dr No Or Under Influence of Alcohol 65+ No Dr	/ Daylight / Dawn/Dusk	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection Not at Intersection/DrivewayRoundabout Intersection Palated but Not at Intersection	Unknown Straight & Level Unknown Straight & Level	No Vulnerable Road User Involved Pedalcydist and Older Road User 65+ Older Road User 65+ Older Road User 65+ Older Road User 65+	None Did Not Grant R/W to Non Motorist Other Contributing Circ Not Listed Under Influence of Alcohol/Drugs/Medication Under Influence of Alcohol/Drugs/Medication	Daylight Dawn/Dusk	Dry At Driveway or Intersection Dry At Driveway or Intersection Dry At Driveway or Intersection Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Fail to Yield No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication	No No No Yes
5466 1001669.06 1015335.71 2016 5467 1001667.16 1014966.17 2015 5468 1001655.75 1014970.32 2016	1 \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \	Property Damage Only Distracted Driving 65+ Property Damage Only Office/Unknown 65+ Minor or Possible Injury Follow Too Closely Under Minor or Possible Injury Inattention Under Minor Or Possible Injury Inattention Under Minor Or Possible Injury Inattention Under 15 or 15 or 1	No	/ Daylight / Daylight / Daylight	Intersection Related but Not at Intersection At Driveway or Intersection Intersection Related but Not at Intersection Not at Intersection/Driveway/Roundabout	Straight & Level Straight & Level Straight & Level Unknown	Older Road User 65+ Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Unknown Distraction Other/Unknown Follow Too Closely Inattention	Daylight Daylight Daylight	Dry Intersection Related but Not at Intersection Dry At Driveway or Intersection Dry Intersection Related but Not at Intersection Dry Not at Intersection/Driveway/Roundabout	Distracted Driving No Risky Driving Behaviors Involved Follow Too Closely Inattention	No No
5470 1001668.46 1015058.59 2018 5471 1001667.23 1014953.86 2019 5472 1001689.22 1015789.75 2018	7 2 Vehides/Paries Rear-End 3 2 Vehides/Paries Sideswipe 9 3 Vehides/Paries Sideswipe 9 3 Vehides/Paries Sideswipe 9 3 Vehides/Paries Rear-End Regile 9 2 Vehides/Paries Regile 9 2 Vehides/Paries Regile 9 1 Vehides/Paries Paries P	Minor or Possible Injury Distracted Driving 25:34 Property Damage Chily Distracted Driving 35:44 Minor or Possible Injury Inattention 65+ Property Damage Chily Inattention 65+ Property Damage Chily Fait to Yeld 65+	No Dr No Dr No Dr No Dr No Dr	/ Daylight / Daylight / Daylight	Intersection Related but Not at Intersection Intersection Related but Not at Intersection Not at Intersection/DrivewayRoundabout At Driveway or Intersection At Driveway or Intersection	Straight & Level Straight & Level Unknown Straight & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Clider Road User 65+ Clider Road User 65+ Clider Road User 65+	Distracted Driving Distracted Driving Inattention Inattention Inattention Fail to Yield	Daylight Daylight Daylight	Dry Intersection Related but Not at Intersection Dry Intersection Related but Not at Intersection Dry Not at Intersection/Driveway/Roundabout Dry At Driveway or Intersection Dry At Driveway or Intersection	Distracted Driving Distracted Driving Inattention Inattention Fail to Yield	Yes No No
5474 1001585.7 1012476.2 2015 5475 1001582.01 1012276.24 2015 5476 1000342.31 1014912.13 2014	S. Vehiclas Parties Angle 3. 2 Vehiclas Parties Angle 3. 2 Vehiclas Parties Rear-End 4. 2 Vehiclas Parties Angle 3. 2 Vehiclas Parties Angle 3. 2 Vehiclas Parties Parties Angle 4. 2 Vehiclas Parties Parties Pedestrian	Property Damage Only Fail to Yield 65+ Minor or Possible Injury Follow Too Closely 55-64 Property Damage Only Inattention 45-54	No	/ Daylight / Daylight / Daylight	At Driveway or Intersection Not at Intersection, Driveway/Roundabout At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Straight & Level	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved Pedestrian	Fail to Yield Fail to Yield Follow Too Closely Instention Other Contributing Circ Not Listed	Daylight Daylight Daylight	Dry At Driveway or Intersection Dry Not at Intersection/Driveway/Roundabout Dry At Driveway or Intersection Dry At Driveway or Intersection Dry At Driveway or Intersection	Fail to Yield Follow Too Closely Inattention No Risky Driving Behaviors Involved	No No No No
5478 999022.43 1014966.59 2016 5479 999022.43 1014966.6 2014 5480 999022.43 1014966.6 2014 5481 999016.6 1014966.5 2017	5 2 Vehides/Parties 4 1 Vehides/Party 5 2 Vehides/Party 5 2 Vehides/Party 6 2 Vehides/Party 7 1 Vehide/Party 7 1 Vehide/Party 7 1 Vehide/Party 7 Struck Object 7 1 Vehide/Party 7 Struck Object	Property Damage Only Distracted Driving 45-54 Property Damage Only Disregard Traffic Control Device/Sign 55-64 Property Damage Only Disregard Traffic Control Device/Sign 45-54 Property Damage Only Distracted Driving 45-54	No Dr WW No Dr No	/ Daylight t Dark-No Street Lights/Street Lights Off		Straight & Level	No Vulnerable Road User Involved	Other/Unknown Disregard Traffic Control Device/Sign None Distactions Outside Vehicle		Dry At Driveway or Intersection Wet At Driveway or Intersection Dry At Driveway or Intersection Dry At Driveway or Intersection Dry At Driveway or Intersection	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Distracted Driving	No Yes No Yes
5482 999024.21 1015016.58 2017 5483 1004269.43 1013452.44 2016 5484 1004269.43 1013452.44 2016 5485 1004269.43 1013452.44 2016	7 2 Vehicles/Parties Angle 5 2 2 Vehicles/Parties Angle 5 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2		25 No		Intersection Related but Not at Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Straight & Level Straight & Level Straight & Level Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Lane Departure Inattention Fail to Yield Disregard Traffic Control Device/Sign	Davlight	Dry Intersection Related but Not at Intersection Wet At Driveway or Intersection Dry At Driveway or Intersection Wet At Driveway or Intersection	No Risky Driving Behaviors Involved Inattention Fail to Yield No Risky Driving Behaviors Involved	Yes No No No
5487 1004248.24 1012785.12 2017 5488 1004221.83 1012121.4 2019 5489 1004221.82 1012121.4 2016	9 2 Vehides/Paries Podalocytist 7 2 Vehides/Paries Aegle 9 2 Vehides/Paries Pedestrian 5 1 Vehide/Pary Suck Colject 5 3 Vehide/Pary Ranke Colject 8 New Colject 9 2 Vehides/Paries Rear-End	Mnor or Possible Injury Other/Unknown 65+ Property Damage Only Other/Unknown 65+ Mnor or Possible Injury Inattention 65+ Property Damage Only Under Influence of Alcohol/Drugs/Medication 45-54	No Dr No Dr No Dr Under Influence of Drugs 45-54 Dr	/ Daylight / Daylight / Daylight / Daylight	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Straight & Grade Unknown Straight & Level Straight & Level	Pedalcydist and Older Road User 65+ Older Road User 65+ Pedestrian and Older Road User 65+ No Vulnerable Road User Involved	None None Inattention Under Influence of Alcohol/Drugs/Medication	Daylight Daylight Daylight Daylight	Dry At Driveway or Intersection	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Inattention Under Influence of Alcohol/Drugs/Medication	No No No Yes
5491 1004221.82 1012121.4 2015 5492 1004379.45 1017378.7 2015 5493 1004414.5 1017367.26 2017	5 2 Vehicles/Parties Rear-End 5 2 Vehicles/Parties Angle 7 1 Vehicle/Party Struck Object	Property Damage Only Distracted Driving Under Influence of Alcohol/Drugs/Medication 35-44 Property Damage Only Fail to Yield Under Influence of Alcohol/Drugs/Medication 45-4 Property Damage Only Inattention 65+	25 No Dr Under Influence of Alcohol 35-44 Dr 25 No W No W	/ Daylight tt Dark-Street Lights On tt Dark-Street Lights On	At Driveway or Intersection At Driveway or Intersection Roundabout Roundabout	Straight & Level Straight & Level Straight & Level Curve & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	Unknown Distraction Under Influence of Alcohol/Drugs/Medication Fail to Yield Inattention	Daylight Dark-Street Lights On Dark-Street Lights On	Dry At Driveway or Intersection Dry At Driveway or Intersection Wet Roundabout Wet Roundabout	Distracted Driving Under Influence of Alcohol/Drugs/Medication Fail to Yield Inattention	No No No Yes
5495 1004379.66 1017377 2016 5496 1004379.44 1017378.72 2017 5497 1004379.44 1017378.72 2017	5 2 Vehioles/Parties Angle 5 1 Vehiole/Party Struck Object 7 1 Vehiole/Party Struck Object 7 2 Vehioles/Parties Angle	Property Damage Only Disregard Traffic Control Device/Sign 65+ Minor or Possible Injury Distracted Driving 35-44 Minor or Possible Injury Other/Unknown 65+ Property Damage Only Fail to Yield 65+	No Dr No Dr No Dr No Ice/Snow	/ Dark-Street Lights On / Daylight //Slush Dark-No Street Lights/Street Lights Off	Roundabout Roundabout Roundabout Roundabout	Curve & Level Straight & Level Straight & Level Curve & Level	Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+	Disregard Traffic Control Device/Sign Unknown Distraction None Fail to Yield	Dark-Street Lights On Daylight Dark-No Street Lights/Street Lights Off Ice/Sn	Dry Roundabout Dry Roundabout Dry Roundabout ow/Slush Roundabout	No Risky Driving Behaviors Involved Distracted Driving No Risky Driving Behaviors Involved Fail to Yield	Yes Yes No
5499 1004379.46 1017378.69 2016 5500 1004442.5 1017433.67 2017 5501 1004414.51 1017367.25 2019	4 2 Vehicles:Parties Angle 3 1 Vehicle:Party Struck Object 2 Vehicles:Parties Angle 3 2 Vehicles:Parties Rear-End 3 Vehicles:Parties Rear-End		No WW 25 No Dr 25 No los/Snow	tt Daylight tt Dark-Street Lights On y Daylight y/Slush Daylight y Dark-Street Lights On	Roundabout Roundabout At Driveway or Intersection Roundabout At Driveway or Intersection	Curve & Level Straight & Level Curve & Level Straight & Level Straight & Level	Older Road User 65+ Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved Pedestrian	Distractions Outside Vehicle Other Contibuting Circ Not Listed Other/Unknown Other Contributing Circ Not Listed Fail to Yield	Dark-Street Lights On Daylight Daylight Ice/Sn	Wet Roundabout Wet Roundabout Dry At Driveway or Intersection ownSlush Roundabout Dry At Driveway or Intersection	Distracted Driving No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Fail to Yield	Yes No No
3992 100425.9 1012431.06 201 5503 1004225.9 1012451.88 2011 5504 1004225.9 1012451.88 2011 5505 1004225.89 1012451.88 2011 5506 1004332.05 101553.37 2011	9 2 Vehides/Paries Pedestrian 7 2 Vehides/Paries Rear-End 9 2 Vehides/Paries Rear-End 1 2 Vehides/Paries Forsk Colject 1 Vehides/Paries Struck Colject Struck Colject Struck Colject		No	y Daylight y Daylight ht Daylight	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Intersection and Not Related	Straight & Level	Pouserable Road User Involved Older Road User 65+ Older Road User 65+ No Vulnerable Road User Involved	Unknown Distraction	Daylight Daylight Daylight	Dry At Driveway or Intersection Dry At Driveway or Intersection Dry At Driveway or Intersection Met At Driveway or Intersection Dry At Intersection and Not Related	Fail to Yield Distracted Driving Fail to Yield Inattention Distracted Driving	No No Yes
5507 1004269.43 1013452.44 2017 5508 1004248.24 1012785.12 2017 5509 1004248.24 1012785.12 2017	7 2 Vehicles/Parties Angle	Property Damage Only Fail to Yield 35-44 Property Damage Only Inattention 65+ Property Damage Only Fail to Yield 65+ Mnor or Possible Injury Fail to Yield 65+	No Dr No Dr No Dr No Dr No W	Daylight Daylight Daylight Daylight Daylight Dark-No Street Lights/Street Lights Off	At Driveway or Intersection	Straight & Level Straight & Level Straight & Level Straight & Level	No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+ Pedestrian and Older Road User 65+	Fail to Yield Inattention Fail to Yield Did Not Grant R/W to Non Motorist		Dry At Driveway or Intersection Dry At Driveway or Intersection Dry At Driveway or Intersection Wet At Driveway or Intersection	Fail to Yield Inattention Fail to Yield Fail to Yield	No No No No
5511 1004221.82 1012121.4 2016 5512 1004221.82 1012121.4 2016 5513 1004296.2 101373.38 2016 5514 1004272.49 1013896.19 2014	2 Webides/Paries Sedestrian 2 Vebides/Paries Rear-End 2 Vebides/Paries Sideswipe 2 Vebides/Paries Sideswipe 2 Vebides/Paries Sideswipe 2 Vebides/Paries Rear-End	Property Damage Only Inattention Under Property Damage Only Fail to Yeld 65+ Property Damage Only Fail to Yeld Undet Property Damage Only Other/Unknown Undet	25 No	/ Daylight / Daylight / Daylight / Daylight / Dark-Street Lights On	At Intersection and Not Related At Driveway or Intersection Driveway Related but Not at Driveway Not at Intersection/DrivewayRoundabout	Straight & Level Straight & Level Straight & Level Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Inattention Fail to Yield Fail to Yield Other Contributing Circ Not Listed	Daylight Daylight Daylight Dark-Street Lights On	Dry At Intersection and Not Related Dry At Driveway or Intersection Dry Driveway Related but Not at Driveway Dry Not at Intersection/Driveway/Roundabout	Inattention Fail to Yield Fail to Yield No Risky Driving Behaviors Involved	No Yes Yes No
5515 1004232.54 1012551.77.2015 5516 1004229.2 1012501.29.2015 5517 1004287.85 1014255.13.2015 5518 1004302.04 101426.97.2015	5 2 Vehicles/Parties Rear-End 1 Vehicles/Party Struck Object 2 Vehicles/Parties Pedalopolist 3 2 Vehicles/Parties Angle Angle	Minor or Passible Injury Follow Too Closely 25:34 Property Damage Only Under Influence of Alcohol/Drugs/Medication Under Minor or Possible Injury Mon Motorist on Wrong Side of Road Under Property Damage Only Fail to Yield Under	No Dr 25 Under Influence of Alcohol Under 25 Wk 25 Index Influence of Alcohol Dr 25 No Dr Dr 25 No Wk Wk	/ Daylight tt Dark-Street Lights On / Daylight tt Daylight	Intersection Related but Not at Intersection Not at Intersection/Driveway/Roundabout At Driveway or Intersection At Driveway or Intersection	Straight & Level Straight & Level Straight & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Pedalcydist No Vulnerable Road User Involved	Follow Too Closely Under Influence of Akohol/Drugs/Medication Fail to Yield	Daylight Dark-Street Lights On	Dry Intersection Related but Not at Intersection Wet Not at Intersection/Driveway/Roundabout Dry At Driveway or Intersection Wet At Driveway or Intersection	Follow Too Closely Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved Fail to Yield	No Yes No No
5519 1004237.7 1012899.06.2017 5520 1004222.19 1012151.42.2018 5521 1000311.49 1017582.84.2011 5522 1004445.56 1017436.39.2018	7 2 Vehicides/Parties Struck Parked Vehicle 8 Agen-End 9 2 Vehicles/Parties Sideswipe 9 2 Vehicles/Parties Backing	Property Damage Only Inattention 65+ Property Damage Only Inattention 65+ Property Damage Only Improper Passing Undet Property Damage Only Inattention 65+	No Dr No Dr 25 No Dr No Dr		Not at Intersection/Driveway/Roundabout Intersection Related but Not at Intersection At Intersection and Not Related Roundabout	Straight & Level Unknown Straight & Level Straight & Level	Older Road User 65+ Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+	Inattention Inattention Improper Backing/Passing/Turning/Parking/Signaling Inattention	Daylight	Dry Not at Intersection/Driveway/Roundabout Dry Intersection Related but Not at Intersection Dry At Intersection and Not Related Dry Roundabout	Inattention Inattention No Risky Driving Behaviors Involved Inattention	Yes No Yes No
5523 1004444.11 1017420.77 2016 5524 1004444.12 1017424.06 2014 5525 1005757.8 1017374.08 2015 5526 996119.8 1008438.32 2016	5 2 Vehicles/Parties Angle	Property Damage Only Distracted Driving 65+ Mnor or Possible Injury Under Influence of Alcohol/Drugs/Medication 65+ Property Damage Only Distracted Driving Under Property Damage Only Distracted Driving Under 165+ 165+ 165+ 165+ 165+ 165+ 165+ 165+	Under Influence of Alcohol 65+ Dr No Dr 25 No Dr	y Daylight y Daylight y Daylight y Daylight	Roundabout Roundabout Not at Intersection/DrivewayRoundabout At Driveway or Intersection	Curve & Level Straight & Level Straight & Level Straight & Level	Older Road User 65+ Older Road User 65+ Older Road User 65+ No Vulnerable Road User Involved	Distractions Outside Vehicle Under Influence of Alcohol/Drugs/Medication Other Contributing Circ Not Listed Unknown Distraction	Daylight Daylight Daylight Daylight	Dry Roundabout Dry Roundabout Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout Dry At Driveway or Intersection	Distracted Driving Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved Distracted Driving	No No Yes No
5527 996119.79 1008438.33.2017 5528 996119.79 1008438.32.2019 5529 996226.92 1011578.24.2014 5530 996290.56 1011657.89.2015	7 2 Vehicles/Paries Angle 2 Vehicles/Paries Angle 4 2 Vehicles/Paries 4 2 Vehicl	Property Damage Only Fail to Yield 65+ Property Damage Only Fail to Yield 55-64 Mnor or Possible Injury Fail to Yield 65+	No Dr No Dr	/ Daylight / Daylight / Daylight / Daylight / Daylight / Daylight	At Driveway or Intersection At Driveway or Intersection Roundabout Roundabout At Driveway or Intersection	Straight & Level Straight & Level Curve & Level Curve & Level Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User involved Motorcycle and Older Road User 65+ No Vulnerable Road User involved No Vulnerable Road User Involved	Inattention Fail to Yield	Daylight	Dry At Drivewayor Intersection Dry At Drivewayor Intersection Dry Roundabout Dry Roundabout Dry At Drivewayor Intersection Dry At Drivewayor Intersection Dry At Drivewayor Intersection	Inattention Fail to Yield Fail to Yield Fail to Yield Inattention	No No No No
5532 996214.44 1011311.87 2014 5533 996200.42 1010921.14 2016 5534 996209.49 1010942.37 2018 5535 996221.11 1011236.12 2018	4 2 Vehicles/Parties Turning	Property Damage Only Inattention 65+ Property Damage Only Follow Too Closely 35-44	No Dr	y Daylight y Daylight tt Daylight tt Daylight y Daylight	At Driveway or Intersection At Driveway or Intersection Not at Hersection/Driveway/Roundabout At Driveway or Intersection At Driveway or Intersection	Straight & Level Straight & Level Straight & Level Straight & Grade Straight & Level	No Vulnerable Road User firvolved Older Road User 65+ No Vulnerable Road User involved No Vulnerable Road User involved Older Road User 65+ Older Road User 65+	Follow Too Closely	Daylight Daylight	Dry At Driveway or Intersection Dry At Driveway or Intersection Wet Not at Intersection/DrivewayRoundabout Wet At Driveway or Intersection Dry At Driveway or Intersection	Inattention Inattention Follow Too Closely Fail to Yield Fail to Yield	No No No
5536 996189.91 1010941.07 2016 5537 996221.41 1011245.16 2016 5538 996193.76 1010938.33 2016 5539 1003663.36 1012147.91 2016	### A Veninos Prants ### 2 Vehinos Prants 3 2 Vehinos Prants 4 Que 2 Vehinos Prants 4 Que 4 4 Que	Minor or Possible Injury Pail to Yield 65+ Property Damage Only Fail to Yield 45-54 Property Damage Only Inattention 55-64 Property Damage Only Fail to Yield Under	No Dr	/ Daylight	At Driveway or Intersection	Straight & Level Straight & Grade Straight & Level Straight & Level Unknown	Uilder Koad User 65+ Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Fail to Yield Fail to Yield Fail to Yield Inattention Fail to Yield	Daylight	Dry At Driveway or Intersection Wet At Driveway or Intersection	Fail to Yield Fail to Yield Inattention Fail to Yield	No No No
5540 1002908.15 1012167.812014 5541 1002893.39 1012139.2015 5542 1001580.89 1012219.77.2017 5543 1001580.88 1012219.77.2017	4 2 Vehicles/Parties Rear-End 3 2 Vehicles/Parties Sideswipe 7 1 Vehicles/Parties Sideswipe 5 2 Vehicles/Parties Pedestrian	Property Damage Only Inattention 55-64 Property Damage Colly Other/Unknown 65+ Property Damage Only Other/Unknown 55-64 Mnor or Possible Injury Fail to Yield 45-54	No	/ Daylight	At Driveway or Intersection Intersection Related but Not at Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved Pedestrian Pedestrian	Inattention Other Contributing Circ Not Listed None Did Not Grant R/W to Non Motorist	Daylight	Dry At Driveway or Intersection Dry Intersection Related but Not at Intersection Dry At Driveway or Intersection Dry At Driveway or Intersection	Inattention No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Fail to Yield	No Yes Yes No
5544 1001580.89 1012219.7[2017 5545 1000110 100829.99.2[017] 5546 1000251.26 1012288.29[2014 5547 1000251.27 1012288.29[2018	2 Vehicles:Parties	Mnor or Possible Injury Fail to Yield 35-44 Property Damage Only Speeding 25-34 Mnor or Possible Injury Fail to Yield Undeat Mnor or Possible Injury Fail to Yield Undeat	No Dr No Wo 225 No Dr 225 No Dr Dr Dr Dr Dr	Daylight	At Driveway or Intersection	Straight & Level Curve & Level Straight & Level Straight & Level	Pedestrian No Vulnerable Road User Involved Pedestrian No Vulnerable Road User Involved	Did Not Grant R.W to Non Motorist Speeding Fail to Yield Fail to Yield	Daylight	Dry At Driveway or Intersection Wet At Driveway or Intersection Dry At Driveway or Intersection Dry At Driveway or Intersection	Fail to Yield Speeding Fail to Yield Fail to Yield	No Yes No No
5548 1000.251.27 101.2288.28 (2016) 5549 1000109.14 1008431.68 2016 5550 996889.66 1011737.55 2016 5551 996901.75 1011745.65 2016	2 Vehicles/Parties Turning	Property Damage Only Fail to Yield 55-64 Property Damage Only Inattention 45-54 Property Damage Only Inattention 65+ Property Damage Only Improper Turn/Merge 55-64	No Dr No Dr No Dr No Dr	/ Dark-Street Lights On / Daylight / Daylight / Daylight	At Driveway or Intersection Not at Intersection.Driveway/Roundabout At Driveway or Intersection At Driveway or Intersection	Straight & Level Curve & Level Curve & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User involved	Fail to Yield Inattention Inattention Improper Turn/Merge	Dark-Street Lights On Daylight Daylight Daylight	Dry At Driveway or Intersection Dry Not at Intersection/Driveway/Roundabout Dry At Driveway or Intersection Dry At Driveway or Intersection	Fail to Yield Inattention Inattention No Risky Driving Behaviors Involved	No Yes Yes No
5552 1009465.74 1009903.81 2015 5553 1009465.75 1009903.82 2015 5554 1004200.61 1011966.15 2016	5 2 Vehicles/Parties Pedalcyclist	Minor or Possible Injury Inattention 35-44 Minor or Possible Injury Improper Backing 25-34	No	t Daylight t Daylight	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Intersection and Not Related	Straight & Grade Straight & Grade Straight & Level Straight & Level	Pedalcyclist No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	Inattention Improper Backing/Passing/Turning/Parking/Signaling Inattention Follow Too Closely		Wet At Driveway or Intersection Wet At Driveway or Intersection Wet At Driveway or Intersection Dry At Intersection and Not Related Wet At Driveway or Intersection Wet At Driveway or Intersection	Inattention No Risky Driving Behaviors Involved Inattention Follow Too Closely	No No No No
5557 1004226.97 1011973.38 2018 5557 1004183.11 1011467.36 2016	2 Vehides/Paries Podalcyclist	Property Damage Only Disregard Traffic Control Device/Sign 65+ Property Damage Only Fail to Yield 65+ Minor or Possible Injury Follow Too Closely 65+	No VIE	tt Daylight tt Daylight / Daylight / Daylight / Daylight / Daylight	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Intersection and Not Related	Straight & Level Straight & Level Straight & Level Unknown	Pedalcyclist and Older Road User 65+ Older Road User 65+ Older Road User 65+ Older Road User 65+	Did Not Grant R/W to Non Motorist Disregard Traffic Control Device/Sign Fail to Yield	Daylight	Dry At Driveway or Intersection Dry At Intersection and Not Related	Fail to Yield No Risky Driving Behaviors Involved Fail to Yield Follow Too Closely	No No No No
5561 100420.97 1011140.72 2017 5562 1004169.64 1011140.65 2017 5563 1004221.83 1012121.4 2019 5564 1004221.82 1012121.4 2019	\$ 2 Vehides/Parties Angle 7 2 Vehides/Parties Angle 7 2 Vehides/Parties Angle 8 1 Vehides/Parties 1 Vehides/Parties 8 2 Vehides/Parties 1 2 Vehides/Parties 1 2 Vehides/Parties 1 2 Vehides/Parties 1 3 Vehides/Parties 1 3 Vehides/Parties 1 4 2 Vehides/Parties 1 5 Vehides/Parties 1 5 Vehides/Parties 1 6 Vehides/Parties 1 7 Vehides/Parties 1 8 Vehides/Parties 1 8 Vehides/Parties 1 9 Vehides/Parties 1 1 Vehides/Parties	Minor or Possible Injury Fail to Yield 55-64-6 Minor or Possible Injury Inattention Under Properly Damage Chiy Institution 65+ Properly Damage Chiy Improper Turn/Merge 55-64 Minor or Possible Injury S5-64 55-25-34 Minor or Possible Injury S5-34 55-34	No Dr 26 No Dr No Dr No Dr No Dr	/ Dark-Street Lights On / Daylight	At Drivewayor Intersection	Straight & Level Straight & Level Straight & Level Unknown Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Fail to Yield Inattension Inattension Inattension Improper Turn/Merge Eating or Drinking	Dark-Street Lights On Daylight	Dry At Driveway or Intersection	Fail to Yield Inattention Inattention No Risky Driving Behaviors Involved Distracted Driving	No No No Yes
5564 1004221.82 1012121.41 2015 5565 1004221.83 1012121.41 2015 5566 1004193.88 1011808.35 2016 5567 1004170.09 1010820.11 2015 5568 1004183.11 1011407.33 2016	2 Vehrides/Partes Rear-End	Mnor or Possible Injury Distracted Driwng 25:34 Mnor or Possible Injury OtherUnknown Under Mnor or Possible Injury OtherUnknown 55:64 Property Damage Cniy Distracted Driving 65+ Property Damage Cniy Under Influence of Alcohol/Drugs/Medication 65+	25 No We	y Daylight tt Dark-Street Lights On tt Dark-Street Lights On y Daylight y Dark-Street Lights On	At Driveway or Intersection	Straight & Level	No Vulnerable Road User Involved Pedaloxilist Pedaloxilist Older Road User 65+ Older Road User 65+	Eating or Drinking Other/Unknown None Unknown Distraction Under Influence of Alcohol/Drugs/Medication	Dark-Street Lights On Dark-Street Lights On	Dry At Driveway or Intersection Wet At Driveway or Intersection Wet At Driveway or Intersection Dry At Driveway or Intersection Dry At Driveway or Intersection	Distracted Driving No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Distracted Driving Under Influence of Alcohol/Drugs/Medication	No No No
5569 1004183.11 1011467.33[2016] 5570 1004169.66 1011140.65 2017 5571 1004199.61 1011621.91 2016 5572 1004223.15 1011727.25 2015	s I Venicios-Party 3 I Vehicles/Parties Pagle 7 2 Vehicles/Parties 8 2 Vehicles/Parties 9 3 V	Property Damage Only Fail to Yield 65+ Minor or Possible Injury Inattention 45-54	Under Insulance of Woohol B5+ UF No No Dr No Dr No Dr No Dr No Dr No Dr Or No Dr Or No Dr Or No Dr	/ Daylight	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection Not at IntersectionDrivewayRoundabout Intersection Related but Not at Intersection	Straight & Level	Uilder Koau User 65+ Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+	Under insuence or reconclutrugs, recolcasion Fail to Yield Inattention Institution Fail to Yield	Davlight	Dry At Driveway or Intersection Dry At Driveway or Intersection Dry At Driveway or Intersection Dry Not at Intersection/Driveway/Roundabout Dry Intersection Related but Not at Intersection	Under initiation Fail to Yield Inattention Inattention Fail to Yield	No No No
	3 z venioss-varies 4 2 Vehioss-Paries 7 2 Vehidas-Paries 8 a Vehidas-Paries 3 3 Vehidas-Paries 5 3 Vehidas-Paries 8 Rear-End 8 Rear-End 9 Rear-End 9 Rear-End	Property Damage Only Property Damage Only Inattention 25:34 Property Damage Colly Inattention 56:4 Minor or Possible Injury Distracted Driving 45:54 Minor or Possible Injury Distracted Driving 45:54	No	/ Daylight / Daylight	intersection Related but Not at intersection Intersection Related but Not at Intersection At Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Straight & Level Straight & Level Straight & Level Straight & Grade Straight & Grade	Uilder Koal User 65+ No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Inattention	Daylight	Dry Intersection Related but Not at intersection Dry Intersection Related but Not at Intersection Dry At Driveway or Intersection Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout	Fall to Held Inattention Inattention Under Influence of Alcohol/Drugs/Medication Distracted Driving	No No Yes
5577 1004216.82 1012049.45 2019 5578 1004448.44 1018424.27 2016	3 2 Vehicle/Parties Rear-End 5 1 Vehicle/Party Struck Object	Minor or Possible Injury Other/Unknown Under Mnor or Possible Injury Other/Unknown Under	25 No Dr Dr	y Daylight y Dark-Street Lights On	Not at Intersection/Driveway/Roundabout At Intersection and Not Related	Straight & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Other/Unknown Other Contributing Circ Not Listed	Daylight Dark-Street Lights On	Dry Not at Intersection/Driveway/Roundabout Dry At Intersection and Not Related	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No Yes

OBJECTID JURISDICTION	COUNTY CITY	PRIMARY TRAFFICWAY	BLOCK NUMBER	INTERSECTING TRAFFICWAY	DIST FROM FT FROI REF_POINT	_DIR REFERENCE POINT_NAME	MILEPOST A SR_ONLY HISTORY SUSPENSE_IND NUMBER	DATE TIME MOST_SEVERE INJURY_TYPE	NUM NUM NUM NUM NUM NUM NUM FAT VEH PEDS BIK	M VEH_1_TYPE	VEH_2_TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD_SURFACE ROADWAY CHARACTERISTIC	LIGHTING CONDITION	FIRST_COLLISION TYPE_OBJECT_STRUCK	VEH_1 ACTION	VEH_2 ACTION	VEH.1.COMPASS DIRECTION_FROM DIRECTION_TO DIRECTION_FROM DIRECTION_TO
5579 City Street 5580 City Street 5581 City Street 5582 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	SEQUIM DUNGENESS V SEQUIM DUNGENESS V SEQUIM DUNGENESS V W ALDER ST	VAY 300 VAY 1300	OLD OLYMPIC HWY	37 F N 330 F N 100 F W	PORT WILLIAMS RD RAINVIEW LN N SEQUIM AVE	No E437606 No E818687 No E406908 No E378016	2015-03-05 13:03 No Apparent Injury	0 0 2 0 0 0 1 0 0 0 3 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	Entering Roundabout Roundabout Related but not at Roundabout Driveway Related but Not at Driveway At Driveway	Clear or Partly Cloudy	Dry Curve & Level Dry Curve & Level Dry Straight & Level Dry Straight & Level	Daylight Daylight Daylight Daylight	Entering at angle Metal Sign Post From same direction - both going straight - both moving - rear-end One parked-one moving	Merging (Entering Traffic) Making Right Turn Going Straight Ahead Making Right Turn	Going Straight Ahead Slowing Stopped in Roadway	West Southeast North South East North South North South North South North West South North North
5583 City Street 5584 City Street 5585 City Street 5586 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W BELL ST W BRACKETT RD W CEDAR ST W CEDAR ST	100 S 808 400 M	S SEQUIMAVE N 5TH AVE N SEQUIMAVE	277 F W	JUNIPER MOBILE ESTS	No E40842 No 3202198 No E335692 No 2767134	2015-02-12 08:05 No Apparent Injury 2015-11-04 14:41 No Apparent Injury 2014-06-17 10:50 Possible Injury 2016-09-08 14:54 No Apparent Injury	0 0 1 0 0 0 3 0 1 0 1 1 0 0 2 0	0 Truck & Trailer 0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Passenger Car 0 Pickup,Panel Truck or Vanette under 10,000 lb		At Intersection and Related At Driveway At Intersection and Related At Intersection and Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level Dry Dry Straight & Level	Daylight Daylight	Street Light Pole or Base One parked-one moving Vehicle turning left hits pedestrian Entering at angle	Making Right Turn Backing Making Let Turn Making Right Turn	Legally Parked, Unoccupied Stopped for Traffic	West South Vehicle Backing South East South West South Vehicle Stopped
5587 City Street 5588 City Street 5589 City Street 5590 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W FIR ST W FIR ST W HAMMOND ST W HENDRICKSON RD	300 M 100 S 0 S	S SEQUIM AVE S SEQUIM AVE N 7TH AVE	98 F W	N SEQUIM AVE	No 2727646 No 3046763 No E887066 No E820657	2015-12-14 12:52 No Apparent Injury 2014-06-09 18:39 No Apparent Injury 2019-01-28 07:42 No Apparent Injury 2018-07-23 18:40 No Apparent Injury	0 0 1 0 0 0 1 0 0 0 2 0 0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup-Panel Truck or Vanette under 10.000 lb Pickup-Panel Truck or Vanette under 10.000 lb	At Intersection and Related At Driveway At Intersection and Related At Intersection and Related	Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Grade Dry Straight & Level	Daylight Daylight	Street Light Pole or Base Unitly Pole Entering at angle Entering at angle	Making Left Turn Making Left Turn Making Left Turn Making Right Turn	Going Straight Ahead Going Straight Ahead	East South North East West North South North South East West East
5591 City Street 5592 City Street 5593 City Street 5594 City Street		W HENDRICKSON RD W HENDRICKSON RD W HENDRICKSON RD W HENDRICKSON RD	900 M 800 M 900 M	N KENDALL RD N KENDALL RD N KENDALL RD	70 F W	GARRY OAK DR	No 3615934 No E357516 No E524893 No 2727885	2015-12-31 15:36 No Apparent Injury 2014-09-17 12:13 Possible Injury 2016-03-13 16:05 No Apparent Injury 2016-07-07 11:55 No Apparent Injury	0 0 2 0 1 0 2 0 0 0 2 0 0 0 2 0	Passenger Car Passenger Car Passenger Car Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Related At Driveway	Clear or Partly Cloudy Clear or Partly Cloudy Raining Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Wet Straight & Level Dry Straight & Level	Daylight Daylight	Entering at angle Entering at angle Entering at angle Entering at angle	Making Left Turn Making Left Turn Making Right Turn Going Straight Ahead	Making Left Turn Making Left Turn Stopped at Signal or Stop Sign Making Left Turn	North East West North North Southeast West North East Northwest Vehicle Stopped Vehicle Stopped West East North South
5595 City Street 5596 City Street 5597 City Street 5598 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W HENDRICKSON RD W HENDRICKSON RD W HENDRICKSON RD W HENDRICKSON RD	500 500 500 700		30 F E 406 F W 286 F E 75 F W	GARRY OAK DR N 5TH AVE N 5TH AVE N 7TH AVE	No E328513	2014-05-07 10:07 Possible Injury 2014-10-09 14:01 No Apparent Injury	1 0 2 0 0 0 2 0 0 0 2 0 0 0 2 0 0 0 2 0	Passenger Car Passenger Car Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway Driveway Related but Not at Driveway	Clear or Partly Cloudy Clear or Partly Cloudy Raining Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Wet Dry Straight & Level	Daylight Daylight Dayl-Street Lights On	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end One parked-one moving From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic Slowing Legally Parked, Unoccupied Stopped in Roadway	East West Vehicle Stopped Vehicle Stopped East West East West West East West East West East West East
5599 City Street 5600 City Street 5601 City Street 5602 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W HENDRICKSON RD	700 600 0 E	E WASHINGTON ST	500 F W 135 F E	N 7TH AVE N SPENCER FARM PL			0 0 2 0	0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Curve at Hilicrest Dry Straight & Grade	Daylight	From opposite direction - all other stages - one stages -	Other* Making Left Turn	Going Straight Ahead Stopped at Signal or Stop Sign	West East East West East North North South Vehicle Stopped Vehicle Stopped
5603 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	W SEQUIMBAY RD W SEQUIMBAY RD W SEQUIMBAY RD	0 N 300 1600	RHODEFER RD	66 F W 50 F NW	FARWEATHER DR FORREST RD	No E371511 No 3803627	2014-11-03 12:42 Suspected Minor Injury 2014-06-20 17:19 Suspected Minor Injury	1 0 1 1	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Raining Clear or Partly Cloudy	Wet Straight & Level Wet Straight & Level Dry Curve & Level	Daylight Daylight	Verlicue versionneu Entering at angle Vehicle going straight hits pedestrian Tree or Stump (stationary)	Going Straight Ahead Making Right Turn Starting in Traffic Lane Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	Cast
5606 City Street 5607 City Street 5608 City Street 5609 City Street 5610 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W SEQUIMBAY RD W SEQUIMBAY RD W SEQUIMBAY RD W SILBERHORN RD	900 2700 0 F	RIVER RD	160 F NW 306 F E 0.19 M SW	FORREST RD INDEPENDENCE DR WHITEFEATHER WAY	No E307077 No 2767054	2019-10-21 10:40 Possible Injury 2014-02-11 06:55 No Apparent Injury 2017-10-22 10:20 No Apparent Injury 2014-06-04 16:17 No Apparent Injury 2018-06-20 14:36 No Apparent Injury	0 0 1 0 0 0 1 0 0 0 3 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related At Intersection and Related At Intersection and Related	Overcast Raining Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Wet Curve & Grade loe Straight at Hiller Dry Straight & Level Dry Straight & Level Dry Straight & Level	Dark-No Street Lights Daylight	Matibox Nation Utility Pole Entering at angle	Going Straight Ahead	Going Straight Ahead Stopped at Signal or Stop Sign	East West East West East West West East West East South North
5610 City Street 5611 City Street 5612 City Street 5613 City Street 5613 City Street 5614 City Street 5615 City Street	Clallam Sequim Clallam Sequim	W SPRUCE ST W SPRUCE ST W SPRUCE ST W SPRUCE ST	0 h 400 h 400 h	N 5TH AVE N 5TH AVE N 5TH AVE N 5TH AVE			No 3615933	2015-12-04 09:00 No Apparent Injury	0 0 1 0	Prokup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Related	Raining Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - one stopped - rear-end Utility Pole Entering at angle Entering at angle	Going Straight Ahead Going Straight Ahead Making Right Turn Making Let Turn Going Straight Ahead	Stopped at Signal or Stop Sign Going Straight Ahead Going Straight Ahead	East West East West East North East South South North West East South North
5614 City Street 5615 City Street 5616 City Street 5617 City Street 5618 City Street 5618 City Street 5619 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	100 M 300 M	N 2ND AVE N 3RD AVE N 4TH AVE	49 F W	N 5TH AVE	No 2728115	2015-08-22 16:53 No Apparent Injury	1 0 2 0 0 0 1 0 0 0 2 0 0 0 2 0 0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related	Clear or Partly Cloudy	Dry Straight & Level Wet Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Daylight Daylight	Fence Entering at angle Entering at angle Street Light Pole or Base	Going Straight Ahead Going Straight Ahead Merging (Entering Traffic) Making Left Turn Making Right Turn	Going Straight Ahead Going Straight Ahead	East West East West South West East West West North West West
5618 City Street 5619 City Street 5620 City Street 5621 City Street 5622 City Street 5622 City Street	Cialiam Sequim Cialiam Sequim Cialiam Sequim Cialiam Sequim Cialiam Sequim Cialiam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	0 N 700 N 700 N	N 4TH AVE N 4TH AVE N 5TH AVE N 5TH AVE			No 3616147	2019-07-23 08:03 No Apparent Injury 2018-09-18 16:07 Possible Injury 2018-10-09 11:12 Possible Injury 2015-06-02 08:09 No Apparent Injury 2014-09-09 17:06 No Apparent Injury	0 0 2 0	Passenger Car Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Truck - Double Trailer Combinations	Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Raining Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Wet Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - one right turn - one straight From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - sideswipe Steet Light Pole or Base	Going Straight Ahead Going Straight Ahead Changing Lanes Making Right Turn	Stopped for Traffic Stopped at Signal or Stop Sign Going Straight Ahead	East West East Vehicle Stopped East West Vehicle Stopped Vehicle Stopped West East West East East North East
5622 City Street 5623 City Street 5624 City Street 5625 City Street	Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	700 M 700 M 700 M	N 5TH AVE N 7TH AVE N 7TH AVE N 7TH AVE			NO 245/U/1	2015-12-17 12:07 No Apparent Injury	0 0 2 0	Truck - Double Trailer Combinations Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	Passenger Car Passenger Car	At Intersection and Not Related At Driveway within Major Intersection At Intersection and Not Related At Intersection and Related	Raining Raining	Dry Straight & Level Wet Straight & Level Dry Straight & Level Wet Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - one stopped - rear-end Entering at angle From same direction - both going straight - both moving - sideswipe From same direction - both going straight - one stopped - rear-end	Making Right Turn Going Straight Ahead Making Left Turn Changing Lanes Going Straight Ahead	Stopped for Traffic Going Straight Ahead Slowing Stopped for Traffic Stopped for Traffic	East West Vehicle Stopped Vehicle Stopped South West West East West Northeast West East Vehicle Stopped West East
5626 City Street 5627 City Street 5628 City Street 5629 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	700 M 700 M	N 7TH AVE N 7TH AVE N 7TH AVE			No 2728107 No E469844 No E599766 No E767236	2014-09-12 15:51 No Apparent Injury 2015-10-07 10:02 No Apparent Injury 2016-10-21 22:52 Possible Injury	0 0 2 0 0 0 2 0 1 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup.Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 ib Pickup Panel Truck or Vanette under 10,000 ib Pickup Panel Truck or Vanette under 10,000 ib Passenger Car Passenger Car Pickup Panel Truck or Vanette under 10,000 ib	At Driveway within Major Intersection At Driveway within Major Intersection At Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Raining Clear or Partly Cloudy Overcast		Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - rear-end Entering at ano	Going Straight Ahead	Slowing Slowing	East West Vehicle Stopped East West East West East West East West North South West East
5628 City Street 5629 City Street 5630 City Street 5631 City Street 5631 City Street 5632 City Street 5632 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST		N 7TH AVE N SEAL AVE N SEAL AVE N SEQUIMAVE			No 3202192 No 2767137	2015-10-26 15:40 No Apparent Injury 2016-09-15 17:23 No Apparent Injury	0 0 0 0	O Dielos Desel Tereb ex Verselle redex 10 000 lb	Distance Depart Tourisms Manager and as 40,000 fb	At leters notice and Net Beleted	Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Level Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Dark-Street Lights On Daylight	Entering at angle Traffic Island From same direction - both going straight - one stopped - sideswipe One car leaving parked position	Going Straight Ahead Going Straight Ahead Overtaking and Passing Merging (Entering Traffic)	Going Straight Ahead Going Straight Ahead Stopped at Signal or Stop Sign Going Straight Ahead	West East South North East West West West East Vehicle Stopped West Suithwest Fast West
5634 City Street 5635 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	100 M	N SEQUIMAVE N SEQUIMAVE RIVER RD			No E363367 No E477690 No 3202127	2014-10-08 14:55 Possible Injury 2015-11-02 09:55 No Apparent Injury 2014-02-18 14:22 No Apparent Injury 2017-07-14 13:47 No Apparent Injury	1 0 2 0 0 0 2 0 0 0 2 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	At Intersection and Related At Intersection and Not Related Entering Roundabout Entering Roundabout	Clear or Partly Cloudy Overcast Raining Clear or Partly Cloudy	Dry Straight & Level Wet Straight & Level Wet Curve & Level Dry	Daylight Daylight Daylight	From same direction - both going straight - both moving - rear-end One car leaving parked position Entering at angle Entering at angle	Going Straight Ahead Starting From Parked Position Merging (Entering Traffic) Merging (Entering Traffic)	Slowing Going Straight Ahead Going Straight Ahead Going Straight Ahead	East West East West East West East West South East West East Court Modificant Continued East
5637 City Street 5638 City Street 5639 City Street 5641 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	200 S	RIVER RD RIVER RD S 2ND AVE			No E322913 No E347812 No 3202126	2014-04-22 18:09 No Apparent injury 2014-08-09 21:30 Suspected Minor Injury 2014-02-18 13:14 No Apparent Injury	0 0 1 0 1 0 1 0 0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Motorcycle Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	Circulating Roundabout Entering Roundabout At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Raining Clear or Partly Cloudy	Dry Curve & Level Dry Curve & Level Wet Straight & Level Dry Straight & Level	Daylight Dark-Street Lights On Daylight	Theory Sumple (stationary) From same direction - both going straight - both moving - sideswipe From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic Stopped for Traffic	North
5641 City Street 5642 City Street 5643 City Street 5644 City Street 5644 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	300 S 300 S 0 S	S 3RD AVE S 3RD AVE S 3RD AVE			No 2727974 No 3202200 No E773487	2015-04-05 17:34 Possible Injury 2015-11-20 10:11 Possible Injury 2018-02-24 16:25 Possible Injury	1 0 2 0 1 0 3 0 6 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Sand/Mud/Dirt Straight & Level	Daylight Daylight Daylight	Entering at angle Entering at angle From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Making Right Turn Going Straight Ahead Going Straight Ahead	Going Straight Ahead Going Straight Ahead Stopped at Signal or Stop Sign	West South South North West East North South East West Vehicle Stopped Vehicle Stopped West West West Vehicle Stopped Vehicle Stopped
5646 City Street 5647 City Street 5648 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	700 S 700 S 700 S	S STH AVE S STH AVE S STH AVE			No 2728112 No 2767139 No 2767151	2015-07-13 11:12 No Apparent Injury 2016-09-16 15:30 No Apparent Injury 2016-03-13 16:34 Possible Injury		Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Overcast	Dry Straight & Level Dry Straight & Level Dry Straight & Level Wet Straight & Level Wet Straight & Level	Daylight Daylight Daylight	From same direction - both going straight - one stopped - rear-end Entering at angle From same direction - both going straight - both moving - rear-end Entering at angle	Going Straight Ahead Making Right Turn Going Straight Ahead Going Straight Ahead	Stopped for Traffic Making Left Turn Going Straight Ahead Going Straight Ahead	West South South West East West East West East West East North South
5649 City Street 5650 City Street 5651 City Street 5652 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	500 S 500 S 700 S	S STH AVE S STH AVE S STH AVE			No E335693 No E351219 No E441681		1 0 2 0 0 0 2 0 0 0 4 0	0 Truck (Flatbad Van.etc) 0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Passenger Car 0 Passenger Car	Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level Dry Dry Straight & Level Dry Straight & Level	Daylight	From same direction - all others From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Backing Going Straight Ahead Stopped for Traffic Going Straight Ahead	Stopped for Traffic Stopped in Roadway Going Straight Ahead Stopped for Traffic	East Vehicle Backing Vehicle Stopped Vehicle Stopped West South Vehicle Stopped Vehicle Stopped West Vehicle Stopped West East West East Vehicle Stopped Vehicle Stopped
5653 City Street 5654 City Street 5655 City Street 5656 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	0 S 0 S 600 S 700 S	STIH AVE STIH AVE STIH AVE STIH AVE			No 2727981 No E307079 No E357517	2016-11-14 15:45 No Apparent Injury 2014-02-10 11:58 Possible Injury 2014-09-17 17:57 Suspected Serious Injury	1 0 2 0 0 0 2 0 1 0 2 0 y 1 0 1 0	1 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Overcast Overcast	Dry Straight & Level Wet Straight & Level Dry Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end Vehicle - Pedaloyclist	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Starting in Traffic Lane Stopped for Traffic Stopped at Signal or Stop Sign	West East West East West East Vehicle Stopped West East Vehicle Stopped South North Vehicle Stopped
5657 City Street 5658 City Street 5659 City Street 5660 City Street 5661 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	700 S 700 S 0 S	S 7TH AVE S 7TH AVE S 7TH AVE S 9TH AVE			No E494571 No E571646 No E747670 No 2457017	2016-12-15 13:10 Possible Injury 2016-08-08 12:40 No Apparent Injury 2017-12-15 14:43 Possible Injury	1 0 2 0 0 0 2 0 1 0 2 0	O Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Related Entering Roundabout	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Raining	Dry Straight & Level Dry Straight & Level Dry Straight & Level Wet Curve & Level	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - sideswipe From same direction - both going straight - one stopped - rear-end Entering at angle	Going Straight Ahead Changing Lanes Going Straight Ahead Making Right Turn Going Straight Ahead Going Straight Ahead	Stopped for Traffic Going Straight Ahead Stopped at Signal or Stop Sign Going Straight Ahead	West East Vehide Stopped Vehide Stopped West East West East West East Vehide Stopped Vehide Stopped South Southeast Southwest Southeast
	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	0 5	S 9TH AVE S 9TH AVE S 9TH AVE S 9TH AVE			No 2728028 No 2978772	2017-01-18 07-40 No Apparent Injury 2018-12-19 14:02 No Apparent Injury 2015-02-19 10:53 No Apparent Injury	0 0 1 0	0 Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Entering Roundabout	Raining Clear or Partly Cloudy Raining Clear or Partly Cloudy	Wet Dry Straight & Level Wet Straight & Level Dry Curve & Level Dry Curve & Level	Dark-Street Lights On Davlight	Entlering at angle Traffic Island From opposite direction - one left turn - one straight From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Making Left Turn Stopped for Traffic Stopped for Traffic	West East West South East West East Vehicle Stopped West East West East
5665 City Street 5666 City Street 5667 City Street 5668 City Street 5669 City Street 5669 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	0 5	S 9TH AVE S 9TH AVE S 9TH AVE S 9TH AVE			No 3803630	2019-09-04 17:19 No Apparent Injury	0 0 2 0 0 0 2 0 0 0 1 0 0 0 1 0	Passenger Car Passenger Car Pickup Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Entering Koundabout	Overcast Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Curve & Level	Daylight Dark-Street Lights On	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end Concrete Barrier/Jessey Barrier - Face Traffic Island	Going Straight Ahead Going Straight Ahead Making Left Turn Going Straight Ahead Making Right Turn	Stopped for Traffic Stopped for Traffic	East West Vehicle Stopped East West East West Southwest Northwest West East
5669 City Street 5670 City Street 5671 City Street 5672 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	0 S	S 9TH AVE S 9TH AVE S 9TH AVE S 9TH AVE			No E615830	2016-12-02 21:35 No Apparent Injury 2018-03-22 20:24 No Apparent Injury	0 0 1 0	Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Entering Roundabout Entering Roundabout Entering Roundabout Entering Roundabout Entering Roundabout	Clear or Partly Cloudy Overcast Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry	Daylight Dark-Street Lights On Dark-Street Lights On Daylight Dark-Street Lights On	Entering at angle Retaining Wall (concrete, rock, brick, etc.) Linear Curb Entering at angle	Making Right Turn Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead Going Straight Ahead	West Southeast North South East West West West East West South North East West
5673 City Street 5674 City Street 5675 City Street 5676 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	0 S 0 S 1000 S	S 9TH AVE S PRIEST RD S PRIEST RD S PRIEST RD			No E874166 No 2728037 No 2728109 No 3046668	2017-03-18 18:35 Possible Injury	3 0 2 0	0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Entering Roundabout At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Raining Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Wet Straight & Level	Dark-Street Lights On Dusk Daylight Daylight	Traffic Island From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic Stopped at Signal or Stop Sign Stopped at Signal or Stop Sign	East West Vehicle Stopped Vest Least West Vehicle Backing Vehicle Stopped East West Vehicle Backing Vehicle Stopped East West Vehicle Backing Vehicle Stopped
5677 City Street 5678 City Street 5679 City Street 5679 City Street 5680 City Street 5681 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	1200 S 1000 S 1000 S	S PRIEST RD S PRIEST RD S PRIEST RD			No 3046766 No 3202177 No 3202187	2015-04-30 11:29 No Apparent Injury 2015-10-08 18:36 Possible Injury	1 0 2 0	0 Passenger Car 0 Passenger Car 0 Passenger Car 0 Passenger Car	Passenger Car Passenger Car Pickup Panel Truck or Vanette under 10,000 lb Passenger Car Pickup Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Raining Operant	Dry Straight & Level Dry Straight & Level Dry Straight & Level Wet Straight & Level Dry		From same direction - both going straight - both moving - rear-end From opposite direction - one left turn - one right turn Entering at angle From same direction - both going straight - one stopped - rear-end	Making Right Turn Going Straight Ahead	Going Straight Ahead Making Left Turn Going Straight Ahead Stopped at Signal or Stop Sign	East West East West West South East South West East North South West East North South
5682 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	0 S 0 S 1000 S	PRIEST RD S PRIEST RD S PRIEST RD S PRIEST RD			No 3759302 No 3759378 No E407543 No E673377	2017-05-04 17:43 No Apparent Injury 2017-08-21 06:40 Possible Injury 2015-03-13 14:25 No Apparent Injury 2017-04-25 17:43 Possible Injury	0 0 2 0	Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related	Raining Clear or Partly Cloudy Clear or Partly Cloudy Overrast	Wet Straight & Level Dry Straight & Level Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Dawn Daylight Davlight	Entering at angle Entering at angle From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead Straight Ahead Starting in Traffic Lane Going Straight Ahead	Going Straight Ahead Going Straight Ahead Stopped at Signal or Stop Sign	East West South North West East North South East West Vehicle Stopped Vehicle Stopped North South West Fast
5685 City Street 5686 City Street 5687 City Street 5688 City Street 5688 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	100 S 100 S 100 S	S SEQUIM AVE S SEQUIM AVE S SEQUIM AVE			No 3202178 No E345633 No E454164 No E562126	2015-05-03 14:03 No Apparent Injury 2014-07-31 16:57 Possible Injury 2015-08-21 17:46 No Apparent Injury 2016-08-21 No Apparent Injury	0 0 2 0 1 0 2 0 0 0 2 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Please part of the Policy Parent Privace or Variette under 10,000 to Policy Parent Privace or Variette under 10,000 to Policy Parent Privace professional Control Parents (Parents under 10,000 to Policy Parents Privace or Variette under 10,000 to Parents Privace or Vari	At Intersection and Related At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Daylight	Same direction both turning right both moving sideswipe From same direction both going straight one stopped rear-end From same direction both going straight one stopped rear-end	Going Straight Ahead Making Right Turn Going Straight Ahead Going Straight Ahead Straight and Traffer Lang	Going Straight Ahead Making Right Turn Stopped at Signal or Stop Sign Stopped at Signal or Stop Sign Stopped at Signal or Stop Sign	West South West South West East Vehicle Stopped Vehicle Stopped
5690 City Street	Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	700 V 700 200	W BRACKETT RD	0 F W 186 F W	7TH AVE N 2ND AVE	No E934520 No 2729723 No 3803701	2019-06-24 13:32 Possible Injury 2015-03-02 12:35 No Apparent Injury 2015-03-02 12:35 No Apparent Injury 2019-12-12 13:56 No Apparent Injury	1 0 2 0 0 0 2 0 0 0 3 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Distance Deep Land to Deep Land to 0,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related Not at Intersection and Not Related Intersection Related but Not at Intersection Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Raining Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Grade Dry Straight & Level Wet Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - sides wipe From same direction - both going straight - one stopped - rear-end	Starting in Traffic Lane Going Straight Ahead Changing Lanes Going Straight Ahead	Stopped at Signal or Stop Sign Stopped for Traffic Going Straight Ahead Stopped for Traffic	West East West Sehide Stopped Wehide Stopped West East West Sehide Stopped West East West West East East East East East East East Ea
5691 City Street 5692 City Street 5693 City Street 5694 City Street 5695 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	200 400 400		275 F W 100 F E 27 F W 95 F W	N 3RD AVE N 4TH AVE N 4TH AVE	No E901616 No 3202166 No 3800253	2019-03-06 15:13 Suspected Minor Injury 2019-03-13 15:15 Possible Injury 2019-06-04 11:08 Suspected Minor Injury	3 0 4 0 1 0 2 0 1 0 3 0	O Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car O Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end	Going Straight Ahead Stopped for Traffic Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead Stopped for Traffic Stopped for Traffic Going Straight Ahead	East West East West West West East Vehicle Stopped Vehicle Stopped East West East West East West East West
5699 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	400 600 400		118 F W 388 F W 129 F E	N 4TH AVE N 5TH AVE N 5TH AVE	No 2727990			0 Pickup,Panel Truck or Vanette under 10,000 lb	Don as Mates Chana	Net at laterareaties and Net Balatad	Clear or Partly Cloudy Clear or Partly Cloudy Overcast Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Wet Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end Entering at angle From same direction - both going straight - one stopped - rear-end	Stopped for Traffic Going Straight Ahead Making Left Turn Overtaking and Passing	Going Straight Ahead Stopped for Traffic Going Straight Ahead Stopped for Traffic	Vehicle Stopped Vehicle Stopped East West West Cast West Vehicle Stopped West Vehicle Stopped West West Vehicle Stopped
5701 City Street 5702 City Street 5703 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST	400 400 600 600		129 F E 218 F E 100 F E 0.12 M E 359 F E	N 5TH AVE N 5TH AVE N 7TH AVE	No 2457018 No 2457067		0 0 2 0	Passenger Car Passenger Car Passenger Car Passenger Car Passenger Car	Bus or Motor Stage Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Bus or Motor Stage Pickup,Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at intersection Not at Intersection and Not Related At Driveway Intersection Related but Not at Intersection	Clear or Party Cloudy Clear or Party Cloudy Clear or Party Cloudy Overcast	Dry Straight & Level Dry Dry Straight & Level Wet Straight & Level	Daylight Dark-Street Lights On	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - sideswipe Entering at angle From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Making Let Turn Going Straight Ahead	Stopped for Traffic Stopped for Traffic Going Straight Ahead Stopped for Traffic	East West Vehide Stopped East West Vehide Stopped North East West East West East East Vehide Stopped
5705 City Street 5706 City Street 5707 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	600 600		359 F E 437 F E 391 F E 500 F E 200 F E	N 7TH AVE N 7TH AVE N 7TH AVE	No 2727712 No 2767157	2015-06-08 15:30 No Apparent Injury 2016-04-22 15:46 No Apparent Injury	0 0 2 0	O Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at intersection and two released Intersection Related but Not at Intersection Not at Intersection and Not Related Intersection Related but Not at Intersection	Clear or Partly Cloudy Clear or Partly Cloudy Raining Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Wet Straight & Level Dry Straight & Level	Daylight Dusk	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic Stopped for Traffic Stopped for Traffic Slowing Stopped at Signal or Stop Sign	East West Last West East West Vehide Backing Vehide Stopped East West East West East West Vehide Backing Vehide Stopped
5709 City Street 5709 City Street 5710 City Street 5711 City Street	Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	700 800 1400		0.1 M E 100 F W 0.19 M W 0.12 M E	N 7TH AVE N 7TH AVE N 7TH AVE N 7TH AVE	No 3202181 No 3759415 No E375464 No E419517	2015-08-18 14:00 No Apparent Injury 2018-04-06 16:06 Possible Injury 2014-11-18 17:05 Suspected Minor Injury 2015-04-23 12:57 No Apparent Injury	0 0 2 0 1 0 2 0 1 0 2 0 0 0 1 0	O Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection Intersection Related but Not at Intersection Not at Intersection and Not Related At Driveway	Clear or Partly Cloudy Clear or Partly Cloudy Unknown Raining	wet	Daylight Dark-No Street Lights Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end Uslity Box	Going Straight Ahead Going Straight Ahead Going Straight Ahead Making Let Turn Going Straight Ahead	Slowing Stopped for Traffic Stopped for Traffic	cast vireat class vireat vehicle Backing Vehicle Stopped East West East West East West East West East West East West East Which Backing Vehicle Stopped East West Vehicle Stopped Vehicle Stopped West Vehicle Stopped Vehicle Stopped West Northeast Ve
5713 City Street 5714 City Street 5715 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	2400 700 700 100		200 F W 239 F W	N 7TH AVE N 7TH AVE N 7TH AVE N SEAL AVE	No E516299 No E726954 No E831806 No 2729724	2016-02-13 17:18 No Apparent Injury 2017-10-24 13:47 No Apparent Injury 2018-08-24 14:27 No Apparent Injury 2015-06-05 13:02 No Apparent Injury	0 0 2 0 0 0 2 0 0 0 2 0 0 0 2 0	Passenger Car Passenger Car Passenger Car Pickup.Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup.Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Pickup.Panel Truck or Vanette under 10,000 lb Pickup.Panel Truck or Vanette under 10,000 lb Passenger Car	At Driveway Intersection Related but Not at Intersection At Driveway Not at Intersection and Not Related	Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Daylight Daylight	Entering at angle From same direction - both going straight - one stopped - sideswipe Entering at angle From same direction - both going straight - one stopped - rear-end	Making Left Turn Starting From Parked Position	Going Straight Ahead Stopped for Traffic Going Straight Ahead Legally Parked, Unoccupied	North South West East West East Vehide Stopped Vehide Stopped South West West East West East Vehide Stopped Vehide Stopped
5716 City Street 5717 City Street 5718 City Street 5719 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	100 100 100 100		81 F W 36 F E 93 F W 21 F SW 211 F W	N SEAL AVE N SEAL AVE N SEAL AVE N SEAL AVE	No 3615941 No 3759303 No 3759410 No E742164	2016-02-19 13:04 No Apparent Injury 2017-05-08 14:56 No Apparent Injury 2018-02-13 15:29 No Apparent Injury 2017-12-02 13:17 No Apparent Injury 2019-05-25 13:01 No Apparent Injury 2019-05-25 13:01 No Apparent Injury	0 0 3 0 0 0 2 0 0 0 3 0 0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Driveway Related but Not at Driveway	Raining Clear or Partly Cloudy Clear or Partly Cloudy Overcast	Wet Straight & Level Dry Straight & Level Dry Straight & Level Wet Straight & Level	Daylight Daylight	une car reaving parked position From same direction - both going straight - one stopped - rear-end One parked-one moving From same direction - all others	Starting From Parked Position Going Straight Ahead Starting From Parked Position Changing Lanes	Going Straight Ahead Stopped for Traffic Legally Parked, Unoccupied Stopped for Traffic	Webite Stopped West East West West East Vehide Stopped West East Vehide Stopped West East Vehide Stopped West East Vehide Stopped
5720 City Street 5721 City Street 5722 City Street 5722 City Street 5723 City Street	Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	9300 100 1200		269 F NE 71 F NW 46 F NW	N SEQUIM AVE RIVER RD RIVER RD RIVER RD	No 2457069 No 2727980	2014-12-06 18:17 Possible Injury 2016-11-06 10:58 No Apparent Injury	1 0 2 0	Passenger Car Pickup Panel Truck or Vanette under 10.000 lb	Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car			Wet Straight & Grade Dry Straight & Level Wet Straight & Level Dry Curve & Level	Daylight	From same direction - both going straight - both moving - sideswipe From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end	Changing Lanes Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead Stopped for Traffic Stopped for Traffic Going Straight Ahead	East East West East East West Vehicle Stopped East West East Vehicle Stopped North South North South
5724 City Street 5725 City Street 5726 City Street 5727 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST	1800 1800 6400 300		51 F NE 52 F NE 118 F E	RIVER RD RIVER RD RIVER RD S 3RD AVE	No 2728035	2017-02-22 16:58 Possible Injury 2017-09-21 11:32 No Apparent Injury 2019-11-02 13:35 No Apparent Injury	1 0 3 0 0 0 2 0	0 Passenger Car 0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb	Roundabout Related but not at Roundabout Not at Intersection and Not Related Roundabout Related but not at Roundabout	Clear or Partly Cloudy	Dry Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end Building	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead Slowing Stopped for Traffic	East West East West East West East West East West Vehicle Stopped Vehicle Stopped East South
5728 City Street 5729 City Street 5730 City Street 5731 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST	200 400 500 500		277 F W 26 F E 182 F W 62 F W 43 F W	S 3RD AVE S 4TH AVE S 5TH AVE S 5TH AVE	No 3202163	2019-02-01 11:34 No Apparent Injury 2019-08-16 12:28 Possible Injury	0 0 3 0 1 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Intersection Related but Not at Intersection	Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Grade Dry Straight & Grade Dry Straight & Level Dry Straight & Level		From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Starting in Traffic Lane Stopped for Traffic Going Straight Ahead Stopped for Traffic	West East West East Supped East West Vehicle Stopped West East West East East East West East West East Vehicle Backing Vehicle Stopped
5732 City Street 5733 City Street 5734 City Street 5735 City Street	Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	600 500 500		43 F W 376 F W 286 F W 272 F W 208 F E	S 5TH AVE S 5TH AVE S 5TH AVE S 7TH AVE	No E464640 No E541293	2015-09-23 10:40 No Apparent Injury 2016-05-02 11:21 Possible Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Making Right Turn Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped at Signal or Stop Sign Stopped for Traffic Stopped for Traffic Stopped for Traffic	Vest Cast Venude Suching Venude Supped East North Vehicle Stopped Vehicle Stopped West East Vehicle Stopped Vehicle Stopped West East Vehicle Stopped Vehicle Stopped East Vehicle Stopped Vehicle Stopped West East West East West
### 22 City Sneet ### 2572 City Sneet ### 2573 City Sneet	Ciallam Sequim	W WASHINGTON ST	700 600 600 700		200 F E 200 F E 270 F E 80 F W	S 7TH AVE S 7TH AVE S 7TH AVE S 7TH AVE	No 3046855 No 3759405 No E744306	2016-06-07 16:40 Possible Injury 2019-01-02 14:53 No Apparent Injury 2017-10-06 12:11 No Apparent Injury 2018-01-02 17:15 Suspected Serious Injury 2018-01-01 17:50 No Apparent Injury 2018-01-01 17:50 No Apparent Injury	0 0 2 0 y 1 0 1 1 0 0 2 0	Passenger Car Passenger Car Passenger Car Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Passenger Car Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	Intersection Related but Not at Intersection	Clear or Partly Cloudy	Dry Straight & Level	Daylight Dark-Street Lights On	From same direction - both going straight - both moving - sideswipe Vehicle going straight hits pedestrian From same direction - both going straight - both moving - rear-end	Soning Stratigut invitedu Changing Lanes Changing Lanes Going Straight Ahead Making Left Turn	Going Straight Ahead	West Southeast West East
5739 City Street 5740 City Street 5741 City Street 5742 City Street 5743 City Street 5744 City Street 5744 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	700 700 900			S 7TH AVE S 7TH AVE S 7TH AVE S 9TH AVE	NO E954512	2019-08-19 08:15 Possible injury	1 0 2 0	0 Passenger Car	Passenger Car Pickup Panel Truck or Vanette under 10,000 lb Passenger Car Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb Passenger Car	Intersection Related but Not at Intersection At Driveway At Driveway	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Grade	Daylight Daylight	Entering at angle From same director - both going straight - both moving - sideswipe Entering at angle Entering at angle Entering at angle	Making Left Turn Changing Lanes Making Left Turn Making Left Turn Making Right Turn Going Straight Ahead	Slowing Going Straight Ahead Stopped in Roadway	South West West East West South West South West South South West East West South West West East West E
5744 City Street 5745 City Street 5746 City Street	Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	1100 1100 900 900		70 F W 263 F W 66 F E 150 F SW 127 F W 230 F W 0.14 M W	S 9TH AVE S 9TH AVE S 9TH AVE	No E620212	2016-10-01 14:08 No Apparent Injury 2017-10-24 12:28 No Apparent Injury 2019-12-12 13:52 No Apparent Injury 2016-12-10 14:00 Possible Injury 2018-11-23 13:23 No Apparent Injury 2018-11-23 13:23 No Apparent Injury	2 0 2 0	0 Passenger Car 0 Passenger Car 0 Passenger Car 0 Pickup/Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Truck (Flatbad,Van,etc) Pickup,Panel Truck or Vanette under 10,000 lb	Circulating Roundabout Not at Intersection and Not Related Roundabout Related but not at Roundabout Roundabout Related but not at Roundabout A Delivery	Overcast	Dry Straight & Grade Dry Curve & Level Wet Straight & Level Dry Wet Dry	Daylight	Entering at angle From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Making Right Turn Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead Making Left Turn	Stopped in Roadway Going Straight Ahead Stopped for Traffic Stopped for Traffic	
57.44 Lry Sixeet 57.45 City Sixeet 57.46 City Sixeet 57.46 City Sixeet 57.47 City Sixeet 57.48 City Sixeet 57.48 City Sixeet 57.49 City Sixeet 57.50 City Sixeet	Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	900 0 0 1000		0.14 M W 157 F E 135 F W 0.12 M E	S 9TH AVE S 9TH AVE S 9TH ST S PRIEST RD	No E818684	2017-01-25 13:27 Suspected Minor Injury	1 0 2 0	Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Intersection Related but Not at Intersection	Clear or Partly Cloudy Overcast Clear or Partly Cloudy Overcast Clear or Partly Cloudy	Dry Wet Straight & Level Dry Straight & Level Wet Straight & Level Wet Straight & Grade	Daylight	Vehicle Strikes Pedalogolist From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Making Left Turn Going Straight Ahead Going Straight Ahead Stopped for Traffic Making Left Turn	Stopped for Traffic Stopped for Traffic Going Straight Ahead Going Straight Ahead	South West West East Vehicle Stopped Vehicle Stopped West East Vehicle Stopped Vehicle Stopped West Vehicle Stopped West East
5751 City Street 5752 City Street 5753 City Street 5754 City Street	Clallarm Sequim Clallarm Sequim Clallarm Sequim Clallarm Sequim Clallarm Sequim Clallarm Sequim	W WASHINGTON ST	900 900 900 1000		0.18 M E	S PRIEST RD S PRIEST RD S PRIEST RD S PRIEST RD	No 2729774 No 3202175 No 3759403 No E559923	2019-01-07 18:05 No Apparent Injury 2019-05-26 17:12 No Apparent Injury 2017-10-13 13:13 No Apparent Injury 2016-07-03 13:03 No Apparent Injury	0 0 2 0 0 0 2 0 0 0 2 0 0 0 2 0	u jnoxup, Panel Truck or Vanette under 10,000 lb 0 Pickup, Panel Truck or Vanette under 10,000 lb 0 Passenger Car 0 Pickup, Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway Intersection Related but Not at Intersection At Driveway	Clear or Parity Cloudy Clear or Parity Cloudy Clear or Parity Cloudy Clear or Parity Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Daylight	Entering at angle Entering at angle From same direction - both going straight - one stopped - rear-end Entering at angle	Making Left Turn Going Straight Ahead	Going Straight Ahead Stopped for Traffic	South West East West North West East West East West Vehicle Backing Vehicle Stopped South West West East
5753 City Street 5754 City Street 5755 City Street 5755 City Street 5756 City Street 5757 City Street 5758 City Street	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST W WASHINGTON ST	900 2000 900 700		444 F E 0.12 M E 0.12 M E 334 F W 0.18 M E 88 F E	S PRIEST RD S PRIEST RD S PRIEST RD S SHAWLN	No E763576 No E920299 No E955986 No 3046860	2018-01-28 17:48 No Apparent Injury 2019-05-10 17:10 Possible Injury 2019-09-02 18:50 No Apparent Injury 2017-11-22 15:41 No Apparent Injury	0 0 2 0 1 0 2 0 0 0 2 0 0 0 2 0	Passenger Car Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	At Driveway Roundabout Related but not at Roundabout At Driveway At Driveway	Overcast Clear or Partly Cloudy Clear or Partly Cloudy Raining	Dry Straight & Level Dry Straight & Grade Dry Straight & Level Dry Straight & Level Wet Straight & Level	Dark-Street Lights On Daylight Daylight Daylight	Entering at angle From same direction - both going straight - one stopped - rear-end	Making Left Turn Making Left Turn Going Straight Ahead Making Left Turn Making Left Turn	Going Straight Ahead Going Straight Ahead Stopped for Traffic Going Straight Ahead Going Straight Ahead	South West East West West East East West South West East West South West East West
•											-		-			-			· · · · · · · · · · · · · · · · · · ·

OBJECTID MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_1_UNIT_1	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_2_UNIT_1	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE 3_UNIT_1 MV_DRIVER_CONTRIBUTING AGE UNIT_1	ER MV_DRIVER GENDER UNIT_1	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_1_UNIT_2	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_2_UNIT_2	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_3_UNIT_2 MV_DRIVER GENDER UNIT_2	BICYCLIST. CONTRIBUTING CIRCUMSTANCE. 1_UNT_1 CIRCUMSTANCE. 2_UNT_	G BICYCLIST_CONTRIBUTING AGE UNIT_1	BICYCLIST GENDER UNIT_1 BICYCLIST_CONTRIBUTING CIRCUMSTANCE_1_UNIT_2	BICYCLIST_CONTRIBUTING CIRCUMSTANCE_2_UNIT_2	BICYCLIST_CONTRIBUTING BICYCLIST BICYCLIST CIRCUMSTANCE_3_UNIT_2 AGE_UNIT_2 GENDE	VICUST PEDESTRIAN_CONTRIBUTING PEDESTRIAN_CONTRIBUTING CIRCUMISTANCE 1_UNT_2 CIRCUMISTANCE 2_UNT	ING PEDESTRIAN_CONTRIBUTING PEDESTRIAN CIRCUMSTANCE_3_UNIT_2 AGE_UNIT_2		FIRST_IMPACT_LOCATION City_St_County_Rd_8_Mise_2010_forward_St
5579 Inattention 5580 Under Influence of Alcohol 5581 Follow Too Closely	Did Not Grant RW to Vehicle		72 Female 50 Female 85 Male	Driver Not Distracted None		70 Maie 73 Female 47 Female								Other Local Lane of Prii	Taffovay Andreway Andrew
5582 Inattention 5583 Improper Turn/Merge 5584 Other Contributing Circ Not Listed 5585 None 5586 Inattention			51 Male 52 Male	None		47 Female						Did Not Grant RW to Vehicle	69	Past the Ou	noulder of Primary Trafficwary utside Shoulder of Primary Trafficwary utside Shoulder of Primary Trafficwary
5586 Inattention 5587 Improper Turn/Merge	Inattention		72 Female 60 Male 16 Male	None		42 Female						DIG YOU SHARE K YY ID VEHICLE	03	Past the Ou	utside Shoulder of Primary Trafficway g Trafficway g Trafficway utside Shoulder of Primary Trafficway
5586 Instantion 5587 Improved Fundament Gire Not Linted 5589 Ober Combusting Gire Not Linted 5589 Ober Not Davander 5590 Ober Not Davander 5590 Ober Not Davander 5591 Ober Not Davander 5592 Instantion 5592 Instantion 5593 Ober Not Davander 5593 Ober Not O			19 Male 61 Male 22 Female	None Driver Not Distracted		22 Female 63 Female								Past the Ou Eane of Prin Lane of Prin	utside Shoulder of Primary Trafficuray imary Trafficuray mary Trafficuray
5591 Did Not Grant RW to Vehicle 5592 Inattention 5593 Improper Turn/Merce	Other Contributing Circ Not Listed Did Not Grant RW to Vehicle Inattention	Exceeding Reas. Safe Speed	75 Female 57 Male 23 Male 75 Female	None None		46 Maie 63 Maie 64 Female								Lane of Prin	mary Trafficway mary Trafficway frafficway mary Trafficway mary Trafficway
5594 None 5595 Inattention	Follow Too Closely		80 Male	Inattention None		39 Maie 81 Maie								Lane of Prin	imary Trafficway
5597 Inattention 5598 Follow Too Closely			91 Male 71 Male 27 Male	None None		76 Male 69 Male								Outside Sh Lane of Pri	mary Traficway uoulder of Primary Trafficway imary Traficway
5599 Operating Defective Equipment 5600 Did Not Grant R/W to Non Motorist 5601 Driver Not Distracted			93 Male	Driver Not Distracted Driver Not Distracted		77 Male 50 Female			None		29 Female				mary Trafficway mary Trafficway mary Trafficway mary Trafficway
5602 Inattention 5603 Did Not Grant RW to Vehicle 5604 None			21 Male 33 Male 79 Female			44 Female						Inattention	43	Lane of Prin	mary Trafficway
5603 Did Not Grant RW to Vehicle 5604 None			49 Female											Past the Ou Past the Ou	imary Trafficway utside Shoulder of Primary Trafficway
5608 Unknown Distraction 5609 Inattention			16 Male 25 Male 19 Male 38 Male	None Driver Not Distracted		18 Maie 54 Female								Past the Ou	utside Shoulder of Primary Trafficway
5006 Under Influence of Airobnol 5007 Exceeding Reas, Sale Speed 5008 Unknown Die traction 5009 Instruction 5010 Follow Too Closely 5011 Authorise Die Tool Sale Sale 5011 Authorise Die Tool Sale 5011 On Home Too Closely 5012 On Home Too Closely 5012 On Home Too Closely 5013 On Home Too Closely 5014 On Home Too Closely 5015 On Home Too Closely 5	Unknown Distraction		74 Male	Driver Not Distracted None											mary Trafficway mary Trafficway usuliad Shoulder of Primary Trafficway imary Trafficway
5613 Did Not Grant RW to Vehicle 5614 Operating Other Electronic Devices (comp 5615 Did Not Grant RW to Vehicle			33 Male 69 Female	None Driver Not Distracted		74 Maie 66 Maie 76 Female								Lane of Prin	imary Trafficway utside Shoulder of Primary Trafficway imary Trafficway
3913 John Verticalari Nivi 2 Venicule 5616 Inatherition 5617 Inatherition 5618 Follow Too Closely 5619 Follow Too Closely 5619 Follow Too Closely 5620 John Vole Grant RW to Vehicle 5621 Driver Not Distracted 5623 Did Not Grant RW to Vehicle 5623 Did Not Grant RW to Vehicle 5623 Did Not Grant RW to Vehicle			73 Male 52 Male 19 Female			51 Male								Lane of Prii Past the Ou	mary Trafficway utside Shoulder of Primary Trafficway mary Trafficway mary Trafficway mary Trafficway
5618 Follow Too Closely 5619 Follow Too Closely 5620 Did Not Grant RW to Vehicle			76 Male	None		67 Male 61 Female 68 Male								Lane of Prii Lane of Prii Lane of Prii	iary Trafficway mary Trafficway
5621 Driver Not Distracted 5622 Inattention 5623 Did Not Grant RW to Vehicle	Follow Too Closely		56 Male 38 Female 21 Female	None None		24 Female 66 Female								Past the Ou Lane of Prin Lane of Prin	mary Trafficway uside Shoulder of Primary Trafficway mary Trafficway mary Trafficway mary Trafficway
5624 Did Not Grant RW to Vehicle 5625 Inattention	Inattention				Mana	73 Female 39 Female 22 Male								Lane of Prin	mary Trafficway
5623 Did Not Grant RVW b Vehicle 5624 Did Not Grant RVW b Vehicle 5625 Inathension 5626 Follow To Closely 5627 Inathension 5628 Policy Prior Not Distracted 5629 Disregard Stop and Go Light 5630 Disregard Stop and Go Light 5631 Under Influence of Acohol 5631 Under Influence of Acohol	Inattention		57 Male 17 Female	None None Failing to Signal	(vo. ID	28 Male								Lane of Prin	mary Traficovay mary Traficovay mary Traficovay mary Traficovay mary Traficovay
5629 Disregard Stop and Go Light 5630 Disregard Stop and Go Light 5631 Under Influence of Alcohol	Inattention		52 Female 64 Female	None		71 Maie 50 Female								Lane of Prin Lane of Prin Outside Sh	imary Trafficway imary Trafficway oudder of Primary Trafficway
5632 Improper Passing 5633 Did Not Grant RW to Vehicle	Inattention		82 Female	None		47 Maie 29 Maie 20 Female								Lane of Prii	mary Trafficway imary Trafficway
5632 Improper Passing 5633 Did Not Grant RW to Vehicle 5634 Distractions Cutside Vehicle 5635 Driver Not Distracted 5636 Inattention			21 Female 87 Male	None Driver Not Distracted None None		30 Female 74 Maie 78 Female								Lane of Prin Lane of Prin Lane of Prin	mary Traflovay mary Traflovay mary Traflovay
5637 Old Not Grant RW to Vehicle 5639 Old Not Grant RW to Vehicle 5638 Driver Interacting with Passengers, Anim 5639 Exceeding Reas, Safe Speed 5640 Inatherelian 5641 Interven Distraction 5642 Instantion 5644 Follow Too Crosely 5645 Old Not Grant RW to Vehicle 5644 Follow Too Crosely	Inattention Operating Handheld Cell Phone		76 Female 66 Female 37 Male	None		80 Female								Lane of Prin	mary Traficway uside Shoulder of Primary Trafficway mary Traficway mary Traficway
5640 Inattention 5641 Unknown Distraction				None Driver Not Distracted		70 Male 69 Male 67 Female									
5643 Did Not Grant RW to Vehicle 5644 Follow Too Closely	Disregard Stop and Go Light	Inattention	24 Female 54 Male 42 Female	None None		67 Female 39 Male 60 Female								Lane of Pri Lane of Pri Lane of Pri	mary Traficway mary Traficway mary Traficway
5645 Other Driver Distractions Inside Vehicle 5646 Under Influence of Alcohol 5647 Inattention 5648 Unknown Distraction	Under Influence of Drugs Follow Too Closely		16 Male	None Driver Not Distracted None Unknown Distraction		68 Male 49 Male 63 Male								Lane of Pris	mary Traficway T Traficway mary Traficway mary Traficway
5648 Unknown Distraction 5649 Improper Backing	relian too diosely		62 Male	Driver Not Distracted		49 62 Female								Lane of Prin	imary Trafficway
5649 Improper Backing 5650 Instrention 5651 None 5652 Driver Interacting with Passengers, Anim	Follow Too Closely			None Inattention None		77 Male 77 Male 66 Male								Lane of Prin Lane of Prin Lane of Prin	mary Trafficway imary Trafficway imary Trafficway
5653 Inattention 5654 Inattention 5655 Follow Too Closely 5655 None			48 Male 39 Female	None None Driver Not Distracted		56 Male 61 Female 54 Female								Lane of Prin	mary Trafficway mary Trafficway mary Trafficway mary Trafficway
5656 None 5657 Inattention			69 Female 54 Male	None		65 Female			Did Not Grant RW to Vehicle		18 Male				
5856 None S857 None S857 None S857 None S857 None S858 Pilotonom Distraction S858 Piloto Pico Closely S858 Piloto Pico Closely S858 Piloto Pico Closely S858 Piloto Pico Closely S858 Piloto Pico Pico Pico Pico Pico Pico Pico Pic	Inattention		54 Male 44 Male 58 Female 88 Female	Driver Not Distracted None None		52 Female 41 Female 59 Male								Lane of Prii Lane of Prii Lane of Prii	many Tradicoway many Tradicoway many Tradicoway many Tradicoway moulder of Primany Tradicoway
5661 Improper Turn/Merge 5662 Did Not Grant RW to Vehicle	Disregard Yield Sign - Flashing Yellow	Inattention	91 Female	None		67 Male									
5664 Inattention 5665 Inattention	Inattention Unknown Distraction		24 Female 55 Male 48 Male	None None		39 Female 34 Female 18 Male								Lane of Prin	mary Traficway imary Traficway mary Traficway
5666 Inattention 5667 Under Influence of Alcohol 5668 Inattention	Follow Too Closely		27 Male 58 Female 83 Female 66 Female	None		77 Female								Other Local Other Local	mary Traficway ston (Cip/CourtyMsc. Traficway) ston (Cip/CourtyMsc. Traficway) mary Traficway
5669 Disregard Yield Sign - Flashing Yellow 5670 Operating Handheld Cell Phone 5671 Instruction	Inattention		66 Female 56 Male	None		75 Male									
3869 144801001 1566 15			31 Male	Inattention		43 Male								Lane of Prin Other Local	sion (Cip(CountyMisc. Trafficway) imary Trafficway sion (Cip(CountyMisc. Trafficway)
5674 Follow Too Closely 5675 Follow Too Closely 5676 Follow Too Closely	Other Contributing Circ Not Listed Unknown Distraction Inattention			Driver Not Distracted		55 Male 65 Male 71 Female								Lane of Prii Lane of Prii Lane of Prii	mary Trafficway mary Trafficway mary Trafficway mary Trafficway
5677 Other Driver Distractions Inside Vehicle 5678 Unknown Distraction 5679 Disregard Stop and Go Light	Eating or Drinking		79 Female	Driver Not Distracted Driver Not Distracted None		71 Female 60 Female 75 Female 67 Male								Lane of Prin	mary Trafficway
5973. Under Influence of Alcohol 5974. Foliow Too Crosely 5974. Foliow Too Crosely 5979. The Thom Too Crosely 5979. The Thom Distractions Inside Whitele 5979. Distraction Too Closely 5989. Distraction Too Closely 5989. The Too Closely 5989. The Too Closely 5989. Distraction Too Closely 59899. Distraction Too Closely 59899. Distraction	Disregard Stop and Go Light		60 Female	None None None Other Contributing Circ Not Listed		67 Male 62 Female 62 Male								Lane of Prin	mary Trafficway mary Trafficway mary Trafficway mary Trafficway
5683 Follow Too Closely 5684 Disregard Stop and Go Light			26 Female	Driver Not Distracted None Driver Not Distracted		54 Female 18 Male 68 Female 31 Female								Lane of Prii	many Tatakoway many Tatakoway many Tatakoway many Tatakoway many Tatakoway
5686 Other Contributing Circ Not Listed 5687 Inattention	Follow Too Closely		33 Female	None None		76 Male 53 Male								Lane of Pris	mary Trafficway imary Trafficway
	Follow Too Closely Follow Too Closely		27 Male 17 Male 80 Female	None None		53 Maie 29 Maie 22 Female								Lane of Prin	mary Trafficway mary Trafficway mary Trafficway
5691 Inattention 5692 None	Follow Too Closely		20 Female 24 Male	None Inattention		71 Male 55 Female 17 Female 68 Male								Lane of Pri	mary Traficway mary Traficway mary Traficway mary Traficway mary Traficway
	Inattention	Distractions Outside Vehicle	24 Male 30 Male 58 Male 67 Female	None None		29 Maie								Lane of Prii Lane of Prii	nary Trafficway mary Trafficway
5696 Follow Too Closely 5698 Inattention			70 Female 42 Female	None Follow Too Closely Driver Not Distracted None	Inattention	23 Female 45 Female 40 Male								Lane of Prin Lane of Prin Lane of Prin	many Traficowy many Traficowy many Traficowy many Traficowy many Traficowy many Traficowy
	Distractions Outside Vehicle Had Taken Medication		65 Male		None	39 Female								Lane of Prin	mary Trafficway
5702 Did Not Grant RW to Vehicle 5703 Follow Too Closely	Had Taken Medication Inattention		41 Female 61 Female 90 Male 30 Male	None None		71 Male 33 Maie 55 Maie 66 Female									many Trafisoway many Trafisoway many Trafisoway many Trafisoway many Trafisoway
5704 Inattention 5705 Inattention 5706 Inattention			19 Male 65 Female 18 Male	None None Driver Not Distracted Driver Not Distracted		60 Male 19 Male 60 Female								Lane of Prin Lane of Prin Lane of Prin	mary Traficway mary Traficway mary Traficway
5707 Unknown Distraction 5708 Eating or Drinking 5709 Follow Ton Closely			17 Male	Driver Not Distracted Driver Not Distracted		71 Maie 63 Female								Lane of Prin	imary Trafficway imary Trafficway
9796 Instension 9797 Instensio			64 Male			82 Male 35 Female								Lane of Prii Lane of Prii Past the Ou	mary Traflicway utside Shoulder of Primary Traflicway
5712 Other Contributing Circ Not Listed 5713 Improper Passing 5714 Inattention	Did Not Grant RW to Vehicle		16 Female 29 Male 28 Male 23 Female	Driver Not Distracted None None		24 Maie 72 Maie 62 Maie								Lane of Prin	mary Traflicway mary Traflicway mary Traflicway mary Traflicway noulder of Primary Traflicway
5715 Other Contributing Circ Not Listed 5716 Did Not Grant RW to Vehicle 5717 Inattention	Inattention		79 Female 33 Female	None Driver Not Distracted None	1	79 Female 51 Maie								Outside Sh Lane of Prin	Juider of Primary Trafficway mary Trafficway mary Trafficway
5718 Other Contributing Circ Not Listed 5719 Improper Passing	Distractions Outside Vehicle		87 Male 85 Female	Driver Not Distracted None None None		74 Male								Outside Sh Lane of Prin	mary Traficway mary Traficway under of Primary Traficway mary Traficway
5720 Did Not Grant RW to Vehicle 5721 Inattention 5722 Follow Too Closely	Other Driver Distractions Inside Vehicle		19 Female 18 Male 22 Female 44 Male	None		77 Female 63 Male 45 Maie 88 Male								Lane of Prin	mary Trafficway mary Trafficway mary Trafficway mary Trafficway
5723 Inattention 5724 Inattention	Follow Too Closely					29 Female								Lane of Prin	mary Trafficway
5726 Operating Handheld Cell Phone 5727 Other Contributing Circ Not Listed	. onew roo olusery		23 Male 54 Male	None Driver Not Distracted		61 Male 38 Male								Lane of Prii Lane of Prii Past the Ou	imary Traflicway utary Traflicway utside Shoulder of Primary Traflicway
5728 Follow Too Closely 5729 Follow Too Closely 5730 Other Contributing Circ Not Listed	Apparently III		73 Female 78 Male	None None None		25 Female 40 Male 73 Maie 46 Female									
5731 Inattention 5732 Improper Turn/Merge	Distractions Outside Vehicle		35 Male 81 Female	None None None None None None None None										Lane of Prii	may Tafacay may Tafacay may Tafacay may Tafacay may Tafacay g Tafacay
5734 Inattention 5735 Other Driver Distractions Inside Vehicle	Follow Too Closely Follow Too Closely		20 Male 52 Female	None Driver Not Distracted		71 Male 55 Male 85 Male									mary Trafficway mary Trafficway mary Trafficway mary Trafficway
5736 Inattention 5737 None 5738 Inattention	Distractions Outside Vehicle		25 Female 55 Male 38 Female	None		63 Female 23 Male 60 Female						Failure to Use Xwalk	69	emale Lane of Prin	mary Trafficway
5739 Improper Turn/Merge 5740 Inattention	Failing to Signal		25 Male 60 Female	None Other Contributing Circ Not Listed Driver Not Distracted		32								Lane of Prin	imary Traficway imary Traficway imary Traficway imary Traficway imary Traficway
5742 Driver Not Distracted 5743 Inattention			19 Female	Driver Not Distracted		41 Female 52 Female 33 Male								Lane of Pri Lane of Pri Lane of Pri	mary Traficway mary Traficway mary Traficway mary Traficway mary Traficway
5744 Exceeding Reas. Safe Speed 5745 Inattention 5746 Inattention	Follow Too Closely Follow Too Closely	industrial in the second secon	48 Female	None		52 Female 33 Male 62 Male 52 Male 33								Lane of Print Lane of Print Lane of Print	sary Trafficway mary Trafficway mary Trafficway
5747 Did Not Grant RW to Vehicle 5748 Inattention			76 Female 26 Male	None Driver Not Distracted Driver Not Distracted		56 Female			None		18 Female			Lane of Pri	mary Traffoway mary Traffoway mary Traffoway mary Traffoway mary Traffoway
5748 Instantion 5759 Operating Detection Equipment 5750 None Control of State Operating 5750 None Control of State Operating 5752 Detection Control of State Operating 5753 State Operating 5754 Special Operating 5754 Special Operating 5755 State Operating 5756 Special Operating 5756 Special Operating 5757 Special Operating 5757 Special Operating 5757 Special Operating 5757 Special Operating 5758 Special Operating 5759 Special Operating 5759 Special Operating 5759 Special Operating 5750 Special Operati	Inattention		28 Male 78 Male 61 Male 58 Male	Univer Not Distracted None	Follow Too Closely	70 Male 28 Female 75 Female								Lane of Prin	mary Trafficway mary Trafficway mary Trafficway mary Trafficway
5752 Did Not Grant RW to Vehicle 5753 Follow Too Closely 5754 Improper Turn Marco	Inattention Inattention Unknown Distraction	Distractions Outside Vehicle		None None		71 Male 64 Male								Lane of Prin	mary Trafficway
5755 Eating or Drinking 5756 Follow Too Closely	Unknown Distraction		54 Female 26 Female 28 Male	None None		20 Male 67 Female 72								Lane of Prin Lane of Prin Lane of Prin	mary Traficway mary Traficway mary Traficway
	i i	1	53 Female	None	1	43 Female 50 Female		1	+ + +	+				Intersecting	g Trafficway mary Trafficway

OBJECTID WA_STATE_PLANE SOUTH_X 2010_FORWARD 2010_FORWARD COL YEAR MIX	COL COL TYPE SEVERITY	COL_HUM FACTOR	COL COL AGE IMPAIRED I	COL_AGE SURFACE CONDITIONS	COL_LIGHT CONDITIONS	COL LOCATION	COL STUDY AREA		COL SEVERITY TEXT	COL_HUMAN FACTOR_Decoded	COL	COL_LIGHT CONDITIONS Decoded	COL_SURFACE CONDITIONS Decoded	COL_LOCATION Decoded	COL_RISKY BEHAVIOR	Intersection Area Emphasis Roadway Departure
5579 1004392.83 1017459.29 2015 2 Vehicles/Paries Angle 5580 1004473.31 10175044.72 018 1 Vehicles/Paries Next Object 5581 1004470.2 1019085.51 2015 3 Vehicles/Paries Reax-End 5582 1004470.2 1013118.72 0014 2 Vehicles/Paries Struck Parkes	Property Damage Only F	Inattention 65 Under Influence of Alcohol/Drugs/Medication 65 Follow Too Closely 65 Inattention 55	No -54 Under Influence of Alcohol	5-54 Dry	Daylight Daylight Daylight Daylight	Roundabout Related but not at Roundabout Driveway Related but Not at Driveway At Driveway or Intersection	Straight & Level	Older Road User 65+	Ur Fo	attention del Influence of Alcohol/Drugs/Medication flow Too Closely attention	Dayligi Dayligi Dayligi Dayligi	ht	Dry Dry Dry Dry	Roundabout Roundabout Related but not at Roundabout Driveway Related but Not at Driveway At Driveway or Intersection	Inattention Under Influence of Alcohol/Drugs/Medication Follow Too Closely Inattention	No Yes No Yes
5582 1004160.95 1013118.7 2014 2 Vehicles.Parles Struck Parles 5583 1004193.88 1011808.35 2015 1 Vehicles.Parles Struck Parles 5584 999471.31 1012282.3 2015 3 Vehicles.Parles Struck Parles 5585 1001586.93 1012242.98 2014 2 Vehicles.Parles Pedestrian	o Vericle Property Damage Cnity t Property Damage Only Ir d Vehicle Property Damage Only If Minor or Possible Injury F		104 No 154 No 154 No 155 No 15	Wet Dry	Daylight		Straight & Level	No Vulnerable Road User Involved		proper Turn/Merge her Contributing Circ Not Listed	Dayligi Dayligi Dayligi	ht ht		At Driveway or Intersection At Driveway or Intersection	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Fail to Yield	Yes Yes
5586 1004225.89 1012451.68 2016 2 Vehicles/Parties Angle 5587 1004269.43 1013452.44 2015 1 Vehicle/Party Struck Object	t Property Damage Only in		664 No nder 25 No	Dry Dry		At Driveway or Intersection At Driveway or Intersection		No Vulnerable Road User Involved Pedestrian and Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	In:	attention proper Turn/Merge	Dayligi Dayligi	ht		At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Inattention No Risky Driving Behaviors Involved	No Yes
5588 1004169.25 1013453.17 2014 1 Vehicle Party Struck Object 5589 1004170.09 1010820.11 2019 2 Vehicles (Parties Angle 5590 1000342.3 1014912.14 2018 2 Vehicles (Parties Angle	t Property Damage Only C Property Damage Only Property Damage Only C	Fail to Yield 55 Other/Unknown Un	nder 25 No 164 No nder 25 No	Dry Dry	Daylight Daylight Daylight	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Straight & Grade Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Fa Ot	her Contributing Circ Not Listed il to Yield her/Unknown	Dayligi Dayligi Dayligi	ht ht	Dry Dry	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	No Risky Driving Behaviors Involved Fail to Yield No Risky Driving Behaviors Involved	No No
5591 999022.44 1014966.59 2015 2 Vehicles.Parles Angle 5592 999022.43 1014966.59 2014 2 Vehicles.Parles Angle 5593 999022.44 1014966.59 2016 2 Vehicles.Parles Angle 5594 1001239.21 1014883.59 2016 2 Vehicles.Parles Angle	Property Damage Only F Minor or Possible Injury Ir Property Damage Only Ir	Fail to Yield 65 Inattention 55 Improper Turn/Merge Un Other/Unknown 65	i+ No i-64 No oder 25 No	Dry	Daylight Daylight Daylight	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Straight & Level	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+		il to Yield attention proper Turn/Merge	Dayligi Dayligi Dayligi			At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Fail to Yield Inattention No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No No
5595 1001338.49 1014873.25 2014 2 Vehicles/Parties Rear-End	Minor or Possible Injury Ir Property Damage Only D	Inattention 65 Distracted Driving 65	i+ No i+ No	Dry	Daylight	At Driveway or Intersection Driveway Related but Not at Driveway	Straight & Level	Older Road User 65+	ln: Di	attention stractions Outside Vehicle	Dayligi Dayligi	ht		At Driveway or Intersection At Driveway or Intersection Driveway Related but Not at Driveway	No Risky Driving Behaviors Involved Inattention Distracted Driving	No No No
5598 1000267:36 1014915.97 2019 2 Vehicles/Parties Rear-End	Property Damage Unity F	Inattention 65 Follow Too Closely 25	i÷ No i-34 No i÷ No	Dry	Daylight Dark-Street Lights On Daylight Daylight	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Unknown Straight & Level	Older Road User 65+		attention flow Too Closely perating Defective Equipment	Dayligi Dark-S Dayligi Dayligi	ht	Dry Wet Dry Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Inattention Follow Too Closely No Risky Driving Behaviors Involved	Yes No Yes
5599 999843.02 1014936.7 2015 2 Vehicles.Parties Sideswipe 5600 1000464.68 1014899.83 2019 2 Vehicles.Parties Pedialogist 5601 1006826.67 1010587.97 2017 2 Vehicles.Parties Reas-End 5602 1010919.35 10105694.3 2015 1 Vehicles.Parties Reas-End	Properly Damage Chily Minor or Possible Injury F Properly Damage Chily Minor or Possible Injury Properly Damage Chily F Properly Damage Chily F	Fail to Yield 65 Other/Unknown 55	e No -64 No -64 No -65 No	Dry Dry	Daylight	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Straight & Level Curve at Hillcrest	Pedalcyclist and Older Road User 65+ No Vulnerable Road User Involved Motorcycle	Di Ot	d Not Grant R.W to Non Motorist her/Unknown	Dayligl Dayligl	ht	Dry Dry	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Fail to Yield No Risky Driving Behaviors Involved	No No Ver
5603 1009478.85 1010617.87 2019 2 Vehicles/Parises Angle 5604 1010210.41 1010590.79 2014 2 Vehicles/Parises Pedestrian 5605 1017279.78 1007968.5 2019 1 Vehicle/Party Struck Object 5606 1017809.72 1006638.71 2019 1 Vehicle/Party Struck Object	Property Damage Only F Mnor or Possible Injury It t Mnor or Possible Injury D	Fail to Yield 25 Inattention 65	-34 No +34 No + No	Wet Wet	Daylight	At Driveway or Intersection Not at Intersection, Driveway/Roundabout	Straight & Level	No Vulnerable Road User Involved Pedestrian and Older Road User 65+ No Vulnerable Road User Involved	Fa No	il to Yield	Dayligi Dayligi		Wet	At Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Fail to Yield Inattention	No No
5606 1017809.72 1006638.71 2019 1 Vehicle/Party Struck Object	t Minor or Possible Injury U t Property Damage Only S	Under Influence of Alcohol/Drugs/Medication 45 Speeding Un	100 20 No. 100 120 No. 100 No.			Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Curve & Grade Straight at Hillcrest	No Vulnerable Road User Involved No Vulnerable Road User Involved	Sp	ider Influence of Alcohol/Drugs/Medication	Dayligi Dark-N	ht No Street Lights/Street Lights Off	Ice/Snow/Slush	Not at Intersection/Driveway/Roundabout	Under Influence of Alcohol/Drugs/Medication Speeding	Yes Yes
5607 1014176.97 1010402.51 (2014 1 Webside Party South Chief 5608 1017615.91 10007411 (2017) (1 Webside Party South Chief 5609 996119.8 1000438.32 (2014) (3 Websides Parties Parties 5610 10101602.97 1012871.77 (2018) (2 Websides Parties Reas Faint 5611 10101002.97 1012871.77 (2015) (1 Webside Party South Chief			-34 No	Dry	Daylight Daylight	At Driveway or Intersection At Driveway or Intersection	Straight & Level Straight & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	Ur In:	known Distraction attention Illow Too Closely	Dayligi Dayligi Dayligi	ht ht	Dry Dry Dry	Not at Intersection/Driveway/Roundabout At Driveway or Intersection At Driveway or Intersection	Distracted Driving Inattention Follow Too Closely	Yes No No
EP42 4004000 07 4042074 77 2045 3 Mahilus Daries Anala		Inattention 65 Fail to Yield Un Fail to Yield 25	ie No nder 25 No n-34 No	Wet Dry	Daylight	At Driveway or Intersection At Driveway or Intersection			Fa Fa	il to Yield	Dayligi Dayligi Dayligi	ht	Wet	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection Not at Intersection/Driveway/Roundabout	Inattention Fail to Yield Fail to Yield	Yes No No
5615 1003563.37 1012147.91 2014 2 Vehicles/Parties Angle	t Property Damage Only D Property Damage Only F Property Damage Only Ir	Fail to Yield 25 Distracted Driving 65 Fail to Yield 35 Inattention 65	i+ No 44 No i+ No	Dry Wet	Daylight Daylight Daylight	Not at Intersection/Driveway/Roundabout At Driveway or Intersection At Driveway or Intersection	Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+	Op Fa	il to Yield serating Other Electronic Devices (comp il to Yield sttention	Dayligl Dayligl Dayligl	ht	Dry Wet Dry	Not at Intersection/Driveway/Roundabout At Driveway or Intersection At Driveway or Intersection	Distracted Driving Fail to Yield Inattention	Yes No No
5616 1002906.16 1012167.8 2015 2 Vehicles/Parles Angle 5617 1002236.87 1012192.49 2019 1 Vehicles/Parles Nuck Object 5618 1002236.87 1012192.49 2018 2 Vehicles/Parles Turning 5619 1002236.87 1012192.49 2018 2 Vehicles/Parles Turning 5619 1002236.87 1012192.49 2018 2 Vehicles/Parles Raer-End		Inattention 45 Follow Too Closely Un Follow Too Closely Un	-54 No	Dry Dry	Daylight Daylight Daylight	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Straight & Level Straight & Grade Unknown	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	In: Fo	attention Ilow Too Closely Ilow Too Closely	Dayligi Dayligi Dayligi	ht		At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Inattention Follow Too Closely Follow Too Closely	Yes No No
5620 1001580.88 1012219.7 2015 2 Vehides/Paries Sideswipe 5621 1001580.87 1012219.89 2014 1 Vehide/Party Struck Cloject 5622 1001580.88 1012219.7 2015 2 Vehides/Paries Rear-End 5623 1000251.26 1012288.29 2015 2 Vehides/Paries Aragie	Property Damage Only F	Fail to Yield 65 Other/Unknown 55 Inattention 35	+ No -64 No		Daylight	At Driveway or Intersection At Driveway or Intersection At Intersection and Not Related	Straight & Level	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved		il to Yield her/Unknown	Dayligi Dayligi	ht	Wet	At Driveway or Intersection At Driveway or Intersection At Intersection and Not Related	Fail to Yield No Risky Driving Behaviors Involved	Yes Yes
5624 1000251 26 1012288 29 2015 2 Vehicles/Parties Sideswipe	Property Damage Only F	Fail to Yield Un Fail to Yield 55	nder 25 No 164 No	Wet	Daylight	At Driveway or Intersection At Intersection and Not Related	Straight & Level	No Vulnerable Road User Involved	Fa	il to Yield il to Yield	Dayligi Dayligi Dayligi	ht ht	Dry Wet	At Driveway or Intersection At Intersection and Not Related	Fail to Yield Fail to Yield	No Yes
5627 1000251.26 1012288.29 2015 2 Vehicles/Parties Rear-End	Property Damage Only In	Inattention 55	:34 No	Dry Dry Wet	Daylight	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved	ln:	attention illow Too Closely sattention	Dayligl Dayligl Dayligl	ht .	Wet	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Inattention Follow Too Closely Inattention In attention	No No No
5628 1000251 26 1012288 29 2016 2 Vehicles/Paries Rear-End 5629 1000251 27 1012288 29 2018 2 Vehicles/Paries Angle 5630 1000251 27 1012288 29 2019 2 Vehicles/Paries Angle 5631 103399 89 10123 43 49 2018 1 Vehicle/Pary Struck Object	Minor or Possible Injury D Minor or Possible Injury D Minor or Possible Injury D	Disregard Traffic Control Device/Sign 45 Disregard Traffic Control Device/Sign 45	nder 25 No 1-54 No 1-54 No	Dry Dry	Dark-Street Lights On Dark-Street Lights On Daylight	At Driveway or Intersection At Driveway or Intersection	Straight & Level Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Di Di	her/Unknown sregard Traffic Control Device/Sign sregard Traffic Control Device/Sign	Dark-S	Street Lights On Street Lights On ht Street Lights On	Dry Dry	At Intersection and Not Related At Driveway or Intersection At Driveway or Intersection	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No No
5632 1003999.89 1012134.94 2015 2 Vehicles/Parties Sideswipe 5633 1004221.82 1012121.4 2016 2 Vehicles/Parties Sideswipe	Property Damage Only Ir Property Damage Only F	Improper Passing 25 Fail to Yield 65		Dry Dry	Daylight	At Intersection and Not Related At Intersection and Not Related Not at Intersection/Driveway/Roundabout	Straight & Level Straight & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	lm Fa	nder Influence of Alcohol/Drugs/Medication proper Backing/Passing/Turning/Parking/Signaling il to Yield	Dark-S Dayligl Dayligl	ht	Dry	At Intersection and Not Related At Intersection and Not Related Not at Intersection/Driveway/Roundabout	Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved Fail to Yield	Yes Yes Yes
5635 1004221.82 1012121.4 2015 2 Vehicles/Parties Sideswipe 5636 996226.92 1011578.24 2014 2 Vehicles/Parties Angle	Minor or Possible Injury D Property Damage Only C	Distracted Driving 25 Other/Unknown Uninattention 65 65	34 No nder 25 No + No	Dry Wet Wet	Daylight Daylight Daylight	At Driveway or Intersection At Intersection and Not Related Roundabout	Straight & Level Straight & Level Curve & Level	No Vuinerable Road User Involved No Vuinerable Road User Involved Older Road User 65+	Di Ot In:	stractions Outside Vehicle her/Unknown attention	Daylig Daylig Daylig	ht	Dry Wet Wet	At Driveway or Intersection At Intersection and Not Related Roundabout	Distracted Driving No Risky Driving Behaviors Involved Inattention	No Yes No
5637 996290.56 1011657.89 2017 2 Vehicles/Parties Angle 5638 996296.89 1011642.51 2014 1 Vehicle Party Struck Object 5639 996226.92 1011578.24 2014 1 Vehicle Party Sideswipe 5640 1003563.37 1012147.91 2014 2 Vehicles/Parties Rear-End	Property Damage Only F t Property Damage Only D Minor or Possible Injury S	Fail to Yield 65 Distracted Driving 65 Speeding 35	* No * No -44 No	Dry Dry Dry	Daylight Daylight Dark-Street Lights On	Roundabout Roundabout Roundabout	Unknown Curve & Level	Older Road User 65+ Older Road User 65+ Motorcycle		il to Yield stracted Driving seeding	Dayligl Dayligl Dark-S	ht ht Street Lights On	Dry Dry Dry	Roundabout Roundabout Roundabout	Fail to Yield Distracted Driving Speeding	No Yes Yes
5641 1003563.36 1012147.91 2018 3 Vehicles/Parties Rear-End	Property Damage Only In Property Damage Only D	Inattention 25 Distracted Driving 45	-34 No -54 No older 25 No	Dry	Daylight	At Driveway or Intersection At Intersection and Not Related	Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved	Ins Ur		Daylig	ht		At Driveway or Intersection At Intersection and Not Related	Inattention Distracted Driving Inattention	No No
5642 1002906.16 1012167.8 2015 2 Vehicles/Parties Angle 5643 1002906.16 1012167.8 2015 3 Vehicles/Parties Angle 5644 1002894.35 1012168.19 2018 2 Vehicles/Parties Rear-End 5645 1002294.35 1012168.19 2018 2 Vehicles/Parties Rear-End	Minor or Possible Injury Ir Minor or Possible Injury F Minor or Possible Injury F	Fall to Yield 45 Follow Too Closely 35	10der 25 No	Dry Sand/Mud/Dirt		At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Intersection and Not Related	Straight & Level Straight & Level Straight & Level Straight & Level	No Vulnerable Road User Involved	Fa Fo	il to Yield Illow Too Closely	Dayligi Dayligi Dayligi	ht ht	Dry Dry Sand/Mud/Dirt		Fail to Yield Follow Too Closely Distracted Driving	No No
5645 1002238.86 1012192.49 2015 2 Vehicles.Parties Rear-End 5646 100180.88 1012219.7 2016 2 Vehicles.Parties Angle 5647 100150.88 1012219.7 2016 2 Vehicles.Parties Rear-End 5648 100150.88 1012219.7 2016 2 Vehicles.Parties Angle	Property Damage Only U Property Damage Only Ir	Inattention 35	100r 25 No	5-44 Dry Dry	Daylight Daylight	At Driveway or Intersection At Intersection and Not Related	Straight & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved		stracted Driving ider Influence of Alcohol/Drugs/Medication attention	Dayligi Dayligi Dayligi	ht	Dry Dry Wet	At Intersection and Not Related At Driveway or Intersection At Intersection and Not Related	Distracted Driving Under Influence of Alcohol/Drugs/Medication Inattention	No No No
5648 100159.0.88 1012219.7 2016 2 Vehicles/Partes Angle 5649 100159.0.87 1012219.9 2014 2 Vehicles/Partes Backing 5650 100159.0.87 1012219.99 2014 2 Vehicles/Parties Rear-End 5651 100159.0.87 1012219.99 2014 2 Vehicles/Parties Rear-End	Property Damage Only Ir Minor or Possible Injury Ir	Distracted Driving Un Improper Backing 55 Inattention 45 Other/Unknown 45	1der 25 No -64 No -64 No	Wet	Daylight Daylight Daylight	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Straight & Level	No Vuinerable Road User Involved	Ur Im	known Distraction proper Backing/Passing/Turning/Parking/Signaling attention	Dayligi Dayligi Dayligi	ht	Wet	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Distracted Driving No Risky Driving Behaviors Involved Inattention No Risky Driving Behaviors Involved	No No No
5651 1001880.87 1012219.69 [2014 2 Vehicles/Parties Rear-End 5652 1001580.88 1012219.7 [2015 4 Vehicles/Parties Rear-End 5653 1001581.1 1012226.24 [2019 2 Vehicles/Parties Rear-End	Property Damage Only C Property Damage Only Minor or Possible Injury Ir	Distracted Driving 35	-54 No -44 No -54 No	Dry	Daylight	At Driveway or Intersection	Straight & Level Unknown	No Vulnerable Road User Involved No Vulnerable Road User Involved	Di In:	one stracted Driving attention	Dayligi Dayligi Dayligi	ht .	Dry	At Driveway or Intersection	No Risky Driving Behaviors Involved Distracted Driving Inattention	No No No
5654 1000251.27 1012288.29 2016 2 Vehides/Parties Rear-End 5655 1000251.26 1012288.29 2014 2 Vehides/Parties Rear-End 5656 1000251.26 1012288.29 2014 2 Vehides/Parties Pedaloyclist	Property Damage Only In Minor or Possible Injury F Suspected Serious Injury C	Inattention 35 Follow Too Closely 55	-44 No -64 No -6 No	Dry Wet		At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Straight & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Pedaloydist and Older Road User 65+	In: Fo	attention flow Too Closely one	Dayligl Dayligl Dayligl	ht ht	Dry Wet Dry	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Inattention Follow Too Closely No Risky Driving Behaviors Involved	No No No
5657 100025126 1012288 29 2015 2 Vehicles/Parties Rear-End 5658 100025126 1012288 29 2016 2 Vehicles/Parties Sideswipe 5659 100025127 1012288 29 2017 2 Vehicles/Parties Rear-End	Minor or Possible Injury Ir	Inattention 45	-54 No -44 No -64 No	Dry	Daylight Daylight Daylight	At Driveway or Intersection At Driveway or Intersection	Straight & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved	Ur	attention sknown Distraction flow Too Closely	Dayligi Dayligi Dayligi	ht	Dry	At Driveway or Intersection	Inattention Distracted Driving Follow Top Closely	No Yes No
5660 998875.09 1012060.93 2015 2 Vehicles/Parties Angle 5661 998875.1 1012060.93 2017 1 Vehicle/Party Struck Object	t Property Damage Only In	Fail to Yield 65 Improper Turn/Merge 65	NO N	Wet Wet	Daylight Dark-Street Lights On	At Driveway or Intersection	Straight & Level Curve & Level Unknown	No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+	Fa Im	il to Yield proper Turn/Merge	Dayligi Dark-S	ht Street Lights On	Wet Wet	At Driveway or Intersection Roundabout At Driveway or Intersection Development	Fail to Yield No Risky Driving Behaviors Involved	No Yes
5662 998859.01 1012099.73 [2018 2 Vehicles:Paries Turning 5663 998875.1 1012090.93 [2015 2 Vehicles:Paries Rear-End 5664 998875.09 1012060.93 [2015 2 Vehicles:Paries Rear-End	Property Damage Only F Property Damage Only Ir	Fail to Yield 65 Follow Too Closely Un Inattention 55	+ NO	Wet Dry	Daylight Daylight Daylight	Roundabout Roundabout Roundabout	Straight & Level Curve & Level	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Fo	il to Yield Illow Too Closely attention	Dayligi Dayligi Dayligi	ht	Dry Wet Dry	Roundabout Roundabout Roundabout	Fail to Yield Follow Too Closely Inattention	No No No
9663 99667.5.07 107.200.93.2,015 2 vernices:raines resar-cho 5666 996858.18 107.21216.2019 2 Verhices:Paries Rear-End 5667 998875.09 107.200.93.2015 1 Verhices:Paries Rear-End 5669 998875.09 107.200.93.2016 1 Verhices:Paries 5669 998875.09 107.200.93.2016 2 Verhices:Paries Angle		Inattention 45 Under Influence of Alcohol/Drugs/Medication 55	104 No 104 No 105 No 10		Daylight Daylight Dark-Street Lights On	Roundabout Roundabout	Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved		attention attention ader Influence of Alcohol/Drugs/Medication	Dayligi Dayligi Dark-S	ht Street Lights On	Dry Dry	Roundabout Roundabout	Inattention Inattention Under Influence of Alcohol/Drugs/Medication	No No Yes
5668 998875.09 1012060.93 2016 1 Vehicle/Party Struck Object																
5670 998875.1 1012060.93 2016 1 Vehicle/Party Struck Object	t Property Damage Only D	Inattention 65 Disregard Traffic Control Device/Sign 65 Distracted Driving 55	+ No No No No	Dry	Daylight Daylight	Roundabood	Straight & Level	No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+ No Vulnerable Road User Involved	In: Di Op	attention sregard Traffic Control Device/Sign perating Handheld Cell Phone	Dayligi Dayligi	ht ht	Dry Dry Wet	Roundabout Roundabout	Inattention No Risky Driving Behaviors Involved	Yes No Yes
5670 998875.1 1012060.93 2016 1 Vehicle/Party Struck Object	t Property Damage Only D	Distracted Driving 55 Inattention Un Inattention 55	+ No	Dry Dry Wet Wet	Daylight Daylight Dark-Street Lights On Dark-Street Lights On	Roundabout	Straight & Level Curve & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Ins Di Op Ins Un	perating Handheld Cell Phone attention	Dayligi Dayligi Dark-S Dark-S Dayligi	ht ht Street Lights On Street Lights On ht	Dry Dry Wet Wet Dry Dry Dry	Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout	Inattention No Risky Driving Behaviors Involved Distracted Driving Inattention Inattention	Yes No Yes Yes Yes No Yes Yes No
5670 99887-5.1 10/12069 39/2016 1 Vehidel-Party Struck Object 5671 998858.18 10/12/12/82/2017 1 Vehidel-Party Struck Object 5672 998858.18 10/12/12/82/2018 2 Vehides/Parties Angle 5673 998858.18 10/12/12/82/2018 1 Vehides/Party Angle 5674 99889.68 10/12/12/82/2017 Vehides/Party Rear-End	t Properly Damage Chily t Properly Damage Chily t Properly Damage Chily t Minor or Possible tripury t Properly Damage Chily t Properly Damage Chily. I Properly Damage Chily. I Properly Damage Chily. I Properly Damage Chily More or Possible injury t More or Possible injury to Properly Damage Chily.	Distracted Driving 55 Instendion Unit Instendion Unit Index Influence of Nicohol/Drugs/Medication 55 Follow Too Closely 55 Follow Too Closely 55 Follow Too Closely 45 Follow Too Closely 25	* No * No *	Dry Dry Wet Wet Dry S-34 Dry Dry	Daylight Daylight Dark-Street Lights On	Roundabout Roundabout Roundabout Roundabout Roundabout Ab Driveway or Intersection	Straight & Level Curve & Level Straight & Level Curve & Level Straight & Level	No Vulnerable Road User Involved	Op In: In: Ur	attention regard Traffic Control Device/Sign sensenger Traffic Control Device/Sign sensenger traffic Control Device/Sign sensenger testention statention dider influence of Alcoholi/Drugs/Medication lideor Too Closely	Dayligi Dayligi Dark-S Dark-S Dayligi	ht ht Street Lights On Street Lights On ht Street Lights On ht Street Lights On	Dry Dry	Roundabout Ab Driveway or Intersection	Inattention No Risky Driving Behaviors Involved Distracted Driving	Yes No Yes Yes No No No No No No No No No
9870 98875 1 101200.09 2016 1 14hhris Barly Shuck Check 1 101201.00 2016 1 14hhris Barly Shuck 1 10	I Properly Damage Chirl I Properly Damage Chirl I Properly Damage Chirl Mor or Prossible Ripry More or Prossible Ripry More or Prossible Ripry Properly Damage Chirl More or Prossible Ripry Properly Damage Chirl More or Prossible Ripry Properly Damage Chirl	Distracted Driving 55 nattention Uninattention Inattention 55 Index Influence of Alcohol/Drugs/Medication 55 Follow Too Closely 55 Follow Too Closely 45 Follow Too Closely 25 Distracted Driving Unin Distracted Driving 65	-64 No -54 No -54 No -54 No -54 No -54 No -54 No -55 No -5	Dry Dry Dry Wet Wet Wet Dry Dry	Daylight Daylight Daylight Daylight Dark-Street Lights On Daylight Dark-Street Lights On Daylight Dark-Street Lights On Daylight Daylight Daylight Daylight Daylight Daylight Daylight	Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout Ab Diveway or inters ection	Straight & Level Curve & Level Straight & Level Curve & Level Straight & Level	No Vulnerable Road User Involved Older Road User Involved Older Road User Involved	Or Institute of In	errating Alandheld Cell Phone titension titension titension dider Infusione of Alcohol/Drugs/Medication llow Too Closely llow Too Closely llow Too Closely stracted Driving Alandheld College stracted Driving Alandheld College stracted Driving Alandheld College stracted Driving Alandheld College stracted Driving	Dayligi Dayligi Dark-S Dark-S Dayligi Dark-S Dayligi Dark-S Dawn/ Dayligi Dayligi Dayligi Dayligi Dayligi Dayligi Dayligi	Int Int Street Lights On Street Lights On Int Street Lights On Int Ousk Int	Wet Dry Dry Dry Wet Dry Dry Vet Dry Dry Dry Dry	Roundsbout	Instantial No Resky Driving Behaviors Involved Distracted Driving Instantial Driving Instantial Control Control Control Instantial Control Instant	Yes No Yes Yes Yes No Yes No
5671 1985 1 10 10 10 1 1 1 1 1	I Properly Chambage Chily Properly Chambage Chily I Properly Chambage Chily I Properly Chambage Chily I Properly Chambage Chily Ment or Properly Chambage Chily Properly Chambage Chily Ment or Properly Chambage Chily Ment or Properly Chambage Chily Properly Chambage Chily Ment or Properly Chambage Chily Ment or Properly Chambage Chily Ment or Properly Chambage Chily	Distracted Driving State State Of Printing State State Of Printing Under with State Sta	-64 No -6	Dry Dry Wet Dry Wet Dry Wet Dry Wet Dry Wet Dry Wet Dry Dry Wet Dry Dry Wet Dry Dry Wet Dry Dry Wet Dry Dry Wet Dry Dry	Daylight Countying Countyi	Soundation II So	Straight & Level Curve & Level Straight & Level	Ne Walmanble Road User Involved No Walmanble Road User Involved	Opination of the control of the cont	earstand, Handheld Cell Phone Interfacion der Influence of Alcohol/Drugs-Medication Interfacion der Influence of Alcohol/Drugs-Medication Inter Tea Closely Inter Tea Closely Inter Tea Closely Interfacion Annual Control Annual Control Annual Control Interfacion Interfaci	Dayligi Dayligi Dark-S Dark-S Dayligi Dark-S Dayligi Dark-S Dawn/ Dayligi Dayligi Dayligi Dayligi Dayligi Dayligi Dayligi	ht ht ht his	West Dry Dry Dry West Dry Dry West Dry Dry Dry West Dry West Dry West Dry West	Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout AD Dreway or Intersection	Institution on No. Risky Driving Behaviors Involved Ostenberd Driving Ostenberd Driv	Yes No No Yes Yes Yes No
Sept	I Projectivi Chambage Chilvi More of Packaste Birginy More of Packaste Birginy Frequency Chambage Chilv More of Packaste Birginy Property Chambage Chily More of Packaste Birginy Property Chambage Chily More of Packaste Birginy Property Chambage Chily More of Packaste Birginy Property Chambage Chily More of Packaste Birginy	Distracted Driving Signature of Acched Driving Under Inhiericon Under Inhiericon Signature of Acched Driving Medication Signature of Acched Driving Medication Signature of Control Driving Signature of Control Driving Under Driving Distracted Driving Distra	-64 No -54 No -54 No -54 No -54 No -54 No -54 No -55 No -5	Dry Dry Wet Wet Dry Wet Dry Wet Dry Dry Wet Dry Dry Dry Dry Dry Dry Wet Dry Wet Dry Dry Wet Dry Dry	Daniple	Repuration of Reputation of Re	Straight & Level Curne & Level Straight & Level	No Vulnerable Road User Involved	Opination of the control of the cont	sensing Nandheld Cell Phone testinon te	Daytigl Daytigl Daytigl Dark-S Dark-S Dark-S Dark-S Dark-S Dark-S Dawn/I Daytigl	Media Marchiston Cristonel Lights Cristo	Wet Dry Dry Dry Wes Dry Wes Dry Dry Ury Ury Ury Ury Ury Ury Ury Ury Ury U	Roundstout	Instantion on No Risky Driving Behaviors Involved Ostenberg Orieting Association of Management Outside Association of Management Outside Instance of Association Orieting Association Foliour Too Closely Foliour Too Closely Ostenberg Orieting Ostenberg Orieting Ostenberg Orieting Ostenberg Orieting Ostenberg Orieting Ostenberg Orieting Ostenberg Ost	\(\text{Yest} \) \(\text{West} \) \(\text{Yest} \) \(\text{Yest} \) \(\text{Yest} \) \(\text{No.} \)
Sept	I Project Chambage Child I Project Chambage Child I Project Chambage Child I More or Passasite Pulpy I More or Passasite Pulpy I Project Chambage Child I More or Passasite Pulpy I Project Chambage Child I More or Passasite Pulpy I More or Passasite Pulpy I More or Passasite Pulpy I Project Chambage Child I More or Passasite Pulpy I Project Chambage Child I More or Passasite Pulpy I Project Chambage Child I Project Child I Project Chambage Child I Project Chambag	Distanced Drining	-64 No -6	Dry Dry Wet Wet Wet Wet Dry Dry Wet Dry Wet Dry Wet Dry	U-budget Dunk-Ormer Lights On Dunk-Ormer L	Sourcidebout Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout AD Description of the section AD Dressey or Interaction	Straight A Level Course & Level Straight A Level Straight & Level	Ne Wildersiche Road User Insched Ne Vollersiche Road User Insched	Ot Institute of the control of the c	earstand, Handheld Cell Phone Interfacion der Influence of Alcohol/Drugs-Medication Interfacion der Influence of Alcohol/Drugs-Medication Inter Tea Closely Inter Tea Closely Inter Tea Closely Interfacion Annual Control Annual Control Annual Control Interfacion Interfaci	Daytigl Daytigl Daytisgl Dark-S Dark-S Dark-S Daytigl Dark-S Daytigl	International Conference Conferen	Wiet Dry Dry Dry Wet Dry Uny Dry Wet Dry Uny Dry Wet Dry Wet Dry Wet Dry Wet Dry Wet Dry Ory Dry Ory Dry Dry Dry Dry Dry Dry Dry Dry Dry D	Roundboot R Roundb	Instantion or Section of Behaviors Insolved to Rose (Darly Dring Behaviors Insolved Section of Behaviors Insolved Section of Behaviors Insolved Section of Parallel Orange Albedosino Under Information of Parallel Orange Albedosino Section of Section of Parallel Orange Section of Section	Yest
5677	I Properly Chambage Child I Properly Chambage Child I More or Pressale Buylory I Properly Chambage Child I Properly Chamba	Distracted Driving 165 (165) 166 (165)	6-64 No	Bry Dry Bry Bry	Li Sadigheir Li Sadigheir Dank- Girmest Lights On Dank	Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout All Dreweay or Interaction All Dreweay or Interactio	Straight A Lovel Curve & Lovel Curve & Lovel Curve & Lovel Curve & Lovel Straight & Lovel	No. Visinershik Road User Involved Odder Road User Involved No. Visinershik Road User Involved No.	Or Institute of the control of the c	erstina Narcheld Cell Phone tentrols der Influence of Accisionage Medication der Influence of Accisionage Medication for Text Closely tion Text Closely tex	Daylej Da	Direct Lights On Direct	Wet Wet Dry Dry Wet Dry Wet Dry Ury Wet Dry Wet Dry Wet Dry Wet Dry Wet Dry Wet Dry	Roundbout Roundb	Instantionio No. Risky Driving Bahavalors Involved Landerston L	Yes
Sept	In Proceed Chambage Child In Proceed Chambage Child In Minor or Proceeding Program of the Proceeding Proceeding In Proceedings	Distracted Driving 65 (1997) 10	6-64 No	Dry Dry Wet Dry Dry Wet Dry Wet Dry Dry Wet Dry Dry Wet Dry Dry Wet Dry Dry	L'Oudgelet L'Oungelet Loungelet Loun	Secretable of Roundation of Ro	Straight A Level Octore & Liveri Octore & Liveri Corre & Liveri Straight A Level	No Visinerable Road User Involved	Or Institute of the control of the c	ensing Alexandreid Cell Phone tentrols der Influence of Alexandreid Cell Phone der Influence of Alexandreid Cell Phone der Influence of Alexandreid Cell Phone der Teo Closely liter Teo Closely liter Teo Closely liter Teo Closely stated Didning stated Didning stated Didning stated Cell Phone der Teo Cell Phone der Teo Cell Phone der Teo Cell Phone der Cell Phone der Cell Phone der Teo Cell Phone der Teo Cell Phone der Cell Phone der Teo Cell Phone der Teo Cell Phone der Cell Phone der Teo Cell Phone	Daylej Da	Manufacture Comment of the Comment o	Wet Wet Dry Dry Wet Dry Wet Dry Ury Wet Dry Wet Dry Wet Dry Wet Dry Wet Dry Wet Dry	Roundbout Roundb	Instantionio No. Risky Driving Bahavalors Involved Landerston L	Yes
Sept	In Proceed Chambase Lebra More or Preside Buylor Properly Chambase Chambase Lebra More or Preside Buylor Properly Chambase Chambase Lebra More or Preside Buylor Properly Chambase Chambas	Distracted Driving Linder Influence of Alcohol Drugs Medication Linder Influence of Alcohol Drugs Medication 250 Driving Influence of Alcohol Driving 250 Driving Influence October Driving 250 Driving Influence 250 Drivin	-64 No -6	Dry Wet Dry Wet Dry Wet Dry Dry	L'Oudighet Daniel Breet Lights On Danie Street Lights On Danie Street Lights On Daniel Stree	Sourcideboot Roundsboot Roundsboot Roundsboot Roundsboot Roundsboot AD Dressey or Interaction AD	Stroight & Lored Corne & Lored Corne & Lored Corne & Lored Stroight & Lored	Ne Vuleranie Road User Involved	Oct Oct	earstina (Barotheld Cell Phone territorio der Influence of Accision (Truga Medication territorio) tion Tea Closely	Dayleg	Service Control of the Control of th	Wet Dry Dry Dry Wet Dry Dry Wet Dry	Roundbout Roundb	Instantion of the Control of the Con	Yes
Methods	In Proceed Chambage Librit I Proceed Chambage Librit I Proceed Chambage Chip I Proceed Chip I Pr	Distracted Driving	6-64 No	Bry Wet Wet Bry Bry	Libodipide Danks General Lipides Cin Danks	Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout A Dineasy to Interaction A Dineasy or Interaction A Din	General & Lean Sengiri & Lean Sengir	New Volumental Read User Involved New Volumental Read User Involved No Volumental Read User Involved Older Read User Edit No Volumental Read User Involved Other Read User Edit No Volumental Read User Involved Other Read User Edit No Volumental Read User Involved Other Read User Edit No Volumental Read User Involved Other Read User Edit No Volumental Read User Involved Other Read User Edit No Volumental Read User Involved Other Read User Edit No Volumental Read User Involved Other Read User Edit No Volumental Read User Involved No Volumental Read User Involved No Volumental Read User Involved Other Read User Edit No Volumental Read User Involved No Volumen	Cr Cr	earsing Narcheld Cell Phone tentrols der Influence of Accision Trugs Medication der Influence of Accision Trugs Medication to Tex Colosely tion Tex Colosely textorial Central Centrol textorial Centrol textorial Centrol textorial Centrol textorial te	Bayleji Dayleji	Direct Lights On Direct	Wet Wet Wet Dry Dry Dry Wet Dry Dry Wet Dry Dry Wet Dry Wet Dry Wet Dry Wet Dry Wet Dry Wet Dry Wet Dry Dry Dry Wet Dry Wet Dry Dry Wet Dry Dry Wet Dry	Roundbout Roundb	Instantion on No. Risky Driving Behaviors Involved Associated to the Instantion of Instantion Instantion of Instantion o	Yes
March	I Property Chambage Chips I Man or or Pressible Business Property Chambage Chips Property Chambage Chips Property Chambage Chips Man or or Pressible Business Property Chambage Chips Man or or Pressible Business Property Chambage Chips Man or or Pressible Business Property Chambage Chips Man or or Pressible Business Man or or Pressible Business Property Chambage Chips Man or or Pressible Business Man or or Pressible Business Property Chambage Chips Property Chambage Chip	Distracted Driving Control of Control of Control Original Control of Control Original Cont	6-64 No	Bry West	Libodipide Danks General Lipides Cin Danks	Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout AD Dreesey or Interaction AD DREESE ORDER AD	General & Lean Sengiri & Lean Sengir	New Volumental Read User Involved New Volumental Read User Involved No Volumental Read User Involved Older Read User Edit No Volumental Read User Involved Other Read User Edit No Volumental Read User Involved Other Read User Edit No Volumental Read User Involved Other Read User Edit No Volumental Read User Involved Other Read User Edit No Volumental Read User Involved Other Read User Edit No Volumental Read User Involved Other Read User Edit No Volumental Read User Involved Other Read User Edit No Volumental Read User Involved No Volumental Read User Involved No Volumental Read User Involved Other Read User Edit No Volumental Read User Involved No Volumen	Cr Cr	earstina Narcheld Cell Phone technical Cell Phone des Influence of Alcohol/Drugs-Medication (des Influence of Alcohol/Drugs-Medication (des Influence of Alcohol/Drugs-Medication (des Text Costaly) des Text Costaly	Dayleg	Manufacture Comment of the Comment o	Wet Wet Wet Dry Dry Dry Wet Dry Dry Wet Dry Dry Wet Dry Wet Dry Wet Dry Wet Dry Wet Dry Wet Dry Wet Dry Dry Dry Wet Dry Wet Dry Dry Wet Dry Dry Wet Dry	Roundbout Roundb	Instantion of the Control of the Con	Yes
Sept	I Property Chambage Chair More or Preside Buylory I Property Chambage Chair More or Preside Buylory Property Chambage Chair More or Preside Buylory Property Chambage Chair More or Preside Buylory Property Chambage Chair Property Chambage Chair Property Chambage Chair More or Preside Buylory More or Preside B	Distracted Driving Control of Control Driving Medication Control of Control Driving Control Driving Control of Control Driving Contr	64.64 No. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1.	By By By By By By By By	Li Sadighei Li Sad	Secretable of Roundebord Roundebo	Gender & Level Gender & Level Gender & Level Gender & Level Grand & Level	No Visinerable Road User Involved Older Road User Involved No Visinerable Road User	CC CC CC CC CC CC CC CC	earstina Narchald Cell Phone technical Cell Phone desired Cell Phone d	Baylej Daylej Da	Direct Lights On Direct	Wet Wet Dry	Roundbout Roundb	Instanction No. Risky Driving Behaviors Involved Associated to the Control of t	Yes
Methods	I Property Chambage Chips I Man or a Preside Injury I Property Chambage Chip Man or a Preside Injury Property Chambage Chip Man or a Preside Injury Property Chambage Chip Man or a Preside Injury Property Chambage Chip Man or a Preside Injury Property Chambage Chip Propert	Districted Driving Linder Influence of Alcohol Charge Medication Linder Influence of Alcohol Charge Medication Section 1	64.64 No. 1 45.41 No. 1 45.42 No. 1 45.43 No. 1 45.44 No. 1 45.44 No. 1 45.44 No. 1 45.45 No. 1 45.45 No. 1 45.45 No. 1 45.46 No. 1 45.46 No. 1 45.46 No. 1 45.47 No. 1 45.48 No. 1 45.48 No. 1 46.48 No. 1 46.49 No. 1 46.49 No. 1 46.40 No. 1 46.41 No. 1 46.41 No. 1 46.41 No. 1 46.42 No. 1 46.44 No. 1 46.46 No. 1 46.47 No. 1 46.47 No. 1 46.48 No. 1 46.49 No. 1 46.49 No. 1 46.40 No. 1 46.40 No. 1 46.40 No. 1 46.41 No. 1 46.41 No. 1 46.41 No. 1 46.42 No. 1 46.44 No. 1 46.44 No. 1 46.44 No. 1 47.44 No. 1 48.44	Bry Wet	L'Oudglett Daniel Breet Lights On Danie Breet Lights On Daniel Breet Lights On	Repuration of Roundation of Ro	Stroight & Level Corne & Scale Cor	No. Visionaribi Road User Involved Older Road User Involved No. Visionaribi Road User Involved No.	Or O	eration (Associated Cell Phone territorion del influence of Accinicithuga Medication (little Teach Teach Cell Phone territorion del influence of Accinicithuga Medication (little Teach Teach Cell Phone territorion (little Teach Cell Phone territorion (littl	Daylegi	Direct Lights On Direct	Wet	Roundbout Roundb	Instantion or to Classify The State of Classify The State of Classify The State of Classify The State of Classification	Yes
Sept	I Property Chambage Chips I Manor or Preside Injury I Property Chambage Chip Manor or Preside Injury Property Chambage Chip Manor or Preside Injury Property Chambage Chip Manor or Preside Injury Manor or Preside Injury Property Chambage Chip Property Chambage Chip Manor or Preside Injury Property Chambage Chip Property C	Distracted Driving	64.64 No. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1.	Dry West Dry Dry	Consight Control (Control Control Cont	Securidation of Roundation of	Signatur & Level	New Volumental Read User Involved New Volumental Read User Involved No Volumental Read User Involved Other Read User Involved No Volumental Read User Involved No Volumenta	Or O	earsing Narcheld Cell Phone tention der Influence of Accinol/Druga-Medication (der Text Cells of	Usylight Darks	See Tourist Committee Comm	Wet Wet Dry Dry Wet Dry Dry Dry Wet Dry Dry Dry Wet Dry Dry Dry Dry Wet Dry Dry Dry Wet Dry Dry Wet Dry Dry Wet Dry Dry Dry Dry Wet Dry	Roundbout Roundb	Instanction or, Kisky Driving Behaviors Involved voic Risky Driving Behaviors Involved sharper of the Control of the Contro	Yest No No No No No No No N
Sept	I Property Chambage Chips I Man or or Preside Injury I Property Chambage Chip Man or or Preside Injury Property Chambage Chip Man or or Preside Injury Property Chambage Chip Man or or Preside Injury Property Chambage Chip Prope	Distracted Driving Linder Industries Linder Industries Linder Industries Linder Industries Linder Industries Political Text Clearing Political Text Clearing Linder Industries	64.6 Mo	Dry West Dry Dry	Danight Consider to the Consideration	Sourcidebout Roundebout Roundebout Roundebout Roundebout Roundebout AD Dressey or Intersection AD Dressey or Intersection AD Dressey or Intersection AD A	Stroight & Level Corne & Korel Stroight & Level	No. Victoriania Road User Involved Older Road User Eder Involved Older Road User Involved No. Victoriania Road User Involved Older	Or O	exertion Liversheld Cell Phone territorion der Influence of Accinistithruga Medication der Influence of Accinistithruga Medication der Influence of Accinistithruga Medication to Tex Colorably	1874 1874	Description Co. Descri	Weet West We	Roundboud Roundb	Instantion or Control of the Control of Cont	Yest No No No No No No No N
Sept	In Proceed Chambage Child In Proceed Chambage Child In Many or Proceeding Program Chambage Child In Proceeding Child In International Child In Proceeding Child In International Child In International Child	Distracted Driving Driving Driving Distracted Driving Driving Driving Driving Driving Driving Driving Driving Driving Dr	64.6 Mos	Bry West	Douglate Dou	Securidation of Roundation of	Storich & Level Corne & Level Corne & Level Corne & Level Storich & General Storich	New Visionaria Road User Involved Older Road User Eder New Visionaria Road User Involved Older Road User Eder New Visionaria Road User Involved Older Road User Involved Older Road User Involved Older Road User Involved New Visionaria Road User Involved Older Road User Involved New Visionaria Road User Involved New Visi	Cr.	existing Narionfield Cell Phone territorion der Influence of Notrois Drugs Medication der Influence of Notrois Drugs Medication der Influence of Notrois Drugs Medication to Tex Colossity to Tex	Designation of the control of the co	Secretary of the secret	Web 19	Roundibout	Instantion or to Classify The Committee of C	Yes
Sept	In Proceed Chambage Child In Proceed Chambage Child In Minor or Proceded In Information Child Informat	Distracted Driving	64.6 Mos	Dry West Dry Dry	Londpide Long Long Long Long Long Long Long Long	Sourcitabios of Roundabout Rounda	Storgish & Level	New Watership Road User Involved Wa	Cr.	earstina Narchridel Cell Phone territorion der Influence of Accholichruge Medication der Influence of Accholichruge Medication der Influence of Accholichruge Medication for Tex Closely lice Te	Designation of the control of the co	See Light Co. Se	Weet We	Roundbout	instantion or control of the control	Yes
Sept	In the Property Chambage Chips More or Preside Injury For Property Chambage Chip More or Preside Injury Property Chambage Chip More or Preside Injury Property Chambage Chip Property Chambage Chip Property Chambage Chip Property Chambage Chip More or Preside Injury Property Chambage Chip More or Preside Injury Property Chambage Chip Property Chambage	Distracted Driving	64.4 No. 1 14. No. 1 15. No. 1 16. No. 1 17. No. 1 18. No. 1	Dry West Dry Dry	Londpilet Dank Direct Lights On Dank Direct Dank Dank Dank Direct Dank Dank Direct Dank Dank Dank Dank Dank Dank Dank Dank	Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout Al Doweway or hitera ection Al at hiterate ection Diversity Roundabout Al A Hiterate ection Diversity Roundabout Al Doweway or hiterate ection Al at hiterate ection Diversity Roundabout Al Doweway or hiterate ection Al Doweway or hiterate ection Al A Doweway or hiterate ection Al A Doweway or hiterate ection Al A Hiter	Simple A Learn Simple A Learn	No. Watershie Road User Involved No. Wa	On the control of the	earstina (Narchield Cell Phone tentrols) der Influence of Aktoriol/Drugs-Medication der Influence of Aktoriol-Drugs-Medication tentrols tentrols of Content Druces-Sign stander Druces-Sign stander Druces-Sign stander-Sign stander-Sign-Medication tentrols tentrols of Content Druces-Sign stander-Sign-Medication tentrols tentrols of Content Druces-Sign stander-Sign-Medication tentrols tent	Designation of the control of the co	Secretary of the secret	Weet Weet Weet Weet Weet Weet Dry	Roundbout Roundb	instanction or control of the contro	Yes No No No No No No No N
March	In Proceed Chambage Child In Proceed Chambage Child In Minor or Proceeding In Information In Information Informati	Distracted Driving Distra	64.64 No. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1.	Dry Dry	Consider Constitution Constitut	Sourcidebout Roundebout Roundebout Roundebout Roundebout Roundebout Roundebout AD Dressey or Intersection AD Dressey or Intersection AD Dressey or Intersection AD Dressey or Intersection AD Dressey or Intersection AD Dressey or Intersection	Simple & Lord Si	New Watership Read User Involved New Wa	Cr.	earstina Narchridel Cell Phone territorion der Influence of Accholichruge Medication der Influence of Accholichruge Medication der Influence of Accholichruge Medication for Tex Closely lice Te	Designation of the control of the co	Secretary of the secret	Wet	Roundbload	Instantion or Control of the Control of Cont	Yes No No No No No No No N
Sept	In Proceed Chambage Childry More or Preside View Control Chambage Childry Properly Chambage Childry More or Preside View Childry More or Preside View Childry Properly Chambage Childry Properly Chambage Childry Properly Chambage Childry Properly Chambage Childry More or Preside View Childry Properly Chambage Childry Properly	Distracted Driving	64.64 No. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1.	Bry West W	Douglaght Dougla	Sourcitabious (Roundabous (Ro	Stocket & Level Stocket & Leve	No. Visionesia Road User Involved Older Road User Ed. No. Visionesia Road User Involved Older Road User Ed. No. Visionesia Road User Involved Older Road User Ed. No. Visionesia Road User Involved Older Road User Ed. No. Visionesia Road User Involved Older Road User Ed. No. Visionesia Road User Involved Older Road User Ed. No. Visionesia Road User Involved No. Visionesia Road User Invol	Co.	earsing Narcheld Cell Phone territorion der Influence of Accinosithrugs Medication der Influence of Accinosithrugs Medication for Tex Closely tion Tex Closely textorion tion Tex Closely textorion tion Tex Closely textorion tion Tex Closely tio	Designation of the control of the co	Secretary of the secret	Weet We	Roundboutd Roundb	instanction Are flasty Driving Behaviors Involved Are f	Yes No No No No No No No N
March	In Proceed Chambage Chips More or Preside View Control Chambage Chips More or Preside View Chips More or Preside View Chips More or Preside View Chips Properly Chambage Chips Process Chambage Chips Process Chambage Chips Process Chambage Chips More or Preside View Chips Process Chambage Chips More or Preside View Chips Process Chambage Chip More or Preside View Chips Process Chambage Chip Process Chambage Chip More or Preside View Chips Properly Chambage Chip More or Preside View Chips Properly Chambage Chip Prop	Distracted Driving	64.64 No. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1.	Day West Day Day	Doublight	Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout AD Dressey or Intern action Not at International Dressey Broundabout And a International Dressey Broundabout And a International Dressey Broundabout Not at International Dressey Broundabout AD Dressey or Internation AD Dressey or Inter	Stocket & Level Stocket & Leve	No. Visionesia Road User Involved Older Road User Involved No. Visionesia Road User Involved Older Road User Involved No. Visionesia Road User Involved Older Road User Involved No. Visionesia Road User Involved Older Road User Involved No. Visionesia Road User Involved Older Road User Involved Older Road User	Co.	earstina Narchridel Cell Phone territorion der Influence of Accholichruge Medication der Influence of Accholichruge Medication der Influence of Accholichruge Medication for Tex Closely lice Te	Designation of the control of the co	Direct Lights On Direct	Weet We	Roundbood Konnistender Annahouse Roundbood Roundbood Annahouse Roundbood Annahouse Annahous	instanction Are flasty Driving Behaviors Involved Are f	Yes
March	In Property Chambage Chips More or Preside Injury Property Chambage Chips More or Preside Injury Property Chambage Chips More or Preside Injury Property Chambage Chips More or Preside Injury Property Chambage Chip Property Cha	Distracted Driving	64 1 No. 1 N	Dry West Dry Dry	Loudgett Danis Breet Lights On Danis Breet	Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout AD Dreesey or Interaction And Interaction	Stocket & Level Stocket & Leve	No. Visionesia Road User Involved Older Road User Road No. Visionesia Road User Involved Older Road User Road No. Visionesia Road User Involved Older Road User Road No. Visionesia Road User Involved Older Road User Road No. Visionesia Road User Involved Older Road User Road No. Visionesia Road User Involved No. Visione	Co.	earsting Narchfeld Cell Phone technical Cell Phone	Designation of the control of the co	Disease Laptins On Disease Lapti	West	Roundboutd	instantion or control of the control	Yes
Bern	I Properly Chambage Chips More or Preside Hayny Properly Chambage Chips Properly Chambage Chips More or Preside Hayny Properly Chambage Chip Pr	Distracted Driving	64.6 Mon	Dry West Dry Dry	Consight Danis Breet Lights On Cans Breet	Roundaboud	Stocket & Level Stocket & Leve	No. Visionesia Road User Involved Older Road User Road No. Visionesia Road User Involved Older Road User Road No. Visionesia Road User Involved Older Road User Road No. Visionesia Road User Involved Older Road User Road No. Visionesia Road User Involved Older Road User Road No. Visionesia Road User Involved No. Visione	Co.	erantica Nariodel Cell Phone territorion der Influence of Accinistings Medication to Tec Costany to Tec Cost	Designation of the control of the co	Secret Lights On Secret	West	Roundboutd Romeway or Internation Roundboutd	instanction was finally privage and process of the	Yes
Sept	In the control of the	Distributed Driving Linder Influence of AlcoholDrugs Medication Linder Influence of AlcoholDrugs Medication Stributed Tool Classify Follow Too Classify	64.64 No. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1.	Day Day	Consider Consid	Roundaboud	Signatur & Level	New Volumentale Road User Involved New Volumentale Road User Involved No Volumentale	On the second se	earsing Narcheld Cell Phone tentrols der Influence of Accinolithugs Medication der Influence of Accinolithugs Medication for Tex Closely time of the Closely time of	Designation of the control of the co	Secretary of the secret	Weet We	Roundboud Roundb	instanction was finally privage and process of the	Yes No No No No No No No N
March	In the Property Chambage Chips More or Preside Injury From Chambage Chips More or Preside Injury Property Chambage Chips More or Preside Injury Property Chambage Chips More or Preside Injury Property Chambage Chips Property Chambage Chips More or Preside Injury Property Chambage Chips More or Preside Injury Property Chambage Chips More or Preside Injury Property Chambage Chips Propert	Distracted Driving	64.4 No. 1 N	Dry West Dry Dry	Londight Dank Breet Lights On Dank Breet L	Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout AD Dreesy or Interaction And In	Signatur & Level	New Volumentale Road User Involved New Volumentale Road User Invo	Columbia	earsing Narcheld Cell Phone technical Cell Phone delirations of Acciding Macdication (der Influence of Acciding Macdication (Design D	Search Lights On Search	West	Roundboutd Romeway or Internation Roundboutd	instanction or control of the contro	Yes Yes
Sept	In Property Chambage Chips More or Preside Burgary E Property Chambage Chips More or Preside Burgary Property Chambage Chips More or Preside Burgary Property Chambage Chips More or Preside Burgary Property Chambage Chip Prop	Description	64.64 No. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1.	Dry West Dry Dry	Condigitat Control (Control Control C	Roundaboud	Stocket & Learn Stocke	Neu-Visional Read User Involved	Co.	earsing Narchald Cell Phone tentrols and Cell Phone te	Designation of the control of the co	Search Lights On Search	West	Roundboud Roundb	instanction or too floating behaviors involved to floating behaviors involved to floating behaviors involved to floating behaviors involved to floating behaviors floating floating behaviors floating behaviors floating floating floating behaviors floating floating floating behaviors floating fl	Yes
Sept	In the Property Chambage Chips More or Prosible Pulyor Property Chambage Chips More or Prosible Pulyor Property Chambage Chips More or Prosible Pulyor Property Chambage Chips More or Prosible Pulyor Property Chambage Chip Prope	Distributed Ohing Linder Influence of AlcoholDrugs Medication Linder Influence of AlcoholDrugs Medication Solitor Influence of AlcoholDrugs Medication Solit	64.4 No. 1 N	Dry West Dry Dry	Consider Constitution Constitut	Sourcitabious (Roundabous (Ro	Signatur & Leaner Signatur & L	Neu-Verlande Road User Involved	Co.	earsing Narchdel Cell Phone technical Cell Phone der Influence of Accidioting Medication in a Valid der Contributing Cell Phone de	Design D	Senert Lights On Senert Lights On Senert Lights On Mark	Weet We	Roundibout Roundibout Roundibout Roundibout Roundibout Roundibout Roundibout Roundib	instanction or too floating behaviors involved to floating behaviors involved to floating behaviors involved to floating behaviors involved to floating behaviors floating floating behaviors floating behaviors floating floating floating behaviors floating floating floating behaviors floating fl	Yes Yes
Sept	I Properly Chambage Chips More or Prosible Playing I Properly Chambage Chips More or Prosible Playing Properly Chambage Chips More or Prosible Playing Properly Chambage Chips More or Prosible Playing Properly Chambage Chip Properly Cha	Description	64.4 No. 1 N	Day West Day West Day West Day West Day West Day West Day West Day	Consider Constitution of the constitution of	Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout Roundabout AD Dreesey or Interaction And Interaction AD Dreesey or Interaction AD Dreesey or Interaction AD Dreesey or Interaction AND Interaction AD Dreesey or Interaction	Stocket & Level Stocket & Leve	No Victoriania Road User Involved No Victoriania R	Co.	earsing Narcheld Cell Phone technical Cell Phone der Influence of Accinolithruga Medication to Teo Cost Teo Cell Cell Phone technical Cell Phone technica	Designation of the control of the co	Desert Lights On Desert	Weet We	Roundibout Konnistioned Roundibout Roun	instanction on Clasty Driving Behaviors involved voice Clasty Driving Behaviors involved submitted submitt	Yes Yes
March	In the Property Chambage Chips More or Preside layery From Chambage Chips More or Preside layery Property Chambage Chips More or Preside layery Property Chambage Chips Property Chambage Chips Property Chambage Chips More or Preside layery Property Chambage Chips Property Chambage Chips Property Chambage Chips More or Preside layery Property Chambage Chips More or Preside layery Property Chambage Chips Property Chambag	Disrepted Priving Linder Influence of AlcoholDrugs Medication Linder Influence of AlcoholDrugs Medication Soliticate Influence of AlcoholDrugs Medication Soliticated Influence of AlcoholDrugs Medicati	64.64 No. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1. 1.	Dry West Dry Dry	Condigitat Control (Control Control C	Roundaboud	Signatur & Level	Neu-Verlande Road User Involved	Co.	earling National Cell Phone desiration (Section Change Medication Cell Phone der Influence of Acciniciting Medication Cell Phone decompliance of Cell Phone decompliance of Acciniciting Cell Phone the Continuing Cell Phone the Continui	Design D	Several Lights On Several Ligh	Weet	Roundibout Round	instanction or control of the contro	Yes
Sept	In the Property Chambage Chips Water or Preside Injury If Property Chambage Chip More or Preside Injury Property Chambage Chip More or Preside Injury Property Chambage Chip More or Preside Injury Property Chambage Chip More or Preside Injury Property Chambage Chip More or Preside Injury More or Preside Injury Property Chambage Chip Property Chambage	Dissupered Driving Linder Influence of AlcoholDrugs Medication Linder Influence of AlcoholDrugs Medication Selling Influence of AlcoholDrugs Medication Sell	64.6 Mon	Dry West Dry Dry	Douglaght	Roundabout	Signatur & Level	No. Victorianità Road Ulear Involued No. Victorianità Road Ulear Invo	Co.	earsing National Cell Phone technical Cell Phone delimination of Microsofthings Medication delimination of Microsofthings Medication delimination of Microsofthings Medication delimination of Microsofthings Medication library for Conselvance stocked Phone decomposition of Conselvance stocked Phone decomposition of Microsofthings Medication is vivide in the Valid decomposition of Microsofthings Medication in the Valid decomposition of Microsofthings Medication decomposition of Microsofthings Med	Designation of the control of the co	Desert Lights On Desert Lights Off Desert Lights	Web 19	Roundboud Roundb	instanction or to floating limitations involved to floating limitations involved to floating limitations involved to floating limitation or floating limitation	Yes Yes

OBJECTID JURISDICTION	COUNTY CITY	PRIMARY BLOCK TRAFFICWAY NUMBER	INTERSECTING DIST ROOM TRAINCE TROOM REF-POINT POINT_NAME MLEPOST B SUSPENSE.ND NAMEER	DATE TIME MOST. SEVERE NUM	BIKES VEH_1_TTPE	VEH_2_TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD_SURFACE ROADWAY CHARACTERISTIC	LIGHTING CONDITION	ARST_COLLISION TYPE_OBJECT_STRUCK	VEH_1 ACTION	VEH_2 ACTION	VEH.1.COMPASS DIRECTION.FROM DIRECTION.FROM DIRECTION.TO DIRECTION.FROM DIRECTION.TO DIRECTION.T
5759 City Street 5760 City Street 5761 City Street 5762 City Street 5762 City Street 5764 City Street 5764 City Street 5766 City Street 5766 City Street 5766 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST 800 W WASHINGTON ST 100	29 F W S SHAWLN No E393960 100 F W SEQUIMAVE No E933960 432 F W SQUIMAVE NO E9338732	2015-01-05 10:02 No Apparent Injury 0 0 2 0 2019-07-08 15:55 No Apparent Injury 0 0 2 0 2 0 2019-07-08 15:55 No Apparent Injury 0 0 2 0 0 2018-05:11 71-04 No Apparent Injury 0 0 2 0 2019-12-04 10:37 No Apparent Injury 0 0 2 0 2019-12-04 10:37 No Apparent Injury 0 0 2 0 0 2019-08:20 13:20 No Apparent Injury 0 0 2 0	Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway Intersection Related but Not at Intersection	Raining Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level Dry Straight & Level	Daylight E	ntering at angle om same direction - both going straight - one stopped - rear-end	Making Left Turn Starting in Traffic Lane Making Left Turn	Going Straight Ahead Stopped for Traffic	South West West East West East Wehicle Stopped Wehicle Stopped
5761 City Street 5762 City Street 5763 City Street	Clallam Sequim Clallam Sequim	W WASHINGTON ST 700 W WASHINGTON ST 700 W WASHINGTON ST 700						Overcast Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level	Daylight Fi	om opposite direction - one left turn - one straight om same direction - one right turn - one straight om same direction - both going straight - one stopped - rear-end	Overtaking and Passing Going Straight Ahead	Going Straight Ahead Making Right Turn Stopped for Traffic	East West West East West South West East West East West State East West East East East East East East East Ea
5764 City Street 5765 City Street 5766 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST 700 W WASHINGTON ST 700 W WASHINGTON ST 700	53 F E WBRACKETTRD No E800228 155 F E WBRACKETTRD No E869614	2018-05-17 15:04 Possible Injury 2 0 2 0 2018-12-06 07:58 No Apparent Injury 0 0 2 0 2019-07-10 16:56 No Apparent Injury 0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Passenger Car Passenger Car Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related At Driveway	Clear or Partly Cloudy Clear or Partly Cloudy Overcast	Dry Straight & Level Dry Wet Straight & Level	Daylight Fr Daylight Fr	om same direction - both going straight - one stopped - rear-end om same direction - both going straight - both moving - sides wipe tering at angle	Slowing Going Straight Ahead	Stopped for Traffic Going Straight Ahead Going Straight Ahead	West East Vehicle Stopped Vehicle Stopped West East West East South West East
5767 City Street 5768 City Street 5769 City Street 5770 City Street	Clallam Sequim Clallam Sequim Clallam Sequim	W WASHINGTON ST 800 W WASHINGTON ST 1095		2019-06-25 12:40 No Apparent Injury 0 0 2 0 2014-03-08 11:26 Possible Injury 1 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Driveway	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Wet Straight & Level	Daylight Fr	om same direction - both going straight - one stopped - rear-end tering at angle	Making Let Turn Going Straight Ahead Making Right Turn Making Let Turn	Going Straight Ahead Stopped for Traffic Going Straight Ahead Going Straight Ahead	East West Vehicle Stopped South Northeast West East
5769 City Street 5770 City Street	Clallam Sequim	W WASHINGTON ST 900 W WASHINGTON ST 1200	No E49943 No E884635 26200	2019-06-25 12-40 No Apparent Injury 0 0 2 0 2014-03-08 11-26 Possible Injury 1 0 2 0 2015-12-29 12-50 No Apparent Injury 0 0 2 0 2019-01-21 13-59 No Apparent Injury 0 0 2 0 2019-01-21 13-59 No Apparent Injury 0 0 2 0	Pickup Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level	Dayright	itering at angle om same direction - both going straight - both moving - rear-end om same direction - both going straight - both moving - sideswipe	Making Left Turn Going Straight Ahead Overtaking and Passing	Going Straight Ahead Slowing Going Straight Ahead	North West East West East West East West East West East West
5771 County Road 5772 County Road 5773 County Road	Cialiam Cialiam Cialiam	36200 36200	0.20 NO E/2/31 0.13 No E871157 36270 0.22 No 3616201	2019-01-21 [13:59] No Apparent Injury 0 0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	proxip, raner fruck or varietie drider 10,000 b	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Wet Curve in Sag loe Curve & Grade loe Curve in Sag	Dark-No Street Lights M Dark-No Street Lights R Daylight Vi	om same unecuon - boin going seaigin - boin moving - sides wipe albox badway Ditch	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Sung Suaight Mieau	East Vest Vest Vest Vest Vest Vest Vest Ve
5774 County Road 5775 County Road 5776 County Road 5777 County Road	Ciallam	36200 42800 43110	0.48 No 3202099 43110 0.00 No 3202098 0.02 No 1202098 No	2015-11-25 09:40 Suspected Minor Injury 1 0 1 0 2015-11-03 13:00 No Apparent Injury 0 0 2 0 2019-11-13 14:18 No Apparent Injury 0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Bus or Motor Stage	Passenger Car	Not at Intersection and Not Related At Intersection and Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level	Daylight E	shicle overturned ntering at angle ver Embankment - No Guardrail Present	Making Right Turn	Slowing	East West North West East
5777 County Road 5778 County Road	Clallam Clallam Clallam	43110 43110 43110	0.25 No E000003 1	2019-08-19 19-37 No Apparent Injury 0 0 1 0 0 1 0 0 0 1 0 0 0 1 0 0 0 0 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	o Passenger Car	At Intersection and Related At Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Level Dry Straight & Level	Daylight Fr Daylight Fr	er Embannient - No Soardan Fresenk noce om same direction - all others	Going Straight Ahead Other* Backing	Stopped in Roadway	East West West East East West
5779 County Road 5780 County Road	Cialiam Cialiam	43110 43110	94120 0.81 No E988580 94120 1.322 No E699104	2019-10-31 15:55 No Apparent Injury 0 0 2 0 0 2017-08-04 13:07 Possible Injury 1 0 2 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Level Dry Straight & Level Dry Straight & Grade		om opposite direction - one left turn - one straight om same direction - both going straight - one stopped - rear-end	Making Left Turn Starting in Traffic Lane Making Left Turn	Going Straight Ahead Stopped at Signal or Stop Sign Going Straight Ahead	North South West North Vehicle Stopped Vehicle Stopped Vehicle Stopped Vehicle Stopped Cont. South North
5782 County Road 5783 County Road	Cialiam	44120 44730	42710 2.75 No 3616213 44930 1.14 No E555135	2016-06-17 07-55 Suspected Minor Injury 2 0 2 0 2016-06-11 04-00 Unknown 0 0 1 0	Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy	Dry Straight & Level	Daylight Fr	itering at angle om same direction - one left turn - one straight Jiliy Pole	Going Straight Ahead Going Straight Ahead	Stopped in Roadway	East South North South South South North
5783 County Road 5784 County Road 5785 County Road 5786 County Road	Clallam Clallam Clallam	44930 44930	0.01 No 3616041 44730 0.57 No E622096	2016-06-11 04:00 Unknown 0 0 1 0 0 1 0 0 1 0 1 0 0 1 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway At Intersection and Related	Clear or Partly Cloudy Overcast Clear or Partly Cloudy	Dry Straight & Level Wet Straight & Level	Daylight E Dark-No Street Lights U	stering at angle sity Pole	Making Right Turn Going Straight Ahead Making Left Turn	Going Straight Ahead	South North East South Vehicle Stopped Vehicle Stopped
	Clallam Clallam Clallam	45230 45350 45350	94120 0.000 No 38151597	2014-02-07 13:59 No Apparent Injury 1 0 2 0 2014-02-07 13:59 No Apparent Injury 0 0 1 0 2019-03-01 14-28 Possible Injury 1 0 1	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Dujiigiis 11	itering at angle ood Sign Post alibox	Making Left Turn Going Straight Ahead Going Straight Ahead	Stopped at Signal or Stop Sign	East South Vehicle Stopped Vehicle Stopped West East Fast West
5788 County Road 5789 County Road 5790 County Road 5791 County Road	Clallam Clallam	45350 45520	3.22 No 3803641	2019-11-14 17:56 No Apparent Injury 0 0 1 0	0 Pickup,Panel Truck or Vanette under 10,000 lb		At Intersection and Not Related	Clear Clear or Partly Cloudy	Wet Curve & Level Dry Straight & Level	Dark-Street Lights On R Daylight Vi Daylight U	oadway Ditch hicle - Pedaloyclist ililir Pole	Going Straight Ahead Overtaking and Passing		North East West East
5791 County Road 5792 County Road 5793 County Road 5794 County Road	Cialiam Cialiam	45730 45730	95000 0.01 No 2728118 0.00 No E694733	2015-09-01 16:15 No Apparent Injury 0 0 1 0 2017-07-18 10:05 No Apparent Injury 0 0 2 0 2014-07-18 10:05 No Apparent Injury 0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Other	Not at Intersection and Not Related At Intersection and Related	Overcast Clear or Partly Cloudy	Dry	Daylight Fr	om same direction - all others	Backing Going Straight Ahead	Backing Color Stroight About	North South Vehicle Backing Vehicle Backing Vehicle Backing Vehicle Backing Vehicle Backing Vehicle Backing
5795 County Road	Cialiam Cialiam	45750 45750	95000 0.00 No E781095 0.10 No 3201988	2018-03-07 14:16 Suspected Minor Injury 5 0 2 0 2014-03-27 18:04 No Apparent Injury 0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Dayright R	ntering at angle om same direction - both going straight - one stopped - rear-end staining Wall (concrete, rock, brick, etc.)	Making Left Turn Going Straight Ahead Going Straight Ahead	Going Straight Ahead Stopped at Signal or Stop Sign	South North Vehicle Stopped Vehicle Stopped South North
5796 County Road 5797 County Road 5798 County Road 5799 County Road	Clallam Clallam	45750 45750	0.31 No E508979 1.23 No E420816	2016-01-19 18:31 No Apparent Injury 0 0 1 0 0 2015-04-28 15:15 No Apparent Injury 0 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Trush (Flather) Non-stel	Not at Intersection and Not Related Not at Intersection and Not Related	Raining Overcast	Wet Straight & Level Wet Straight at Hillor Dry Straight & Level	Dark-No Street Lights Fr Daylight V	nnce	Going Straight Ahead Going Straight Ahead	Coine Steelaha Aband	North South North South South South South
5799 County Road 5800 County Road	Clallam	45750 45750	45350 1.33 No 3202069 45350 1.33 No 3202069	2014-03-07 15:40 No Apparent Injury 2 0 2 0 2016-07-28 14:49 Possible Injury 2 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Level	Daylight E	mening at angle stering at angle stering at angle	Going Straight Ahead Going Straight Ahead	Going Straight Ahead Going Straight Ahead Going Straight Ahead	South North East West West East North South
5801 County Road 5802 County Road	Cialiam Cialiam Cialiam	45750	45350 1.33 No 3202204 45350 1.33 No 3615767	2015-05-25 14:45 Suspected Minor Injury 2 0 2 0 2017-09-02 16:00 Possible Injury 1 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Grade	Daylight E	stering at angle stering at angle	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Southeast Northwest Northeast Southwest Vehicle Backing East Vehicle Backing South
5804 County Road 5805 County Road	Cialiam Cialiam	45750 45750 45750	45350 1.33 No 3616031 45350 1.33 No 3616134 45350 1.33 No 3616144	2014-00-23 11:42 Possible injury 1 0 2 0 2014-07-27 14:10 Possible injury 4 0 2 0 2014-01-19 15:14 No Apparent Injury 0 0 2 0	Passenger Car Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy	Wet Straight & Level Dry Dry Straight & Level	Daylight E	itering at angle Itering at angle Itering at angle	Going Straight Ahead Going Straight Ahead	Going Straight Ahead	West East South North South West North South West East South North
5804 County Road 5805 County Road 5806 County Road 5807 County Road	Clallam Clallam Clallam	45750 45750	45350 1.33 No 3769387 45350 1.33 No E391718	2017-12-21 10:00 Possible Injury 1 0 2 0 2015-01-13 09:00 No Apparent Injury 0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Overcast	Dry Straight & Level Wet Straight & Level	Dayright	itering at angle	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead Going Straight Ahead Going Straight Ahead	West East South North South North West East
5808 County Road 5809 County Road 5810 County Road	Clallam Clallam Clallam	45750 45750 45750	45350 1.33 No E780537 45350 1.33 No E90592 1.33 No E90592	2017-05-06-07-07-07-07-07-07-07-07-07-07-07-07-07-	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight E Daylight E	tering at angle tering at angle	Going Straight Ahead	Going Straight Ahead Going Straight Ahead Going Straight Ahead	East West South North East West South North East West South North
5811 County Road 5812 County Road	Clallam Clallam	45750 45750	1.33 No En3ember 1.43 No 3202090 2.02 No 3202189	2014-10-08 21:30 No Apparent Injury 0 0 1 0 0 2015-10-19 10:41 No Apparent Injury 0 0 1 0	0 Passenger Car 0 Passenger Car	The second secon	Not at Intersection and Not Related Not at Intersection and Not Related	Fog or Smog or Smoke	Dry Straight & Grade Dry Straight & Level Dry Straight & Level Dry Curve & Level	Dark-No Street Lights Vi	itering at angle shicle Strikes Deer badway Ditch	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead Making Right Turn	y mgm.comi	South North South North
5812 County Road 5813 County Road 5814 County Road 5815 County Road	Cialiam Cialiam	45750 45750 46110	47530 2.25 No 3202136 47530 2.25 No E533186 1 30 No 202096	2014-06-28 06:12 Possible hjury 1 0 1 0 2016-04-08 02:10 Dead at Scene 0 1 1 0 2016-01-10 15:18 No Apparent hjury 0 0 0	Pickup Panel Truck or Vanette under 10,000 lb Motorcycle Pickup Panel Truck or Vanette under 10,000 lb) Passenger Car	At Intersection and Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Level Dry Curve & Level Dry Straight & Level	Daylight O Dark-No Street Lights O Daylight Fr	ver Embankment - No Guardrail Present ver Embankment - No Guardrail Present om same direction - both going straight - both moving - rear-end	Making Right Turn Going Straight Ahead Going Straight Ahead	Going Straight Ahead	South East South North South North North North North North North
5816 County Road	Clallam Clallam	46110 47550	1.30 No 3202096 1.50 No 3202399 45350 0.00 No E533185	2015-10-20 15:18 No Apparent Injury 0 0 2 0 0 2 0 0 0 0 0 0 0 0 0 0 0 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Motorcycle	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related At Intersection and Related	Unknown Clear or Partly Cloudy	Wet Curve & Level	Dark-No Street Lights Tr	ee or Stump (stationary)	Going Straight Ahead	Going Straight Ahead	South North South North West East South North
5817 County Road 5818 County Road 5819 County Road 5820 County Road	Clallam Clallam Clallam	47990 47990	47850 0.25 No 3202145 0.61 No 3202145	2016-06-21 14:15 Possible Injury 2 0 2 0 2 0 2 2014-03-23 13:20 Possible Injury 1 0 2 0	Bus or Motor Stage Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight E	ntering at angle tetring at angle on opposite direction - both going straight - sideswipe om same direction - one left turn - one straight	Going Straight Ahead Going Straight Ahead Going Straight Ahead Overtaking and Passing Making Let Turn	Going Straight Ahead Going Straight Ahead Making Left Turn	North South West East East West West East
5821 County Road	Clallam Clallam Clallam	47990 47990	47530 1.16 No E737687 45350 1.28 No E402078 1.44 No 3202159	2014-01-11 2029 No Apparent Insury 0 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 0 1 0	о Роскир, Paner Truck or Vanette under 10,000 lb 0 Passenger Car 0 Pickup, Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Driveway	Raining Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level			Going Straight Ahead	Making Left Turn Going Straight Ahead Making Right Turn	
5823 County Road 5824 County Road	Clallam Clallam Clallam	49200 50400	0.02 No E99886 1.38 No E87300	2019-12-28 05:00 No Apparent Injury 0 0 1 0 0 2018-12-14 01:56 No Apparent Injury 0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Clear Clear or Partly Cloudy	Wet Straight & Level	Dark-No Street Lights B	neuma untilige om same direction - one right lum - one straight er Embankmert - No Guardrail Present ulder (stational)	Going Straight Ahead Going Straight Ahead		South North East West
5825 County Road 5826 County Road 5827 County Road	Cialiam Cialiam Cialiam	50400 51590 51740	1.84 No E579173 0.56 No 2727719	2016-08-24 21:52 No Apparent Injury 0 0 1 0 2015-07-28 15:05 Possible Injury 2 0 3 0 2015-08-24 17:10 No Apparent Injury 0 0 0 2	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway Not at Intersection and Not Related At Driveway	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Level Dry Straight & Level Dry Straight & Level Dry Straight & Level	Dark-No Street Lights B Daylight Fr	sudder (stationary) om same direction - both going straight - one stopped - rear-end tering at angle	Making Left Turn Going Straight Ahead Making Left Turn	Stopped for Traffic	East South North South Vehicle Stopped South North West East
5828 County Road 5829 County Road	Clallam Clallam	51740 51740	51590 0.88 No 2727994 51590 0.88 No E347702	2018-05-07 16:30 Possible Injury 1 0 2 0 2014-07-12 21:26 No Apparent Injury 0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Dark-No Street Lights U	om same direction - both going straight - one stopped - rear-end ility Pole	Slowing Going Straight Ahead	Going Straight Ahead Stopped for Traffic	West East West East South North
5830 County Road 5831 County Road	Cialiam Cialiam	51740 51740	51590 0.88 No E358133 1.47 No 2767005	2014-09-13 11:55 No Apparent Injury 0 0 2 0 2016-10-20 14:22 No Apparent Injury 0 0 1 0 0 2017-09-16 21:20 No Apparent Injury 0 0 1 0 0 1 0	0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast Clear or Partly Cloudy	Dry Straight & Level	Daylight Fi Daylight U Dark-No Street Lights Ti	om same direction - both going straight - one stopped - rear-end	Stopped at Signal or Stop Sign Going Straight Ahead	Going Straight Ahead	East Vehicle Stopped East West West East
5832 County Road 5834 County Road	Cialiam Cialiam	51740 51890 52270	1.49 No 2727916 0.53 No 3759390 94120 0.00 No 3202377	2017-09-16 21:20 No Apparent Injury 0 0 1 0 2019-12-26 22:00 No Apparent Injury 0 0 1 0 2016-09-24 08:44 No Apparent Injury 0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb			Clear Clear or Partly Cloudy	Dry Curve & Grade	Dark-No Street Lights Li	ee or soump (stationary) near Curb om same direction - both going straight - one stopped - rear-end	Going Straight Ahead Making Left Turn Starting in Traffic Lane	Stopped at Signal or Stop Sign	South East East Southwest North South Vehicle Backing Vehicle Stopped
5833 County Road 5834 County Road 5835 County Road 5836 County Road	Cialiam Cialiam	52270 52270	94120 0.00 No 5502879 94120 0.076 No 3616148	2019-12-26 22:00 No Apparent Injury 0 0 1 0 0 0 1 0 0 1 0 0 1 0 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 0 1 0 0 0 1 0 0	0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related	Raining Clear or Partly Cloudy	Dry Straight & Level	Daylight Fi Daylight E	om same direction - both going straight - one stopped - rear-end itering at angle	Starting in Traffic Lane Going Straight Ahead Going Straight Ahead	Stopped at Signal or Stop Sign Stopped at Signal or Stop Sign	North South Vehicle Stopped Vehicle Stopped East West North South
5837 County Road 5838 County Road	Clallam Clallam	52500 52500 52500	54970 0.65 No E940698 0.94 No 2727915 0.96 No 2301696	2019-06-18 17:05 No Apparent Injury 0 0 2 0 2017-07-23 15:58 Suspected Minor Injury 1 0 1 0 2014-01-13 19:14 No Apparent Injury 0 0 1 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb		At Intersection and Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy		Daylight E. Daylight M Dark-No Street Lights W Dark-Street Lights On Tr	ntering at angle silbox	Going Straight Ahead	Slowing	East North South North South Northwest Northwest
5840 County Road 5841 County Road	Cialiam Cialiam	52500 52500 52500	0.96 No 3501990 0.96 No 3616140 1.00 No E561961	2014-09-20 20-47 No Apparent Injury 0 0 1 0 2016-07-05 18:18 Suspected Serious Injury 3 0 1 0	0 Passenger Car 0 Passenger Car 0 Passenger Car		At Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade Dry Straight at Hillor	Dark-Street Lights On Tri Daylight R	ood sign Fost see or Stump (stationary) sadway Ditch	Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East West East South North
5842 County Road 5843 County Road	Cialiam Cialiam	52500 52500	1.01 No 3759332 2.19 No 3202196	2018-05-10 12:24 No Apparent Injury 0 0 1 0 0 2015-10-23 15:12 Suspected Minor Injury 3 0 1 0	0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Overcast	Dry Straight & Grade	Daylight Fr	ence ee or Stump (stationary)			South North East West
5844 County Road 5845 County Road 5846 County Road	Cialiam Cialiam	52800 53810 53810	95250 0.00 No 3816142 54300 1.47 No E499410 64300 1.47 No F517014	2011-50-ER 1170-0 No Appaired Horse Property 0	0 Passenger Car 0 Passenger Car 0 Passenger Car	Passenger Car	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Overrast	Dry Straight & Grade Dry Straight & Level Dry Straight & Grade	Daylight Ti Daylight E Dark-No Street Lights R Dark-No Street Lights III	ntering at angle badway Ditch	Going Straight Ahead Making Left Turn Going Straight Ahead Going Straight Ahead	Making Right Turn	West South North East South North South North
5846 County Road 5847 County Road 5848 County Road 5849 County Road	Clallam Clallam Clallam	53840 53840	5330 1.20 No E87701- 53330 2.17 No E877228	2016-02-16 18:30 No Apparent Injury 0 0 1 0 2014-09-29 18:36 No Apparent Injury 0 0 2 0 2018-09-11 19:25 No Apparent Injury 0 0 2 0 2018-09-11 19:25 No Apparent Injury 0 0 0 1 0 2016-07-04 15:25 Unknown 0 0 1 0	Truck Tractor & Semi-Trailer Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Curve at Hillcrest	Daylight Fr Daylight E	ility Box om same direction - one right turn - one straight tering at angle once	Making Right Turn Making Left Turn Going Straight Ahead	Going Straight Ahead Going Straight Ahead	East North East West North East East West
5850 County Road	Ciallam Ciallam	53840 53840 53840	3.26 No 3202385	2016-04-18 14:00 No Apparent Injury 0 0 1 0	0 Other		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Grade Dry Curve & Grade Dry Straight & Level	Daylight R Daylight R Dark-No Street Lights W	ence soadway Ditch	Going Straight Ahead		East West West East West Fact
5853 County Road	Clallam Clallam Clallam	53840 53880	3.78 No 3201991 0.73 No E674417	2015-05-03 03:20 No Apparent Injury 0 0 1 0 0 1 0 2014-08-06 00:34 No Apparent Injury 0 0 1 0 0 0 1 0 0 0 17-05-03 16:09 No Apparent Injury 0 0 1 0 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Grade	Daylight R Dark-No Street Lights W Dark-No Street Lights M Daylight Fi	albox allon tree hit by vehicle (on the road)	Going Straight Ahead		East West West East
5854 County Road 5855 County Road 5856 County Road 5857 County Road	Clallam Clallam Clallam	54070 54070	0.93 No 3759365 1.34 No 375937	2017-08-05 00:06 Suspected Minor Injury 1 0 1 0 2018-11-21 21:00 No Apparent Injury 0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb)	Not at Intersection and Not Related Not at Intersection and Not Related At Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Dark-No Street Lights Vi	shicle Strikes Elik	Going Straight Ahead Going Straight Ahead		North South North South
5857 County Road 5858 County Road	Cialiam Cialiam	54300 54300	53810 0.50 No 2787059 53810 0.50 No E622106	2017-06-29 16:09 No Apparent Injury 0 0 0 1 0 1 0 0 1 1 0 0 1 1 0 0 1 1 0 0 1 1 0 0 1 1 0 0 1 1 0 0 1 1 0 0 1 0 0 1 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 0 1 0 0 0 0 1 0 0 0 0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	5	At Intersection and Related At Intersection and Related At Intersection and Related	Raining Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level	Dark-No Street Lights R Dark-No Street Lights Fi Dark-No Street Lights R Dark-No Street Lights Fi	andersy Ditch	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead		South North South North
5859 County Road 5860 County Road	Clallam Clallam	54300 54300	53810 0.50 No E961996 0.61 No 3202367	2019-09-06 14:30 No Apparent Injury 0 0 2 0 2 2017-01-09 07:56 No Apparent Injury 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 0 1 0 0 0 0 1 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Truck Tractor & Semi-Trailer	At Intersection and Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry loe Straight & Level	Daylight R	ntering at angle sadway Ditch sadway Ditch	Making Left Turn Going Straight Ahead Going Straight Ahead	Going Straight Ahead	South West West East West East
5862 County Road 5863 County Road	Cialiam Cialiam Cialiam	54300 54300 54300	0.61 NO E633223 1.00 No 3615882 1.00 No E471442	2017-01-04 14:35 Suspected winor injury 1 0 1 0 2016-05-21 19:08 No Apparent Injury 0 0 1 0 2015-10-02 20:30 Possible Injury 1 0 1 0	0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related		Dry Straight & Grade Dry Curve & Grade	Daylight Ti Dark-No Street Lights Ti	saoway unch ee or Stump (stationary) ee or Stump (stationary)	Going Straight Ahead Going Straight Ahead		west East West East East Northwest
5862 County Road 5863 County Road 5864 County Road 5865 County Road	Clallam Clallam	54300 54300	1.20 No E496049 1.36 No 3616203	2016-00-21 13-08. No Applantit injuty 0 0 1 1 0 2015-10-22 (2016) Post Silo Rijuty 1 1 0 1 1 0 2015-12-14 08:00 Unkinown 0 0 2 0 0 2 1 0 2015-12-14 08:00 Unkinown 0 0 0 1 0 0 2016-01-01 18:30 No Apparent Injuty 0 0 1 1 0 2019-04-12 12:37 No Apparent Injuty 0 0 1 1 0 2019-04-12 12:37 No Apparent Injuty 1 0 0 1 0 1 0 1 0 1 0 1 0 1 0 1 1 1 0 1 1 0 1 1 1 0 1 1 1 0 1 1 1 0 1 1 1 0 1 1 1 0 1 1 1 1 0 1 1 1 1 0 1 1 1 1 0 1 1 1 1 1 0 1 1 1 1 1 1 1 1 1 1 1	0 Truck (Flatbad, Van, etc) 0 Pickup, Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	loe Straight & Level	Daylight Fi Dark-No Street Lights Fi	om same direction - all others ence	Backing Going Straight Ahead	Stopped for Traffic	Vehicle Backing Vehicle Backing East West East West
5868 County Road	Ciallam Ciallam	54300 54300 64300	1.41 No 3202170 1.50 No 3759322 1.62 No F96084	2019-04-12 12:37 No Apparent Injury 0 0 1 0 2018-09-02 02:00 Suspected Minor Injury 1 0 1 0 2019-09-08 12:52 No Apparent Injury 0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Truck (Flathad Van etc))	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy		Daylight R Dark-No Street Lights Ti Daylight R		Going Straight Ahead Going Straight Ahead Going Straight Ahead		East West East West West West
5869 County Road 5870 County Road	Clallam Clallam	54300 54800				Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Clear or Partly Cloudy Fog or Smog or Smoke	Dry Straight & Level	Dusk Fr	om same direction - boil going straight - boil moving - real-end	Going Straight Ahead Backing	Going Straight Ahead Stopped for Traffic	West East West East Vehicle Backing Vehicle Backing Vehicle Backing Vehicle Stopped
5871 County Road 5872 County Road 5873 County Road	Cialiam Cialiam Cialiam Cialiam Cialiam	54800 55740 56830	95250 0.00 No E593001 55740 0.67 No E938001 66970 0.19 No 256538	2014-01-15 52-24 No Apparent injury 0 0 2 0 2016-10-07 16-47 No Apparent injury 0 0 2 0 2016-10-07 16-47 No Apparent injury 0 0 2 0 2014-08-09 13-36 Possible injury 1 0 1 0 2014-08-09 13-36 Possible injury 4 0 2 0 2014-08-09 13-36 Possible injury 4 0 2 0 2019-07-21 11-43 Possible injury 1 0 1 0 2019-07-21 11-43 Possible injury 1 0 1 0 1 0 2019-07-21 11-43 Possible injury 1 0 1 0 1 0 2019-07-21 11-43 Possible injury 1 0 1 0 1 0 2019-07-21 11-43 Possible injury 1 0 1 0 1 0 2019-07-21 11-43 Possible injury 1 0 1 0 1 0 1 0 1 0 1 0 1 0 1 0 1 0 1 0 1 0 1 0 1 0 1 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup Panel Truck or Vanette under 10 000 lb	At Intersection and Related Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight R	om same direction - all others staining Wall (concrete, rock, brick, etc.) om same direction - both going straight - one stopped - rear-end	Backing Going Straight Ahead Making Left Turn	Stopped in Roadway Making Left Turn	West Vehicle Backing Vehicle Stopped Vehicle Stopped South North Fast South
5874 County Road 5875 County Road	Cialiam	56830 56910	0.05 No E944132	2014-05-10 11:10 No Apparent Injury 0 0 2 0 2019-07-21 11:43 Possible Injury 1 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Motorcycle	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Level Dry Curve & Level			Making Left Turn Making Right Turn Going Straight Ahead	Going Straight Ahead	East West South North East West
5876 County Road 5877 County Road	Clallam Clallam Clallam	57140 57140	0.35 No 3202064 1.32 No 3799333	2019-07-21 11-33 Possible Injury 1 0 1 0 2014-02-09 05:00 Possible Injury 1 0 0 1 0 0 10-00 2014-02-09 05:00 Possible Injury 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight O	er Embankment - No Guardrail Present ee or Shum (stationary) ee or Shum (stationary) ee or Shum (stationary) ee or self-en shu	Going Straight Ahead Making U-Turn Going Straight Ahead	Going Straight Ahead Going Straight Ahead	Northeast Southwest West East
5879 County Road 5880 County Road	Clallam	57140 57250	337 No F791440 0.44 No E830138	2018-04-23 13:21 No Apparent Injury 0 0 2 0 2018-08-15 17:00 No Apparent Injury 0 0 1 0	Passenger Car Passenger Car	Other	Not at Intersection and Not Related	Clear or Partly Cloudy Fog or Smog or Smoke				Overtaking and Passing	Going Straight Ahead	East West East West South North
5879 County Road 5880 County Road 5881 County Road 5882 County Road 5882 County Road	Clallam Clallam Clallam Clallam	57250 57250 57250	1.20 No 2767012 2.10 No 3201750 2.20 No Espain	2018-04-23 153-21 No Apparent Injury 0 0 2 0 2018-04-23 153-21 No Apparent Injury 0 0 1 0 2018-06-15 17:700 No Apparent Injury 0 0 1 0 2018-06-15 17:700 No Apparent Injury 0 0 1 0 2018-06-15 17:00 No Apparent Injury 0 0 1 0 2018-06-15 17:00 No Apparent Injury 0 0 1 0 2018-06-15 17:00 No Apparent Injury 0 0 1 0	0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Overcast	Wet Curve & Grade Wet Curve at Hillcrest	Dark-No Street Lights Vi Daylight O Dark-No Street Lights O	ailbox ilbox thickerturned ter Embankment - No Guardrail Present back Phincher	Going Straight Ahead Going Straight Ahead Going Straight Ahead Making Left Turn		South North South North South North
3882 Courty Road 3884 Courty Road 3884 Courty Road 3886 Courty Road 3886 Courty Road 3886 Courty Road 5887 Courty Road 5887 Courty Road 5889 Courty Road 5889 Courty Road 5899 Courty Road 5899 Courty Road 5899 Courty Road	Clallam Clallam Clallam	58250 58250	2.29 No E882883 0.01 No 3799386 0.06 No E674415	2014-02-15 14:30 No Apparent Injury 0 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Wet Straight & Grade	Daylight Fi	one collects one same direction - all others of Bank or Ledge	Making Left Turn Making U-Turn Going Straight Ahead Going Straight Ahead	Going Straight Ahead	North South South North South North
5886 County Road 5887 County Road	Clallam	58250 58250 68260	0.30 No 3202066 0.30 No E502768	2014-02-20 07:38 Possible hjury 2 0 1 0 2016-01-02 14:32 No Apparent Injury 0 0 1 0 2016-01-03 14:30 No Apparent Injury 0 0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Dickup Panel Touch as Manager 1 4 4 4 4 4 4	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Ice Straight & Grade Ice Straight & Grade	Dawn R Daylight Ti	ee or Stump (stationary)	Going Straight Anead	Going Straight About	North South South South North South North South North South
5889 County Road 5890 County Road	Clallam Clallam Clallam	58250 94120	1.01 No 2727997 1.01 No 2979011 141020 0.34 No 3615715	2014-04-29 19-49 No Apparent Injury 0 0 2 0 0 0 0 1 0 0 0 1 0 0 0 0 0 0 0 0	Passenger Car Pickup,Panel Truck or Vanette under 10.000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Truck (Flatbad,Van,etc)	Not at Intersection and Not Related Not at Intersection and Not Related At Intersection and Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Wet Curve & Grade Dry Curve & Grade Dry Straight & Level	Daylight Fi Daylight E Daylight Fi	om opposite direction - all others arth Bank or Ledge om same direction - one left turn - one straight	Going Straight Ahead Going Straight Ahead Making Left Turn	Going Straight Ahead Going Straight Ahead	South North South North South North South North
5891 County Road 5892 County Road	Clallam	94120 94120	0.50 No 2727914 0.50 No 3615713	2017-05-28 16:10 Possible Injury 1 0 3 0 2014-08-11 14:40 Possible Injury 1 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight E Daylight U	itering at angle	Making Left Turn Going Straight Ahead Going Straight Ahead	Going Straight Ahead Going Straight Ahead	Vehicle Stopped North East West East West
	Clallam Clallam Clallam	94120 94120 94120	0.70 No 3615751 0.71 No E447280 41030 0.80 No 3202387	2011-12-29 21-30 No Apparent Injury 0 0 0 1 0 0 1 0 0 1 1 0 0 1 1 0 1 0 1	u Pickup,Panel Truck or Vanette under 10,000 lb 0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Passenger Car	Pickup Panel Truck or Vanette under 10 000 is	Not at Intersection and Not Related Not at Intersection and Not Related At Intersection and Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Grade Dry Straight & Level	Dark-No Street Lights G Daylight U Daylight Fr	uardrail - Face ility Pole om same direction - all others	Going Straight Ahead Going Straight Ahead Making U-Turn	Going Straight Ahead	reormeast Southwest East West East South East West
5896 County Road 5896 County Road 5897 County Road 5898 County Road 5899 County Road	Cialiam Cialiam Cialiam	94120 94120	9.030 No E92559 0.99 No E92559 0.90 No 3759374	2016-06-19 13-00 Suspected More Huny 4 0 2 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	D OUD, D I D O O O O O O O O O O O O O O O O O	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Grade	Daylight Vi	shicle Strikes Deer	Going Straight Ahead		South North West East
5898 County Road 5899 County Road	Clallam	94120 94120	1.00 No 3202286 1.00 No 3616230	2017-01-15 19:40 Suspected Minor Injury 1 0 1 0 2016-12-06 11:02 No Apparent Injury 0 0 2 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Ice Straight & Grade Snow/Slush Straight & Grade	Dark-Street Lights On R Daylight Fr	ility Pole addway Dirch om opposite direction - all others	Going Straight Ahead Going Straight Ahead	Going Straight Ahead	East West West East
5901 County Road 5901 County Road 5902 County Road	Clallam Clallam Clallam	94120 94120	1.00 No 3616231 1.13 No E800473 1.20 No E737688	2016-05-16 15:27 Suspected Minor Injury 0 0 2 0 0 2018-05-16 15:27 Suspected Minor Injury 2 0 2 0 2017-11-17 07:04 No Apparent Injury 0 0 1 1 0	о Ріскир, Panei Truck or Vanette under 10,000 lb 0 Pickup, Panel Truck or Vanette under 10,000 lb 0 Pickup, Panel Truck or Vanette under 10 nnn lh	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Grade Ice Straight & Level	Daylight Fr Daylight Fr Dawn E	om opposite direction - all others om same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead	Stopped for Traffic Slowing	
5903 County Road 5904 County Road	Cialiam	94120 94120	1.25 No 277906 1.25 No E62293	2015-11-27 06:50 Suspected Serious Injury 1 0 1 0 2016-12-11 03:09 No Apparent Injury 0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Raining	loe Straight & Level	Dark-No Street Lights G	urft Bank or Ledge oncrete Barrier/Jerse y Barrier - Face uardrall - Face	Going Straight Ahead Going Straight Ahead Going Straight Ahead		East West West East
5905 County Road 5906 County Road 5907 County Road	Clallam Clallam Clallam	94120 94120 94120	1.26 No E995433 1.28 No E298121 1.30 No E299222	2019-12-11 (09:24 No Apparent Injury 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 0 1 0 0 0 1 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Overcast Clear or Partly Cloudy	Dry Straight & Grade Ice Straight & Level	Daylight G Daylight G Daylight	uardrail - Leading End uardrail - Face idge Rail - Face	Going Straight Ahead Going Straight Ahead Going Straight Ahead		East West East West West West
5908 County Road 5909 County Road 5910 County Road 5911 County Road	Clallam Clallam Clallam Clallam Clallam	94120 94120	41750 1.59 No 3616157 1.94 No 3616003	2015-02-13 09-23 Suspected Minor Injury 2 0 3 0 0 2016-07-24 19:58 No Apparent Injury 0 0 1 0	Pickup Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related Not at Intersection and Not Related	Clear or Partly Cloudy	loe Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Fi Daylight Vi	om same direction - both going straight - one stopped - rear-end	Making Left Turn	Making Left Turn	East West East West East West
5910 County Road 5911 County Road		94120 94120	2.03 No 2978806 2.69 No 3047269	2014-09-30 19:05 No Apparent Injury 0 0 1 0 0 2015-10-14 07:37 No Apparent Injury 0 0 1 0 0 0 1 0 0 0 1 0 0 0 0 0 0 0 0	Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car	Natura Densil Trust a 25	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Dusk Vi Daylight Vi	shicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Coins Stanishs ** · · ·	West East East West
5913 County Road	Clallam Clallam Clallam	94120 94120	2.76 No 3202060 2.85 No 3615972 43110 3.50 No 3615702	2015-05-13 09:22 September Florer 2 2 0 3 2 0 15 05:05 09:05	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Driveway Related but Not at Driveway At Intersection and Related	Overcast Overcast Overcast	Wet Straight & Level Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Fr	om opposite direction - all others om same direction - all others om same direction - one left turn - one straight	Making U-Turn Making U-Turn	Going Straight Ahead Going Straight Ahead Overtaking and Passing	West West East West West North West East East West East
5915 County Road 5916 County Road	Clallam	94120 94120	43110 3.50 No 3615973 43110 3.50 No E748255	2014-08-01 12:30 No Apparent Injury 0 0 2 0 2017-12-16 11:26 Possible Injury 2 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight E	tering at angle tering at angle	Making Left Turn Going Straight Ahead Going Straight Ahead	Going Straight Ahead	East West South East South North East West
5917 County Road 5918 County Road 5919 County Road	Ciallam Ciallam Ciallam Ciallam	94120 94120 94120	43110 3.50 No E821426 43110 3.50 No E910474 43110 3.50 No E968523	2018-07-10 12:16 Possible Injury 1 0 2 0 2019-03-27 07:52 Possible Injury 1 0 2 0 2019-08-16 08:38 Possible Injury 2 0 2	Pickup, Panel Truck or Vanette under 10,000 lb Truck (Flatbad, Van,etc) Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry	Daylight Fr	ntering at angle om opposite direction - one left turn - one straight tering at angle	Going Straight Ahead Making Left Turn	Going Straight Ahead Going Straight Ahead	South North West East East South West East South North West East
5920 County Road 5921 County Road	Clallam	94120 94120	43110 3.50 No E9882234 3.50 No 3202234 3.90 No E997864	2019/07/10 12-16 19-20-16-10-10-10-10-10-10-10-10-10-10-10-10-10-	Passenger Car Truck (Flatbad, Van, etc)	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear	Dry Straight & Level loe Straight & Level		ntering at angle me direction - both going straight - one stopped - rear-end her Objects	Going Straight Ahead Stopped at Signal or Stop Sign Going Straight Ahead	Going Straight Ahead Going Straight Ahead	West East West East East West
5922 County Road 5923 County Road 5924 County Pend	Clallam Clallam	94120 94120 94120	3.91 No 2767149 4.00 No 3202356 6.06 No F622112	2019-12-15 1007 Possible Injury 1 0 1 0 0 1 0 0 1 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 0 1 0	Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Pickup Panel Truck or Vanette under 40,000 lb	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Grade	Daylight Fr Daylight R	om same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic	East West Vehicle Backing Vehicle Stopped East West West Vehicle Backing Vehicle Stopped
5925 County Road 5926 County Road	Clallam Clallam Clallam	94120 94120	4.06 No E622112 4.11 No E98142 4.16 No E913233	2019-12-15 10:35 No Apparent Injury 0 0 1 0 0 2019-12-15 10:35 No Apparent Injury 0 0 1 0 0 1 0 0 0 1 0 0 0 0 0 0 0 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear	loe Straight in Sag Dry Straight in Sag	Daylight R	arth Bank or Ledge oadway Dritch ood Sign Post	Going Straight Ahead		West East West East
5925 County Road 5926 County Road 5927 County Road 5928 County Road	Clallam Clallam Clallam	94120 94120	4.24 No E551253	2016-05-25 17:40 Possible Injury 2 0 2 0	0 Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Raining	Dry Straight in Sag Dry Straight & Level Wet Straight & Level	Daylight Fr Daylight Fr	ood Sign Post om oppositie direction - one left turn - one straight om same direction - one left turn - one straight	Going Straight Ahead Making Left Turn Going Straight Ahead	Going Straight Ahead Making Left Turn	East South West East East West East
5929 County Road 5930 County Road		94120 94120	4.39 No E653532 4.46 No 3616143	2017-02-25 07:17 Suspected Minor Injury 1 0 1 0 2014-10-07 14:54 No Apparent Injury 0 0 2 0 0 2 0 0 2 1 0 2	Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Bickup Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Ice Straight & Level	Daylight Fr	ence om same direction - one left turn - one straight	Overtaking and Passing		West East West North
5932 County Road 5933 County Road	Clallam	94120 94120 94120	45230 4.50 No 3816000 45230 4.50 No 3616048 45230 4.50 No E830587	2018-08-17 08:24 Suspected Mnor Injury 2 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level		ntering at angle om same direction - both going straight - one stopped - rear-end om same direction - both going straight - one stopped - rear-end	Going Straight Ahead Making Left Turn Going Straight Ahead Going Straight Ahead	Making Left Turn Making Left Turn Stopped for Traffic Stopped for Traffic	South West East South East West East West East West Vehicle Stopped Vehicle Stopped
992-0 County Road 592-0 County Road 5930 County Road 5930 County Road 5931 County Road 5932 County Road 5932 County Road 5934 County Road 5934 County Road 5935 County Road 5936 County Road	Clallam Clallam	94120 94120	4.70 No E610031 4.73 No 2767051	2015-04-10 (1655) Peacible hypuy 1 0 0 2 0 0 2015-04-10 (1655) Peacible hypuy 1 0 0 1 0 2 0 0 2014-10-20 (1655) Peacible hypuy 1 0 0 0 2 0 0 2014-10-20 (1455) Peacible hypuy 0 0 0 2 0 0 0 2 0 0 0 0 0 0 0 0 0 0 0	Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related At Driversery	Raining Clear or Partly Cloudy	Wet Straight & Level Wet Straight & Level	Dark-No Street Lights R Dark-No Street Lights D	oadway Dinch omestic animal (horse, cow, sheep, etc) om same direction - one right sum - one straight	Making Left Turn Going Straight Ahead Going Straight Ahead	Stopped in Roadway Making Right Turn	Vehicle Backing East Vehicle Stopped East West East North East West East North
5936 County Road 5937 County Road 5938 County Road	Clallam Clallam Clallam	94120 94120	4.75 No 3799320 4.80 No 3800264 4.92 No 3202279	2019-08-11 03:00 Suspected Minor Injury 1 0 1 0 2014-02-11 02:10 No Apparent Injury 0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	O O O O O O O O O O O O O O O O O O O	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast	Dry Straight & Level loe Straight & Level	Dark-No Street Lights Fit Dark-No Street Lights R	ence padway Ditch	Going Straight Ahead Going Straight Ahead	g ogn tun	West East East West
								-		-				

CITID CIRCUMSTANCE_1_UNIT_1 CIRCUMSTANCE_2_UNIT_1 CIRCUMS	VER_CONTRIBUTING MV_DRIVER MV_DRIVER AGE GENDER UNIT_1 UNIT_1		MV_DRIVER MGE_UNIT_2	DRIVER BICYCLIST_CONTRIBUTING CIRCUMSTANCE_1_UNT_1 BICYCLIST_CONTRIBUTING CIRCUMSTANCE_2_UNT_1	BICYCLIST_CONTRIBUTING CIRCUMSTANCE_3_UNIT_1 BICYCLIST AGE UNIT_1	T BICYCLIST BICYCLIST_CONTRIBUTING CIRCUMSTANCE_1_UNIT_2 BICYCLIST_CONTRIBUTING CIRCUMSTANCE_2_UNIT_2	BICYCLIST_CONTRIBUTING BICYCLIS CIRCUMSTANCE_3_UNIT_2 AGE_UNIT	T BICYCLIST PEDESTRIAN_CONTRIBUTING CIRCUMSTANCE_1_UNT_2 CIRCUMSTANCE_1_UNT_2	
59 Ond Not Clean RVV to Vehicle 50 Divine No Districted 50 Divine No D	44 Male 78 Male	Driver Not Distracted Driver Not Distracted Driver Not Distracted Driver Not Distracted	57 Fe 66 Ma 23 Ma	aale					Lane of Primary Traficowry Lane of Primary Traficowry Lane of Primary Traficowry Lane of Primary Traficowry
62 Did Not Grant RW to Vehicle 63 Other Driver Distractions Inside Vehicle 64 Distractions Outside Vehicle	72 Female 79 Male 60 Female 69 Male 68 Male 68 Male	None None None	47 Ma 59 Fe 37 Ma 32 Fe	ale					Lane of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway
			29 Fe	nale					Lane of Primary Trafficowy
66 Inattention Did Not Grant RW to Venicle 69 Inattention 70 Follow Too Closely	16 Female 74 Male 42 Male	None None None	31 Fe 31 Ma 50 Ma	sale					Lane of Primary Traficovay Lane of Primary Traficovay Lane of Primary Traficovay
	ng 43 Male 33 Female	None	28 Ma						Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
73	27 Male 17 Male 72 Female 90 Female	None	19 Ma	2					Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
78 Under Influence of Alcohol None	59 51 Female 17 Male	None	87 Fe 58 Ma						Lane of Primary Trafficusty Prast the Outside Shoulder of Primary Trafficusty Oner Location (ChyCounsyMas. Chafforway) Lane of Primary Trafficusty Lane of Primary Trafficusty Lane of Primary Trafficusty
79	59 59 59 59 59 59 59 59	None None	63 Ma 79 Ma 31 Ma	2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2					Jone of Dinney Traffoowy Lane of Primay Traffoowy
Decision	39 Female 63 Male	Driver Not Distracted	21 Ma						Past the Outside Shoulder of Primary Trafficway
86 Inattention		Driver Not Distracted None	49 Fe	sale					Lane of Primary Trafficway Past the Outside Snoulder of Primary Trafficway Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
### Operating #### ### #### #### #### #### #### #### #### ##### ##### ######	81 Male 29 Male 38 Male					Failing to Signal	48	Male	Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway
	58 Male 34 Male 54 Female	Improser Backing None Acros	34 Ma 83 Fe	e sale					Lane of Primary Trafficowy
193 Disregard Stop Sign - Flashing Red Unknown Distraction	76 Female 27 Male 22 Male	None	30 Fe	sale					Past the Outside Shoulder of Primary Trafficway
98 Disregard Stop Sign - Flashing Red Inattention	30 Female 66 Male 53 Male	None Operating Delective Equipment Other Contributing Circ Not Listed	25 Ma 45 Fe	b sale					Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway
99	68 Male 32 Male 33 Male	None Operating Defective Equipment Other Contributing Circ Not Listed None Obsespand Step and Go Light Institute Obsespand Step and Go Light Insti	59 Ma 40 Fe 57 Ma 22 Ma						Lane of Primary Trafficway
03 hattention 04 hattention 55 Discreard Stop Stop - Flashing Red leastertion	29 Male 50 Male 61 Female	Driver Not Distracted Driver Not Distracted Driver Not Distracted Driver Not Distracted	42 Fe	nale					Lane of Primary Trafficway
06 Disregard Stop Sign - Flashing Red	18 Female 50 Male	Ower Not Distracted Ower Not Distracted Other Contributing Circ Not Listed Other Contributing Circ Not Listed Other Contributing Circ Not Listed	66 Ma 61 Ma 71 Ma	3 3					Lane of Primary Trafficusy
77 None Disregard Stop Sign - Flashing Red Inattention	46 Male 19 Male 69 Female	Dimer Vol De tracked Nome Nome Nome	39 Fe 66 Ma 57 Ma						Lane of Primary Trafficusty
11 None 12 Apparently Asleep or Fatiqued 13 Under Influence of Alcohol Exceeding Stated Speed Limit Inattention	70 Female 82 Male 30 Male 47 Male 28 Female								Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
12 Apparently Acteop or Fatigued 13 Under influence of Actobal Exceeding Stated Speed Limit Institution of Actobal Over Center Line 14 Under Influence of Actobal Over Center Line 15 Follow 10c Closely 16 Questing Fatigued			53 Ma	3					Past the Outside Shoulder of Primary Trafficovay Lane of Primary Trafficovay Past the Outside Shoulder of Primary Trafficovay
17 Disregard Stop Sign - Flashing Red Did Not Grant RW to Vehicle Distractions Outs 18 Disregard Stop Sign - Flashing Red Hashing Red Hashing Red Hashing Red 19 Apparently Liellegor Faligued Over Center Line Hashing Red 20 Improper Passing Hisproper Passing	52 Female 73 Male 88 Male 35 Female	None	54 Ma 92 Ma 66 Fe	a ale					Lane of Primary Trafficway
20	35 Female 80 Female 40 Male	None None Orientated	43 Fe 33 Fe 57 Fe	nale					Lane of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
23 Inattention Exceeding Reas. Safe Speed 24 Distractions Outside Vehicle 25 Inattention	24 Male 21 Male 13 Female								Past the Outside Shoulder of Primary Trafficway
27 Did Not Grant RW to Vehicle 28 Under Influence of Alcohol Eating or Drinking	13 Perinate 13 Perinate 14 Perinate 15 Marie 15 Marie 16 Marie 17 Marie 18 Marie 19 Marie 19 Marie 19 Marie 19 Marie 19 Marie	None None	76 Ma 35 Fe 67 Fe	sale sale					Lane of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway
29	36 Male 56 Male 18 Male	Ination	40 Ma						Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
32 hattention 33 Other Contributing Circ Not Listed 4 hattention	19 Male 18 Male 50 Female	None	66 Ma	3					Outside Shoulder of Primary Trafficway
22	50 Female 63 Female 25 Female 20 Male	None None None	88 Fe 79 Fe 27 Ma	cale					Lane of Primary Traflicway Lane of Primary Traflicway Intersecting Traflicway Intersecting Traflicway
38 Exceeding Reas. Safe Speed Other Contributing Circ Not Listed 39 Apparently III Apparently III 40 Under Influence of Alcohol Exceeding Stated Speed Limit	20 Male 37 Female 62 Female 58 Female 18 Female								Inters ecting Trafficway Past the Outside Shoulder of Primary Trafficway
42 Inattention Operating Other Electronic Devices (comp									Past the Outside Shoulder of Primary Trafficway
45 Under Influence of Alcohol Disregard Stop Sign - Flashing Red	92 Female 55 Female	Driver Not Distracted	65 Fe						Past the Outside Shouldear of Primary Trafficway Lant of Primary Trafficway Past the Outside Shouldear of Primary Trafficway Past the Outside Shouldear of Primary Trafficway Past the Outside Shouldear of Primary Trafficway
45 Inattention	80 Male 65 Male 35 Male	None None	70 Ma 71						Lane of Primary Tatiloway Intersecting Trafficway Past the Outside Shoulder of Primary Trafficway
50 Inattention 51 Exceeding Reas. Safe Speed Improper Turn/Merge 52 Other Combination Circumstance	78 Male								Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway
Second Continues	20 Female 79 Female 28 Male								
55 Disregard Stop Sign - Flashing Red Apparently Asleep or Fatigued Other Driver Distri	20 Male 62 Female tractions Inside Vehicle 54 Female 63 Male								Past the Outside Shoulder of Primary Trafficway
59 Disregard Stop Sign - Flashing Red 60 None 61 Care State	64 Male		61 Ma						Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Dart the Outside Shoulder of Primary Trafficway
37 Disregard Sop Sign Rashing Red Apparently fellesp or Fatigued Other Diner District		Driver Not Distracted	43 Mb						Line of Philimsy Traditionary Past the Outside Shoulder of Phinary Traditionary Past the Outside Shoulder of Phinary Traditionary Past the Outside Shoulder of Phinary Traditionary Past Location Calcinomithes. Traditionary Past Location Calcinomithes. Traditionary Land Christian Traditionary Past to Outside Shoulder of Phinary Traditionary Past to Outside Shoulder of Phinary Traditionary
Exceeding Reas: Safe Speed Instention Driver Interacting with Passengers, Anim Driver Contributing Circ Not Listed	47 Male 19 Female								Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
88 None 99 Other Contributing Circ Not Listed Other Contributing Circ Not Listed Improper Backing	24 Male 48 Male 32 Male 72 Male	None None	84 Ma	3					Past the Outside Shoulder of Finner Findleway Lane of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway
71 Improper Backing 1 Improper Backing 1 Improper Backing 2 Unknown Distraction	47 Maie 47 Maie 43 Maie 29 Maie 19 Maie	None None	76 Ma						Lame of Primary Traffoway Past the Outside Shoulder of Primary Trafficway Lane of Primary Traffoway Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
74 Exceeding Stated Speed Limit Exceeding Reas. Safe Speed 75 Exceeding Stated Speed Limit Unknown Distraction	19 Male 40 Male	None	3, 10	CASTAC CONTROL					Past the Outside Shoulder of Primary Trafficway
90	57 Female 66 Male	Penne Marchitestad Diner Net Chistosted Diner Net Chistosted None None	73 Ma 57 Ma						Past the Outside Shoulder of Primary Trafficovay Lane of Primary Trafficovay Lane of Primary Trafficovay
/9 instensor improper Passing 80 Apparently Fatigued 81 hattention	20 Female 59 Female 21 Female 69 Male	None	54 MB	5					Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
82 Apparently Fatigued 33 None 84 Did Not Grant RW to Vehicle Improper U-Turn	69 Male 59 Male 35 Female 52 Male 18 Female	None .	86 Ma	3					Other Location (City/County/Misc, Trafficway)
85 Inattention 86 Orient Not Distracted 87 Under Influence of Alcohol Exceeding Reas. Safe Speed	52 Male 18 Female 40 Female 17 Female								Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
88 Exceeding Stated Speed Limit 89 Exceeding Stated Speed Limit Distractions Outside Vehicle 90 None Driver Not Distracted	17 Female 18 Female 43 Male 31 Male	None	66 Fe	ale					Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway
91 Did Not Grant RW to Vehicle 92 Apparently ke leep or Fatigued 93 Other Contributing Gire Not Listed Unknown Distraction	31 Male 84 Male 30 Male	None	52 Ma	,					
94 Apparently Fatiqued 95 instention Improper U-Turn 96 Driver Not Distracted	3 a) debte 3 b) Marie 5 b) Marie 6 b) Male 6 b) Male 2 b) Male 2 b) Male 2 b) Male 7 p) Male 5 f) Male 6 f) Male 5 f) Male 6 f) Male 6 f) Male 6 f) Male 7 g) Male 6 f) Male 7 g) Male 8 g	None .	66 Ma						Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway
97 Exceding Reas. Safe Speed	33 Male 34 Male 79 Male	None	41 Ma	,					Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway
Other Contributing Circ Not Listed	53 Female 54 Male 31 Male	None None	55 Fe 58 Ma	sale					Outside Shoulder of Primary Trafficway Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
03 Exceeding Reas. Safe Speed 04 Other Contributing Circ Not Listed 05 Apparently Jesleep or Fatigued	42 Male 32 Male 76 Male								Outside Shoulder of Primary Trafficway
96 Other Contributing Circ Not Listed 97 Other Contributing Circ Not Listed 98 Driver Not Distracted 90 Driver Not Distracted	30 Female 16 Male		59 Ma						Outside Shoulder of Primary Trafficway Outside Shoulder of Primary Trafficway Outside Shoulder of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway
09 None 10 None 111 None	27 Female 70 Female	Oriner Not Distracted	59 Ma						Care of Primary Tatilioway Lane of Primary Tatilioway
11 None 12 Did Not Grant RW to Vehicle Improper U-Turn Inattention 13 Improper U-Turn Did Not Grant RW to Vehicle 14 Improper LTumMerres	62 Male	None	22 Ma 20 Ma 23 Ma	,					Lane of Primary Trafficway Lane of Primary Trafficway
15 Instantion Instanti	16 Female 56 Female	Other Contributing Circ Not Listed Instantion None	23 Ma 60 Fe 60 Fe	sale sale					Lane of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway
B Did Not Grant RW to Vehicle 19 Did Not Grant RW to Vehicle 19 Did Not Grant RW to Vehicle 20 Notes	43 Male 71 71 72 72 72 72 72 72 72 72 72 72 72 72 72	None Driver NO Distracted Driver NO Distracted	46 Fe 61 Ma 54 Fe	sale					Lane of Primary Trafficway
22 Other Driver Distractions Inside Vehicle	28 Male 76 Female 17 Female	None	41 Ma 36 Fe	rale					Past the Outside Shoulder of Primary Trafficway
13	20 Note: 70 Female 17 Female 17 Female 17 Female 19 Female 19 Male 33 Male 65 Male 33 Female 25 Male 27 Female								Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
26 Other Contributing Circ Not Listed Other Driver Distractions Inside Vehicle 27 Other Contributing Circ Not Listed 28 Follow Too Closely	65 Male 33 Female 26 Male	None	39 Fe 17 Fe						Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway
28		Dimer Not Distracted None None	49 Ma 65 Fe	sale					Past the Obtained Shoulder of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway
32 1-follow Too Closely 33 Follow Too Closely 34 Other Contributing Circ Not Listed	79 Female 20 Male 25 Male	None	75 Ma 57 Ma 45 Ma	1					Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway
35 None Screeding Reas. Safe Speed Unknown Distraction 27 Apparently Asleep or Fatigued	20 Male 25 Male 29 Male 20 Female 19 Male		42 Ma						Lane of Primary Trafficway Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
38 Other Contributing Circ Not Listed	19 Male 56 Male					1 1	4		Past the Outside Shoulder of Primary Trafficway

OBJECTID WA_STATE_PLANE SOUTH_Y 2010_FORWARD COX			E IMPAIRED	COL_AGE COL COL_LIGHT SURFACE CONDITIONS CONDITIONS	COL CO GEOMI	IEAT IEAT		COL_LIGHT COL_SUR CONDITIONS CONDITION Decoded Decode	FACE COL_LOCATION Decoded	COL_RISKY Intersection BEHAVIOR Area_Name Area
5760 1004122.18 1012127.5 2019 5761 1000064.22 1012273.22 2018	5 2 Vehicles-Paries Angle 9 2 Vehicles-Paries Raus-End 8 2 Vehicles-Paries Tuming 9 2 Vehicles-Paries Tuming 9 2 Vehicles-Paries Rear-End	Property Damage Only Other/Unknown 65+ Property Damage Only Fail to Yield 65+ Property Damage Only Distracted Driving 65+	No	Wet Daylight Dry Daylight Dry Daylight Dry Daylight Wet Daylight Dry OtherUnknown	At Driveway or Intersection Straight & L At Driveway or Intersection Straight & L Intersection Related but Not at Intersection Straight & L	Level No Vuincrable Road User Involved Level Older Road User 65+ Level Older Road User 65+ Level Older Road User 65+	Other/Unknown Other/Unknown Fail to Yield Distracted Driving			Fail to Yield
5762 1000076.57 101227.33.2011 5763 101227.41 101227.33.2011 5764 99983.02 101226.487 20101.72 5766 99983.08 101227.44 2011.22 2	8 2 Vehindes/Parties Rear-End 2 Vehindes/Parties Sidoswipe 9 2 Vehindes/Parties Angle 9 2 Vehindes/Parties Angle 4 2 Vehindes/Parties Angle	Minor or Possible Injury Inattention Unde	No	Dry Daylight Dry Daylight Wet Daylight Dry Daylight Dry Daylight Dry Daylight	At Driveway or Intersection Straight & L	Older Road User 65+	Fall to Yield Inattention Inattention	Daylight Dry Daylight Dry Daylight Wet Daylight Dry Daylight Dry Daylight Dry	merseculori Related but vos a menseculori Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout At Driveway or Intersection Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Distracted Driving
5771 932115.78 1024903.02 2017	4 2 Websides Partes Angle	Property Damage Only Inattention 65+ Property Damage Only Follow Too Closely 35-4	No	Dry Daylight Wet Daylight Dry Daylight Wet Daylight Use Daylight Wet Daylight Lec.RonowSlusch Davin-No Street Lights /Street Lights Off Loc.Snow/Slusch Davin-No Street Lights /Street Lights Off	At Driveway or Intersection Straight & L Not at Intersection/Driveway/Roundabout Straight & L Not at Intersection/Driveway/Roundabout Curve in Sa Not at Intersection/Driveway/Roundabout Curve & Gri	Level No Vulnerable Road User Involved ag No Vulnerable Road User Involved ade No Vulnerable Road User Involved	Inattention Follow Too Closely Inattention	Dark-No Street Lights/Street Lights Off Ice/Snow/ Dark-No Street Lights/Street Lights Off Ice/Snow/	AD Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection Not at Intersection Driveway/Roundabout Not at Intersection/Driveway/Roundabout Slush Not at Intersection/Driveway/Roundabout Slush Not at Intersection/Driveway/Roundabout	Inathension Follow Too Closely N
5775 972639.14 1021171.48 2015 5776 971202.8 1017731.43 2016 5777 971279.56 1019148.78 2019 5778 971273.94 1019902.6 2011	5 1 Nehicle-Party Overturmed 5 2 Vehicles-Parties Angle 8 1 Vehicle-Party Over Embankment - No Guardrall Present 9 1 Vehicle-Party Struck Object 7 2 Vehicles-Parties Backing	Minor or Possible Injury Speeding Und Property Damage Only Property Damage Only 65+5 Property Damage Only Distracted Driving 65+ Property Damage Only Under Influence of Nicohol/Drugs/Medication 45-6 Property Damage Only Under Influence of Nicohol/Drugs/Medication 45-6	7.25 No No No No Under Influence of Acohol		Not at intersection/Universal/Roundabout Curve & Let At Driveway or Intersection Straight & L Not at Intersection/Driveway/Roundabout Straight & C At Driveway or Intersection Unknown	Level Older Road User 65+ Grade Older Road User 65+	Speeding Improper Turn/Merge Distracted Driving Inattention	Daylight Ice/Snow: Daylight Dry Daylight Dry	At Driveway or Intersection	Y Ne Ne Ne Ne Ne Ne Ne
5779 972553.1 1019891.54 2019 5780 972679.92 1022510.43 2011 5781 1004601.12 1022643.07 2018 5782 968743.53 1022696.57 (2016	9 2 Vehicles/Paries Tuming 7 2 Vehicles/Paries Rear-End 8 2 Vehicles/Paries Rear-End 8 2 Vehicles/Paries Angle 6 2 Vehicles/Paries Tuming 6 1 Vehicle/Paries Tuming 6 1 Vehicle/Paries Struck Object 7 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	Property Damage Only Inattention Und Minor or Possible Injury Distracted Driving 35-4 Minor or Possible Injury Fail to Yield 65+ Minor or Possible Injury Inattention Und	7:25 No	Dry Daylight	At Driveway or Intersection Straight & L	vel No Vulnerable Road User Involved Level No Vulnerable Road User Involved Grade Older Road User 65+ Level No Vulnerable Road User Involved No Vulnerable Road User Involved	Inattention Unknown Distraction Fail to Yield Inattention	Daylight Dry Daylight Dry Daylight Dry Daylight Dry	At Driveway or Intersection	Inathention
5784 988290.7032 1012324.063.2015 5785 988398.12 1015335.13.2016 5786 977986.09 1022347.33.2016 5787 982135.5534 1031459.076.2014		Properly Damage Only Fail to Yeld Properly Damage Only Dis regard Traffic Control Device/Sign S5-6 Minor or Possible Injury Apparently Alelsep, Fatigued or III Apparently Alelsep, Fatigued or III 65-6 Minor or Possible Injury Apparently Alelsep, Fatigued or III 66-6 66-6 66-6 66-6 66-6 66-6 66-6	No No No Apparently Asleep, Fatigued or III Apparently Asleep, Fatigued or III Apparently Asleep, Fatigued or III	Dry Daylight	At Driveway or Intersection Straight & L At Driveway or Intersection Straight & L At Driveway or Intersection Straight & L Not at Intersection/Driveway/Roundabout Straight & L	Level No Vulnerable Road User Imolved Level No Vulnerable Road User Imolved Level No Vulnerable Road User Imolved Level Older Road User imolved Level Older Road User 65+	Fail to Yield Disregard Traffic Control Device/Sign Inattention Apparently Asleep or Fatiqued	Daylight Dry Dark-No Street Lights/Street Lights Off Wet Daylight Dry Daylight Dry	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection Not at Intersection/Driveway/Roundabout	Fail to Velid N. No Risky Driving Behaviors Involved Y. No Risky Driving Behaviors Involved N. Apparently Asleep, Fatigued or III Y. Apparently Asleep, Fatigued or III Y. Y. Apparently Asleep, Fatigued
5791 987182.43 1020047.66 2015 5792 986145.76 1026048.99 2017	9 1 Vehida/Party Struck Object 7 2 Vehida/Parties Pedalcyclist 5 1 Vehida/Party Struck Object 7 2 Vehidas/Parties Sideswipe	Property Damage Only Speeding 25-3 Minor or Possible Injury Other/Unknown 35-4 Property Damage Only Improper Backing 55-6 Property Damage Only Other/Unknown 25-3	No No No No No	Wet Dark-Street Lights On Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight	At Intersection and Not Related Curve & Let At Driveway or Intersection Straight & L Not at Intersection/Driveway/Roundabout Straight & L At Driveway or Intersection Unknown Unknown	No Vulnerable Road User Involved	Speeding None	Dark-Street Lights On Wet Daylight Dry Daylight Dry	At Intersection and Not Related At Driveway or Intersection Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Speeding Y No Risky Driving Behaviors Involved N No Risky Driving Behaviors Involved Y No Risky Driving Behaviors Involved Y
5796 988838.59 1027586.65 2016	4 2 Whicks-Paries Royle 2 Whicks-Paries Rear-End 4 Ukhicks-Paries Rear-End 4 Ukhicks-Paries Rear-End 5 Whicks-Paries 5 Whicks-Paries 5 Whicks-Paries 5 Whicks-Paries 6 Whicks-Paries 6 Whicks-Paries 6 Paries 6 Whicks-Paries 6 Paries 6 Whicks-Paries 6 Paries 6 Whicks-Paries 6 Paries 6 Whicks-Paries 6 Whicks-Paries 6 Paries 6 Whicks-Paries 6 Paries 6 Whicks-Paries 6 Paries 6 Whicks-Paries 6 Paries 6 Whicks-Paries 7 Whicks-Paries 6 Whicks-Paries 7 Whicks-Paries 7 Whicks-Paries 7 Whicks-Paries 8 Whicks-Paries 8 Whicks-Paries 9 Whicks	Minor or Possible Injury Disregard Traffic Control DeviceSign 45-5 Monror Possible Injury Disregard Traffic Control DeviceSign 65-5 Pepperty Damage Coly Speeding 25-3 Spe	No No 100 100 100 100 100 100 100 100 100 10	Dry Daylight Dry Daylight Dry Daylight Under 25 Wet Dark-No Street Lights/Street Lights Off Wet Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Straight & L	Hillcrest No Vulnerable Road User Involved	Disregard Traffic Control Device/Sign Speeding Under Influence of Alcohol/Drugs/Medication	Daylight Dry Daylight Dry Dark-No Street Lights/Street Lights Off Wet	At Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	No Risky Driving Behaviors involved
5800 989249.83 1032903.99 2016 5801 989249.83 1032903.99 2016 5802 989249.83 1032903.98 2017	4 2 Vehicles/Parties Angle 6 2 Vehicles/Parties Angle 7 2 Vehicles/Parties Angle 7 2 Vehicles/Parties Angle 7 2 Vehicles/Parties Angle 8 2 Vehicles/Parties Angle 9 2 Vehicles/Parties Angle 9 1	Property Damage Only Oherd Unknown 45-5 Minor or Possible Injury Oisregard Traffic Control Device/Sign 65- Minor or Possible Injury Oherd Unknown 25-3 Minor or Possible Injury Disregard Traffic Control Device/Sign 25-3 Minor or Possible Injury 25-3 25-3	No No No No No No No	Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight Wet Daylight	At Driveway or Intersection Straight & C At Driveway or Intersection Straight & L At Driveway or Intersection Straight & C At Driveway or Intersection Straight & C	Grade No Vulnerable Road User Involved Level Older Road User 65+	None Disregard Traffic Control Device/Sign Other/Unknown Disregard Traffic Control Device/Sign	Daylight Dry	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	No Risky Driving Behaviors Involved
5804 989247.07 1032903.8 2014 5805 989247.07 1032903.8 2014 5806 989238.82 1032898.69 2017 5807 989247.0677 1032903.804 2014	4 2 Vehicles/Parties Angle 4 2 Vehicles/Parties Angle 7 2 Vehicles/Parties Angle 5 2 Vehicles/Parties Angle	Minor or Possible Injury Inattention 45-5 Property Damage Only Disregard Traffic Control Device/Sign 55-6 Minor or Possible Injury Disregard Traffic Control Device/Sign Under Property Damage Only Other/Unknown 45-5	No N	Dry Daylight Dry Daylight Dry Daylight Wet Daylight Dry Daylight Wet Daylight Dry Daylight	At Driveway or Intersection Unknown	No Vulnerable Road User Involved No Vulnerable Road User Involved Level No Vulnerable Road User Involved Level No Vulnerable Road User Involved Level No Vulnerable Road User Involved	Inattention Disregard Traffic Control Device/Sign Disregard Traffic Control Device/Sign None	Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight Wet	At Driveway or Intersection	Institution No Risky Driving Behaviors Involved No Risky Driving B
5809 989238.82 1032898.69 2018 5810 989238.82 1032898.69 2018 5811 989252.4 1033432.31 2014 5812 989238.89 1038535.79 2018	8 2 Vehicles/Parties	Monor or Possible Injury Disregard Traffic Control Device/Sign Undit Monor or Possible Injury Under Influence of Alcohol/Drugs/Medication 65+ Property/Damage Only Ohen/Unknown 65+ Property/Damage Only Apparently Aleep, Fatigued or III 65+ Monor or Possible Injury Under Influence of Alcohol/Drugs/Medication 25-35	r 25 No Under Influence of Medication No Apparently Asleep, Fatigued or III	Dry Daylight S5+ Dry Daylight Dry Dark-No Sweet Lights / Street Lights Off S5+ Dry Daylight	At Driveway or Intersection Straight & L At Driveway or Intersection Straight & L Not at Intersection/Driveway/Roundabout Straight & C Not at Intersection/Driveway/Roundabout Straight & C	Level No Vulnerable Road User Imolved Level Older Road User 65+ Grade Older Road User 65+ Level Older Road User 65+		Daylight Dry Daylight Ory Daylight Ory Dark-No Street Lights/Street Lights Off Dry Daylight Dry	At Driveway or Intersection At Driveway or Intersection Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Under Influence of Alcohol Drugs/Medication No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Apparently Asleep, Fatigued or III Your Under Influence of Alcohol Drugs/Medication Your Research Alcohol Drugs/Medication Your Research
\$813 989316.2 1037800.51 2014 \$814 989302 1037803.51 2016 \$815 994272.62 103218.57 2015 \$816 994302.5409 1033251.611 2014 \$817 989249.83 1032903.99 2016 \$818 990543.16 1032490.39 2016	6 1 VehicleiParty Over Embankment - No Guardrail Present 5 2 Vehicles/Parties are-End 4 1 Vehicle/Party Struck Object 6 2 Vehicles/Party Struck Object 6 2 Vehicles/Parties 4	Fatality Under Influence of Alcohol/Drugs/Medication 45-5 Property Damage Only Follow Too Closely 25-3 Property Damage Only Distracted Driving 35-4 Minor or Possible Injury Disreaard Traffic Control Device/Sign 45-5	Under influence of Acohol No No No No No	25-34 Dry Davlicht 45-64 Dry Davlich No Street Lights (Street Lights Off Dry Davlight Wet Dark-No Street Lights (Street Lights Off Dry Davlight Dry Davlight	Not at Intersection/Driveway/Roundabout Curve & Let Not at Intersection/Driveway/Roundabout Straight & L Not at Intersection/Driveway/Roundabout Curve & Let At Driveway or Intersection Straight & G Straight & G	Motorcycle	Under Influence of Alcohol/Drugs/Medication	Dark-No Street Lights/Street Lights Off Dry Daylight Dry Dark-No Street Lights/Street Lights Off Wet Daylight Dry Daylight Dry Daylight Dry Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Under Influence of Alcohol/Drugs/Medication Y Follow Too Closely No Statacted Driving No Risky Driving Behaviors Involved N
5821 996014.1503 1035983.952 2015	6 2 Ushbides Parties . Angle	Manor or Possible Injury Manor or Possible Injury Property Damage Only Property Damage Only Property Damage Only Property Damage Only Manor or Possible Injury American Only Manor or Possible Injury Manor or Possible Injury Manor or Possible Injury Under Influence of Alcohol/Drugs/Medication Under Influence of Alcohol/Drugs/Medication Under Influence Only Index	Apparently Asleep, Fatigued or III No No Under Influence of Drugs	## Dry Daylight Wet Dark-No Street Lights/Street Lights Off Dry Daylight		Level Older Road User 65+ Level No Vulnerable Road User Involved Older Road User 65+	Apparently Askep or Fatigued Improper Backing/Passing/Turning/Parking/Signating Fail to Yield Under Influence of Alcohol/Drugs/Medication	Daylight Dry Dark-No Street Lights/Street Lights Off Wet Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry	At Driveway or Intersection Not at Intersection/Driveway/Roundabout At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Apparently Relep. Fatigued or III Y No Risky Driving Behaviors Involved N Risky Driving Behaviors Involved N Tail to Yield N Under Influence of Alcohol/Drugs/Medication N Lasteentinc N
5825 997947.63 1008361.63 2016 5826 997009.7651 1014722.191 2015 5827 998173.7034 1014998.379 2015	8 I Vehicie/Party Struck Object 6 I Vehicie/Party Suruck Object 5 I Vehicie/Party Suruck Object 5 I Vehicies/Parties Rear-End 5 I Vehicies/Parties Angle 8 I Vehicies/Parties Rear-End	Property Damage Only	125 No 125 No N	35-44 Dry Das/knNb Street Lights/Street Lights Off Dry Dark-Nb Street Lights/Street Lights Off Wet Dark-Nb Street Lights/Street Lights Off Dry Dark-Nb Street Lights/Street Lights Off Dry Daylight Dry Daylight 0F Daylight 0F Daylight	Not at Intersection/Driveway/Roundabout Straight & L At Driveway or Intersection Out at Intersection Straight & L At Driveway or Intersection At Driveway or Intersection Straight & L Straight & L Straight & L	rvel No Vulnerable Road User Involved Level Older Road User 65+	Inattention Distractions Outside Vehicle Inattention Follow Too Closely Fall to Yield Under Influence of Alcohol/Drugs/Medication	Dark-No Street Lights/Street Lights Off Dry. Dark-No Street Lights/Street Lights Off Wet Dark-No Street Lights/Street Lights Off Dry. Daylight Dry. Daylight Dry. Dr	Not at Intersection/Driveway/Roundabout At Driveway or Intersection Not at Intersection/Driveway/Roundabout At Driveway or Intersection At Driveway or Intersection	Distracted Driving V
5829 997015 1016039.71 2014 5830 997015 1015039.71 2014 5831 993939.2 1015176.41 2014 5832 993831.65 1015178.34 2017	4 1 Vehicle,Party Struck Object 4 2 Vehicles,Parties 6 1 Vehicle Party Struck Object 7 1 Vehicle,Party Struck Object 7 1 Vehicle,Party Struck Object	Property Damage Only Under Influence of Alcohol/Drugs/Medication 35-4 Property Damage Only Other/Unknown 55-6 Property Damage Only Speeding Und Property Damage Only Inattention Und	Under Influence of Alcohol No			No Vulnerable Road User Involved Level No Vulnerable Road User Involved vel No Vulnerable Road User Involved vel No Vulnerable Road User Involved vel No Vulnerable Road User Involved	Under Influence of Alcohol/Drugs/Medication None Speeding Inattention	Daylight Dry Daylight Wet Dark-No Street Lights/Street Lights Off Dry	Not at Intersection/Driveway/Roundabout At Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Intersection Related but Not at Intersection	Under Influence of Alcohol/Drugs/Medication Y No Risky Driving Behaviors Involved N Speeding Y Instrumenton Y No Risky Driving Behaviors Involved Y Y No Risky Driving Behaviors Involved Y Y
5834 996578.42 1021687.7 2016 5835 996578.42 1021687.7 2016 5836 996725.37 1025725.41 2016 5837 1008097.03 1025266.99 2015	6 2 Vehicles/Parties Rear-End	Property Damage Only Inattention 45-5 Minor or Possible Injury Speeding 55-6 Minor or Possible Injury Other/Unknown 25-3	No No : No : 25 No	Dry Daylight Wet Daylight Dry Daylight Dry Daylight	At Driveway or Intersection Straight & L	Level No Vulnerable Road User Involved	hattenfon Speeding None Improper Turn/Merge	Daylight Dry Daylight Wet Daylight Dry Daylight Dry	At Driveway or Intersection Not at IntersectionDriveway/Roundabout	Inatention N Speeding N No Risky Driving Behaviors Involved N No Risky Driving Behaviors Involved N
5839 1009756.032 1025052.271 2014 5840 1009755.99 1025052.3 2014 5841 1009936.59 1024903.29 2016	4 1 VehicleiParty Struck Object 4 1 VehicleiParty Struck Object 6 1 VehicleiParty Struck Object 6 1 VehicleiParty Struck Object 8 1 VehicleiParty Struck Object	Property Damage Only Apparently Asleep, Fatigued or III 55-6 Property Damage Only Under Influence of Alcohol/Drugs/Medication Suspected Serious Injury Speeding Under Property Damage Only Inattention Under	Apparently Asleep, Fatigued or III Under Influence of Arcohol 25 No 25 No 275 No 275 No	Dry Daylottet	Not at Intersection/Driveway/Roundabout Curve & Let Al Intersection and Not Related Ourve & Gra Al Intersection Oriveway/Roundabout Straight at I Not at Intersection/Driveway/Roundabout Straight at I Not at Intersection/Driveway/Roundabout Curve & Gra Curve & Gra Curve & Gra	Ivel No Vulnerable Road User Involved rade No Vulnerable Road User Involved Hillicrest No Vulnerable Road User Involved Grade No Vulnerable Road User Involved	Under Influence of Alcohol/Drugs/Medication Speeding Inattention	Daylight Dry Dark-No Street Lights/Street Lights Off Wet Dark-Street Lights On Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout At Intersection and Not Related Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Speeding Y Apparently Acleep, Fatigued or III V Under Influence of Alcohol/Drugs/Medication V Inatention V Inatention V Inatention V V Inatention V V
5845 1007075.01 1017319.13 2016 5846 1007075.01 1017319.13 2016	5 1 Vehicle/Party Struck Object 6 1 Vehicle/Party Struck Object	Monor or Possable Injury Property Damage Only Pal Is V Yeld Property Damage Only Possable Only Property Damage Only Instituted Only Property Damage Only Stated Only	No Under Influence of Alcohol No No No	Dry Daylight Dry Daylight Dry Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off Dry Daylight Dry Daylight	At Driveway or Intersection Straight & G At Driveway or Intersection Straight & L At Driveway or Intersection Straight & G At Driveway or Intersection Straight & G	Grade		Daylight Dry Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off Dry Daylight Dry Daylight Dry	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Operating National Control of Machell Drugs Medication National Operation National Operational Operati
5850 1018518.32 1002519.25 2016	6 Vehicle Party Struck Object 6 Vehicle Party Struck Object 5 Vehicle Party Struck Object 4 Vehicle Party Struck Object 4 Vehicle Party Struck Object 7 Vehicle Party Struck Object 7 Vehicle Party Struck Object 7 Vehicle Party Struck Object Object Object 8 Vehicle Party Struck Object Objec	Unkr Unkr Unkr Unkr	own No	Dry Daylight Dry Daylight Dry Daylight Dry Dark-No Street Lights /Street Lights Off Dry Dark-No Street Lights /Street Lights Off Dry Daylight	Not at Intersection/Driveway/Roundabout Straight & G Not at Intersection/Driveway/Roundabout Curve & Gra		Other Contributing Circ Not Listed Inattention Speeding Other Contributing Circ Not Listed None	Daylight Dry Daylight Dry Daylight Dry Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off Dry Daylight Dry Daylight Dry	At Driveway or Intersection Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Y Instantian Y Y Instantian Y Y Speeding Y Y No Risky Driving Behaviors Involved Y Y No Risky Driving Behaviors Involved N Y No Risky Driving Behaviors Involved N Y No Risky Driving Behaviors Involved N No Risky Driving Behaviors Involved N N No Risky Driving Behaviors Involved N N No Risky Driving Behaviors Involved N N N N N N N N N N N N N N N N N N N
\$854 1015028.927 1017066.082.2011 \$855 1015126.73 1019109.63 2018 \$856 1007075.01 1017319.14 2011 \$857 1007075.01 1017319.14 2011	7 Vehicle/Party	Minor or Possible Injury Distracted Driving 25-3	No r25 No	Dry Dark-No Street Lights (Street Lights Off Wet Dark-No Street Lights Off Port Dark-No Street Lights Off Dry Dark-No Street Lights Off Up Dark-No Street Lights Off Wet Dark-No Street Lights (Street Lights Off Dry Dark-No Street Lights (Street Lights Off Dry Dark-No Street Lights Off	Not at Intersection/Driveway/Roundabout Straight & L Not at Intersection/Driveway/Roundabout Straight & L At Intersection and Not Related Straight & L At Driveway or Intersection Straight & L	Level No Vulnerable Road User Involved	Other Contributing Circ Not Listed Disregard Traffic Control Device/Sign Disregard Traffic Control Device/Sign	Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off Wet Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off Wet Dark-No Street Lights/Street Lights Off Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout At Intersection and Not Related At Driveway or Intersection	Distracted Driving No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Y No Risky Driving Behaviors Involved Y No Risky Driving Behaviors Involved Y Under Influence of Alcohol Drugs/Medication Y
5859 1007075.01 1017319.14 2019 5860 1007820.87 1017347.35 2017 5861 1007674.33 1017299.83 2017 5862 1010155.37 1018044.99 2016	9 2 Vehicles/Parties Angle 7 1 Vehicle/Party Struck Object 7 1 Vehicle/Party Struck Object 6 1 Vehicle/Party Struck Object	Property Damage Only Disregard Traffic Control Device/Sign 65+ Property Damage Only Other/Unknown Unikr Minor or Possible Injury Speeding 55-6	No own No	Dry Daylight Ice/Snow/Slush Daylight Ice/Snow/Slush Daylight Dry Daylight	At Driveway or Intersection Unknown Not at Intersection. Driveway Roundabout Straight & L Not at Intersection. Driveway Roundabout Curve & Le Not at Intersection. Driveway Roundabout Straight & C Not at Intersection. Driveway Roundabout Curve & Gre Not at Intersection. Driveway Roundabout Curve & Gre On the Straight & Curve & Gre On the	Older Road User 65+ No Vulnerable Road User Involved wel Motorcycle Grade No Vulnerable Road User Involved	Disregard Traffic Control Device/Sign None Speeding Other Contributing Circ Not Listed	Daylight Dry Daylight LecSnowl Daylight LecSnowl Daylight LecSnowl Daylight Dry Dark-No Street Lights/Street Lights Off Dry	At Driveway or Intersection Slush Not at Intersection/Driveway/Roundabout Slush Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Y Speeding Y No Risky Driving Behaviors Involved Y No Risky Driving Behaviors Involved Y
5865 1011584.01 1018672.12 2016 5866 1012170.4 1018709.34 2019 5867 1011920.03 1018765.5 2018	5 2 Vehicles/Parties Backing	Unknown Improper Backing 55-6 Property Damage Only Speeding 45-5 Property Osmage Only Instension Und	No No No 125 No 125 No	loe/Snow/Slush Dark-No Street Lights/Street Lights Off Dry Daylight	Not at Intersection/Driveway/Roundabout Unknown Not at Intersection/Driveway/Roundabout Straight & L Not at Intersection/Driveway/Roundabout Curve & Let Not at Intersection/Driveway/Roundabout Curve & Let Not at Intersection/Driveway/Roundabout Curve & Let	No Vulnerable Road User Involved	Improper Backing/Passing/Turning/Parking/Signaling Speeding Inattention	Daylight Wet Dark-No Street Lights/Street Lights Off Ice/Snow/	Not at Intersection/Driveway/Roundabout	New First New First
\$869 101270.37 101889.25 2018 5870 102455.746 102265.754 2014 5871 1004695.446 102265.764 2014 5872 1004601.11 102264.36 2015 5872 1009885.16 1022462.15 2015 5873 10226407.91 990339.41 2014 5874 1031319.209 994774.6218 2015	8 Z Vehicles Paries Rean End 2 Vehicles Paries Blacking 6 Z Vehicles Paries Blacking 1 Vehicles Paries Blacking 1 Vehicles Paries Blacking 1 Vehicles Paries Slacking 2 Vehicles Paries Slack Object 4 Z Vehicles Paries Slack Object 2 Vehicles Paries Slack Object 3 Vehicles Paries Slack Object 4 Vehicles Paries Blacking 4 Vehicles Paries Paries Blacking 4 Vehicles Paries 4 Vehicles Paries 4 Vehicles Paries 4 Vehicles Parie	Minor or Possible Injury Other/Unknown 25-3 Property Damage Only Inattention 65+	No N	Dry Daylight Dry Daylight Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Curve & Let At Driveway or Intersection Straight & L At Driveway or Intersection Straight & L Not at Intersection/Driveway/Roundabout Straight & L	No Vulnerable Road User Involved	Other Contributing Circ Not Listed Inattention Improper Backing/Passing/Turning/Parking/Signaling Unknown Distraction Inattention	Daylight Dry Dawn/Dusk Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry	Not at Intersection/Driveway/Roundabout At Driveway or Intersection At Driveway or Intersection Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving No Risky Drivin
5876 1031279.393 991248.3355 2014 5877 988867 1023233.04 2018 5878 1024867 97 993203.13 2018	4 2 Vehicles/Paries Struck Object 9 1 Vehicles Pariy Oner Emankemen No Guardrail Present 4 1 Vehicle Pariy Struck Object 8 2 Vehicles/Paries Sides wipe 2 2 Vehicles/Paries Sides wipe	Property Damage Only Speeding Undid Menor or Possible Injury Speeding 36-4 Minor or Possible Injury Distracted Driving 26-4 Minor or Possible Injury Improper TurnNerge 55-6 Property Damage Only Other Unknown 66+	725 No No No No No No No	Dry Daylight Dry Daylight Dry Daylight Dry Daylight LosSnowSlush Dark-No Street Lights/Street Lights Off Dry Daylight Dry Daylight	A Driwway or Intersection Straight & L. Not at Intersection. Driwway/Roundabout Curve & Ler Not at Intersection. Driwway/Roundabout Curve & Green &	ruel Motorcycle rade No Vulnerable Road User Involved Level No Vulnerable Road User Involved	Speeding Speeding Unknown Distraction Improper Backing/Passing/Turning/Parking/Signaling Other/Unknown	Daylight Dry Daylight Dry Dark-No Street Lights/Street Lights Off Dry Daylight Dry Daylight Dry Daylight Dry	At Driveway or Intersection Not at Intersection/Driveway/Roundabout	Speeding Y Speeding Y Speeding Y Speeding Y Y Speeding Y Y Speeding Y Y Speeding Y No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Y Y Speeding Speedin
5879 1040954.35 999225.67 2015 5880 1029536.93 995094.82 2016 5881 1028040.88 998800.91 2017 5882 1027365.561 1003096.86 2014 5883 1027597.14 1003362.92 2018	8 2 Vehioles Parties Rean-End	Property Damage Only Inattention Und Property Damage Only Apparently Aslaep, Fatigued or III 55-6 Property Damage Only Inattention Und Property Damage Only Apparently Aslaep, Fatigued or III 65+	r 25 No Apparently Asleep, Fatigued or III 25 No Apparently Asleep, Fatigued or III No No	Dry Daylight 55-64 Dry Daylight Wet Dark-No Street Lights/Street Lights Off 85+ Wet Daylight Wet Daylight Wet Daylight	Not at Intersection/DrivewayRoundabout Straight & C Not at Intersection/DrivewayRoundabout Straight & L Not at Intersection/DrivewayRoundabout Curve & Gr Not at Intersection/DrivewayRoundabout Curve at Hit At Driveway or Intersection Curve & Gr	Grade No Vulnerable Road User Involved Level No Vulnerable Road User Involved ade No Vulnerable Road User Involved illcrest Older Road User 65+	Inattention Driver Physical Condition Inattention Driver Physical Condition	Daylight Dry Daylight Dry Daylight Dry Dark-No Street Lights/Street Lights OH Wet Daylight Wet Dark-No Street Lights/Street Lights OH Wet	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Inatention Naparention Naparention Naparentiy Asleep, Fatigued or III Y Apparently Asleep, Fatigued or III Naparently Asleep, Fatigued or III Y Apparently Asleep, Behaviors Involved Y Y Naparently Asleep Naparently Asleep
5884 1040928.93 1000300.01 [201] 5885 1041081.01 1004467.7 [201] 5886 1040992.545 1001808.342 [2014 5887 1041005.81 1001822.33 [2014 5888 1041923.82 100589.7 1] [2014	7 2 Vehicles/Parties Turning 7 1 Vehicle/Party Struck Object 4 1 Vehicle/Party Struck Object 6 1 Vehicle/Party Struck Object 6 1 Vehicle/Party Struck Object 7 2 Vehicle/Party Struck Object 8 2 Vehicle/Party Struck Object 8 2 Vehicle/Parties Velescope 9 2 Vehicle/Parties 9 2 Vehicle/Party Velescope 9 2 Vehicle/Parties Velescope 9 2 Velescope 9 2 Vehicle/Parties Velescope 9 2 Velescope Velescope 9 2 Velescope Velescope 9 2 Velescope Velescope 9 2 Velescope Velescope Velescope Velescope Velescope 9 2 Velescope Velescope Velescope Velescope Velescope Velescope	Property Damage Only Fail to Yield 35-4 Minor or Possible Injury Inattention 45-5 Minor or Possible Injury OtherUnknown Under Property Damage Only John Linder Influence of Alcohol/Drugs/Medication 35-4 Property Damage Only Speeding Under	No	Wet Daylight	Not at Intersection/Driveway/Roundabout Straight & C Not at Intersection/Driveway/Roundabout Straight & C Not at Intersection/Driveway/Roundabout Straight & C Not at Intersection/Driveway/Roundabout Straight & G Not at Intersection/Driveway/Roundabout Curve & C	Grade No Wilnerable Road User Involved	Fail to Yield Interest on Other/Unknown Under Influence of Alcohol/Drugs/Medication Speeding	Daylight Wet Daylight Dry Dawn/Dusk Ice/Snow! Daylight Ice/Snow! Daylight Ice/Snow! Daylight Ice/Snow!	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Slush Not at Intersection/Driveway/Roundabout Slush Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Fail to Vield No.
5899 1041107.886 1005550.63 2014 5890 956881.3 1020813.23 2014 5891 980626.6 1022259.66 2011 5892 957517.66 1021365.03 2014	4 1 Vehicles Party 7 3 Vehicles Party 7 3 Vehicles Partes 1 Vehicles Party 5 Struck Object	Property Damage Only Speeding Under	r 25 No : No	Dry Daylight	Not at Intersection Driveway/Roundabout Curve & Gr. At Driveway or Intersection Straight & L At Driveway or Intersection Straight & L Not at Intersection Driveway/Roundabout Straight & L Not at Intersection Driveway/Roundabout Straight & L Not at Intersection Driveway/Roundabout Straight & L		Speeding None Fail to Yield Apparently Acleep or Fatigued	Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry	Not at Intersection/Driveway/Roundabout At Driveway or Intersection At Driveway or Intersection Not at Intersection/Driveway/Roundabout	Y Ne Risky Driving Behaviors Involved N N N N N N N N N
5896 958078 32 1021765 12 2019	5 1 Vehrider-Parry Struck Object 2 Vehrider-Restra Turning 9 1 Vehrider-Parry Struck Object 1 Vehrider-Parry Struck Object 7 1 Vehrider-Parry Sideswape	Suspected Senous Injury Apparently Asleep, Fatigued or III 65+ Minor or Possible Injury Inattention 65+	Apparently Asleep, Fatigued or III	Day Daylight	At Driveway or Intersection Straight & L Not at Intersection/Driveway/Roundabout Straight & G Not at Intersection/Driveway/Roundabout Straight & L	Level Older Road User 65+ Grade No Vulnerable Road User Involved Level No Vulnerable Road User Involved	Inattention		At Driveway or Intersection	Apparently felselp, Fatigued or III Initiationion No. Risky Driving Behaviors Involved No. Risky Driving Behaviors Involved No. Risky Driving Behaviors Involved Y No. Risky Driving Behaviors Involved Y Y
5900 959852.88 1022599.32 2016 5901 960600.53 1022955.13 2016 5902 961129.61 1023158.67 2017 5903 991694.79 1022871.34 2016	6 2 Vehicles/Parties Sideswipe 8 2 Vehicles/Parties Rean-End 7 1 Vehicle/Party Struck Object 5 1 Vehicle/Party Struck Object	Property Damage Only Other/Unknown 45-5 Minor or Possible Injury Follow Too Closely 45-5	No	looiSnowiSlush Daylight looiSnowiSlush Daylight Daylight Daylight looiSnowiSlush DawnOusk looiSnowiSlush DawnOusk	Not at Intersection. Driveway/Roundabout Straight & C Not at Intersection. Driveway/Roundabout Straight & C Not at Intersection. Driveway/Roundabout Straight & C Not at Intersection. Driveway/Roundabout Straight & L Not at Intersection. Driveway/Roundabout Straight & L Not at Intersection. Driveway/Roundabout Straight & L		Other Contributing Circ Not Listed Follow Too Closely Speeding Speeding	Daylight ce/Snow/ Daylight Dry Dawn/Dusk Ice/Snow/ Dawn/Dusk Ice/Snow/	Slush Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Slush Not at Intersection/Driveway/Roundabout Slush Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Y No Risky Driving Behaviors Involved Y Follow Too Closely N Speeding Y Speeding Y
5904 961079.87 1023141.66 [2015] 5905 961738.19 1023334.39 [2015] 5906 961184.9641 1023175.114 [2014] 5907 961284.5229 1023209.671 [2014] 5908 962773.578 1023517.481 [2015]	6 1 Vehiole/Party Struck Object 9 1 Vehiole/Party Struck Object 4 1 Vehiole/Party Struck Object 4 1 Vehiole/Party Struck Object 5 1 Vehiole/Party Struck Object 6 1 Vehiole/Party Struck Object 7 1 Vehiole/Party Struck Object	Property Damage Only Oher/Unknown 25:3 Property Damage Only Apparenty Asleep, Fasigued or III 65+ Property Damage Only Oher/Unknown 25:3- Property Damage Only Oher/Unknown Und Moor or Possible Indius Oher/Unknown 56:6-	No Apparently Asleep, Fatigued or III No 225 No No No	loo_Snow/Slush Dark-No Street Lights/Street Lights Off 55+ Dry Daylight Daylight loo_Snow/Slush Daylight Dry Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight Daylight	Not at Intersection/DrivewayRoundabout Straight & C Not at Intersection/DrivewayRoundabout Straight & C Not at Intersection/DrivewayRoundabout Straight & I. Not at Intersection/DrivewayRoundabout Straight & I. At Driveway	Grade No Witnerable Road User Involved	Apparently Asleep or Fatigued Other Contributing Circ Not Listed Other Contributing Circ Not Listed	Dark-No Street Lights/Street Lights Off Ice/Snow/ Daylight Dry Daylight Ice/Snow/ Daylight Ice/Snow/	Slush Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Slush Not at Intersection/Driveway/Roundabout Slush Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Apparently Asleep, Falligued or III Y Apparently Asleep, Falligued or III Y No Risky Driving Behaviors Involved Y No Risky Driving Behaviors Involved Y No Risky Driving Behaviors Involved
5909 964508.18 1023290.15 201 5910 964879.3 1023246.85 201 5911 968549.8926 1022728.538 2015 5912 968876.1475 1022684.747 2011 5913 969336.42 102639.76 2011	6 1 Vehicle-Party Arimal 4 1 Vehicle-Party Arimal 5 1 Vehicle-Party Arimal 4 2 Vehicles-Parties Sideswipe 6 2 Vehicles-Parties Turning	Property Damage Only OherUnknown 25-3 Property Damage Only OherUnknown 65+7 Property Damage Only OherUnknown 45-5 Minor or Possible Injury Fall to Yield 65+8 Property Damage Only Improper Turn/Merge 55-6	No	Dry Daylight	Not at Intersection/DrivewayRoundabout Straight & L Driveway Roundabout Straight & L Driveway Related but Not at Driveway Straight & L	Level No Wilnerable Road User Involved Level Older Road User 65+ Level No Vulnerable Road User Involved Level Older Road User 65+ Level No Vulnerable Road User Involved	None None Fail to Yield Improber Backing/Pas sing/Turning/Parking/Signaling	Daylight Wet Daylight Dry	At Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Driveway/Related but Not at Driveway	No Risky Driving Behaviors Involved
5914 972768.2848 1022507.312 2014 5915 972768.27 1022507.31 2014 5916 972676.96 1022500.06 2015 5917 972676.96 1022500.06 2016	4 2 Vehicles/Parties Turning 4 2 Vehicles/Parties Angle 7 2 Vehicles/Parties Angle 8 2 Vehicles/Parties Angle	Property Damage Only Improper Turn/Merge 65+ Property Damage Only Other/Unknown Undid Minor or Possible Injury Inattention 55-6 Minor or Possible Injury Fail to Yield 65+	No N	Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight	At Driveway or Intersection Straight & I.	Level Older Road User 65+ Level No Vulnerable Road User Involved Level No Vulnerable Road User Involved Level Older Road User 65+	Improper Turn/Merge Inattention Fail to Yield Fail to Yield	Daylight Dry	At Driveway or Intersection	No Risky Driving Behaviors Involved N No Risky Driving Behaviors Involved N Institution N Fall to Yield N Fall to Yield N N N
5920 972768.27 1022507.31 2014 5921 974833.99 1022443.29 2019	4 2 Vehicles/Parties Rear-End 9 1 Vehicles/Party Struck Object 7 2 Vehicles/Parties Rear-End	Minor or Possible Injury Fail b Yeld 65+ Property Damage Only Other/Unknown 25-3 Minor or Possible Injury Speeding 65+ Minor or Possible Injury Distracted Driving Und	No	Dry Daylight Dry Daylight loe/Snow/Slush Daylight Dry Daylight	Al Driveway of Intersection Unknown Not at Intersection/Driveway/Roundabout Straight & L Not at Intersection/Driveway/Roundabout Straight & L Not at Intersection/Driveway/Roundabout Straight & U Not at Intersection/Driveway/Roundabout Straight & G Not at Intersection/Driveway/Roundabout Straight & G	Level No Vulnerable Road User Involved Level Older Road User 65+ Grade No Vulnerable Road User Involved	Fail to Yeld None Speeding Distracted Driving	Daykight	Not at Intersection/Driveway/Roundabout Slush Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Fail to Yield N No Risky Driving Behaviors Involved N Speeding Y Distracted Driving N
5925 975757.45 1022414.82 2019 5926 976134.86 1022400.51 2019 5927 976586.39 1022384.94 2016 5928 976586.39 1022384.94 2016	9 1 Vehida/Party Struck Object 9 1 Vehida/Party Struck Object 6 2 Vehidas/Parties Turning 5 2 Vehidas/Parties Turning	Property Damage Only Other Unknown 25-3 Minor or Possible Injury Other Unknown 65+ Minor or Possible Injury Other Unknown 25-3 Minor or Possible Injury Follow Too Closely 25-3	No N	Dry Daylight Ieo-Snow/Slush Daylight Ieo-Snow/Slush Daylight Dry Daylight Dry Daylight Wet Daylight Wet Daylight	vot at mersection/unwaykoundatout Unknown Not at Intersection/DrivewayRoundatout Straight in 9 Not at Intersection/DrivewayRoundatout Straight in 1 At Driveway or Intersection Straight & At Driveway or Intersection Straight & I	No Vulnerable Road User Involved Sag No Vulnerable Road User Involved Sag Older Road User 66+ Level No Vulnerable Road User Involved Level No Vulnerable Road User Involved	None Other Contributing Circ Not Listed Other Contributing Circ Not Listed Follow Too Closely	Daylight loc/Snowl Daylight Dry Daylight Dry Daylight Wet	Not at hitersection/Driveway/Roundabout Slush Not at hitersection/Driveway/Roundabout Slush Not at hitersection/Driveway/Roundabout Not at hitersection/Driveway/Roundabout At Driveway/or Intersection At Driveway or Intersection	Speeding Y No Resky Driving Behaviors Involved No Resky
5929 977283.32 1022368.2 [201] 5930 977821.07 102235.3.43 [201] 5931 977986.08 1022347.34 [201] 5932 978031.6023 1022347.069 [2016]	7 1 Vehicle/Barty Struck Object 4 2 Vehicles/Parties Turning 7 2 Vehicles/Parties Angle 5 2 Vehicles/Parties Rear-End 8 2 Vehicles/Parties Rear-End 8 2 Vehicles/Parties Rear-End	Minor or Possible Injury Improper Passing 25-3	No No No No 125	los/Snow/Slush Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Straight & L At Driveway or Intersection Straight & L Straight & L	Level No Vulnerable Road User Involved Level No Vulnerable Road User Involved Level No Vulnerable Road User Involved Level Older Road User 65+ Level No Vulnerable Road User Involved	Improper Backing/Passing/Turning/Parking/Signaling Follow Too Closely Fail to Yield Follow Too Closely Follow Too Closely Follow Too Closely	Daylight lee/Snowl Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry	Slush Not at Intersection/Driveway/Roundabout At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	No Risky Driving Behaviors Involved Y Follow Too Closely N Fail to Yfeld N Follow Too Closely N Follow Too Closely N Follow Too Closely N
5934 979043.44 1022313.9 2016 5935 976932.42 1022377.76 2016 5936 979683.09 1022294.84 2018	6 2 Vehicles/Parties Struck Object 6 1 Vehicles/Parties Animal 8 2 Vehicles/Parties Turning	Property Damage Only Other/Unknown 25-3 Property Damage Only Other/Unknown 25-3 Property Damage Only Speeding Undit	No No 125 No	Wet Dark-No Street Lights (Street Lights Off Wet Dark-No Street Lights Off Wet Daylight Dark-No Street Lights Street Lights Off Under 25 Dry Dark-No Street Lights (Street Lights Off Inder 25 Dark-No Street Lights (Street Lights Off Inder 25 Dark-No Street Light	Not at Intersection/Driveway/Roundabout Straight & L Not at Intersection/Driveway/Roundabout Straight & L At Driveway or Intersection Straight & L	Level No Vulnerable Road User Involved Level No Vulnerable Road User Involved Level No Vulnerable Road User Involved	Other Contributing Circ Not Listed None Speeding	Dark-No Street Lights/Street Lights Off Wet Dark-No Street Lights/Street Lights Off Wet Daylight Wet	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout At Driveway or Intersection Not at Intersection/Driveway/Roundabout Siush Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Y No Risky Driving Behaviors Involved N Speeding N

OBJECTID JURISDICTION	COUNTY CITY	PRIMARY BLOCK TRAFFICWAY NUMBER	INTERSECTING DIST ML_O' COMP_DR REFERENCE TROOM TRANSCORD TO REPORT TO REPORT TO REPORT TO SUPPOSE NO NAMEER		BIKES VEH_1_TYPE	VEH_2_TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD_SURFACE ROADWAY CHARACTERISTIC	LIGHTING CONDITION	RRST_COLLISION TYPE_OBJECT_STRUCK	VEH_1 ACTION	VEH_2 ACTION	VEH.1_COMPASS DIRECTION_FROM DIRECTION_TO DIRECTION_FROM DIRECTION_TO
5939 County Road 5940 County Road	Cialiam 94 Cialiam 94	4120 4120	4.94 No 2767053 4.99 No E475129	2016-12-28 07:35 Suspected Serious Injury 3 0 2 0 2015-10-12 08:46 No Apparent Injury 0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry	Daylight Ma	m opposite direction - all others libox	Going Straight Ahead Going Straight Ahead	Going Straight Ahead	East West West East West East
5941 County Road 5942 County Road	Cialiam 94 Cialiam 94	4120 4120		2017-11-02 17:03 No Apparent Injury 0 0 2 0 2017-01-02 13:30 No Apparent Injury 0 0 2 0	0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related	Raining Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level	Daylight En	ering at angle ering at angle	Going Straight Ahead Making Left Turn	Going Straight Ahead Going Straight Ahead	North South West East Vehicle Stopped West West East
5942 County Road 5943 County Road 5944 County Road 5945 County Road 5945 County Road	Cialiam 94 Cialiam 94	4120 4120	94170 5.00 No 3615678 94170 5.00 No 3615884		Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight En	ering at angle ering at angle	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead Going Straight Ahead Going Straight Ahead	South North East West East West South North
5945 County Road 5946 County Road	Cialiam 94	4120 4120	94170 5.00 No 3615998 94170 5.00 No 3616032	2016-05-28 13:05 No Apparent Injury 0 0 2 0 2017-08-03 14:05 No Apparent Injury 0 0 2 0 2017-08-03 14:05 No Apparent Injury 0 0 2 0 2014-07-01 13:16 No Apparent Injury 0 0 3 0 0 3 0 0 0 0 0 0 0 0 0 0 0 0 0	0 Passenger Car	Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy	Dry Straight & Level	Daylight En	ering at angle ering at angle	Going Straight Ahead	Going Straight Ahead	South North East West
5947 County Road 5948 County Road	Cialiam 94 Cialiam 94	4120 4120 4120	94170 5.00 No E896568 5.30 No 3759354 6.60 No 3890368	2019-01-15 12:12 Possible Injury 2 0 2 0 2 0 2017-05-27 12:40 Suspected Minor Injury 2 0 2 0 2 0 2019-05-29 13:65 Suspected Minor Injury 1 0 1 0 2017-07-08 14:50 Suspected Minor Injury 2 0 1 0 0 2017-07-08 14:50 Suspected Minor Injury 2 0 1 0 0 0 0 0 0 0 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Passanger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Fro	ering at angle m opposite direction - one left turn - one straight iicle Strikes Deer	Making Left Turn Making Left Turn Going Straight Ahead	Going Straight Ahead Going Straight Ahead	Rorin East East West East South West East
5950 County Road 5951 County Road	Cialiam 94	4120	5.66 No 2767150	2017-07-08 14:50 Suspected Minor Injury 2 0 1 0	Pickup, Panel Truck or Vanette under 10,000 lb Page anger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level	Daylight Tre	od Sign Post	Going Straight Ahead Making Left Turn		West East Cart Southwart
5951 County Road 5952 County Road 5953 County Road	Clallam 94 Clallam 94 Clallam 94	4120 4120	5.89 No E426335	2015-05-13 15:01 No Apparent Injury 0 0 2 0 2016-08-29 19:00 No Apparent Injury 0 0 1 1 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Driveway Related but Not at Driveway Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Fro		Slowing Going Straight Ahead Making Left Turn	Going Straight Ahead	West East West East Fast Southeast
5954 County Road 5955 County Road	Cialiam 94 Cialiam 94	4120 4120	94270 0 6.26 No 2978766 94270 6.26 No 3616036	2018-07-20 10:25 No Apparent Injury 0 0 2 0 2014-12-02 13:05 No Apparent Injury 0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Curve & Level Dry	Daylight En Daylight Fro	ening at angle m opposite direction - one left turn - one straight	Making Left Turn Making Left Turn	Making Left Turn Going Straight Ahead	South West West South Southeast South Southeast East
5956 County Road 5957 County Road	Cialiam 94 Cialiam 94	4120 4120	94270 6.26 No E766326 94270 6.26 No E892063	2016-05-15 105-05	0 Passenger Car 0 Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Overcast Snowing	Snow/Slush Straight & Level	Dark-No Street Lights En Daylight En	ering at angle ering at angle	Starting From Parked Position Stopped at Signal or Stop Sign	Going Straight Ahead Making Right Turn	South East West East Vehicle Stopped Vehicle Stopped West South
5958 County Road 5959 County Road	Cialiam 94 Cialiam 94	4120 4120	6.26 No 3759357 6.29 No 3202399	2017-06-08 20:43 No Apparent Injury 0 0 2 0 2014-02-11 06:10 No Apparent Injury 0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related Not at Intersection and Not Related	Raining Overcast	Wet Straight & Level loe Curve & Level	Daylight Fro Dark-No Street Lights Ea	m opposite direction - one lett turn - one straight th Bank or Ledge	Going Straight Ahead	Making Left Turn	West East East South North West
5960 County Road 5961 County Road	Cialiam 94 Cialiam 94	4120 4120	6.32 No E635097 45580 6.40 No E876011	2017-05-08 203-3 No Apparent Injury 0 0 2 0 0 2 0 0 2 0 0 0 0 0 0 0 0 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	P. I. P. I.T. I.	Not at Intersection and Not Related At Intersection and Not Related	Other Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level	Dawn Ro Daylight Ow	adway Ditch er Embankment - No Guardrail Present	Going Straight Ahead Going Straight Ahead	0.1011-11-1	East West West East
5963 County Road 5964 County Road	Cialiam 94	4120 4120 4130						Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Wet Straight & Level	Daylight En	ering at angle ering at angle ering at angle	Going Straight Ahead Going Straight Ahead Miking Left Turn	Going Straight Ahead Going Straight Ahead Going Straight Ahead	West East North South East South South South North
5963 County Road 5964 County Road 5965 County Road 5966 County Road	Cialiam 94 Cialiam 94	4120 4120	94290 (8.67 No 36/15/48) 94290 (6.67 No 36/15/48) 94290 (6.67 No 36/16/128)	2014-11-24 15:03 Possible Injury 1 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car Pickup Panel Truck or Vanette under 10.000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level Dry Dry Straight & Level	Daylight En	eering at angle m same direction - both going straight - one stopped - rear-end	Making Left Turn Going Straight Ahead Going Straight Ahead	Going Straight Ahead Going Straight Ahead Making Left Turn	East West South North South North East
5967 County Road 5968 County Road	Cialiam 94 Cialiam 94	4120 4120	94290 6.67 No 3759320 94290 6.67 No 3769400	2014-06-07 0938 Possible hijuy	Passenger Car Passenger Car	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Level	Daylight En Dark-No Street Lights En	tering at angle	Making Left Turn Going Straight Ahead	Going Straight Ahead Going Straight Ahead	West South South North East West South North
5967 County Road 5968 County Road 5969 County Road 5970 County Road 5971 County Road	Cialiam 94 Cialiam 94	4120 4120	45850 6.93 No 3616047 6.98 No 2767170	2015-03-29 03:00 No Apparent Injury 0 0 1 0 0 2016-05-30 12:59 No Apparent Injury 0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level Dry Curve & Level	Dark-Street Lights On Gu		Going Straight Ahead	Going Straight Ahead	South North East West East West
	Cialiam 94 Cialiam 94	4120 4120	7.12 No E916867 45910 7.18 No E96082	2019-04-09 14:15 No Apparent Injury 0 0 1 0 0 1 0 0 1 0 0 1 0 0 0 1 0	 Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb 	Passenger Car	Not at Intersection and Not Related At Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight Fro	ity Box m same direction - both going straight - one stopped - rear-end	Stopped for Traffic Going Straight Ahead Going Straight Ahead	Stopped for Traffic	West East East West Vehicle Stopped Vehicle Stopped
5972 County Road 5973 County Road 5974 County Road	Cialiam 94 Cialiam 94	4120 4120	7.20 No E450014 7.81 No E55136	2015-08-03 11:16 No Apparent Injury 0 0 1 0 2016-06-19 01:04 Died in Hospital 0 1 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Curve & Level	Daylight Vel Dark-No Street Lights Fer	ce	Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East West East
5976 County Road 5977 County Road	Cialiam 94	4120 4120 4130	51950 7.97 No E687451 7.90 No E714606	2019-04-0914-15 No Apparent Injury 0 0 1 1 0 2 0 1 1 0 2 1 0 1 1	Pickup,Panel Truck or Vanette under 10,000 lb Passanger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy		Dark-No Street Lights Vel Daylight Fro	ince versionieu m same direction - both going straight - one stopped - rear-end m same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead	Making Left Turn Stopped for Traffic	Vest
5978 County Road 5979 County Road	Cialiam 94 Cialiam 94	4120 4120 4120	8.12 No 2727924 8.18 No F89061	2017-06-16 14:55 Possible Injury 2 0 2 0 2 0 2017-11-01 13:50 Possible Injury 2 0 2 0 0 2018-10-99 20:39 No Apparent Injury 0 0 1 0 2019-02-08 10:57 No Apparent Injury 0 0 2 0 0 2019-02-08 10:57 No Apparent Injury 0 0 2 0 0 0 0 0 0 0 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb		Clear or Partly Cloudy Clear or Partly Cloudy Snowing	Dry Straight & Level Snow/Slush Curve & Grade		m same unecom room going seaguir rone supped reamend icle Strikes Deer m opposite direction - all others	Going Straight Ahead Going Straight Ahead	Going Straight Ahead	West East Venice Supped Venice
5980 County Road 5981 County Road	Cialiam 94 Cialiam 94	4120 4120	52270 8.22 No 3202142	2015-02-18 17:00 No Apparent injury 0 0 2 0	0 Passenger Car 0 Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Raining	Dry Curve & Grade	Dusk En	ering at angle	Going Straight Ahead	Making Left Turn Going Straight Ahead	East West North East North South East West
5982 County Road 5983 County Road	Cialiam 94 Cialiam 94	4120 4120	52270 8.22 No EA8300 52270 8.22 No EA8300	2018-07-13 14:45 Possible Injury 1 0 2 0	0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Raining Clear or Partly Cloudy	Wet Straight & Level Wet Straight & Grade Dry Curve & Level	Daylight Me Daylight En	nering at angle tal Sign Post tering at angle	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Making Left Turn	North South East West South West
5984 County Road 5985 County Road 5986 County Road 5987 County Road	Cialiam 94 Cialiam 94 Cialiam 94	4120 4120	8.27 No 3615995	2016-12-25109:35 No Apparent Injury 0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Intersection and Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	lice Curve & Level Dry Straight & Level Dry Straight & Level	Daylight Uti	ity Pole m same direction - both going straight - both moving - rear-end m opposite direction - one left turn - one straight	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing	East West West East West North
5985 County Road 5987 County Road	Clallam 94	4120 4120	8.35 No 3759384 8.60 No 3615908	2018-06-23 12:12 No Apparent Injury 0 0 2 0 2017-10-07 15:36 No Apparent Injury 0 0 2 0 2017-02-16 56:58 No Apparent Injury 0 0 2 0 2015-07-17 18:18 No Apparent Injury 0 0 2 0	0 Passenger Car 0 Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway Not at Intersection and Not Related	Clear or Partly Cloudy	Ice Straight & Level	Fro	m opposite direction - one left turn - one straight m opposite direction - all others	Going Straight Ahead Going Straight Ahead Stopped for Traffic	Making Left Turn Going Straight Ahead	yvest East East West West East East West
5989 County Road 5990 County Pond	Cialiam 94	4120 4120	51840 8.98 No 3202361 51840 8.98 No E43883 9.95 No E43883	2015-06-16 17:33 No Apparent Injury 0 0 2 0 2 0 2015-06-16 17:33 No Apparent Injury 0 0 2 0 2 0 2019-03-29 07-51 No Apparent Injury 0 0 0 2 0 0 2 0 0 2 0 0 2 0 0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Level Dry Curve & Level Dry Straight & Level	Daylight Fro Daylight Fro Daylight Fro	m same direction - both qoing straight - one stopped - rear-end m same direction - both going straight - one stopped - rear-end m same direction - both going straight - both moving - rear-end	Going Straight Ahead	Going Straight Ahead Stopped for Traffic Going Straight Ahead	North East North South North Vehicle Stopped West Fast West Fast
5991 County Road 5992 County Road	Ciallam 94 Ciallam 94 Ciallam 94	4120 4120	9.05 No E910475 51890 9.16 No 3759397 9.18 No 2227727	2015-06-16 17:33 No Apparent Injury 0 0 2 0 2019-03-29 07:51 No Apparent Injury 0 0 2 0 2019-03-21 21:10 No Apparent Injury 0 0 1 0 2015-06-30 12:23 Possible Injury 1 0 1 0	Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		At Intersection and Related At Intersection and Not Related Not at Intersection and Not Related	Clear or Parity Cloudy Clear or Parity Cloudy Clear or Parity Cloudy	Dry Straight & Level	Dark-No Street Lights Ea	m same direction - both going straight - both mowing - rear-end th Bank or Ledge e or Stump (stationary)	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	owny ovargin relead	North South
5992 County Road 5993 County Road 5994 County Road 5995 County Road	Cialiam 94 Cialiam 94	4120 4120	51890 9.19 No E737689	2017-11-20 19:22 No Apparent Injury 0 0 2 0	0 Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related Intersection Related but Not at Intersection	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Level Dry Curve & Level Dry Curve & Level Curve & Level	Dark-Street Lights On En Daylight Fro	ering at angle	Making Left Turn	Making Left Turn Going Straight Ahead	North West West South South West East West
5996 County Road	Clallam 94 Clallam 94	4120 4120	9.24 No E631061	2017-01-02 08:25 No Apparent Injury 0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		At Intersection and Not Related At Intersection and Not Related	Clear or Partly Cloudy	Sand/Mud/Dirt Curve & Level	Daylight Wo	m same direction - both going straight - both moving - sideswipe od Sign Post od Sign Post	Merging (Entering Traffic) Making Right Turn Other*		West South West West
5997 County Road 5998 County Road 5999 County Road 6000 County Road	Cialiam 94 Cialiam 94	4120 4120	51880 9.47 No E461943	2015-09-05 09:05 Possible Injury 1 0 2 0	0 Pickup, Panel Truck or Vanette under 10,000 lb	Pickup, Panel Truck or Vanette under 10,000 lb	At Intersection and Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight Fro	m same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead	Stopped for Traffic	West East Vehicle Stopped Vehicle Stopped West East Stopped Vehicle Stopped
	Cialiam 94	4120 4120		2014-09-04 00:13 No Apparent Injury 0 0 1 0 0 2017-11-17 19:19 No Apparent Injury 0 0 2 0 0 2 0 0 0 0 0 0 0 0 0 0 0 0 0	Passenger Car Passenger Car Passenger Car	Passenger Car	At Intersection and Related At Intersection and Related	Raining	Wet Straight & Level	Dark-No Street Lights En	ering at angle ering at angle ering at angle	Going Straight Ahead Going Straight Ahead Going Straight Ahead Making Right Turn	Going Straight Ahead Making Left Turn Going Straight Ahead	East South North East West South West Ward South West
6001 County Road 6002 County Road 6003 County Road	Cialiam 94 Cialiam 94 Cialiam 94	4120 4120		2017-02-02 11:171 No Apparent Injury 0 0 2 0 2017-06-28 13:07 Unknown 0 0 2 0 2018-05-02 08:00 No Apparent Injury 0 0 3 0 2018-05-02 17:15 No Apparent Injury 0 0 2 0	0 Passenger Car 0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight En	iering at angle ering at angle m same direction - both going straight - both moving - rear-end	Making Right Turn Going Straight Ahead	Going Straight Ahead Going Straight Ahead Slowing	East West North South West Seast West Fast
6004 County Road 6005 County Road	Cialiam 94	4120 4120	9.68 No E802868 51850 9.68 No E802868	2018-05-23 17:15 No Apparent Injury 0 0 2 0 0 2018-05-29 10:36 No Apparent Injury 0 0 2 0 0 2 0 0 0 0 0 0 0 0 0 0 0 0 0	0 Passenger Car 0 Passenger Car			Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight En	m same careties — som going statight - both moving - real-ciril.	Going Straight Ahead Making Right Turn Going Straight Ahead	Going Straight Ahead Going Straight Ahead	West South North South South North Fast West
6006 County Road 6007 County Road	Clallam 94 Clallam 94	4120 4120	51650 9.68 No E824319 51650 9.68 No E981603	2018-07-17 13:03 No Apparent Injury 0 0 2 0 2019-11-03 14:45 No Apparent Injury 0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight En	ering at angle ering at angle	Going Straight Ahead	Going Straight Ahead	East West South North North South West East
6008 County Road 6009 County Road	Cialiam 94 Cialiam 94	4120 4120	51650 9.68 No EA18026 9.68 No 2728013	2017-02-02 11:17 No Apparent Injury 0 0 2 0 2017-11-30 17:54 Possible Injury 1 0 2 0	0 Passenger Car 0 Passenger Car		At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Wet Straight & Level	Daylight En	ering at angle ering at angle	Going Straight Ahead Making Right Turn Starting in Traffic Lane	Going Straight Ahead Going Straight Ahead Starting in Traffic Lane	West South North South East West South North
6010 County Road 6011 County Road	Cialiam 94 Cialiam 94	4120 4170	9.68 No 3759383 94120 0.00 No E951112	2017-10-04 17:39 Suspected Minor Injury 1 0 1 0 2019-08-06 15:30 Suspected Minor Injury 2 0 2 0 2017-09-28 12:50 No Apparent Injury 0 0 2 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Vel Daylight En	ering at angle	Going Straight Ahead	Going Straight Ahead Stopped for Traffic	West North South North East West
6012 County Road 6013 County Road 6014 County Road	Cialiam 94 Cialiam 94	4170 4170	0.01 No 3759402 0.02 No E339705	2017-09-28 12:50 No Apparent Injury 0 0 2 0 2 0 2014-05-19 16:54 No Apparent Injury 0 0 2 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car Truck (Flatbad, Van, etc)	Intersection Related but Not at Intersection At Driveway	Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Grade	Daylight Fro	m same direction - both going straight - one stopped - rear-end m same direction - one right turn - one straight	Overtaking and Passing	Making Right Turn	North South North South South North South East
6015 County Road	Cialiam 94 Cialiam 94	4170 4170	0.97 NO 3799419 1.41 No 320238 0.4420 1.47 No 2202182	2014-05-19 16:54 No Apparent Flury	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet Straight & Level Wet Straight & Level Dry Straight & Level	Daylight En Dark-No Street Lights Ro Daylight En	ering at angle add av Dirch	Backing Going Straight Ahead Going Straight Ahead	Going Straight Ahead Going Straight Ahead	South North South South North South North South North South North Mart East
6017 County Road 6018 County Road	Cialiam 94 Cialiam 94	4170 4170	95120 1.58 No 3202197 95000 2.23 No 2767055	2015-11-03 07:11 No Apparent Injury 0 0 1 0 2017-10-27 15-47 No Apparent Injury 0 0 2 0	Passenger Car Passenger Car	Passenger Car	Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Uti			Going Straight Ahead	North South West East
6019 County Road 6020 County Road	Cialiam 94 Cialiam 94	4170 4170	95000 2.23 No 2767156 95000 2.23 No 3615704	2016-04-19 14:00 Unknown 0 0 1 0 2014-04-17 17:08 No Apparent Injury 0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Overcast	Dry Straight & Level	Daylight Gu	ardrail - Face ering at angle	Going Straight Ahead Making Right Turn Making Left Turn Going Straight Ahead Making Picht Turn	Going Straight Ahead	South North East South South North
6021 County Road 6022 County Road	Cialiam 94 Cialiam 94	4170 4170	45350 3.20 No 3803645 45350 3.22 No E731600	2017-10-27 15-37 No. Apparent highery	 Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb 		Not at Intersection and Not Related At Intersection and Not Related	Clear or Partly Cloudy Overcast	Dry Straight & Level Dry Curve & Level Wet Curve & Level	Daylight Ro				North East South East
6023 County Road 6024 County Road	Cialiam 94 Cialiam 94	4170 4170	45350 3.22 NO E731002	2017-10-16-10-46 Died II Hospital I I 2 0	O Dielos Desel Trusk as Venette under 10,000 lb	Pickup, Parier Truck or varietie drider 10,000 ib	Not at Intersection and Not Related At Intersection and Not Related	Raining Clear or Partly Cloudy	Wet Curve & Level Dry Curve & Level	Dark-No Street Lights Ro	m opposite direction - all others adway Ditch	Going Straight Ahead Going Straight Ahead	Going Straight Ahead	South North East West South North
6025 County Road 6026 County Road	Ciallam 94 Ciallam 94 Ciallam 94	4270 4270	44730 0.51 No 375935	2016-12-30 09:23 No Apparent Injury 0 0 1 0 2 0 2017-07-22 14:47 Suspected Minor Injury 1 0 2 0	0 Passenger Car 0 Passenger Car	Motorcycle Motorcycle	Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level	Daylight Fro	m same direction - one right turn - one straight	Going Straight Ahead Making Right Turn	Going Straight Ahead Overtaking and Passing	South North South North South North South North
6026 County Road 6027 County Road 6028 County Road 6029 County Road	Cialiam 94 Cialiam 94	4270 4270 4270	447,30 U.51 NO E597,870 45520 U.63 NO E893496 U.63 NO E89340 U.63 NO E89340 U.63 U.63 NO E89340 U.63 U.63 U.63 U.63 U.63 U.63 U.63 U.63	2017-07-22 1447 Suspended Minor Injury 1 0 2 0 0 2 0 0 1019-124 1150 Ulminows 0 0 0 2 0 0 0 2 0 1019-124 1150 Ulminows 0 0 0 0 2 0 1019-126 1150 Ulminows 0 0 0 0 2 0 1019-126 1150 Ulminows 0 0 0 0 3 0 0 0 1019-127 1150 1150 Ulminows 0 0 0 0 3 0 0 0 0 1019-1170 1150 No Apparent Injury 0 0 0 3 0 0 1019-1170 1150 No Apparent Injury 0 0 0 2 0 1019-12-10 1200 Possible Injury 1 0 2 0 0 1019-1170 1150 Ulminows 0 1019-1170 Injury 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related Not at Intersection and Not Related	Clear or Partly Cloudy Snowing Clear or Partly Cloudy	loe Straight & Level Dry Straight & Level	Daylight Fro	zeing at angle m same direction - one right turn - one straight m opposite direction - both going straight - sideswipe	Making Right Turn Making Left Turn Going Straight Ahead Going Straight Ahead	Going Straight Ahead Making Right Turn Going Straight Ahead	East South South North South North South East South North North South
6030 County Road 6031 County Road	Cialiam 94 Cialiam 94	4270 4270	0.75 No E493490 45520 0.87 No 3615937	2015-12-10 12:00 Possible Injury 1 0 2 0 2016-01-21 16:29 Suspected Minor Injury 1 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car Passenger Car	Not at Intersection and Not Related At Intersection and Related	Raining	Wet Straight & Level Wet Straight & Level	Daylight Fro	m same direction - one right turn - one straight		Making Right Turn Making Left Turn	North South North West East South North East
6032 County Road 6033 County Road	Cialiam 94 Cialiam 94	4270 4270	0.88 No E49083 1.10 No 2767165	2015-11-22 09:30 Unknown 0 0 2 0 2016-05-10 15:00 No Apparent Injury 0 0 3 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight Fro Daylight Fro	m same direction - both going straight - one stopped - rear-end m same direction - both going straight - one stopped - rear-end	Making Left Turn Making Left Turn Going Straight Ahead	Going Straight Ahead Stopped for Traffic	South West South North North South North South
6034 County Road 6035 County Road	Cialiam 94 Cialiam 94 Cialiam 94	4270 4270	1.20 No 3759310 1.25 No 2727925	2017-07-07 15:45 No Apparent Injury 0 0 2 0 2 0 2018-12-04 17:52 No Apparent Injury 0 0 1 0 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Driveway	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight En	ering at angle ow Bank		Going Straight Ahead	East South South North South East
6036 County Road 6037 County Road	Clallam 94	4270 4270	94120 1.76 No E426333	2015-12-10 12-00 Possible Injury 1 0 2 0 0 0 0 0 0 0 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy	Dry Straight & Level	Daylight En	adway Ditch ering at angle	Making Right Turn Going Straight Ahead Making Left Turn	Going Straight Ahead	South North East South South North
6039 County Road 6040 County Road	Clallam 94 Clallam 94 Clallam 94	4290 4290	0.46 No E622108 45520 0.63 No E893494	2016-01-27 23:30 Unknown 0 0 1 0 2019-02-08 10:54 No Apparent Injury 0 0 2 0 2018-01-24 18:12 No Apparent Injury 0 0 2 0 2016-03-04 13:10 No Apparent Injury 0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Intersection and Related	Overcast Snowing	Wet Straight & Level	Dark-No Street Lights Ma Daylight En	ering at angle libox ering at angle	Making Left Turn Going Straight Ahead Making Right Turn	Going Straight Ahead Stopped at Signal or Stop Sign	North South South East Vehicle Stopped Vehicle Stopped
6041 County Road 6042 County Road	Clallam 96 Clallam 96 Clallam 96	5000 5000	0.01 No 2729770 0.31 No E521410	2018-11-24 18:12 No Apparent Injury 0 0 2 0 2018-03-04 13:10 No Apparent Injury 0 0 1 0 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb		Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Dusk Fro	m same direction - both going straight - both moving - rear-end libox	Slowing Going Straight Ahead	Going Straight Ahead	East West East West East West
6042 County Road 6043 County Road 6044 County Road 6045 County Road 6046 County Road	Cialiam 96 Cialiam 96	5000 5000	0.46 No E992857 0.50 No E674416	2019-11-22 06:30 Possible Injury 1 0 1 0 0 1 0 2017-05-16 11:15 Suspected Minor Injury 1 0 1 0 0 1 0 0 1014-05-10 16:00 No Apparent Injury 0 0 1 0 0 10 0 10 0 10 0 10 0 10 0 10	Other Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related At Intersection and Not Related	Unknown Clear or Partly Cloudy	Unknown Dry Straight & Level	Unknown Ro Daylight Fer	adway Ditch nce	Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East East West
6045 County Road 6046 County Road	Cialiam 98 Cialiam 98	5000	0.66 No 3616030 0.75 No 272724	2014-05-10 16:00 No Apparent Injury 0 0 1 0 2015-08-27 20:42 No Apparent Injury 0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy			nicle Strikes Deer	Going Straight Ahead		East West East West
6048 County Road	Cialiam 95 Cialiam 95 Cialiam 96	5000 5000	45650 0.88 No 2728113	2015-08-13 23:23 Suspected Minor Injury 1 0 1 0	0 Passenger Car		Not at Intersection and Not Related At Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Vel Dark-No Street Lights Tre Daylight Vel	inde stakes Deer e or Stump (stationary) ide Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead		East West West West West
6049 County Road 6050 County Road 6051 County Road 6052 County Road 6053 County Road 6054 County Road 6054 County Road 6055 County Road	Cialiam 96 Cialiam 96	5000	1.21 No E724447 2.12 No E49084	2014-06-22 (08-11 No Apparent Injury	Passenger Car Pickup Panel Truck or Vanette under 10.000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight Fro	m same direction - one left turn - one straight adway Ditch	Going Straight Ahead Going Straight Ahead	Stopped for Traffic	West East Vehicle Stopped Vehicle Stopped East West
6052 County Road 6053 County Road	Cialiam 96 Cialiam 96 Cialiam 96 Cialiam 96 Cialiam 96	5000 5000	2.13 No 3615727 2.14 No 3615730	2018-09-15 13:42 No Apparent Injury 0 0 2 0 2 0 2019-03-18 19:50 No Apparent Injury 0 0 1 0	0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level	Dusk Fe	m same direction - both going straight - both moving - rear-end nce	Going Straight Ahead Going Straight Ahead	Slowing	West East West East West Southwest
6054 County Road 6055 County Road	Cialiam 90	5000	46110 2.24 No E398378	2015-01-22 14:15 No Apparent Injury 0 0 1 0	0 Passenger Car		Not at Intersection and Not Related At Intersection and Related	Raining Overcast	Wet Straight & Level Wet Straight & Level	Daylight Ro	adway Ditch	Going Straight Ahead Making Left Turn Making Left Turn	Going Straight Ahead	Sourn East West East West North
6055 County Road 6056 County Road 6057 County Road 6058 County Road	Clallam 96 Clallam 96 Clallam 96	5000 5000	46110 2.24 No E652517 2.28 No E995435 2.29 No 3302400	2017-02-23 14:59 No Apparent Injury 0 0 2 0 2019-12-19 07:28 Unknown 0 0 1 0 0 2014-02-19 08:00 Suspected Minor Injury 1 0 1 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear Clear or Partly Cloudy	Wet Straight & Level loe loe Straight & Level	Dawn Bri Dawlight n.:	ering at angle dge Rail - Face dge Rail - Face	Making Left Turn Going Straight Ahead Going Straight Ahead	Going Straight Ahead	East West East West
6059 County Road 6060 County Road	Clallam 95	5000 5000	250 No 2729771	2019-11-23 18:19 No Apparent Injury 0 0 1 0	Passenger Car	Passenger Car	Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy	Dry Straight & Grade	Dark-No Street Lights Vel	nicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped in Roadway	East West East West Vehicle Stopped Vehicle Stopped
6059 County Road 6060 County Road 6061 County Road 6062 County Road	Clallam 96 Clallam 96 Clallam 96	5000 5000	2.00 NO EXTENSION 2.20 NO 2727891 2.77 No 2727649	2019-12-23 18:22 No Apparent Injury 0 0 2 0 2016-09-09 18:53 No Apparent Injury 0 0 1 0 2015-12-22 06:49 No Apparent Injury 0 0 1 0	Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related At Intersection and Not Related	Fog or Smog or Smoke Clear or Partly Cloudy Clear or Partly Cloudy	Ice	Ro	adway Ditch	Going Straight Ahead Going Straight Ahead Going Straight Ahead		East West East West
	Cialiam 95 Cialiam 95	5000 5000	2.97 No 2727904 3.02 No E804732	2016-0-0-0 118-53 Ne Apparent Isiury 0 0 0 1 0 0 1 0 0 1 0 0 1 0 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 0 1 0 0 0 0 1 0 0 0 0 1 0	0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Level		nicle Strikes Deer m opposite direction - one left turn - one straight	Going Straight Ahead	Going Straight Ahead	West East West East
6065 County Road 6066 County Road	Ciallam 96 Ciallam 96 Ciallam 96	5000 5000	52270 3.03 No E387816 52270 3.03 No E387204	2014-12-20 17:15 Suspected Minor Injury 2 0 2 0 2 0 2 2018-09-02 19:15 Suspected Serious Injury 1 0 2 0 0 0 0 0 0 0 0	u Pickup,Panel Truck or Vanette under 10,000 lb 0 Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related	Raining Clear or Partly Cloudy	Dry	Daylight En	ering at angle ering at angle	Making Left Turn Going Straight Ahead Going Straight Ahead Starting From Parked Position	Going Straight Ahead Going Straight Ahead Starting in Traffic Lane	reoran South East West West East South North
6068 County Road	Cialiam 95 Cialiam 95	5000 5000	52270 3.03 No E947460 52270 3.03 No E96551 3.03 No E96551	2018-10-01 08:08 Possible Injury	Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb Motorsvole		At Intersection and Related	Raining Raining Clear or Partly Cloudy	Wet Straight & Level Wet Straight & Level Dry Straight & Level	Daylight En	ering at angle	Going Straight Ahead	Going Straight Ahead	North South East West North South West East Northeast Fast
6070 County Road 6071 County Road	Clallam 96 Clallam 96 Clallam 96	5000 5000	3.03 No 2767027 3.13 No 2767027 3.33 No E777390	2016-10-12 14:50 Possible Injury 1 0 2 0 2016-12-31 22:45 Unknown 0 0 1 0 2018-01-31 16:13 Possible Injury 1 0 1 0	0 Not Stated 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy Unknown Overcast	Snow/Slush Straight & Level Wet Straight & Level	Daylight Fro Dark-No Street Lights Fer Daylight Ro	m same direction - both going straight - both moving - rear-end noe adway Ditch	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Making Right Turn	West East West East
6072 County Road 6073 County Road	Clallam 95	5000 5000	5.3.5 NO E775395 52410 3.53 No E653533	2018-01-31 (6:13) Possible Injury 1 0 1 0 1 0 2 0 0 2017-10-11 (1428) No Apparent Injury 0 0 2 0 0 2017-10-11 (1428) No Apparent Injury 1 0 2 0 0 2017-03-19 (9950) Possible Injury 1 0 0 2 0 0 2017-03-19 (9950) Possible Injury 0 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight Fro	m same direction - one left turn - one straight m same direction - one left turn - one straight	Overtaking and Passing	Making Left Turn Making Left Turn	East West East South East West East South
6075 County Road	Clallam 96 Clallam 96 Clallam 96	5000 5000	4.02 No 3202364 4.02 No 3202365	2017-01-09 09:47 No Apparent Injury 0 0 1 0 0 2017-01-09 09:12 No Apparent Injury 0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level loe Straight & Grade loe Straight & Grade	Daylight Fro	nicle overturned m same direction - both going straight - both moving - rear-end	Overtaking and Passing Going Straight Ahead Going Straight Ahead	Going Straight Ahead	East West East West
6076 County Road 6077 County Road 6078 County Road 6079 County Road	Cialiam 96 Cialiam 96	5000 5000	95250 4.54 No 3202165 95250 4.54 No 3202360	2019-02-22 18:50 No Apparent Injury 0 0 3 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Dark-Street Lights On En	ering at angle	Starting in Traffic Lane	Going Straight Ahead Making Left Turn Making Left Turn	West East South North North East East North County Marth
6079 County Road	Cialiam 95	5250 5250	1.41 No 2457074	2015-12-09 12:03 Possible Injury	0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car.	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Level	Daylight En	m same sanction - point going sealight - one stopped - real-end ering at angle	Making Left Turn Going Straight Ahead Making Left Turn Making Left Turn	Going Straight Ahead	East South South North Fact South South North
6081 County Road 6082 County Road	Cialiam 96 Cialiam 96 Cialiam 96	5250 5250	54400 1.50 No 3616028 1.56 No E86028 1.58 No E663530	2017-05-30 13:40 No Apparent Injury 0 0 2 0 2017-02-22 19:00 No Apparent Injury 0 0 1 1 0	0 Passenger Car 0 Passenger Car	Passenger Car Truck (Flatbad, Van, etc)	At Intersection and Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Raining Unknown	Dry Straight at Hillor Wet Unknown Straight & Grade	Daylight On Dark-No Street Lights Ou	ering at angle parked-one moving r Embankment - No Guardrail Present icle Strikes Deer	Making Left Turn Going Wrong Way on Divided Hwy Going Straight Ahead	Going Straight Ahead Legally Parked, Occupied	East South North North South North South North
6083 County Road 6084 County Road	Cialiam 95	5250 5250	1.59 No E824325 1.60 No E792532	2017-05-30 13:40 No Apparent Injury 0 0 2 0 0 2017-02-22 19:00 No Apparent Injury 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Interrection and Not Related	Clear or Partly Cloudy Overcast			incle Strikes Deer kes or Was Struck by Object from the Load of Another Vehicle	Going Wrong Way on Divided Hwy Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	North South South North
6085 County Road 6086 County Road	Cialiam 95 Cialiam 95	5250 5250	1.61 No 3202390 1.70 No 2767129	2016-04-10 (1-222 No Apparent Injury 0 0 2 0 2 0 2 0 2 0 2 0 2 0 2 0 2 0 2	0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight at Hillor Dry Straight & Grade	Daylight Vel Dark-Street Lights On Vel	nicle Strikes Deer	Going Straight Ahead		South North South North
6087 County Road 6088 County Road	Cialiam 98	5250 5250	1.79 No E951114 54800 2.00 No 2227222	2019-08-10 13:13 No Apparent Injury 0 0 2 0 2 0 2015-07-17 11:19 No Apparent Injury 0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Not at Intersection and Not Related At Intersection and Related	Raining Clear or Partly Cloudy	Dry Straight & Level	Daylight Fro Daylight Fro	m same direction - both going straight - both moving - rear-end m opposite direction - one left turn - one straight	Going Straight Ahead Making Left Turn	Slowing Going Straight Ahead	North South North South North East South North
6088 County Road 6089 County Road 6090 County Road 6091 County Road	Claliam 96 Claliam 96 Claliam 96 Claliam 96 Claliam 96	5250 6250	54800 2.00 No 3202362 54800 2.00 No 3616127 64800 2.00 No 3616127	2015-07-17 11:19 No Apparent Injury 0 0 2 0 02015-07-17 11:19 No Apparent Injury 0 0 2 0 02015-09-21 16:11 No Apparent Injury 0 0 2 0 02014-05-26 11:105 Possible Injury 1 0 2 0 02016-05-01 17:36 No Apparent Injury 0 0 2 0 02016-03-10 17:36 No Apparent Injury 0 0 2 0 02016-03-10 17:36 No Apparent Injury 0 0 0 2 0 0 02016-03-10 17:36 No Apparent Injury 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	u Pickup,Panel Truck or Vanette under 10,000 lb 0 Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight En	kering at angle ering at angle m opposite direction - one left tum - one straight	Making U-Turn Making Right Turn Making Left Turn	Going Straight Ahead Going Straight Ahead Going Straight Ahead	vest East North South East North South North Morth South North
6092 County Road	Cialiam 95	5250 5250	2.01 NO E092002	2015-02-04 11:28 (NO Apparent Injuly 0 0 1 0	o Pickup, Pariet Huck of Varietie diluter 10,000 to		Not at intersection and Not Related	Clear or Partly Cloudy Snowing Clear or Partly Cloudy	loe Straight & Grade	Daylight Ro	adway Ditch	Going Straight Ahead	Going Straight Ahead Going Straight Ahead	North South North South North
6094 County Road 6095 County Road	Clallam 95 Clallam 95 Clallam 95	5250 5250 5250	2.03 No 3618202 2.11 No E652518 2.15 No E47145	2015-11-27 13:27 No Apparent Injury 0 0 2 0 2017-02-16 13:32 Suspected Minor Injury 1 0 1 0 2015-10-06 19:30 No Apparent Injury 0 0 1 0	0 Passenger Car 0 Passenger Car	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related		Dry Straight & Grade Dry Straight & I evel	Daylight Fro Daylight Ea Dark-No Street Lights Vel		Going Straight Ahead Making U-Turn Going Straight Ahead	Going Straight Ahead	South North South North South North North North South
6096 County Road 6097 County Road	Cialiam 96 Cialiam 96	5250 5250	2.15 NO E471445 2.16 No E725646 2.22 No E652034	All Del	Pickup Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Overcast	Dry Straight & Grade Dry Straight & Grade	Daylight Fro	m same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	South North North North South
6098 County Road 6099 County Road	Clallam 95	5250 5250	224 No E960400 51650 225 No E960803	2019-09-30 08-22 No Apparent Injury 0 0 1 0 1 0 2019-08-30 13:30 No Apparent Injury 0 0 2 0	O Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade	Daylight Vel	m same direction - both going straight - one stopped - rear-end ms ame direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead Stopped in Roadway	Stopped at Signal or Stop Sign	
6100 County Road 6101 County Road	Clallam 96 Clallam 96 Clallam 96 Clallam 96	5250 5250	51650 2.26 No 3615858 51650 2.26 No 3616050	2017-10-06 17:24 No Apparent Injury 0 0 2 0 2 0 2015-05-06 17:55 No Apparent Injury 0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related	Overcast Clear or Partly Cloudy	Dry Straight at Hillor Dry Straight & Grade	Daylight Fro Daylight En	m same direction - both going straight - one stopped - rear-end m same direction - both going straight - one stopped - rear-end ering at angle	Making Lett Turn	Going Straight Ahead Going Straight Ahead	Vehicle Backing North Vehicle Backing North West North North South
6102 County Road 6103 County Road	Clallam 96 Clallam 96 Clallam 96 Clallam 96	5250 5250	2.27 No 3616141 2.40 No E993792	2015-00-06 17-56 No Apparent Injury 0 0 2 0 0 2014-09-22 025 No Apparent Injury 0 0 1 0 0 2014-09-22 025 No Apparent Injury 0 0 1 0 0 2019-12-16 [23-55 No Apparent Injury 0 0 1 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 1 0 0 0 0 1 0	0 Passenger Car 0 Passenger Car	Dieles Denet T. 1 124 124	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear	Dry Straight & Level Dry Straight & Grade	Dark-Street Lights On Vel Dark-No Street Lights Uti	ering at angle isles Sinkes Deer it iy Pole m opposite direction - all others	Going Straight Ahead Making U-Turn		North South South South
6105 County Road	Clallam 95	5250 6250	2.46 No 272011 95000 2.51 No E885069 67200 2.52 No E790716	2018-11-23 13:49 INO Apparent Injury 0 0 2 0 2 0 2 2018-11-23 13:43 Possible Injury 1 0 2 0 2 0 2018-01-20 18:18 Support 155-15 2018-01-20 18:18 Support 155-15 2018-01-20 18:18 Support 155-15 2018-01-20 18:18 Support 155-15	Passenger Car Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Page appar Car.	At Intersection and Related At Intersection and Related	Overcast Clear or Partly Clouds	wet Straight & Level	Daylight En	ering at angle	Going Straight Ahead Making Right Turn	Going Straight Ahead Going Straight Ahead Making Left Turn	West South North South North West South North South
6106 County Road 6107 County Road 6108 County Road	Clallam 96 Clallam 96 Clallam 96 Clallam 96	5250 5250	52500 2.52 No E399715 95000 2.52 No 2227715 95000 2.52 No 2227715	2015-01-30 15:15 Suspected Minor Injury 1 0 2 0 2015-05-18 17:15 No Apparent Injury 0 0 2 0 2015-05-15 11:09 No Apparent Injury 0 0 3 0 0 3 0 0 0 0 0 0 0 0 0 0 0 0 0	0 Passenger Car 0 Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Raining Clear or Partly Cloudy	Dry Straight & Grade Wet Straight & Level Dry Straight & Level	Daylight Fro	m opposite direction - one left turn - one straight m opposite direction - one left turn - one straight m opposite direction - one left turn - one straight	Going Straight Ahead Making Left Turn Making Left Turn	Making Left Turn Going Straight Ahead Going Straight Ahead	South West North South South West North
6109 County Road 6110 County Road	Clallam 95	5250 5250	95000 2.52 No 2727905 95000 2.52 No 2767153 95000 2.52 No 3202092	2016-03-15 113:09 No apparent rijury 0 0 3 0 2016-03-29 15:57 Suspected Minor Injury 1 0 2 0 2014-11-14 18:50 Possible Injury 1 0 2 0	0 Passenger Car 0 Passenger Car			Clear or Partly Cloudy	Dry Straight & Level	Daylight En	ering at angle	Making Left Turn	Going Straight Ahead	North North South
6109 County Road 6110 County Road 6111 County Road 6112 County Road 6112 County Road 6113 County Road	Clallam 96 Clallam 96 Clallam 96 Clallam 96	5250 5250	95000 2.52 NO 302014 95000 2.52 No 302144 95000 2.52 No 3615829	2016-02-03-18-57 Suspected Monor Houry 0 0 2 0 0 2 0 0 0 0	0 Passenger Car 0 Passenger Car	Passenger Car Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight En	ering at angle ering at angle ering at angle	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead Making Left Turn Going Straight Ahead	West East South West West East South North
6113 County Road 6114 County Road	Cialiam 96 Cialiam 96 Cialiam 96	5250 5250	95000 2.52 No 3615999 95000 2.52 No 3759372	2017-10-13 14:20 Possible Injury 1 0 2 0 2018-07-30 14:56 No Apparent Injury 0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight En Daylight En	ering at angle	Going Straight Ahead	Going Straight Ahead	East West North South South North East West
6114 County Road 6115 County Road 6116 County Road 6117 County Road 6117 County Road 6118 County Road	Clallam 95	5250 5250	95000 2.52 No E633222 95000 2.52 No E731601	2017-01-03 09:20 Unknown 0 0 2 0 2017-10-24 09:16 Suspected Minor Injury 2 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight En	ering at angle ering at angle	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead Going Straight Ahead Going Straight Ahead	North South West East West East North South
	Clallam 95	02.00	95000 2.52 No E759209	2010 08 07 17 10 Pusselle Figury 1 0 2 0	O Description of Variette under 10,000 lb	District Penni Truck or vanette under 10,000 lb	A later seties and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry	Daylight Fro	m opposite direction - one left turn - one straight m opposite direction - one left turn - one straight	Going Straight Ahead Going Straight Ahead	Going Straight Ahead Making Left Turn	North South South West

MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_1_UNIT_1 5939 Exceeding Reas. Safe Speed	MV_DRIVER_CONTRIBUTING	MV_DRIVER_CONTRIBUTING MV_DRIVER MV_DRIVER MV_DRIVER	ER MV_DRIVER_CONTRIBUTING MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_1_UNIT_2 CIRCUMSTANCE_2_	IBUTING MV_DRIVER_CONTRIBUTING MV_DRIVE	MV_DRIVER	BICYCLIST_CONTRIBUTING CIRCUMSTANCE_1_UNIT_1 BICYCLIST_CONTRIBUTING CIRCUMSTANCE_2_UNIT_1	BICYCLIST_CONTRIBUTING BICYCLIST	T BICYCLIST BICYCLIST_CONTRIBUTING BICYCLIST_CONT	RIBUTING BICYCLIST_CONTRIBUTING BI	BICYCLIST	BICYCLIST PEDESTRIAN_CONTRIBUTING PEDESTRIAN_CONTRIBUTING CIRCUMSTANCE_1_UNIT_2 CIRCUMSTANCE_2_UN	ITING PEDESTRIAN_CONTRIBUTING PEDESTRIAN PEDESTRIAN	FIRST_IMPACT_LOCATION
	CIRCUMSTANCE_2_UNIT_1		CIRCUMSTANCE_1_UNIT_2 CIRCUMSTANCE_2_ Driver Not Distracted		2 UNIT_2 36 Female	CIRCUMSTANCE_1_UNIT_1 CIRCUMSTANCE_2_UNIT_1	CIRCUMSTANCE_3_UNIT_1 UNIT_1	UNIT_1 CIRCUMSTANCE_1_UNIT_2 CIRCUMSTANCE_	_UNIT_2 CIRCUMSTANCE_3_UNIT_2 AG	GE_UNIT_2	GENDER_UNIT_2 CIRCUMSTANCE_1_UNIT_2 CIRCUMSTANCE_2_UN		City_St_County_Rd_&_Misc_2010_forward_St ane of PrimaryTraflicway
5939 Exceeding Reas. Safe Speed	Inattention Did Not Grant RW to Vehicle Disreg	81 Male	Driver Not Distracted		47 Female 16 Female								ane of Primary Trafficway ast the Outside Shouldar of Primary Trafficway ane of Primary Trafficway ane of Primary Trafficway
5943 Did Not Grant RW to Vehicle 5944 None 5945 Inattention		86 Male 18 Male 76 Male	Oriver Not Distracted Instention Did Not Grant RW to Vehicle None		42 Male 42 Male 17 Female 29 Female								ane of Primary Trafficway
5946 Distractions Outside Vehicle 5947 Did Not Grant RW to Vehicle 5948 Improper Turn/Merge	Other Contributing Circ Not Listed Did Not Grant RW to Vehicle				29 Female 66 Male 63 Male								ane of Primary Trafficway
5949 None 5950 Other Contributing Circ Not Listed 5951 Exceeding Stated Speed Limit		56 Female 38 Female 17 Female	Driver Not Distracted										ane of Primary Trafficway ane of Primary Trafficway ane of Primary Trafficway Trafficway Trafficway Trafficway Trafficway
5951 Exceeding Stated Speed Limit 5952 Other Contributing Circ Not Listed 5953 Apparently II 5954 Instention		69 Male 46 Male 77 Male	Other Contributing Circ Not Listed		20 Male 62 Female								ane of Primary Trafficway sat the Outside Shoulder of Primary Trafficway ane of Primary Trafficway ane of Primary Trafficway
5955 Improper Turn/Merge 5956 Under Influence of Alcohol 5957 None 5958 Driver Not Distracted	Inattention Under Influence of Drugs Did No	80 Female ot Grant RW to Vehicle 34 Female	Other Contributing Clies Not Lisited Other Not Distracted None Other Not Distracted None Other Not Distracted Distracted Distracted Other Contribution Other Not Distracted Other Contribution Other Contribution		63 Female								ane of Primary Trafficway
5957 None 5958 Driver Not Distracted 5959 Exceeding Reas, Safe Speed 5960 Driver Not Distracted		26 Male 29 Male	None Did Not Grant RW to Vehicle Over Center Line		45 Male 64 Male 63 Male								ane of Primary Trafficway tersecing Trafficway ane of Primary Trafficway rast the Outside Shoulder of Primary Trafficway
5962 Disregard Stop Sign - Flashing Red	Distractions Outside Vehicle Operating Handheld Cell Phone Inatten	54 Male 71 Female 1100 24 Female 24 Female 82 Male	None		61 Female								vast the Outside Shoulder of Primary Trafficovay hast the Outside Shoulder of Primary Trafficovay ane of Primary Trafficovay
5963 Inattention 5964 Did Not Grant RW to Vehicle 5965 Disregard Stop Sign - Flashing Red 5966 Under Influence of Alcohol		82 Male 79 Female 35 Female 38 Male	None None None		27 Male 54 Male 53 Male 50 Female								ane of Primary Trafficway
5966 Under Influence of Alcohol 5967 Inattention 5968 Under Influence of Alcohol	Inattention Disregard Stop Sign - Flashing Red	82 Female	Driver Not Distracted		50 Female 73 Male 68 Male								ane of Primary Trafficway ane of Primary Trafficway ane of Primary Trafficway
5967 Inattention 5968 Under Influence of Alcohol 5969 Under Influence of Alcohol 5970 Driver Not Distracted 5971 Apparently Asleep or Fatigued		21 Male 59 Male 81 Male	Driver Not Distracted Inattention		67 Male								Jutside Shoulder of Primary Trafficway ane of Primary Trafficway ast the Outside Shoulder of Primary Trafficway
5972 Exceeding Reas. Safe Speed 5973 None 5974 Other Contributing Circ Not Listed	Exceeding Reas. Safe Speed	46 Male 71 Male	Oner Not Distanced Institution None Description Description Description Description		82 Male								ane of Primary Trafficway
5976 Exceeding Reas, Safe Speed 5976 Distractions Outside Vehicle 5977 Distractions Outside Vehicle		60 Male 62 Male	Driver Not Distracted		38 Female 66 Male								ane of Primary Trafficway ast the Outside Shoulder of Primary Trafficway asts the Outside Shoulder of Primary Trafficway ane of Primary Trafficway
5978 Other Contributing Circ Not Listed	Follow Too Closely	18 Female 48 Male 60 Female	Driver Not Distracted None None		56 Male								ane of Primary Trafficway
5980 None 5981 Disregard Stop Sign - Flashing Red 5982 Other Contributing Circ Not Listed 5983 None		83 Female 24 Female	None None		72 Female 59 Female								ane of Primary Trafficway ane of Primary Trafficway Trafficway ane of Primary Trafficway) ane of Primary Trafficway
EDDE Fellow Ton Classic		92 46 Male 94 Male	Did Not Grant RW to Vehicle		20 Female 30 Male								ast the Outside Shoulder of Primary Trafficway
EDDY Franchise Dans Cafe Canad	Over Center Line	80 Male 27 Male	Distractions Outside Vehicle Note Note that the state of		30 Male 24 Female 73 Male 30 Female								ane of Primary Trafficway
Exceeding reas. Sale speed	Expanding Para Cole County	65 Male	None		30 Female 55 Male 54 Male								ane of Primary Trafficway and the Outside On Outside O
5993 Other Contributing Circ Not Listed	Exceeding Reas. Safe Speed	78 Male			19 Female								ast the Outside Shoulder of Primary Trafficway ane of Primary Trafficway
5996 Under Influence of Drugs	Exceeding Stated Speed Limit	66 Male 78 Male 75 Male	Driver Not Distracted None		20 Male								ane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
5998 Under Influence of Alcohol 5999 Did Not Grant RW to Vehicle	Other Driver Distractions Inside Vehicle Inattention Operat Disregard Stop Sign - Flashing Red Other (33 Female	None		53 Maie 17 Maie 17 Maie								ane of Primary Trafficway ast the Obtside Shoulder of Primary Trafficway ane of Primary Trafficway ane of Primary Trafficway
		52 Maie 90 Maie 56 Female	None None Driver Not Distracted		68 Male								ane of Primary Trafficway ane of Primary Trafficway ane of Primary Trafficway
6003 Inattention 6004 Did Not Grant RW to Vehicle 6005 Disregard Ston Ston - Flagbling P - 4	Follow Too Closely Inattention		None Driver Not Distracted None Driver Not Distracted None Driver Not Distracted None		66 Male 64 Male 53 Male 41 Male								ane of Primary Trafficuray
9000 bistegard sop Sagni-riassing ked 9001 bid No Grant RW to Whicle 9002 bisregard Sop Sign - Flashing Red 9003 bisregard Sop Sign - Flashing Red 9005 bisregard Sop Sign - Flashing Red 9005 bisregard Sop Sign - Flashing Red 9005 bisregard Sop Sign - Flashing Red 9006 bid Not Grant RW to Whicle 9007 bisregard Sop Sign - Flashing Red 9007 bisregard Sop Sign - Flashing Red 9007 bisregard Sop Sign - Flashing Red 9008 bid Not Grant RW to Webicle 9008 bid Not Grant RW to Webicle	Inattention	95 Female 61 Female 18 Female 90 Male 78 Female			41 Male 37 Female 75 Male 68 Male								ane of Primary Trafficway
	Inattention Other Contributing Circ Not Listed	78 Female 40 Female	None None		43 Female						Male		ane of Primary Trafficway
6010 Did Not Grant RW to Vehicle 6011 Did Not Grant RW to Vehicle 6012 Follow Too Closely 6013 Improper Passing	Inattention	50 Female 67 Female 38 Male	None Driver Not Distracted None		61 Female 30 Male 46 Male								ane of Primary Trafficway ane of Primary Trafficway ane of Primary Trafficway ane of Primary Trafficway
6014 Inattention 6015 Over Center Line 6016 Did Not Grant RW to Vehicle	Unknown Distraction Operat Inattention	ting Defective Equipment 34 Male	Driver Not Distracted		28 Female 66 Male								ane of Primary Trafficway Past the Ousside Shoulder of Primary Trafficway ane of Primary Trafficway
6017 Apparently As leep or Fatigued 6018 None 6019 Improper Turn/Merge 6020 Inattention	Unknown Distraction				17								
6021 Under Influence of Alcohol		79 Male 15 Female 76 Male	Disregard Stop Sign - Flashing Red None		30 Female								ass tel Culsante attituate un immary ratincinary ann of Primary Trafforway ten excing Trafforway ten excing Trafforway ann of Primary Trafforway ast the Outside Shoulder of Primary Trafforway ast the Outside Shoulder of Primary Trafforway ast the Outside Shoulder of Primary Trafforway
6022 Other Contributing Circ Not Listed 6023 Under Influence of Alcohol 6024 Operating Defective Equipment 6025 None		76 Male 50 Male 29 Male 22 Female 20 Male 22 Female 20 Male 22 Female 20 Male 22 Female 20 Male 20 Mal	None		61 Male								asa tale Outstode officiale or imitaaly traincinary and of Primary Traillicovay ast the Outstide Shoulder of Primary Traillicovay and of Primary Traillicovay and of Primary Traillicovay
		22 Female 29 Male 16 Male	inatention Improper Passing None None None		68 Male 77 Female								
6027 Did Not Grant RW to Vehicle 6028 Exceeding Reas. Safe Speed 6029 Over Center Line 6030 Exceeding Reas. Safe Speed					77 Female 55 Female 72 Male 64 Male								ane of Primary Trafficway
6030 Exceeding Reas. Safe Speed 6031 Did Not Grant RW to Vehicle 6032 Driver Not Distracted 6033 Follow Too Closely	Inattention Inattention	76 Male 34 Female 25 Female	None Distractions Outside Vehicle None Oriver Not Distracted		17 Female								ane of Primary Trafficway ane of Primary Trafficway ane of Primary Trafficway
6034 Did Not Grant RW to Vehicle 6035 Inattention 6036 Apparently III 6037 None	Inattention	53 Female 54 Female 73 Female	Driver Not Distracted		30 Male 60 Male								ane of Primary Trafficway ast the Outside Shoulder of Primary Trafficway asts the Outside Shoulder of Primary Trafficway ane of Primary Trafficway
					72 Female 65 Male								
903B Disregard Slop Sean - Histhing Red 9 904 Chee Chee Chee Chee Chee Chee Chee Che		55 Female 64 Female 41 Female	None Under Influence of Alcohol		76 Male 33 Female								and Co Findley Hausday Trafficway tetre octing Trafficway and of Pimary Trafficway and of Pimary Trafficway and of Pimary Trafficway ast the Outside Shoulder of Pimary Trafficway
6043 None 6044 Apparently As leep or Fatigued 6045 Driver Not Distracted		62 Male 66 Female											as it in Custode Shoulder of Primary Trafficway ast the Outside Shoulder of Primary Trafficway ast the Outside Shoulder of Primary Trafficway and of Primary Trafficway and of Primary Trafficway and of Primary Trafficway
6046 Other Contributing Circ Not Listed 6047 None		47 Female 74 Male											ane of Primary Trafficway
6049 None 6050 Driver Adjusting Audio or Entertainmen	ent	74 Male 33 Male 61 Female 30 Female			65 Male								hast the Outside Shoulder of Primary Trafficuray ane of Primary Trafficuray ane of Primary Trafficuray
6051 Driver Not Distracted 6052 Inattention 6053 Distractions Outside Vehicle	Unknown Distraction	31 Female 19 Male 17 Female	Driver Not Distracted None		49 Male								rast the Outside Shoulder of Primary Trafficway ane of Primary Trafficway stat the Outside Shoulder of Primary Trafficway ane of Primary Trafficway
5049 None	inattention	19 Male 18 Female 46 Male	None None		41 Male 17 Male								ane of Primary Trafficway ant the Outside Folduler of Primary Trafficway ane of Primary Trafficway butside Stoodard or Primary Trafficway butside Stoodard or Primary Trafficway butside Shootafe of Primary Trafficway
6057 None 6058 Exceeding Reas. Safe Speed 6059 None		18 Female 18 Female 46 Male 64 Female 20 Female 59 Female											
6060 Exceeding Reas. Safe Speed 6061 None 6062 None		68 Male 49 Male) The rec		24 Female								ane of Primary Trafficway ane of Primary Trafficway ant the Outside Shoulder of Primary Trafficway
6063 None 6064 Other Contributing Circ Not Listed 6065 Did Not Grant RW to Vehicle	Inattention Did No	74 Male ot Grant RW to Vehicle 66 Female 71 Female	None None Did Not Grant RW to Vehicle None		35 Male 78 Female								ane of Primary Trafficway
6066 None 6067 hattention 6068 Disregard Ston Ston - Flagbing Prof	Unknown Distraction	48 Male 73 Female	Did Not Grant RW to Vehicle None None		76 35 Male 38 Male								ane of Primary Trafficway
enen la attactic	THE OWNER OF THE OWNER				-Jimen								ane of Primary Traticusy ast the Outside Shoulder of Primary Trafficway ast the Outside Shoulder of Primary Trafficway ane of Primary Trafficway ane of Primary Trafficway
6070 Unknown Distraction	Institution	27 Male 28 Male											
patientson Unknown Distraction Cother Contributing Circ Not Listed Cother Contributing Circ Not Listed Improper Passing Cother Contributing Cother Contributing Cother Contributing Cother Contributing Cother Contributing	Inattention Had To Inattention	27 Male 28 Male 3ken Medication 66 Female	Driver Not Distracted None		22 Male 56 Female								ast the Outside Shoulder of Primary Tramovay ane of Primary Trafficway ane of Primary Trafficway
nattenson	Inattention Had To Inattention	27 Male 28 Male 3ken Medication 66 Female	Driver Not Distracted None None None None		22 Male 56 Female 21 Male 63 Male								and to Primary I ratioway ast the Outside Shoulder of Primary Trafficway and of Primary Trafficway and of Primary Trafficway
susue principle of the property of the propert	Inattention Had Tr Inattention	27 Motion 28 Motion 38 Motion 48 Motion 48 Motion 49 Female 40 Female 47 Motion 47 Motion 47 Female 47 Female	Umer Not Distracted None None None None None One None One None One One One One One One One One One O		22 Male 56 Female 21 Male 63 Male								are to Primary Tradecay are to Chamber Tradecay are of Primary Tradecay
sweez anamenon Common Distraction Not Listed 2071 Universe Distraction Not Listed 2071 Ohne Constituting Client Not Listed 2071 Memory Plant Street S	Sateriorio Had Ti tatteriorio Interiorio Interiorio Interiorio Interiorio Interiorio	2 dealer	Dinor No Distanced Done No Distanced Notes Done No Distanced		22 Male 56 Female 21 Male								are to Primary Translatory are to Primary Translatory are of Primary Translatory
www. practices of the control of the	National Had To Part T	27 Moles 27 Moles 28 Moles 29 Moles 29 Moles 29 Moles 27 Moles 28 Moles	Dimer Net Destanded Notice Not		22 Maie 56 Female 21 Maie 63 Maie 46 Female 54 Maie 23 Female 54 Maie								Assistant of County Studies of Primary Trafficacy and Primary Trafficacy
www. promotion for the promotion of the	Saturation Had Ty Institution Institution Institution Institution Institution Institution Institution Institution	27 below 38 hand Medication 56 Female 66 Female 66 Female 77 Female 77 Female 78 Female 79 Female 79 Female 70 Mate 70 Mate 70 Mate 70 Mate 70 Mate 70 Mate 70 Female	Direct Feb Oystacked None None None None None Owner None		22 Male 56 Female 21 Male 63 Male 46 Female 58 Male 61 Male 63 Male 44 Female 54 Male 41 Male 77 Female								Text the Coulcus Shoulder of Primary Trafficeary are of Primary Text (Incomy are of Primary are of Pri
processor proces	National Had Till Parkette And Till Parkett A	27 below 38 hand Medication 56 Female 66 Female 66 Female 77 Female 77 Female 78 Female 79 Female 79 Female 70 Mate 70 Mate 70 Mate 70 Mate 70 Mate 70 Mate 70 Female	Direct Feb Oystacked None None None None None Owner None		22 Male 56 Female 21 Male 63 Male 46 Female 58 Male 61 Male 63 Male 44 Female 54 Male 41 Male 77 Female								Test Teo Clauder Shoulder of Primary Trafficuscy and of Primary Testificuscy and of Primary Testificuscy and of Primary Testificuscy test exceeding Testificuscy and of Primary Testificuscy
water provinces of the control of th	National Had To control Had To contr	2 de Marie 2 de Marie 3 de Marie 4 de Francis de 1 Marie 4 de 1 Marie 5 de Marie 5 de Marie 6 de Marie 7 de Marie 7 de Francis de 1 Marie 7 de Marie 7 de Marie 7 de Marie 7 de Marie 8 de Marie 8 de Marie 9 de	Direct Hot Detected		22 Male 56 Female 21 Male 63 Male 63 Male 46 Female 58 Male 61 Male 23 Female 54 Male 41 Male								Test the Colactif Shoulder of Pinnary Trafficeary and of Pinnary Test Rosey
www. promotion beneation between beneating of the beneating of the United States of the States of th	National Had To Part T	2 de de la company de la compa	Direct Moto Destracted Nonce Nonce Nonce Nonce Nonce Over Hot Destracted Direct Not Destracted Direct Not Destracted Nonce Nonce Nonce Nonce Nonce Nonce Nonce Nonce Nonce Nonce Nonce		22 Male 56 Female 21 Male 23 Male 33 Male 63 Male 64 Female 58 Male 61 Male 23 Female 54 Male 41 Male 41 Male 77 Female 77 Female 72 Female 73 Male 74 Male 77 Female 75 Female 76 Male								van teo Colondi Shoulder of Pinnary Trafficeary area of Pinnary Trafficeary are of Pinnary Trafficeary area of Pinnary Trafficeary
second parameters of the control of	National Add To Control Add To Contr	2 de de la company de la compa	Direct Moto Destracted Nonce Nonce Nonce Nonce Nonce Over Hot Destracted Direct Not Destracted Direct Not Destracted Nonce Nonce Nonce Nonce Nonce Nonce Nonce Nonce Nonce Nonce Nonce		22 Male 50 Female 21 Mate 33 Mate 63 Mate 64 Female 55 Mate 61 Mate 23 Female 54 Mate 64 Mate 64 Mate 65 Female 65 Female 65 Female								vast ter Colade Stocider of Primary Trafficency and of Primary Trafficency
608) Naturalism Control of Contro	National Add To	2 de la company	None Notes None None None None None None None None		22 Male 56 Female 21 Male 23 Male 33 Male 63 Male 64 Female 58 Male 61 Male 23 Female 54 Male 41 Male 41 Male 77 Female 77 Female 72 Female 73 Male 74 Male 77 Female 75 Female 76 Male								vast ter Outdet Shoulder of Primary Trafficency and the Trafficency and the Trafficency and the Trafficency and the Trafficency and of Primary Trafficency
water provinces of the control of th	National Add Tr statement statement statement valenders valenders statement stateme	2 de la company	None Notes None None None None None None None None		22 Male 60 Female 21 Mate 60 Male 60 Male 60 Male 61 Male 65 Male								van ter Christel Shoulder of Pinnary Trafficeury and of Pinnary Trafficeury
6101 Inattention 6102 Driver Not Distracted 6103 Other Contributing Circ Not Listed 6104 None 6105 Did Not Grant RW to Vehicle 6106 None 6107 hastention	Improper U-Turn	2 (Marie Land Communication Co	Disner Mot Destructed		22 Male 66 Female 71 Nate 72 Mark 73 Mark 74 Mark 74 Mark 77 Female 77 Mark 77 Female 77 Mark 77 Female 77 Mark 78 Mark 79 Mark 79 Mark 70 Mark 70 Mark 70 Mark 70 Mark 70 Mark 71 Mark 71 Mark 72 Mark 72 Mark								vast ter Colade Shoulder of Primary Trafficesy ans of Primary Trafficesy ans of Primary Trafficesy ans of Primary Trafficesy and of Primary Trafficesy
6103 Other Contributing Circ Not Listed 6104 None 6105 Did Not Grant RW to Vehicle 6106 None 6107 Inattention	Improper U-Turn	2 death of the control of the contro	Shore Net Detected		22 Malte 66 Female 21 Malte 63 Malte 63 Malte 64 Female 64 Malte 64 Female 65 Malte 67 Malte 67 Malte 68 Female 68 Malte 68 Malte 69 Malte 61 Malte 61 Malte 64 Malte 64 Malte 64 Malte 64 Malte 64 Malte 65 Female 65 Malte 65 Female 65 Female 65 Female 66 Malte 67 Malte 68 Female 69 Malte 69 Malte 60 Malte 60 Malte 60 Malte 60 Malte 60 Malte 60 Malte 61 Female 61 Female 61 Female								van teo Cohade Shoulder of Pinnary Trafficusy are of Pinnary Trafficusy
6103 Other Contributing Circ Not Listed 6104 None 6105 Did Not Grant RW to Vehicle 6106 None 6107 Inattention	Improper U-Turn	2 de date 2 de date 3 de date 5 fermate 6 fermate 6 fermate 6 fermate 6 fermate 6 fermate 6 fermate 7 fermate 7 fermate 6 fermate 6 fermate 7 fermate 6 fermate 7 fermate	Direct Hot Detended		22 Male 22 Male 24 Male 25 Female 21 Male 21 Male 26 Female 26 Male 26 Female 36 Male 46 Female 36 Male 46 Female 46 Female 46 Female 41 Male 41 Male 41 Male 42 Female 43 Male 44 Male 45 Male 46 Female 46 Male 47 Female 48 Male 48 Male 48 Male 48 Male 49 Male 48								was the Orbital Shoulder of Primary Trafficusy and of Dirinary Trafficusy and of Dirinary Trafficusy and of Dirinary Trafficusy and of Dirinary Trafficusy and of Primary Trafficusy and of Primary Trafficusy and of Primary Trafficusy and of Dirinary Trafficusy
6050 Destrotion Control of the Control of	Improper U-Turn	alven Medication	Shore Net Detected		22 Male 52 Marie 63 Female 64 Marie 65 Female 66 Female 67 Marie 68 Marie 68 Marie 68 Marie 69 Marie 69 Marie 69 Marie 69 Marie 60 Marie 60 Marie 60 Marie 61 Female 61 Female 63 Female								varia the Coulodd Shoulder of Primary Trafficuscy and of Primary Trafficusc

WA STATE PLANE OBJECTID SOUTH X 2919_FORWARD	COL_SURFACE COL_LOCATION COL_RISKY Intersection Emphasis Roadw Decoded BBWAYIOR Area, Name Departs
	Ico Brow/Wolan Not at hieres circlos Dineway/Roundabout Speeding Yes Dry Not at hieres circlos Dineway/Roundabout No Roksy Driving Behaviors Involved Yes Wet All Dineway or Inters action No Dry All Dineway or Inters action Fall to Widd No No No No No No
	Dry McDrivewayor Intersection Hall to Yield No Dry McDrivewayor Intersection No Risky Driving Behaviors Involved No Dry McDrivewayor Intersection Inatention No
5947 980626.6 1022259.66[2019 12 Vehicles-Parties Angle Minor or Possible Injury Fail to Yield Daylight A Driveway or Intersection Straight & Level Older Road User 65+ Fail to Yield Daylight	Dry At Driveway or Intersection Distracted Driving No Dry At Driveway or Intersection Fail to Yield No
5946 958251.48 102227/2 2017 [2 Vehicles Planes Tuming Monor or Peasable Insury Monor or Peasable Insury<	Dry Not at Intersection/Driveway/Roundabout No Risky Driving Behaviors Involved No Dry Not at Intersection/Driveway/Roundabout No Risky Driving Behaviors Involved Yes
8981 984627/4 10227/28/20 (2016) 1 Whited Park Supplied Opened Designed O	Wet A Dimessay or Interfacetion Speeding Organization Speeding Dry Dimension of Dry Dry Dimension of Dry
985 99729.16 1920/04.1014 [2 Whites Bridge Thomas Tho	
1995 1997	Viet pt Drieway or intersection No Risky Driving senaviors involved No Lights Off Ice/Snow/Stush Not at Intersection/Driveway/Roundabout Speeding Yes Ice/Snow/Stush Not at Intersection/Driveway/Roundabout No Risky Driving Behaviors Involved Yes
9961 9880f.35 10222875 2018 1 WhiteheaParts 2018 A Interaction and Not Related Staglet A Interaction	Wet M Intersection and Not Related Inattention Yes Dry M Driveway or Intersection No Risky Driving Behaviors Involved No Dry At Driveway or Intersection Inattention No
Select S	Wet At Driveway or Intersection Fall to Yeld No Dry 4 Driveway or Intersection No Riskly Driving Behaviors Involved No Dry 4 Driveway or Intersection Under Influence of Alcohol Drugs Medication No Under Influence of Alcohol Drugs Medication No
9947 98866.71 102224 1 3 2016 3 Vehicles Planter More September More of Possible Hour Management More of Possible Hour More of More of Possible Hour More of More of Possible Hour More of More of More of Possible Hour More of More	Dy At Oneway or Intersection Inatention No Inatention No In At Oneway or Intersection Under Influence of Alcohol Drugs Affection No No Wet At Oneway or Intersection Under Influence of Alcohol Drugs Affection No Yes Dry Not at Neteroscion Novel No
991375.15 1022992.23 2109 2 Vehicles:Parties Rear-End Minor or Possible Injury Speeding 45-54 No Dry Daylight Intersection and Not Related Straight & Level No Vulnerable Road User Involved Speeding Daylight	Dry Not at Intersection/Driveway/Roundabout Apparently Asleep, Fatigued or III Yes Dry At Intersection and Not Related Speeding No
9972 99178-15 1922982-22 (1919 Meltinos Pharter New York	Dy Not at htersection Dinewsylkoundabout No Risky Driving Behaviors Involved No Dy Not at htersection Dinewsylkoundabout No Risky Driving Behaviors Involved Yes Lights Off Dy Not at htersection Dinewsylkoundabout No Systy Driving Behaviors Involved Yes Dy Not at htersection Dinewsylkoundabout Speeding Yes Dy Add Drivesylkoundabout Districted Driving No
5977 99533.31 1027/19/0 (2017) Vehicles Physics Name of Passable lighty N	Dry Driveway Related but Not at Driveway Distracted Driving No Lights Off Dry Not at Intersection/Driveway/Roundabout No Risky Driving Behaviors Involved No
998 95554-51 (12) (12) (18) (18) (12) (15) (18) (18) (18) (18) (18) (18) (18) (18	tosisrowsusan for at mineracions universignous speedung of the
9982 996578.41 102/18677 [2019] I Vehiode Parties Straight & Grade No Vehiorise Parties	Dry Pt Driveway or intersection No Risky Driving Benaviors Involved No Ice/Snow/Slush Not at Intersection/Driveway/Roundabout Speeding Yes
9986 997035 1027222 5323077 [2 Mehides Parises Luming Properly Jumage Chris Debet Unknown 165 No. Dy Julygipt All Dimessay in Interaction Disagnit & Level Older Road Letter 654 No. Disagnit Debet Disa	Dry At Driveway or Intersection Foliow Tox Cisosely No Dry At Driveway or Intersection No No Instruction No No No Instruction No No No Dry At Driveway or Intersection No No No No No No
	Dry A Driveway or Intersection
5982 9991 12.04 1017/828 (20) 2015 1 Whiteling Party Study Clipser Separately Markets Separately Markets Flying Apparently Markets Not at Interestation Onlineary/Roundabout. Cure & Level Older Roundabout. Cure & Level Other Physical Condition Data/short 9993 (1.57) 17/19/87 38/2017 (2) 15/19/87 38/2017 (2) 16/19/87 38/2017 (2) 1	Dry Not at Intersection/Driveway/Roundabout Apparently As leep, Fatigued or III Yes Dry At Driveway or Intersection No Risky Driving Behaviors Involved No
5995 99201.83 101771129 [2016 WehldenParty Struck Object Property Damage Only Improper TurnMerge 65+ No Wet Dark-Street Lights Cn A Intersection and Not Related Straight & Grade Older Road User 65+ Improper TurnMerge Dark-Street Lights Cn 5996 99201.83 1017771129 [2017 Wehlden Party Struck Object Property Damage Only Improper TurnMerge 65+ No Wet Dark-Street Lights Cn 5996 99201.83 1017771129 [2017 Wehlden Party Struck Object Property Damage Only Improper TurnMerge 65+ No Wet Dark-Street Lights Cn 5996 99201.83 1017771129 [2017 Wehlden Party Struck Object Property Damage Only Improper TurnMerge 65+ No Wet Dark-Street Lights Cn 5996 99201.83 1017771129 [2017 Wehlden Party Struck Object Property Damage Only Improper TurnMerge 16-1-1-1-1-1-1-1-1-1-1-1-1-1-1-1-1-1-1-	Wet At Intersection and Not Related No Risky Driving Behaviors Involved Yes SandMudDin At Intersection and Not Related Under Influence of Alcohol/Drugs/Medication Yes Dry M Drivingary of Intersection Institution International Internation International International International International International International International Internation
998 1000716.47 101795.18] 2014 1 Vehicle Part Stuck Cloject Property Damage Only Unider Influence of Alcohol Drugs/Medication 25-34 Unider Influence of Alcohol Drugs/Medication University Univer	Dry Not at Interest pro-insing forces Inter
6001 1001756.82 1017513.99[2017 2 Vehicles Parties Angle Property Chamage Only Fail to Yield 65+ No Day (pht 4 Disvessory Intersection Straight & Level Older Road User 65+ Fail to Yield Daylight	Dry At Driveway or Intersection Pair to Held ING Dry At Driveway or Intersection No Risky Driving Behaviors Involved No
6005 1001757.19 1017525.12 2018 2 Vehicles:Parine Magle Propenty Damage Chry Disregard Traffic Control Device/Sign B5+ No Dry Daylight Al-Driveway or Intersection Straight & Level Older Road User 65+ Disregard Traffic Control Device/Sign Daylight	Dry M Driveway or Intersection Fail to Yield No Dry M Driveway or Intersection No Risky Driving Behaviors Involved No
1007/17/19 1/17/25/13/2017 2 enterocology of the control of the co	Dry A Dineway or Intersection Fall to Yeld No Dry A Dineway or Intersection No Risky Dining Behaviors Involved No Dry A Dineway or Intersection Fall to Yeld No Wet A Dineway or Intersection Fall to Yeld No No No No No
	Dry At Driveway or Intersection Fail to Yield No Dry At Driveway or Intersection Fail to Yield No Dry Intersection Related Wint at Intersection Follow Too Closely No
6913 9803329 1014584 2014 2 Vehicles Parties Tuning Property Damage Chiy Improper Passing Surject A Dinewayor Intersection Straight & Grade No Valencable Road User Involved Improper Passing Daylight R Dinewayor Intersection Straight & Level No Valencable Road User Involved Improper Passing Daylight R Dinewayor Intersection Straight & Level No Valencable Road User Involved Improper Passing Daylight R Dinewayor Intersection Straight & Level No Valencable Road User Involved Improper Passing Daylight R Dinewayor Intersection Daylight R Dinewayor Intersection Daylight R Dinewayor Intersection Daylight R Dinewayor Intersection Daylight Day	Dry M Drivewayor Intersection No Risksy Driving Behaviors Involved No Wet 4 Drivewayor Intersection Instantision No Wet 4 Driveway or Intersection Instantision No Wet Not at Intersection/Driveway/Roundabout No Risksy Driving Behaviors Involved Yes
9016 900168 II 10222988 2012 20122988 2012 2 th Indicis Parts Fugin Property Damage Clay Part Yeard 25-54 No.	Dry M Driveway or Intersection Fall to Yeld No Dry Not at Intersection Driveway/Roundabout Apparently Asleep, Fatigued or III Dry A Driveway or Intersection No No No
	Dry Mc Driveway or Intersection No Risky Driving Behaviors Involved Yes Dry At Driveway or Intersection Intersection Intersection Intersection No Dry Not all intersection Driveway/Roundabout Under Influence of Alcohol Drugs Medication Yes
	Wet At Intersection and Not Related No Risky Driving Behaviors Involved Yes Wet Not at Intersection/Driveway/Roundabout Under Influence of Alcohol/Drugs/Medication Yes Uights Off Dry At Intersection and Not Related No Risky Driving Behaviors Involved Yes Ves
9035 9969883 1 013298 212016 2 Vehicles Paries Res-End Properly Damage City in Other 25 No Wet Objects Not at Interaction-Dreway/Poundation Straight & Level No Vehicrobre ded Vene Objects Note of Daylight All Development of Straight & Level No Vehicrobre ded Vene Objects Note of Daylight All Development of Straight & Level No Vehicrobre ded Vene Objects Note of Daylight All Development of Da	Wet Not at Intersection/Driveway/Roundabout No Risky Driving Behaviors Involved No Dry At Driveway or Intersection No Risky Driving Behaviors Involved No Dry At Driveway or Intersection Fall b Yield No
6028 98707985 101600384 (2017) B 1 Vehicles Paries Unine Properly Jamage Carly Properly Properly Jamage Carly Properly Jamage Carly Properly Properly Properly Jamage Carly Properly Properly Pr	Dry Au-Droway or Inters.ection Fail to Yeld No. LedSnowSubsub. Aff Droway or Inters.ection Speeding No. Dry Not at Inters.ection Officers.ection Speeding No. Dry Not at Inters.ection/Droway/Roundabout No. Riskyl Driving Behaviors Involved Yes Wet Not at Inters.ection/Droway/Roundabout Speeding. No.
	Wet At Driveway or Intersection Fall to Yeld No Dry At Driveway or Intersection No Risky Driving Behaviors Involved No Dry At Driveway or Intersection Folion Too Closely No No
	Dry Not at Intersection/Driveway/Roundabout Apparently Asleep, Fatigued or III Yes
6038 98867.57 1022245.99 (2014 2 Vehicles Paries Angle Minor or Possible Injury Disregard Traffic Control Device Sign 85+ No Dry Dark-No Street Lights Off A Driveway or Intersection Straight & Level Older Road User 65+ Disregard Traffic Control Device Sign Dark-No Street Lights Off A Driveway or Intersection Straight & Level Older Road User 65+ Disregard Traffic Control Device Sign Dark-No Street Lights Off A Driveway or Intersection Straight & Level Older Road User 65+ Disregard Traffic Control Device Sign Dark-No Street Lights Off A Driveway or Intersection Straight & Level Older Road User 65+ Disregard Traffic Control Device Sign Dark-No Street Lights Off A Driveway or Intersection Dark-No Street Lights Off A Driveway or Intersection Dark-No Street Lights Off Dark-No	
980884.48 1026227.9812018 12 Vehicles:Parties Rear-End Property Damago Chilv Other Unknown 155.64 No Intersection Related but Not at Intersection Straight & Level No Vulnerable Road User Involved None IDawn/Dusk	Dry Intersection Related but Not at Intersection No Risky Driving Behaviors Involved No Pry Not at Intersection/Driveway/Roundabout No Risky Driving Behaviors Involved Yes
18942 95 20 20 20 20 20 20 20 20 20 20 20 20 20	OtherUnknown Not at IntersectionDinewsyRoundabout No Risky Dhing Behaviors Involved Yes Dry All Intersection and Not Related Apparently Actient, Facility of Utility Office of
6947 98508142 1038095442 2019 1 Unbelow Park Proper Damage Only Description of the Control o	Lights Off Dry Not at hierare-dison/Dineway/Roundabout No Risky/Driving Behaviors involved No Dry Not at hierare-dison/Dineway/Roundabout No Risky/Driving Behaviors involved No Upts Off Dry Not at hierare-dison Dineway/Roundabout No Risky Driving Behaviors involved Yes Dry Not at hierare-dison/Dineway/Roundabout No Risky/Driving Behaviors Involved No
989 997210.05 102000.08 2077 2 Whitedes Parks 1 Standard Driving Paperty Damage Chry Distracted Driving 1997210.05 102000.08 2077 2 Whitedes Parks 1 Standard Driving Paperty Damage Chry Distracted Driving 1997210.05 102000.08 2077 2 Whitedes Parks 1 Standard Driving 1997210.05 102000.08 2077 2 Whitedes Parks 1 Standard Driving 1997210.05 102000.08 2077 2 Whitedes Parks 1 Standard Driving 1997210.05 102000.08 2077 2 Whitedes Parks 1 Standard Driving 1997210.05 102000.08 2077 2 Whitedes Parks 1 Standard Driving 1997210.05 102000.08 2077 2 Whitedes Parks 1 Standard Driving 1997210.05 102000.08 2077 2 Whitedes Parks 1 Standard Driving 1997210.05 102000.08 2077 2 Whitedes Parks 1 Standard Driving 1997210.05 102000.08 2077 2 Whitedes Parks 1 Standard Driving 1997210.05 102000.08 2077 2 Whitedes Parks 1 Standard Driving 1997210.05 102000.08 2077 2 Whitedes Parks 1 Standard Driving 1997210.05 102000.08 2077 2 Whitedes Parks 1 Standard Driving 1997210.05 102000.08 2077 2 Whitedes Parks 1 Standard Driving 1997210.05 102000.08 2077 2 Whitedes Parks 1 Standard Driving 1997210.05 102000.08 2077 2 Whitedes Parks 1 Standard Driving 1997210.05 102000.08 2077 2 Whitedes Parks 1 Standard Driving 1997210.05 102000.08 2077 2 Whitedes Parks 1 Standard Driving 1997210.05 102000.00 2 Whitedes Parks 1 Standa	Dry At Driveway or Intersection Distracted Driving No Ice/Snow/Slush Not at Intersection/Driveway/Roundabout No Risky Driving Behaviors Involved Yes
6054 992605.3799 1026933.707 2015 2 Vehicles-Paries Angle Property Damage Chily Fail to Yield Under 25 No Wet Daylight Not at htersection-Driveway/Roundabout Straight & Level No Vulnerable Road User Involved Fail to Yield Daylight	Wet IA Drieway or Intersection Inattention No Dry Net at Intersection Drieway/Roundabout Yes Yes Wet Not at Intersection Drieway/Roundabout Fall to Yield No
5955 992005-3790 102583-3070 2015 Whelmick Plant Novemany or Intersection Staginft & Level No Volumenable Road User morked Speeding Daulyight A Diseasemant of Intersection Staginft & Level No Volumenable Road User morked Speeding Daulyight Daulyight A Diseasemant of Intersection Staginft & Level No Volumenable Road User morked Speeding Daulyight Daulyight 6007 950307-579 1025806-80 2019 1 Vehicles Plant Staginft & Level No Volumenable Road User morked Speeding David David 6008 950307-579 1025806-80 2019 1 Vehicles Plant Staginft & Level No Volumenable Road User morked Nove 9608 950307-579 1025806-80 2019 1 Vehicles Plant Staginft & Level No Volumenable Road User morked Nove 9608 950307-579 1025806-80 2019 Nove Clipter David Clipter Nove Clipter David Clipter Nove Clipter Nove Clipter David C	Wet AD newsy or Interaction Facility (No. Ready Delivery Interaction Facility
6059 994124.07 1025799.9312018 If Vehicle-Party Verimal Property Damage Only Other Unknown 55-64 No Dry Dark-No Street Lights (Street Lights	Lights Off Dry Not at Intersection/Driveway/Roundabout No Risky Driving Behaviors Involved No
666 996410.63 1025731.36[2015 Vehicle Party Jenimal Property Damage Only Other Unknown 65+ No Daylight Straight & Grade Older Road User 65+ None Daylight	lco/Snow/Slush At Intersection and Not Related No Risky Driving Behaviors Involved Yes Dry Not at Intersection Invested No. Risky Driving Behaviors Involved No. No. Risky Driving Behaviors Involved No.
1995 1997-74-21 1097-79-21 2014 2 Marked Septimes Project 2014 2 Marked Septimes 2014 2 Marked Septimes 2014 2 Marked Septimes 2014 2 Marked Septimes 2 Marke	Lights Off Wet At Driveway or Intersection Fall to Yield No Dry At Driveway or Intersection No Risky Driving Behaviors Involved No Wet At Driveway or Intersection Inatemier No No No
40068 998725.37 1025725.42 2019 2 Nobines Parks Applies Applie	Wet At Driveway or Intersection No Risky Driving Behaviors Involved No
999075.1 1025621.44 2017 2 Vehicles/Parties Truming Properly/Damage Only Improper Backing/Passing/Turming-Pasking/Signaling Daylight	Wet Not at Intersection/Driveway/Roundabout No Risky Driving Behaviors Involved Yes Dry At Driveway or Intersection No Risky Driving Behaviors Involved No
	Dry At Driveway or Interaction No Risky Driving Behaviors Involved Yes too StrondStush Not at Interaction/Drivings/Roundatout No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Dry At Drivings or Interaction Drivings Office No. Risky Driving Behaviors Involved No Dry At Drivings or Interaction Office No. Risky Driving Behaviors Involved No.
	Dry McDriewayor Intersection No Risky Driving Behaviors Involved No Wet Alt Driewayor Intersection Follow Too Closely No Dry McDriewayor Intersection Fall to Yield No
Control Cont	Dry At Driveway or Intersection Fail to Yield No Wet Not at Intersection/Driveway/Roundabout No Risky Driving Behaviors Involved Yes
	Dry Not at Intersection/Driveway/Roundabout No Risky Driving Behaviors Involved No Dry Not at Intersection/Driveway/Roundabout No Risky Driving Behaviors Involved No
6088 1004601.11 1022643.06 2015 2 Vehicles Parties Turning Property Damage Only Fail to Yield 65+ No Dry Daylight At Driveway or Intersection Straight & Level Older Road User 65+ Fail to Yield Daylight	Dry Stat at Interaction Dineway/Roundatout No Raks/ Driving Behaviors Involved No Dry Stat at Interaction Driveway/Roundatout No Raks/ Driving Behaviors Involved No Dry Stat at Interaction Driveway/Roundatout No Raks/ Driving Behaviors Involved No Dry At Driveway of Interaction Driveway/Roundatout No Raks/ Driving Behaviors Involved No Dry At Driveway of Interaction Driving State State No Raks/ Driving Behaviors Involved No No Dry At Driveway of Interaction Driving State State No Raks/ Driving Behaviors Involved No No Dry At Driveway of Interaction Driving State S
6889 10044871.11 1022264.30 (2015 £ Vehicles Parties Legel Property Damage Only Fail to Yeld 56.4 No Dry Daylight At Diveway or Intersection Staight & Level No Vehicles Parties Accordance Fail to Yeld Daylight 6991 1004497.11 102264.30 (2016 £ Vehicles Parties Very Legel Montro Possible (in the Yeld Description Dry Daylight At Diveway or Intersection Staight & Level No Vehicles Parties Vehicles Parties<	Dry At Driveway or Intersection Fail to Yield No Dry At Driveway or Intersection Inattention No Dry At Driveway or Intersection Fail to Yield No No No No No
1004/2013/09 1002	Ice/Snow/Slush Not at Intersection/Driveway/Roundabout No Risky Driving Behaviors Involved Yes Dry Not at Intersection/Driveway/Roundabout Follow Ton Closely No No.
1899 1 1004550.33 1002544.29 [2015] I Valueda Party Permit 1 1004550.30 [2015	Dry Not at Interaction Orienway/Roundabout No Reaky Driving Behaviors Involved No
6098 10/0464 127 10/2374 26.3 [2019 1 Whick Parts Animal Property Quanage Chiy Other Unknown 55 4 No Dry Daylight Not at Intersection Oneway/Roundabout Straight & Grade Older Road User Erich None Daylight	Dry Not at Intersection/Drieway/Roundabout No Risky/Driving Behaviors Involved No Dry At Drieway or Intersection Distanced Driving Dry At Drieway or Intersection No Risky Driving Behaviors Involved No No No
5501 1004467348 1024052003075 [2 hehides Parise Rode Properly Damage Only Institution 155 No Dry Daylight R Dimessyor intersection Straight & Grade Older Road parise Reads Older Road parise Road Parise Road parise Roads Older Road parise Road parise Roads Older Road parise Road par	Dry At Driveway or Intersection Inattention No Dry Not at Intersection/Driveway/Roundabout No Risky Driving Behaviors Involved No
6104 1004880.38 1025081.96[2017 2 Vehicles Parties Sides wice Procenty Damage Only Other Unknown 45-54 No Wet Davight Vehicles Parties Sides wice No Vulnerable Road User Involved None Davight	Lights Off Dry Not at htersection Dinewsylfkoundsboot No Riskyl Driving Behaviors Innoleed Yes Wet Not at htersection Dinewsylfkoundsboot No Riskyl Driving Behaviors Innoleed Yes Wet A Drivingway or Intersection Processing Not Not No Riskyl Driving Behaviors Innoleed No Dry 4t Drivingway intersection No Riskyl Driving Behaviors Innoleed No No No No Riskyl Driving Behaviors Innoleed No No No No No Riskyl Driving Behaviors Innoleed No
100,000 100,	
	Wet At Driveway or Intersection Inattention No Dry At Driveway or Intersection Fail to Yield No Dry At Driveway or Intersection Inattention No
1.00 1.00	West KD messary or Interaction Institution No No Part KD messary or Interaction Institution No No RD messary or Interaction Fall to Visid No No RD messary or Interaction Institution No No RD messary or Interaction Institution No No RD messary or Interaction Speeding No No RD messary or Interaction Fall to Visid No No RD messary or Interaction No RD messary or Interaction No RD messary or Interaction No RD messary or Interaction RD messary or Interaction RD messary or Interaction No RD messary or Interaction RD messary or Interactio
6109 1004/99/22 10253778.22/015 2 Vehicles-Parises Project More or Possible Injury 9 and 19 Wel	Dry At Driveway or Intersection Inattention No Lights Off Dry At Driveway or Intersection Speeding No Dry At Driveway or Intersection Fail to Yield No

OBJECTID JURISDICTION	COUNTY CITY	PRIMARY BLOCK TRAFFICWAY NUMBER	KET-FORT KET-FORT SUSPENSE, IND	DATE TIME MOST_SEVERE NUM		JUNCTION RELATIONSHIP	WEATHER	ROAD_SURFACE ROADWAY CHARACTERISTIC	LIGHTING CONDITION	RRST_COLLISION TYPE_OBJECT_STRUCK	VEH_1 ACTION	VEH. 2 ACTION	VEH.1_COMPASS DIRECTION_FROM DIRECTION_FROM DIRECTION_FROM DIRECTION_FROM DIRECTION_TO
6120 County Road 6121 County Road	Cialiam 9 Cialiam 9 Cialiam 9 Cialiam 9	95250 95250 95250	95000 2.52 No E855878 95000 2.52 No E901874 2.74 No 2978774	2018-11-01 08:15 Possible Injury 1 0 3 0 0 2019-03-20 07:16 No Apparent Injury 0 0 2 0 0 2018-12-24 17:24 No Apparent Injury 0 0 1 0 0	Pickup Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Grade	Daylight Dawn	venicle Strikes Deer	Starting in Traffic Lane Going Straight Ahead Going Straight Ahead	Going Straight Ahead Going Straight Ahead	Vest East North South West East North South South North South
	Cialiam 9 Cialiam 9 Cialiam 9	95250 95250 95250		2019-09-12 16:25 No Apparent Injury 0 0 1 0 0 2017-11-27 18:49 No Apparent Injury 0 0 2 0 0	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Wet Curve & Grade Dry Curve & Level Loe Curve & Grade Dry Curve & Grade Dry Curve & Level Loe Curve & Grade Dry Cu		From same direction - all others	Going Straight Ahead Backing Going Straight Ahead		South North North Vehicle Backing Vehicle Stopped Vehicle Stopped South North
6126 County Road 6127 County Road	Cialiam 9 Cialiam 9	95250 95250 95250		2019-02-05 13:10 No Apparent Injury 0 0 1 0 0	Trock tractor Passenger Car Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	lce Curve & Grade (Daylight Daylight	Guardrail - Face	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Channel for Tolk-	South North South North South North
6127 County Road 6128 County Road 6129 County Road 6130 County Road	Cialiam 9 Cialiam 9 Cialiam 9	95250 95250 95250	291 No	2013-08-05 15:55, No Apparent injury 0 0 2 0 0 0 0 0 1 0 0 0 0 1 0 0 0 0 1 0 0 0 0 0 1 0	Passenger Car Plocup Panel Truck or Vanette under 10,000 lb Passenger Car Plocup Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related Not at Intersection and Not Related At Intersection and Related	Clear or Party Cloudy Clear or Party Cloudy Overcast	lce Curve & Grade C Dry Curve & Level C Dry Straight & Level C			Going Straight Ahead Going Straight Ahead Overtaking and Passing	Stopped for Traffic Going Straight Ahead	Southeast Northwest Wehicle Stopped Sorthwest Southeast South South North South
6131 County Road 6132 County Road 6133 County Road	Cialiam 9 Cialiam 9 Cialiam 9	95250 95250 95250	3.34 NO 3202003 4.42 No 2728029 4.65 No E416020	2014-01-26 13-30 No Apparent Injury 0 0 2 0 0 0 0 0 0 0	Plokup,Panel Truck or Vanete under 10,000 lb Plokup,Panel Truck or Vanete under 10,000 lb Plokup,Panel Truck or Vanete under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Curve & Level Dry Straight & Level Dry Straight & Level Dry Straight & Level Dry Curve & Grade Dry Straight & Level Dry Straight &	Dark-No Street Lights Dark-No Street Lights Daylight	Vehicle Strikes Deer Mailbox Uslity Box	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	ast yest East west South North
6135 County Road 6136 County Road 6137 County Road 6137 County Road 6138 County Road	Cialiam Cialiam Jefferson 3	32750	90 1825527 1.89 No E625527 1.89 No E614661	2016-03-16 99-93 No Apparent Injury 0 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Not at Intersection and Not Related At Intersection and Related Not at Intersection and Not Related	Clear or Partly Cloudy Raining	Dry Straight & Level L	Daylight	Entering at angle	Making Right Turn	Going Straight Ahead	North South North South North South
6138 County Road 6139 County Road	Jefferson 3	33480 34880 50360	93150 5.22 No E944075 93150 9.31 No E94508	2016-11-23 23:30 Unknown 0 0 1 1 0 0 0 2014-0-117 18:30 No Apparent Injury 0 0 1 1 0 0 0 2014-0-117 18:30 No Apparent Injury 0 0 1 1 0 0 0 2019-07-28 18:30 Possible Injury 1 0 1 0 0 0 2017-07-21 15:35 Possible Injury 1 0 1 0 0 0 0 1014-13-04 13:30 Evented Safety Injury 1 0 1 0 0 0 0 0 1014-13-04 13:30 Evented Safety Injury 2 0 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Passenger Car Pickup Panel Truck or Vanette under 10,000 lb Passenger Car	Not at Intersection and Not Related At Intersection and Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy				Going Straight Ahead Going Straight Ahead Making Left Turn Going Straight Ahead		Courth South South South
6139 County Road 6140 County Road 6141 County Road 6142 County Road	Jefferson 5	52340 52340 52340	0.62 No E990207 0.83 No E835992	2019-12-06 06:32 No Apparent Injury 0 0 1 0 0 2018-09-08 21:45 Possible Injury 2 0 1 0 0	Passenger Car Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Overcast Clear or Partly Cloudy	Dry Straight & Grade L Dry Straight at Hillor Dry Straight & Level Dry S	Dark-No Street Lights Dark-No Street Lights Dark-No Street Lights	Roadway Dirch Vehicle Strikes Deer Time or Stump (stationary)	Going Straight Ahead Going Straight Ahead Going Straight Ahead		ast vest Vest East Vest East
	Jefferson 5 Jefferson 5 Jefferson 5	52340 52340 52340	0.87 NO E444,65.2 0.92 No E38491 0.95 No E347199	2014-08-20 06:30 No Apparent Injury 0 0 1 0 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Truck (Flatbad, Van, etc)	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Fog or Smog or Smoke Clear or Partly Cloudy	Wet Etraight & Grade E	Daylight Daylight	Over Embankment - No Guardrail Present Tree or Stump (stationary)	Going Straight Ahead Going Straight Ahead Going Straight Ahead		vest East ast West ast West
6146 County Road 6147 County Road 6148 County Road	Jefferson 5 Jefferson 5	52340 52340 52340	0.99 NO E7696322 1.50 No E668222 1.87 No E311951	2017-04-20 10:95 No Apparent Injury 0 0 1 0 0 0 2014-02-20 12:01 Possible Injury 1 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade [I Dry Curve & Grade [I	Daylight Daylight	Earth Bank or Ledge Guide Post	Going Straight Ahead Going Straight Ahead Going Straight Ahead		vest vest Vest Vest Vest Vest Vest Vest Vest V
6147 County Road 6148 County Road 6149 County Road 6150 County Road 6151 County Road	Jefferson 5 Jefferson 5 Jefferson 5	52340 52340 52340	2.03 No E482.176 2.30 No E92339 2.47 No E46858	2017-04-30 1005 No Apparent Injury 0 0 1 1 0 0 0 1 1 0 1 0 1 0 1 0 1 0 1	Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related At Driveway Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level L	Daylight	Tree or Stump (stationary) Tree or Stump (stationary) Entering at angle	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	Vest East Vest East Vorth South East West
6152 County Road 6153 County Road 6154 County Road 6155 County Road	Jefferson 5 Jefferson 5 Jefferson 5	53400 556990 56990	56990 0.01 No E940715 0.93 No E783659	2019-07-16 13:07 No Apparent Injury 0 0 1 0 0 2018-03-16 14:30 No Apparent Injury 0 0 1 0 0 0	Prickup, Panel Truck or Vanette under 10,000 lb Passenger Car	Intersection Related but Not at Intersection Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade C	Daylight Daylight	Tree or Stump (stationary) Earth Bank or Ledge Vehicle Strikes Deer Tree or Stump (stationary) Tree or Stump (stationary)	Going Straight Ahead Making Left Turn Going Straight Ahead Going Straight Ahead		vest East korthwest Southeast couth North
6155 County Road 6156 County Road 6157 County Road 6158 County Road 6159 County Road	Jefferson	56990 56990	1.11 NO EMB0917 125 No E78556 130 No E58601 130 No E584601		Passenger Car Truck Tractor & Semi-Trailer Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related At Driveway	Overcast Overcast Clear or Partly Cloudy	Dry Curve & Grade II	Daylight Daylight	Utility Pole	Going Straight Ahead	Going Straight Ahead	ass vest Southeast Northwest South North West East
6158 County Road 6159 County Road 6160 County Road	Jefferson 5	56990 56990	1.57 No E950083 2.18 No E550880 2.54 No E314192	2019-08-12 19:28 Suspected Minor Injury 1 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Motorcycle Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Sleet or Hail or Freezing Rain	Snow/Slush Curve & Grade	Daylight	Tree or Stump (stationary)	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead		Southeast Northwest South South South
6163 County Road	Jefferson 5 Jefferson 5 Jefferson 5	56990 56990	2.54 No E595436 2.58 No E44520 2.61 No E555699	2016-10-13 (07:07 No Apparent Injury 0 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Passenger Car Truck (Plablad,Van,etc) Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Unknown	Wet Curve & Grade [Wet Curve & Grade [Wet Curve & Grade [Dark-No Street Lights	Building	Going Straight Ahead Going Straight Ahead Going Straight Ahead		South North South North South South
6164 County Road 6165 County Road 6166 County Road 6167 County Road	Jefferson 5 Jefferson 5 Jefferson 5	56990 56990	2.65 No E449344 2.69 No E45394 3.02 No E41157	2015-07-23 (02:90 No Apparent Injury 0 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Strai	Daylight Daylight	Roadway Ditch Utility Pole Culvert and/or other Appurtenance in Ditch	Going Straight Ahead Going Straight Ahead Going Straight Ahead		dorthwest Southeast South South North Indian South Sou
6168 County Road 6169 County Road 6169 County Road	Jefferson 5 Jefferson 5 Jefferson 5	56990 56990	3.33 No E719090 3.62 No E843322 3.84 No E420067	2017-10-04 08:11 No Apparent Injury	Passenger Car Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Fog or Smog or Smoke	Dry Straight & Level I Dry Straight & Level I Dry Curve & Level I	Daylight Daylight Daylight	Roadway Ditch From same direction - both going straight - one stopped - rear-end Metal Sign Post	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic	South North Aorth South North South Aorth South
6168 County Road 6169 County Road 6170 County Road 6171 County Road 6172 County Road	Jefferson 5 Jefferson 5	0990 0990 0990	4.23 No E430945 4.51 No E910447 4.53 No E841153	2015-06-04 16:40 No Apparent Injury	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level II Dry Curve & Level II Dry Straight & Grade II	Daylight Daylight Daylight	From same direction - both going straight - one stopped - rear-end Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic	Southeast Northwest Vehicle Stopped Vehicle Stopped Sortheast Northwest ast West
6173 County Road 6174 County Road 6175 County Road 6176 County Road 6176 County Road 6177 County Road	Jefferson 5 Jefferson 5 Jefferson 5	56990 56990	4.84 No E446118 4.85 No E88009 5.13 No E381103	2019-0-0-1 (1468 No. Apparent Brigory 0 0 1 1 0 0 0 2019-0-0-1 (1468 No. Apparent Brigory 0 0 1 1 0 0 0 2019-0-0-1 (1468 No. Apparent Brigory 0 0 1 1 0 0 0 2019-0-0-1 (1469 No. Apparent Brigory 0 0 1 1 0 0 0 2019-0-1 (1469 No. Apparent Brigory 0 0 1 1 0 0 0 2019-0-1 (1469 No. Apparent Brigory 0 0 1 1 0 0 0 2019-0-1 (1469 No. Apparent Brigory 0 0 1 1 0 0 0 2019-0-1 (1469 No. Apparent Brigory 0 0 1 1 0 0 0 2019-0-1 (1469 No. Apparent Brigory 0 0 1 1 0 0 0 2019-0-1 (1469 No. Apparent Brigory 0 0 1 0 0 0 2019-0-1 (1469 No. Apparent Brigory 0 0 1 0 0 0 2019-1-1 (1469 No. Apparent Brigory 0 0 1 0 0 0 2019-1-1 (1469 No. Apparent Brigory 0 0 1 0 0 0 2019-1-1 (1469 No. Apparent Brigory 0 0 1 0 0 0 2019-1-1 (1469 No. Apparent Brigory 0 0 1 0 0 0 0 2019-1-1 (1469 No. Apparent Brigory 0 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Passenger Car Passenger Car Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level [Dawn	Mailbox Tree or Stump (stationary) Earth Bank or Ledge Tree or Stump (stationary)	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead		asst West South North forth South
6176 County Road 6177 County Road 6178 County Road 6179 County Road	Jefferson 5 Jefferson 5 Jefferson 5 Jefferson 5	56990 56990		2010-03-31 15:37 Possible Injury 1 0 1 0 0 2016-11-06 15:22 No Apparent Injury 0 0 1 0 0 2018-02-20 03:34 Suspected Minor Injury 1 0 1 0 0	Prickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Raining						vormeast Southwest Southwest Southwest vorth South
	Jefferson 5 Jefferson 5	56990 56990		2016-10-22 09:30 No Apparent Injury 0 0 1 0 0 0 2015-12-14 14:45 No Apparent Injury 0 0 1 0 0 0 2015-12-14 14:45 No Apparent Injury 0 0 1 0 0 0 2015-01-29 02:00 Unknown 0 0 1 0 0	Passanger Car Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Wet Curve & Level (Wet Straight & Grade (Dry Straight & Grade (Daylight Daylight Dark-No Street Lights	Over Embankment - No Guardrail Present Tree or Stump (stationary) Tree or Stump (stationary) Mailbox	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East Aorth South South North
6181 County Road 6182 County Road 6183 County Road 6184 County Road	Jefferson 5 Jefferson 5 Jefferson 5	56990 56990	6.35 No E479350 6.47 No E409351 6.53 No E519716	2015-11-01 19:22 No Apparent Injury 0 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Passenger Car Passenger Car Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear Clear or Partly Cloudy	Dry Straight & Grade (Wet Straight & Grade (Wet Curve & Grade (Dry Straight & Grade (Dark-No Street Lights Dark-No Street Lights Dark-No Street Lights	Vehicle Strikes Deer Utsity Pole Roadway Ditch	Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East 4certh South 8certh South
6185 County Road 6186 County Road 6187 County Road 6188 County Road	Jefferson 5 Jefferson 5 Jefferson 5	56990 56990	6.78 No E702907	2017-08-17 (20:55 Suspected Mnor Injury 1 0 1 0 0 0 2 2 0 0 0 2 0 0 0 0 0 0 0 0	Motorcycle Passenger Car Passenger Car Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Overcast	Dry Straight & Level	Dark-No Street Lights Dark-No Street Lights	Vehicle Strikes Deer Roadway Ditch	Going Straight Ahead Going Straight Ahead	Legally Parked, Unoccupied	South North North North South North South North South North N
6188 County Road 6189 County Road 6190 County Road	Jefferson 5 Jefferson 5	56990 56990 56990	7.55 No E822761	2018-07-28 [21:11] No Apparent Injury 0 0 1 1 0 0 0 2014-06-30 12:44 No Apparent Injury 0 0 0 2 0 0 0 0 10:07:02-25 18:45 Possible Injury 1 0 0 1 0 0 0	Phasenger Car Ph	At Intersection and Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Overcast	Dry Straight & Level E	Daylight	From opposite direction - one left turn - one straight	Going Straight Ahead Making Left Turn Going Straight Ahead	Going Straight Ahead	North South dorth East South North South North
6189 County Road 6190 County Road 6191 County Road 6192 County Road 6193 County Road	Jefferson 5 Jefferson 5 Jefferson 5	56990 56990 56990	8.29 No E826338 8.30 No E403664 8.47 No E496431	2018-08-08 13:49 No Apparent Injury 0 0 2 0 0 0 2 0 0 2 0 15:05-26 (6:54 No Apparent Injury 0 0 1 0 0 0 1 0 0 0 0 1 0 0 0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup.Panel Truck or Vanette under 10,000 lb Pickup.Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy Overcast				Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic	Northwest Southeast Vehicle Stopped Vehicle Stopped South North South North
6194 County Road 6195 County Road 6196 County Road	Jefferson 5 Jefferson 5 Jefferson 5	56990 56990 56990	51000 8.8.6 No E319026 8.8.6 No E354760 8.8.6 No E354760 8.7.3 No E300583	2015-12-12 23.37 Possible bijury 3 0 1 0 0 0 2016-07-16 21:30 No Apparent Injury 0 0 0 2 0 0 0 2014-09-05 11:38 No Apparent Injury 0 0 0 2 0 0 0 2014-09-113 19:10 No Apparent Injury 0 0 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Passenger Car Passenger Car	At Intersection and Related At Intersection and Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade Dry Straight & Grade Dry Curve & Grade Dry Curve & Grade	Dark-Street Lights On Davlight	From same direction - one left turn - one straight Entering at angle	Making Left Turn Making Left Turn Going Straight Ahead	Going Straight Ahead Making Left Turn	South West South North ast South North East Octheast Southwest
6197 County Road 6198 County Road 6199 County Road 6200 County Road 6201 County Road	Jefferson 5 Jefferson 5 Jefferson 5 Jefferson 5	56990 56990 56990	8.74 No E950846 9.64 No E459997 9.84 No E47474	2019-06-11 1866 Supposed Minor Pury 1	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	Not at Intersection and Not Related At Intersection and Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Grade [Dry Straight & Level [Dusk			Making Left Turn	Vest East South North East South South
6200 County Road 6201 County Road 6202 County Road	Jefferson 5 Jefferson 5 Jefferson 6	56990 56990 60120	1204 NO 1204 474 10.11 No 1204 474 10.11 No 1203 10.11 No 1203 10.11 10.51 No 1203 10.11 10.51 No 1203 10.11 10.51 No 1203 10.11 10.51 10.	2017-10-13 08:05 No Apparent Injury 0 0 1 0 0 0 2016-09-23 14:05 No Apparent Injury 0 0 1 1 0 0 0 2016-09-23 14:05 No Apparent Injury 0 0 1 1 0 0 0 2019-02-27 20:25 Possible Injury 1 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Truck (Flatbad,Van.etc) Pickup,Panel Truck or Vanette under 10,000 lb Passenner Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level (Dry Curve & Level (Dry Curve & Level (Dry Straight & Level (Dry Straight & Level (Daylight Daylight Dark-No Street Lights	Roadway Ditch Road Bank or Ledge Illililin Pole	Making Left Turn Going Straight Ahead		South South ass West South
6202 County Road 6203 County Road 6204 County Road 6205 County Road	Jefferson 6	60120 60120 60120	0.71 No E915974 0.82 No E80333 0.85 No E66333	2019-04-27 18:50 No Apparent Injury 0 0 1 0 0 0 2015-12-14 11:15 Dead at Scene 0 1 1 0 0 0 2017-04-13 08:45 Suspected Serious Injury 2 0 1 0 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level (Davlight	Mailbox Tree or Stump (stationary) Earth Bank or Ledge	Going Straight Ahead Going Straight Ahead Going Straight Ahead		Vest East Vest East Sast West
6206 County Road	Jefferson 9 Jefferson 9	93150 93150 93150	3.01 No E923359 7.08 No E311952 7.08 No E776026	2019-05-23 18:08 Possible hijury	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy				Going Straight Ahead Going Straight Ahead		Forth South South South North South North South North
6210 County Road	Jefferson 9 Jefferson 9	93150 93150 93150	17.06 NV 27.0002.0 77.14 No 23.0002.0 77.5 No 3365548 77.7 No 12.0002.0	2014-01-26 08:30 No Apparent Injury 0 0 1 1 0 0 0 2015-08-22 15:35 No Apparent Injury 0 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Fog or Smog or Smoke Overcast Overcast	loe Straight & Grade [Dry Straight & Level [Wet Straight & Level [Daylight Daylight Dark-No Street Lights		Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead		South North South North South North South South
6211 County Road 6212 County Road 6213 County Road 6213 County Road	Jefferson 9 Jefferson 9 Jefferson 9 Jefferson 9	93150 93150 93150	7.67 NO E864274 7.97 NO E864274 8.14 NO E940714 9.16 No E77742	2016-09-10 14:20 No Apparent Injury 0 0 2 0 0 0 2019-07-10 23:10 Suspected Minor Injury 1 0 1 0 0 2018-02-24 08:32 No Apparent Injury 0 0 1 0 0	Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	At Intersection and Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Raining Steet or Mail or Free ring Pain	Dry Straight & Level E Wet Straight & Level E	Daylight Dark-No Street Lights	From same direction - one left turn - one straight Over Embankment - No Guardrail Present	Overtaking and Passing Going Straight Ahead Going Straight Ahead	Making Left Turn	South Northeast South North North South South
6214 County Road 6215 County Road 6216 County Road 6217 County Road	Jefferson 9 Jefferson 9	93150 93150 93150	8.20 No E739388 8.52 No E644888	2017-11-23 21:46 No Apparent Injury 0 0 2 0 0 0 2017-02-22 12:10 Possible Injury 1 0 1 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Raining Sleet or Hail or Freezing Rain Raining Sleet or Hail or Freezing Rain Overrast	Wet Straight & Grade	Dark-No Street Lights Daylight Daylight	Earth or statement and the statement of	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	South North South North South North South North South North South North
	Jefferson 9 Jefferson 9	93150 93150 93150	8.96 No E912250 9.31 No E610250 9.41 No 260650	2015-02-12-11-03 No Appearent Hypry 0 0 1 1 0 0 0 1 0 0 0 1 0 0 0 0 1 0 0 0 0 1 0	Passenger Car Plickup Panel Truck or Vanette under 10,000 lb Parsenger Car	Not at Intersection and Not Related	Raining Clear or Partly Cloudy Clear or Partly Cloudy	Wet Curve at Hillcrest	Dark-No Street Lights	Roadway Ditch Earth Bank or Ledge	Going Straight Ahead Going Straight Ahead	Going Straight Ahead	South North South North South
6221 County Road 6222 County Road 6222 County Road	Jefferson 9	93150 93150	9.48 No E512024 9.50 No E622143 9.50 No E622143	2016-02-01 (07:00 No Apparent Injury 0 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	**Security of Car Passenger Car Passenger Car Passenger Car Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy				Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead		South North Douth North Douth North
6224 County Road 6225 County Road 6226 County Road	Jefferson 9 Jefferson 9 Jefferson 9	93150 93150 93150	9.59 No E402869 9.79 No E402869 9.79 No E623457 9.79 No E623457	2015-02-21 (20:50 No Apparent Injury 0 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Passanger Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Snowing Clear or Partly Cloudy Clear or Partly Cloudy Clear	Dry Straight & Grade Dry Straight & Level Dry Strai	Dark-No Street Lights Dark-No Street Lights Dark-No Street Lights	velocia delette Luer velocia delette veloc	Going Straight Ahead Going Straight Ahead	Going Straight Ahead	South North North South Sorth South South South
6227 County Road 6228 County Road 6229 County Road	Jefferson 9 Jefferson 9	93150 93150 93150	9.87 No E927125 10.22 No E932125 10.22 No E932115 10.27 No E932115	2019-06-03 16:11 Possible Injury 1 0 1 0 0 0 2018-11-16 10:29 Possible Injury 1 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Parezoner Cr	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Level Dry Strai	Daylight Daylight	Over Embankment - No Guardrail Present Roadway Ditch Usbride Strieger Dear	Going Straight Ahead Going Straight Ahead Going Straight Ahead		oorth South South North Jourth Courts
6229 County Road 6230 County Road 6231 County Road	Jefferson 9 Jefferson 9 Jefferson 9	93150 93150	10.37 No E939713 10.30 No E939713 10.34 No E939713	2015-02-06 18-53 No Apparent Injury 0 0 1 0 0 0 2018-09-08 03:25 No Apparent Injury 0 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Plackup Panel Truck or Vanette under 10,000 lb Passenger Car Distance Car Distance Car Distance Car	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Raining Overcast				Going Straight Ahead Going Straight Ahead Going Straight Ahead		South
6231 County Road 6232 County Road 6233 County Road 6234 County Road	Jefferson 9 Jefferson 9 Jefferson 9 Jefferson 9	93150 93150 93150	10.72 No E747535 11.14 No E814568 11.63 No E625342	2018-0-90 2018-0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Level C Snow/Slush Curve in Sag	Daylight Dark-No Street Lights	Roadway Ditch Tree or Stump (stationary) Earth Bank or Ledge	Going Straight Ahead Going Straight Ahead Going Straight Ahead	2000	South North South
6235 County Noad 6236 County Road 6236 County Road 6236 County Road 6237 County Road 6238 County Road 6238 County Road 6240 County Road 6240 County Road 6240 County Road 6241 County Road 6242 County Road 6242 County Road	Jefferson 9 Jefferson 9 Jefferson 9	93150 93150 93150	11.72 No £830743 12.04 No £830743 12.04 No £147095 12.23 No £14095 12.56 No £734000	2014-09-17 14:37 Possible Injury 1 0 1 0 0 2014-09-17 16:24 Dead at Scene 1 1 2 0 0 2014-09-07 08-04 No Angagery House	Plakup, Panel Truck or Vanette under 10,000 ib Passenger Car Plakup, Panel Truck or Vanette under 10,000 ib Plakup, Panel Truck or Vanette under 10,000 ib Passenger Car Plakup, Panel Truck or Vanette under 10,000 ib	At Intersection and Related Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy			From same direction - both going straight - one stopped - rear-end Earth Bank or Ledge	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic Going Straight Ahead	loorth South Southwest Southwest Northeast Southwest Southwest Northeast
6239 County Road 6240 County Road	Jefferson 9 Jefferson 9 Jefferson 9	93150 93150	12.60 No E371/203	2014-11-04 (04-03 Possible Injury 1 0 1 0 0 0 2014-11-04 (04-03 Possible Injury 1 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Flokup, Panel Truck or Vanette under 10,000 lb Truck Tractor & Semi-Trailer	Not at Intersection and Not Related	Clear or Partly Cloudy Overcast Clear or Partly Cloudy	Wet Curve & Grade [Dark-Street Lights Off Daylight	Over Embankment - No Guardrail Present	Going Straight Ahead Going Straight Ahead Going Straight Ahead		South South South South South South South South South
6242 County Road 6243 County Road 6244 County Road	Jefferson 9 Jefferson 9	93150 93150 93150		2016-07-11 08:31 Possible Injury 1 0 1 0 0 0 2017-12-08 18:30 No Apparent Injury 0 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0		Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy			Earth Bank or Ledge Over Embankment - No Guardrail Present	Going Straight Ahead Going Straight Ahead		South North South North South North
6243 County Road 6244 County Road 6245 County Road 6245 County Road 6246 County Road	Jefferson 9 Jefferson 9 Jefferson 9	93150 93150 93150	13.57 NO E004-11 13.58 No E799045 13.58 No E720716	2018-05-17 12-45 Possible Injury 1 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Passenger Car Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Raining	Dry Curve & Grade Dry Curve &	Daylight Daylight	Earth Bank or Ledge Roadway Dirth Ower Embankment - No Guardrail Present Tean or Summ in Estimated Control Control Tean or Summ in Estimated C	Going Straight Ahead Going Straight Ahead Going Straight Ahead		Aorth South Aorth South Aorth South
6247 County Koad	Jefferson 9 Jefferson 9 Jefferson 9	93150 93150 93150 93150	13.61 No E753.27 13.96 No E772.25 14.16 No E772.25 14.17 No E772.27	2017-11-12 00:00 Suspected Mnor Injury 1 0 1 0 0 0 2016-08-15 20:48 No Apparent Injury 0 0 1 1 0 0 0 2017-09-10 14:30 No Apparent Injury 0 0 0 1 0 0 0 2017-09-10 14:30 No Apparent Injury 0 0 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Passenger Car Pickup Panel Truck or Vanette under 10,000 lb Passenger Car	Not at Intersection and Not Related	Raining Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade (Dark-No Street Lights	Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead		South North South North South North South South
6251 County Road 6251 County Road 6252 County Road	Jefferson 9 Jefferson 9 Jefferson 9	93150 93150	14.51 No E70895 14.51 No E70895 14.51 No E70895 14.51 No E70895 14.82 No E64887 14.8	2017-08-27 [22:52 No Apparent Injury 1 0 1 0 0 0 2 0 1 0 0 2 0 1 0 0 0 0 0 0	Plackup,Panel Truck or Vanette under 10,000 lb Passenger Car Pokup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	vvet Straight & Level t	Jayngiit	One parked-one moving	Going Straight Ahead	Legally Parked, Unoccupied Legally Parked, Unoccupied	Aorth South Aorth Vehicle Backing Aorth South Aorth South
6252 County Road 6253 County Road 6254 County Road 6254 County Road 6255 Africal Inners Trafficurary	Jefferson Jefferson	7 CED ADS CASING TRIBITION	No E932553 No E965740 No E972546 W 101 No E772546	2019-09-25 0723 No Apparent Injury 0 0 1 0 0 2019-09-25 0723 No Apparent Injury 0 0 1 0 0 2019-10-18 18:09 No Apparent Injury 0 0 1 0 0 2017-04-01 18:09 No Apparent Injury 0 0 1 0 0	Passenger Car Truck Tractor & Semi-Trailer Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Overcast Clear or Partly Cloudy	Dry Straight & Grade [Dry Straight & Level [Wet Curve & Level [Dry Straight & Grade [Daylight Daylight	Vehicle Strikes Deer Other Objects Wehicle Strikes Deer Union Country Un	Going Straight Ahead Going Straight Ahead Going Straight Ahead		South North Dozeth North
6257 Miscellaneous Trafficway 6258 Miscellaneous Trafficway 6258 State Route	Cialiam L Cialiam N	7 CEDARS CASINO THRU WY 270756 LOOPER LN PRIVATE N MASTERS RD	W 101 No 2729043	2016-01-02 08:30 No Apparent Injury 0 0 2 0 0 0 2019-09-07 21:30 No Apparent Injury 0 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Other Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Other Descriptions Ctr. Descriptions Ctr	At Driveway	Clear or Partly Cloudy Raining Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade (Wet Straight & Level (Dry Curve at Hillcrest (Dry Straight & Level (Daylight Dark-Street Lights On		Going Straight Ahead Going Straight Ahead Making Right Turn Going Straight Ahead	Stopped at Signal or Stop Sign Stopped for Traffic	South North Vest South Vehicle Stopped Vehicle Stopped Vest South Vest South Moret Eart Moret Eart
			249.61 No 3797561 249.62 No 3815440 249.62 No E917710 249.62 No E917710	2018-11-28 15-37 Poosible Injury 1 0 1 0 0 2019-14-29 15-14 No Apparent Injury 0 0 2 0 0	Passenger Car Passenger Car Passenger Car Passenger Car Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel	Not at Intersection and Not Related Intersection Related but Not at Intersection	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	wet Straight & Level L	Daylight	From same direction - both going straight - one stopped - rear-end Other Objects From same direction - both going straight - one stopped - rear-end Grown same direction - both going straight - one stopped - rear-end Grown same direction - both going straight - both rearing - searced	Going Straight Ahead Going Straight Ahead Starting in Traffic Lane Going Straight Ahead Going Straight Ahead	Stopped for Traffic	West
6263 State Route 6264 State Route	Cialiam Port Angeles 1	101		2019-11-28 [13-27 16-17 16	Pickup Panel Truck or Vanette under 10,000 ib Pickup Panel Truck or			Dry Straight & Level Dry	Daylight			Going Straight Ahead Stopped at Signal or Stop Sign Stopped at Signal or Stop Sign Stopped at Signal or Stop Sign	West East West East Sast West East Vehicle Stopped Sast West Vehicle Stopped Vehicle Stopped
6265 State Route 6267 State Route 6267 State Route	Cialiam Port Angeles 1 Cialiam Port Angeles 1 Cialiam Port Angeles 1	101 101 101	249.63 No E328074 249.63 No E333849	2014-05-02 08:30 No Apparent Injury 0 0 2 0 0 2 0 0 2014-12-05 14:38 No Apparent Injury 0 0 2 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Overcast Clear or Partly Cloudy	Dry Curve & Grade (I Wet Straight & Level (I Dry Straight & Grade (I	Daylight Daylight Daylight	From same direction - both going straight - one stopped - rear-end Entering at angle Tree or Suurp (stationary)	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped at Signal or Stop Sign Going Straight Ahead	ast West East Vehicle Stopped West East South North South North
\$250 State Roote \$254 State Roote \$254 State Roote \$255 State Roote \$257 State Roote	Cialiam Port Angeles 1 Cialiam Port Angeles 1 Cialiam Port Angeles 1 Cialiam Port Angeles 1	101	1249.63 No E470973 249.63 No E630068 249.63 No E670415	2017-02-04 (3:54 No Apparent Injury 0 0 2 0 0 0 2 0 0 0 2 0 17-05-11 (2:04 No Apparent Injury 0 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Pricipip Pared Truck or Vanette under 10,000 b Passenger Car	At Intersection and Related At Intersection and Related At Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Grade Dry Strai	Daylight Dark-Street Lights On	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic Stopped at Signal or Stop Sign	ast West East Vehicle Stopped South North South Vehicle Stopped Vest East Vehicle Stopped
6271 State Route 6272 State Route 6273 State Route	Cialiam Port Angeles 1 Cialiam Port Angeles 1	101	249.63 No E823537 249.63 No E95358 249.65 No 2867161	2016-07-15 14:00 No Apparent Injury 0 0 2 0 0 0 2 0 0 0 2 0 0 0 0 0 0 0 0	Passenger Car Pa	At Intersection and Related At Intersection and Related At Intersection and Related	Raining Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level [Wet Straight & Level [Dry Straight & Level [Daylight Dark-Street Lights On Daylight	Entering at angle Entering at angle From same direction - both going straight - one stopped - rear-end	Making Right Turn Starting in Traffic Lane	Going Straight Ahead Going Straight Ahead Stopped at Signal or Stop Sign	Vest East South North South East West East East Northwest Vehicle Stopped
6274 State Route 6275 State Route 6276 State Route	Cialiam Port Angeles 1 Cialiam Port Angeles 1 Cialiam Port Angeles 1	101 101 101	249.66 No E546473 249.67 No 2741397 249.67 No E662839	2019-04-11 11:46 Suspected Minor Injury 0 0 2 0 0 2019-04-11 11:46 Suspected Minor Injury 2 0 2 0 0 2017-06-13 14:59 No Apparent Injury 0 0 2 0 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	intersection Related but Not at Intersection At Driveway Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Dry Straight & Grade I Wet Straight & Level I Dry Straight & Level I	Daylight Daylight Daylight	From same direction - both going straight - both moving - rear-end Entering at angle From same direction - both going straight - both moving - rear-end	Starting in Traffic Lane Going Straight Ahead Going Straight Ahead	Starting in Traffic Lane Making Left Turn Slowing	ass West East West South East West South East West East West
6277 State Route 6278 State Route 6279 State Route	Cialiam Port Angeles 1 Cialiam Port Angeles 1 Cialiam Port Angeles 1 Cialiam Port Angeles 1		249.67 No E914313 249.67 No E881657 249.68 No 2867115	2018-06-22 13:53 No Apparent Injury 0 0 2 0 0 2019-01-03 17:26 No Apparent Injury 0 0 4 0 0 2017-12-02 17:06 No Apparent Injury 0 0 2 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Not Stated Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Raining Clear or Partly Cloudy			Entering at angle From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - rear-end	Making Left Turn Slowing Going Straight Ahead	Making Left Turn Slowing Going Straight Ahead	ast South North East sast West East West ast West East West
6280 State Route 6281 State Route 6282 State Route	Cialiam Port Angeles 1 Cialiam Port Angeles 1 Cialiam Port Angeles 1	101 101 101	249.68 No E755560 249.69 No 2741394 249.69 No E42215 249.69 No E42215	2018-01-02 17:39 No Apparent Injury 0 0 5 0 0 2019-03-25 14:31 No Apparent Injury 0 0 2 0 0 2015-12-08 17:15 No Apparent Injury 0 0 3 0 0	Pickup, Panel Truck or Vanette under 10,000 ib Passenger Car	Not at Intersection and Not Related At Driveway Intersection Related but Not at Intersection	Overcast Clear or Partly Cloudy	Dry Straight & Level L	Daylight	Entering at angle	Going Straight Ahead Going Straight Ahead	Slowing Making Left Turn	ast West East West sast West South East Vest East West East
6281 State Route 6282 State Route 6283 State Route 6284 State Route 6285 State Route	Cialiam Port Angeles 1	101			Pickup Panel Truck or Vanette under 1,000 b Pickup Panel Truck or Vanette under 10,000 b Pickup Panel Truck or		Overcast Raining Raining	Wet Straight & Grade	Dusk	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Making Left Turn Going Straight Ahead Going Straight Ahead	Slowing Going Straight Ahead Stopped for Traffic Stopped for Traffic	West North East West East West East Vehicle Stopped East Webicle Stopped East Vehicle Stopped
9239 Sate Route 9236 Sate Route 9236 Sate Route 9236 Sate Route 9237 Sate Route 9238 Sate Route 9238 Sate Route 9238 Sate Route 9238 Sate Route 9230 Sate Route 9230 Sate Route 9230 Sate Route 9231 Sate Route 9231 Sate Route 9231 Sate Route 9232 Sate Route 9232 Sate Route 9232 Sate Route 9233 Sate Route	Cialiam Port Angeles 1 Cialiam Port Angeles 1 Cialiam Port Angeles 1	101 101 101	249.70 No E568197 249.71 No E544677 249.71 No E544677 249.71 No E598582	2016-07-29 16:52 No Apparent Injury 0 0 2 0 0 0 2 0 0 0 0 0 0 0 0 0 0 0 0	Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Overcast	Dry Straight & Level (Dry Straight & Level (Wet Straight & Level (Daylight Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing Stopped for Traffic	Sast West East West East West East Vehicle Stopped Sast West East West
6289 State Route 6290 State Route 6291 State Route	Claliam Port Angeles 1 Claliam Port Angeles 1 Claliam Port Angeles 1	101 101 101		2016-11-21 17:35 No Apparent Injury 0 0 2 0 0 2018-02-01 16:58 No Apparent Injury 0 0 2 0 0 2017-01-11 15:56 No Apparent Injury 0 0 2 0 0 2019-10-16 10:55 Possible Injury 1 0 1 1 0	Passenger Car Pickup Panel Truck or Vanette under 10,000 Ib Passenger Car Pickup Panel Truck or Vanette under 10,000 Ib Passenger Car Pickup Panel Truck or Vanette under 10,000 Ib Pickup Panel Truck or Vane	Not at Intersection and Not Related Intersection Related but Not at Intersection At Driveway	Clear or Partly Cloudy Raining Clear or Partly Cloudy	Dry Straight & Level Wet Straight & Level Dry Straight & Level	Dusk Dusk Daylight	From same direction - both going straight - both moving - rear-end Entering at angle	Going Straight Ahead Making Left Turn	Slowing Going Straight Ahead Slowing Going Straight Ahead	East West East West ast West East West bouth West East West
6292 State Route 6293 State Route 6294 State Route	Cialiam Port Angeles 1	101 101 101					Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade [Dry Straight & Grade [Daylight Dark-Street Lights On	Vehicle turning right hits pedestrian Tree or Stump (stationary)	Making Right Turn Going Straight Ahead		South East East West East Vehicle Stopped
6232 State Route 6233 State Route 6234 State Route 6235 State Route 6236 State Route 6236 State Route 6237 State Route 6238 State Route 6238 State Route	Cialiam Port Angeles 1 Cialiam Port Angeles 1 Cialiam Port Angeles 1 Cialiam Port Angeles 1 Cialiam Port Angeles 1	101 101 101	249.75 No E397873 249.75 No E46226 249.76 No 267093	2015-02-03 14-21 Possible Injury 2 0 2 0 0 0 0 2015-11-15 07-30 Possible Injury 1 0 2 0 0 0 2014-12-11 04-37 No Apparent Injury 0 0 2 0 0	Passenger Car Pickup Parel Truck or Vanette under 10,000 ib Passenger Car Not Stated Truck (Flabback Wanetc) Pickup Parel Truck or Vanette under 10,000 ib Pickup Parel Truck or Vanette under 10,000 ib	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Raining Raining	Dry Straight & Level [Wet Straight & Level [Wet Straight & Level [Daylight Daylight Dark-Street Lights (2)	From same direction - both going staight - one stopped - rear-end From same direction - both going staight - both moving - rear-end From same direction - both going staight - one stopped - rear-end From same direction - both going staight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic Going Straight Ahead Stopped for Traffic Going Straight Ahead	Vest East West East Vehicle Stopped Vest East West Sast Vehicle Stopped Vest East West South
6298 State Route	Cialiam Port Angeles 1 Cialiam Port Angeles 1	101	249.76 No 2867093 249.76 No E980762	2019-11-08 13:49 Possible Injury 1 0 4 0 0	Truck (Flatbad, Van,etc) Pickup, Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level [Daylight	From same direction - both going straight - both moving - sideswipe From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead	Going Straight Ahead Going Straight Ahead	ast West East West

	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_3_UNIT_1 MV_DRIVER AGE UNIT_1 MV_DRIVER GENDER UNIT_1 42 Male	MV_DRIVER_CONTRIBUTING MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_1_UNIT_2 CIRCUMSTANCE_2_UNIT_2	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_3_UNIT_2 MV_DR AGE_UI	MV_DRIVER GENDER UNIT_2 66 Female	R BICYCLIST_CONTRIBUTING BICYCLIST_CONTRIBUTING CIRCUMSTANCE_1_UNIT_1 CIRCUMSTANCE_2_UNIT_1	BICYCLIST_CONTRIBUTING CIRCUMSTANCE_3_UNIT_1 BICYCLIST AGE UNIT_1	BICYCLIST GENDER UNIT_1 BICYCLIST. CONTRIBUTING GRCUMSTANCE 1_UNIT_2 GRCUMSTANCE 2	BICYCLIST_CONTRIBUTING INIT_2 CIRCUMSTANCE_3_UNIT_2	BICYCLIST AGE_UNIT_2	BICYCLIST PEDESTRIAN_CONTRIBUTING PEDESTRIAN_C GRODER_UNIT_2 GIRCUMSTANCE_1_UNIT_2 GIRCUMSTANC	INTRIBUTING PEDESTRIAN. CONTRIBUTING PEDESTRIAN PEDESTRIAN GENORE. UNIT. 2 CIRCUMSTANCE. 3. UNIT. 2 GENORE UNIT. 2	
nattention	42 Maie 87 Maie 22 Female	None		63 Male								Lane of Primary Traficoway Lane of Primary Traficoway Lane of Primary Traficoway
ad Taten Medicanton ad Taten Medicanton propose Backer aceding Reas. Sale Speed ther Contributing Circ Not Listed aceding Reas Sale Speed ther Contributing Circ Not Listed aceding Reas. Sale Speed	52 Male 56 Female 35 Female	None		23 Female								Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway Outside Shoulder of Primary Trafficway
hther Contributing Circ Not Listed xxxxxxxxxxxxxxxxxxxxxxxxxxxxxxxxxxx	35 Female 56 84 Female											Outside Shoulder of Primary Trafficway Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
xxxxxxxxxxxxxxxxxxxxxxxxxxxxxxxxxxxxxx	68 Male 86 Male 23 Female 35 Female	None		55 Female								Lane of Primary Trafficway
lid Not Grant RW to Vehicle Improper Passing	23 Female 35 Female	Improper Signal		51 Male								Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway
lone Inder Influence of Alcohol Internation Control of Alcohol				60 Female								Lanc of Primary Traticusy Past the Outside Shoulder of Primary Tratificway Past the Outside Shoulder of Primary Tratificway Outside Shoulder of Primary Tratificway Outside Shoulder of Primary Tratificway
ilistractions Outside Vehicle tone tone tone tone tone tone tone ton	33 Male 20 Male 34 Male 82 Female	Mona		82								Past the Outside Shoulder of Primary Trafficway Use of Primary Trafficway Use of Primary Trafficway
nattention Inknown Distraction weeding Stated Speed Limit	oz remare	NOTE		03								Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
Informat Distraction Jacceding Stated Speed Limit Exceeding Stated Speed Limit Exceeding Reas. Safe Speed	16 Male 17 Fernale 72 Male 32 Male 65 Fernale 45 Male											Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
pparetniy rasgueu lone Iriser Not Distracted	32 Male 62 Female											Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
ione when Not Distracted (one opparently Asless or Fatiqued	45 Male											Past the Outside Shoulder of Primary Trafficway
pipenkinny professor by languages. International professor by the Passengers, Anim inknown Distraction one one of the Passengers, Anim inknown Distraction one one of the Passengers by the Pas	27 Male 29 Male 39 Male											Bast the Outside Shoulder of Finnery Trafficway Past the Outside Shoulder of Pinnery Trafficway Past the Outside Shoulder of Pinnary Trafficway Past the Outside Shoulder of Pinnary Trafficway
lone lone	39 Male 18 Male											Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
Inknown Distraction pparently Asleep or Fatigued	20 Male 81 Male											Past the Outside Shoulder of Primary Trafficway
Ione Individual Control Indiv	20 Male 81 Male 35 Female 80 Female	None		49 Male								Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway
xpeeding Reas, Safe Speed	30 Female											Past the Outside Shoulder of Primary Trafficway
ther Contributing Circ Not Listed triver Not Distracted lad Taken Medication	76 Male 42 Male											Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
Inknown Distraction Id Not Grant RW to Vehicle perating Detective Equipment xxxxeding Reas. Safe Speed	26 Male 75 Male 40 Female 17 Female 63 Male	None		23 Male								Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway Lane Of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
perating Defective Equipment exceeding Reas. Safe Speed	40 Female 17 Female											Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
one szeeding Reas. Safe Speed szeeding Reas. Safe Speed szeeding Reas. Safe Speed szeeding Reas. Safe Speed	63 Male 18 Female											Past the Outside Shoulder of Primary Trafficway
occeding Reas. Safe Speed occeding Reas. Safe Speed	18 Female 20 Male 68 Male											Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
	24 Male 57 Male											
pparently Fatigued Linknown Distraction	66 Female 18 Female	Other Contributing Circ Not Listed										Other Location (ChyCountyMac. Trafficway) Past the Guistide Shoulder of Primary Trafficway Past the Guistide Shoulder of Primary Trafficway
Ather Contributing Circ Not Listed	77 Male 40 Male 31 Male 27	Other Contributing Circ Not Listed		53 Female								
istractions Outside Vehicle sastention tone				49 Female								Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway
pparently Acleep or Fatigued nattention	31 Female											Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
popurently Asleep or Fatigued attention papurently Asleep or Fatigued papurently Asleep or Fatigued attention attention attention attention	17 Male 46 Male 82 Female 16 Male 22 Mala											Past the Outside Shoulder of Primary Traffioway Past the Outside Shoulder of Primary Traffioway Past the Outside Shoulder of Primary Traffioway
river Adjusting Audio or Entertainment	16 Male 22 Male								$\pm \equiv$			Past the Outside Shoulder of Primary Trafficway
pparently Aeleep or Fatigued instractions Cutsids Vehicle wher Contributing Circ Not Listed	22 Male 28 Male 47 Female 78 Male											Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
	78 Male											Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
veneding Reas, Safe Speed	64 Male 24 Female 20 Female											Past the Custor Shoulder of Primary Trafficway Bast the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
Inder Influence of Alcohol Exceeding Reas. Safe Speed												Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway
Ione Ione Ione Ione Ione Ione Ione Ione	18 Male 29 Male 60 Male 67 Male 77 Male 12 Male 13 Female 16	None										Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Outside Shoulder of Primary Trafficway Lane of Primary Trafficway
lone Unknown Distraction	60 Male 57 Male	None		69 Male								
inder Influence of Alcohol olilow Too Closely When Adjusting Audio or Entertainment	32 Male 18 Female	None		41 Male								Past the Outside Shoulder of Primary Traffioway Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
	66 Male											Past the Outside Shoulder of Primary Trafficway
pparently Fatigued iid Not Grant RW to Vehicle	33 Female 79 Female 68 Male	None None		74 Male 61 Female								Lane of Primary Trafficway Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
	68 Male 92 Male						None		27	Female		Past the Outside Shoulder of Primary Trafficway Outside Shoulder of Primary Trafficway
lid Not Grant R/W to Non Motorist id Not Grant R/W to Vehicle one sattention	31 Male 74 Male	None		66 Female								Lane of Primary Trafficway Lane of Primary Trafficway
pparently Fatigued	92 Male 31 Male 74 Male 25 Male 64 Male											Outside Shoulder of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
Ione Sastention Other Driver Distractions Inside Vehicle Operanity III Unknown Distraction Inside Vehicle	40 Female 59 Male											
pparently Asleep or Fatigued	75 Male 31 Female											Past the Outside Shoulder of Primary Trafficway
pparently Asleep or Fatigued Inattention	73 Male 46 Female											Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
lone	50 Mate 75 Mate 31 Female 77 Mate 44 Female 20 Mate 44 Female											Past the Outside Shoulder of Primary Trafficway
pparently Fatigued Other Contributing Circ Not Listed lattention Over Center Line												Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
attention Over Center Line paparently Asleep or Fatigued lone axeeding Reas. Sale Speed	25 Maie 39 Male	Driver Not Distracted		43 Male								Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
xeeding Reas. Safe Speed xattention xattention xxeeding Reas. Safe Speed ther Contributing Circ Not Listed inder Influence of Alcohol		None		26								Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway Death - Outside Shoulder of Death
Meer Contributing Circ Not Listed	89 Female	New		E E Mala								Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
vier Commoning Circ Not Exeeu Index Filliagues of Notonol Index Influence of Drugs paperenty Faligued Operating Handheld Cell Phone sattention	46 Female 43 Maie 53 Female	NOTE		55 Male								Past to Coulide Studied or Pinnary Tutlicowy Past to Coulide Studied or Pinnary Tutlicowy Past to Coulide Studied Pinnary Tutlicowy Past to Coulide Studied Pinnary Tutlicowy Past to Coulide Studied or Pinnary Tutlicowy Lane of Pinnary Tutlicowy Past to Coulide Studied or Pinnary Tutlicowy Lane of Pinnary Tutlicowy Past to Coulide Studied or Pinnary Tutlicowy Past to Coulide Studied or Pinnary Tutlicowy
apparently rangued eparating nationed cert i note	43 Mere 4 4 5 Mere 4 5 Mere 4 5 Mere 5 Mere 5 Mere 6 Mere											Past the Outside Shoulder of Primary Trafficway
Ione xxxeding Reas. Safe Speed paparently Aeleep or Fatigued pparently Aeleep or Fatigued	31 Female 25 Male	None		46 Male								Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway
	21 Male 57 Female											Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
pparently As leep or Fatigued	59 Maie 37 Maie 50 Female											Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
Ione Irriver Not Distracted	63 Male											Pass the Obliside Shoulder of Primary Trafficway Lane of Primary Trafficway Lane of Primary Trafficway Pass the Obliside Shoulder of Primary Trafficway Lane of Primary Trafficway Pass the Obliside Shoulder of Primary Trafficway
ione nattention	20 Maie 74 Maie											Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
atting or Lumenag atting or Lumenag long long	18 Male											Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
triver Interacting with Passengers, Anim perating Defective Equipment Apparently Fatigued Exceedin		Driver Not Distracted		31 Female								Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
perating Defective Equipment Apparently Fatigued Exceedir pparently Asleep or Fatigued Over Center Line pparently Asleep or Fatigued	22 Male 27 Female	None		70 Male								Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
Operating Handheld Cell Phone Wher Contributing Circ Not Listed Inattention	48 53 Maie											
lone xceeding Reas. Safe Speed	48 53 Male 22 Male 29 Male 19 Male							-				Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
Inder Influence of Alcohol Operating Other Electronic Devices (comp Inder Influence of Alcohol Exceeding Reas. Safe Speed	19 Male 32 Male											
xceeding Stated Speed Limit Unknown Distraction pparently Fatigued	39 Female 26 Female											Past the Outside Shoulder of Pinnary Trafficuray
xxxeeaing reas. Safe Speed	19 Male 32 Male 33 Female 26 Female 17 Male 21 Female 28 Male											Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway
Content of Missigned Exceeding Shasin Spleed Limit Content of Missigned Exceeding Shasin Spleed Limit Content of Missigned Exceeding Shasind Spleed Limit Content of Missigned Content of Missi	20 Mote 49 Fernale 49 Fernale 22 Mote 50 Mote 50 Mote 50 Mote 60 Mote 77 Fernale 61 Mote 63 Mote 64 Mote 65 Mo	Name of the state										Past the Outside Shoulder of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway
pparently Ac leep or Fatigued	22 Male 80 Male	None										
attention long	34 Male 60 Male											Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway Lane of Primary Trafficway
ione	75 Female 88 Male	New		40.00								Outside Shoulder of Primary Trafficway
Ather Contributing Circ Not Listed ther Contributing Circ Not Listed	35 Male 67 Male	Driver Not Distracted		49 Male								Lane of Primary Trafficway Past the Outside Shoulder of Primary Trafficway
pparently III	57 Female 37 Male	Name and Distriction		63 Female								Lane 3 Increasing Miepost In Median Increasing Miepost
attention Unknown Distraction	18 Male 54 Female	None Ovier Not De Israele Ovier Not De Israele None None None None None None None Non		31 Male 69 Female 25 Male 57 Male								Lane 2 Increasing Milepost Lane 1 Increasing Milepost Lane 1 Decreasing Milepost
Inform Distraction	61 Male 78 Male	Driver Not Distracted		57 Male								Left Turn Lane Decreasing Milepost
sattention Follow Too Closely lisregard Stop and Go Light benefit on Defective Equipment	Female 58 Female	Driver Not Distracted		74 Male								Lane 2 Decreasing Milepost Lane 1 Increasing Milepost Lane 1 Increasing Milepost Lane 2 Increasing Milepost
perating Defective Equipment Inknown Distraction Follow Too Closely	28 Male 19 Male	None Driver Not Distracted None None None		44 Female 16 Female								Intersecting Road Increasing Milepost Lane 1 Decreasing Milepost
onow roo crosely Index Influence of Alcohol	47 36 Female 51 Female	None										Intersecting Road Increasing Milepost Right Shoulder Increasing Milepost Lane 2 Increasing Milepost
safeticities Control Interest Control Interes				83 Female 40 Male 19 Female								Lane 1 Increasing Milepost
ollow Too Closely perating Defective Equipment Other Contributing Circ Not Listed	20 Male 26 Male	Other Contributing Circ Not Listed Driver Not Distracted		28 Male								Lane 1 Decreasing Miepost Lane 2 Increasing Miepost Left Turn Lane Decreasing Miepost
ollow Too Closely	34 Female 32 Male	Driver Not Distracted Other Contributing Circ Not Listed Driver Not Distracted None Other Contributing Circ Not Listed		28 Male 61 Male 57 Male								Lane 1 Decreasing Milepost
one oliow Too Closely				20 Female 17 Male								Left Turn Lane Decreasing Miepost Lane 2 Decreasing Miepost Lane 2 Decreasing Miepost
ollow Too Closely	59 Maie 46 Maie	Follow Too Closely Driver Not Distracted None		33 i ciliare								Lane 2 Decreasing Milepost
Interior Distraction Unknown Distraction Follow T	35 Male Too Closely 39 Female wn Distraction 78 Female	None		33 Male								Left Turn Lane Decreasing Milepost Lane 1 Increasing Milepost
attention Unknown Distraction Follows T ther Contributing Circ Not Listed Did Not Grant RW to Vehicle Unknown Inknown Distraction Follow Too Closely	wil Diseaction 78 Female 17 Female	None		25 Male 67 Male 57 Male 56 Female								Lane 1 Decreasing Miepost Lane 2 Decreasing Miepost
ollow Too Closely ollow Too Closely	40 Male 20 Male	Over Not Distracted		56 Female 83 Female 37 Male								Lane 1 Decreasing Milepost Lane 2 Decreasing Milepost
ollow Too Closely attention	71 Male 53 Male	None None		39								Lane 2 Decreasing Milepost Lane 1 Decreasing Milepost
ollow I do Closely ollow Too Closely	29 Male 26	None None		65 Female 48 Male 79 Female								Lane 2 Decreasing Mlepost
na not Grant RW to Vehicle sattenbon	26 49 Male 63 Female	None		79 Female						Inattention	25 Male	Lane 2 Decreasing Milepost Lane 2 Decreasing Milepost Right Shoulder Increasing Milepost
ulterrison of lotton Toc Closely oldow Toc Closely oldow Toc Closely oldow Toc Closely which userding Reas. Sate Speed userding Reas. Sate Speed userding Reas. Sate Speed ulterrison Follow Toc Closely oldow Toc	27 Male 51 Male 23 Male 54 Female	None										
satemion Follow Too Closely ollow Too Closely	23 Male 54 Female	None		32 Female 45 Female 63 Male								Lane 2 Decreasing Milepost Lane 2 Increasing Milepost Lane 2 Decreasing Milepost
ione.	30 Pelliale			57 Female		1			+	+ + + - + +		Lane 1 Increasing Milepost Lane 2 Decreasing Milepost
attention	41 Male	Industrison		D) I ciliale								

		OBJECTID WA. STATE. PLANE SOUTH. X SOUTH. Y 2010_FORWARD COL.		COL COL_HUM SEVERITY FACTOR	COND	COL COL_LIGHT RFACE CONDITIONS CONDITIONS	COL COL STUD LOCATION GEOMETRY AREA	IEXT IEXT	COL_HUMAN COL FACTOR_Decoded MVIW	COL_LIGHT COL_SUR CONDITIONS CONDITI Decoded Decod		COL_RISKY Intersecti BEHAVIOR Area_Nar	tion Emphasis Roadway Departure
	Column	6121 1004721.31 1026533.41 2018	1 Vehicle/Party Animal 1 Vehicle/Party Struck Object	Property Damage Only Inattention Property Damage Only Other/Unknown Property Damage Only Under Influence of Alcohol/Drugs/Medication	15+ No	Dry Dawn/Dusk Dry Other/Unknown Wet Daylight	At Driveway or Intersection Straight & Level Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Curve & Grade	Older Road User 65+ Ins No Vulnerable Road User Involved No No Vulnerable Road User Involved Ur	attention one Inder Influence of Alcohol/Drugs/Medication Page 18 solving Page 19 of Turning Page 19 of Signature	Daylight Units On Day	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Inattention No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication	No No No Yes
	The content of the	6124 1004515.49 1027345.49 2019 6125 1004685.22 1027119.81 2017 6126 1004661.59 1027139.58 2019 6127 100421.85 102745.23 2019	1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object	Property Damage Only Speeding Property Damage Only Other/Unknown Property Damage Only Speeding		ow/Slush Daylight Dry Dark-Street Lights On ow/Slush Daylight	Not at Intersection/Driveway/Roundabout Curve & Grade Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Curve & Grade	No Vuinerable Road User Involved Sp No Vuinerable Road User Involved Ot Older Road User 65+ Sp	peeding ther Contributing Circ Not Listed peeding	Daylight Ice/Snow/ Dark-Street Lights On Dry Daylight Ice/Snow/ Daylight Ice/Snow/	Slush Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Slush Not at Intersection/Driveway/Roundabout Slush Not at Intersection/Driveway/Roundabout	Speeding	Yes Yes Yes Yes
The content of the		6128 1004273.32 1027583.87 2015 6129 1004101.45 1027765.24 2018 6130 1002988.87 102888.87 2017 6131 1002913.04 1028954.77 2014	2 Vehicles/Parties Turning 1 Vehicle/Party Struck Object 2 Vehicles/Parties Sideswipe 3 Vehicles/Parties (Sideswipe	Property Damage Only Follow Too Closely Unknown Under Influence of Alcohol/Drugs/Medication Property Damage Only Fail to Yield Property Damage Only Others Inknown	15+ No	Dry Daylight Dry Dark-No Street Lights/Street Lights Off Dry Daylight	At Driveway or Intersection Curve & Level Not at Intersection/Driveway/Roundabout Unknown At Driveway or Intersection Straight & Level	Older Road User 65+ Fo No Vulnerable Road User Involved Ur No Vulnerable Road User Involved Fa	ollow Too Closely nder Influence of Alcohol/Drugs/Medication ail to Yield	Daylight Dry Dark-No Street Lights/Street Lights Off Dry Daylight Dry	At Driveway or Intersection Not at Intersection/Driveway/Roundabout At Driveway or Intersection Not at Intersection/Driveway/Roundabout	Fail to Yield	No Yes Yes No
Second Process Seco	Column	6132 999925.12 1034539.45 2017 6133 999728.67 1034993.05 2015 6134 1004460.29 1027418.48 2014 6135 1001756.82 1017514 2016	2 Vehicles/Parties Struck Object 1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object	Property Damage Only Under Influence of Alcohol/Drugs/Medication Property Damage Only Distracted Driving Property Damage Only Other/Unknown	25:34 Under Influence of Alcohol 25:34 D Judger 25 No D 0 15:534 No looSno looSno	Dry Dark-No Street Lights/Street Lights Off Dry Daylight ow/Slush Daylight Dry Davlight	Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Curve & Grade At Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved Ur No Vulnerable Road User Involved Di No Vulnerable Road User Involved No	nder Influence of Alcohol/Drugs/Medication istractions Outside Vehicle one	Dark-No Street Lights/Street Lights Off Dry Daylight Dry Daylight Ice/Snow/	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Slush Not at Intersection/Driveway/Roundabout	Under Influence of Alcohol/Drugs/Medication Distracted Driving No Risky Driving Behaviors Involved Inattention	Yes Yes Yes No
	The content of the	6136 1063311.03 915576.26 2016 6137 1063362.25 914387.9 2014 6138 1065211.09 931756.98 2019 6139 1100942.35 951240.65 2017	1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object 1 Vehicle/Party Over Embankment - No Guardrail Present 1 Vehicle/Party Struck Object	Unknown Distracted Driving Property Damage Only Speeding Minor or Possible Injury Operating Defective Equipment	Inknown No	Wet Dark-No Street Lights/Street Lights Off Wet Daylight Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Curve & Grade Not at Intersection/Driveway/Roundabout Unknown At Driveway or Intersection Not at Intersection/Driveway/Roundabout Curve & Grade Onter Straight & Level	No Vulnerable Road User Involved Ur No Vulnerable Road User Involved Sp No Vulnerable Road User Involved Op	perating Defective Equipment river Physical Condition	Daylight Dry Daylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout At Driveway or Intersection Not at Intersection/Driveway/Roundabout	Speeding No Risky Driving Behaviors Involved Apparently Asleep, Fatigued or III	Yes Yes Yes Yes
The content of the	Company	6140 1072230.86 986007.5 2014 6141 1073452.35 986100.11 2019 6142 1074201.59 986146.46 2018 6143 1074531.853 986163.6003 2015	1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object 1 Vehicle/Party Over Embankment - No Guardrail Present	Suspected Serious Injury Other/Unknown Property Damage Only Other/Unknown Mnor or Possible Injury Other/Unknown Property Damage Only Apparenty Asteen Fatigued or III	25:34 No W 35:64 No D 35:54 No D	Wet Dark-No Street Lights /Street Lights Off Dry Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off Dry Davijoht	Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight at Hillcrest Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Curve & Grade	No Vulnerable Road User Involved No No Vulnerable Road User Involved Ot No Vulnerable Road User Involved No No Vulnerable Road User Involved Ac	one theriUnknown one parently Asleep or Fatigued	Dark-No Street Lights/Street Lights Off Wet Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off Dry Daylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	Yes No Yes Yes
The content of the	Company	6144 1074821.35 986155.56 2014 6145 1074988.2 986129.6095 2014 6146 1075194.813 986085.8718 2018 6147 1076850.86 985124.76 2017	1 Vehicle/Party	Property Damage Only Distracted Driving Property Damage Only Distracted Driving Property Damage Only Other/Unknown Property Damage Only Other/Unknown	25:34 No W 25:34 No D 25:34 No D 25:44 No W 10:44 No D	Wet Daylight Dry Daylight Wet Daylight Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Unknown Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Curve & Grade Not at Intersection/Driveway/Roundabout Curve & Grade	No Vulnerable Road User Involved Di No Vulnerable Road User Involved Ur No Vulnerable Road User Involved Nr No Vulnerable Road User Involved Nr	istracted Driving	Daylight Wet Daylight Dry Daylight Wet Daylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Distracted Driving Distracted Driving No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	Yes Yes Yes Yes
The content of the	Company	6148 1079239.317 986000.4891 2014 6149 1078748.43 985625.77 2015 6150 1079603.68 986587.08 2019 6151 1079707.47 986888.59 2015	1 Vehide/Party Struck Object 1 Vehide/Party Struck Object 1 Vehide/Party Struck Object 2 Vehides/Parties Ingle	Minor or Possible Injury Apparently Asleep, Fatigued or III Suspected Serious Injury Under Influence of Alcohol/Drugs/Medication	Inder 25 No D 05+ Apparently Asleep, Fatigued or III 65+ VI 8544 Under Influence of Drugs and Apparently Asleep, Fatigued or III 35-44 D 55+ No D	Dry Daylight Wet Dark-No Street Lights/Street Lights Off Dry Dawn/Dusk Dry Daylight	Not at Intersection/Driveway/Roundabout Curve & Grade Not at Intersection/Driveway/Roundabout Straight & Grade	Older Road User 65+ Ap No Vulnerable Road User Involved Ap	pparently Asleep or Fatigued pparently Asleep or Fatigued	Daylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Apparently Asleep, Fatigued or III Under Influence of Alcohol/Drugs/Medication	Yes Yes Yes No
	The content of the	6152 1081924.7 990742.5 2016 6153 1101945.72 966282.89 2019 6154 1091435.45 989478.56 2018 6155 1091771.2 989345.9 2019	1 Vehicle/Party Struck Coject 1 Vehicle/Party Animal 1 Vehicle/Party Struck Object	Minor or Possible Injury Property Damage Chily Other/Unknown Property Damage Chily Other/Unknown Property Damage Chily Under Influence of Alcohol/Drugs/Medication	Inknown No D	Dry Daylight Dry Daylight Wet Dark-No Street Lights/Street Lights Off	Intersection Related but Not at Intersection Unknown Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Unknown	No Vulnerable Road User Involved Ot Older Road User 65+ Ot No Vulnerable Road User Involved Ur	ther Contributing Circ Not Listed ther/Unknown nder Influence of Alcohol/Drugs/Medication	Daylight Dry Daylight Dry Dark-No Street Lights/Street Lights Off Wet	Intersection Related but Not at Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication	Yes Yes No Yes
The content of the		6156 1095457.4 982579.71 2018 6157 1093003.03 988280.52 2016 6158 1093883.83 987107.9 2019 6159 1095838.33 980686.82 2016	2 Vehides/Party Angle 1 Vehide/Party Overtumed 1 Vehide/Party Over Embankment - No Guardrail Present	Property Damage Only Distracted Driving Property Damage Only Fail to Yield Minor or Possible Injury Operating Defective Equipment Minor or Possible Injury Speeding	15+ No	Dry Daylight Dry Daylight Dry Daylight Dry Dark-No Street Lights/Street Lights Off	At Driveway or Intersection Curve & Grade Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Curve & Grade	Older Road User 65+ Fa Motorcycle Op No Vulnerable Road User Involved Sp	ail to Yield perating Defective Equipment peeding	Daylight Dry Daylight Dry Dark-No Street Lights/Street Lights Off Dry	At Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Fail to Yield No Risky Driving Behaviors Involved Speeding	Yes No Yes Yes
The content of the		6160 1095602.528 982371.996.2014 6161 1095615.288 982349.1033 2016 6162 1095666.2 982231.78 2015 6163 1094705.67 983693.33 2016	1 Vehide/Party Struck Object 1 Vehide/Party Struck Object 1 Vehide/Party Struck Object	Property Damage Only Speeding Minor or Possible Injury Speeding Minor or Possible Injury Speeding	Inder 25 No Vi Juder 25 No Vi S5+ No Vi	Wet Dawn/Dusk Wet Daylight Wet Dark-No Street Lights/Street Lights Off	Not at Intersection/DrivewayRoundabout Curve & Grade At Intersection and Not Related Curve & Grade Not at Intersection/DrivewayRoundabout Curve & Grade	No Vulnerable Road User Involved Sp No Vulnerable Road User Involved Sp Older Road User 65+ Sp	peeding peeding peeding	Dawn/Dusk Wet Daylight Wet Dark-No Street Lights/Street Lights Off Wet	Not at Intersection/Driveway/Roundabout At Intersection and Not Related Not at Intersection/Driveway/Roundabout	Speeding Speeding Speeding	Yes Yes Yes Yes
Column	The content of the	6164 1095738.23 981692.85 2015 6165 1095743.92 981632.85 2015 6166 1095915.491 979886.3717 2015 6167 1095835.14 980718.66 2017	1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object	Suspected Serious Injury Apparently Asleep, Fatigued or III Property Damage Only Apparently Asleep, Fatigued or III Property Damage Only Inattention	15-64 Apparently Asleep, Fatigued or III 15-64 D 15-64	Dry Daylight Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level	No Vuinerable Road User Involved Ap Older Road User 65+ Dr No Vuinerable Road User Involved Ins	pparently Askeep or Fasigued pparently Askeep or Fasigued inver Physical Condition sattention	Daylight Dry Daylight Dry Daylight Dry Daylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Apparently Asleep, Fatigued or III Apparently Asleep, Fatigued or III Inattention	Yes Yes Yes Yes
			1 Vehicle/Party Struck Object 2 Vehicles/Parties Rear-End 1 Vehicle/Party Animal	Property Damage Only Distracted Driving Property Damage Only Inattention Property Damage Only Other/Unknown	15-44 No D 15-34 No D 15-534 No D	Dry Daylight Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Curve & Level Intersection Related but Not at Intersection Straight & Level Not at Intersection/Driveway/Roundabout Curve & Level	No Vulnerable Road User Involved Di No Vulnerable Road User Involved In: No Vulnerable Road User Involved No	istractions Outside Vehicle attention one	Daylight Dry Daylight Dry Daylight Dry	Not at Intersection/Driveway/Roundabout Intersection Related but Not at Intersection Not at Intersection/Driveway/Roundabout	Distracted Driving Inattention No Risky Driving Behaviors Involved	Yes No No
The content of the		6173 1099303.479 971214.2542 2015 6174 1099277.35 971235.35 2019 6175 1100015.63 969818.89 2014 6176 1100178.111 969560.3415 2015	1 Vehide/Party Struck Object 1 Vehide/Party Struck Object 1 Vehide/Party Struck Object 1 Vehide/Party Struck Object	Property Damage Only Inattention Property Damage Only Apparently Asleep, Fatigued or III Property Damage Only Inattention	Jnder 25 No	Wet Dawn/Dusk Dry Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved In: No Vulnerable Road User Involved Ap Older Road User 65+ In:	attention	Dawn/Dusk Wet	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Inattention	Yes Yes Yes Yes Yes Yes
		6177 1101825.37 968096.23 2016 6178 1099751.89 970309.1 2018 6179 1101982.07 967503.67 2016 6180 1101972.46 967353.98 2015	1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object	Property Damage Only Distracted Driving Minor or Possible Injury Apparently Aleepe, Fatigued or III Property Damage Only Distracted Driving		Dry Daylight Wet Daylight Wet Daylight Wet Daylight Wet Daylight Wet Daylight	Not at Intersection/Driveway/Roundabout Curve & Grade Not at Intersection/Driveway/Roundabout Curve & Crade Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Straight & Grade	No Vulnerable Road User Involved Dr No Vulnerable Road User Involved Ap No Vulnerable Road User Involved Di	pparently Asleep or Fatigued istractions Outside Vehicle	Dry Dark-No Street Lights/Street Lights Off Wet Darkight Wet	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Apparently Asleep, Fatigued or III Distracted Driving	Yes Yes Yes Yes
	The content of the	6181 1101930.174 966680.3259 2015 6182 1101817.21 964873.88 2015 6183 1101777.97 963838.46 2019 6184 1096728.82 976589.63 2018	1 Vehicle/Party Struck Object 1 Vehicle/Party Animal 1 Vehicle/Party Animal 1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object	Unknown Other/Unknown Property Damage Only Other/Unknown	No.	Dry Dark-No Street Lights/Street Lights Off Wet Dark-No Street Lights/Street Lights Off Wet Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Curve & Grade Not at Intersection/Driveway/Roundabout Straight & Grade	No Vulnerable Road User Involved No No Vulnerable Road User Involved No No Vulnerable Road User Involved Sp		Dark-No Street Lights/Street Lights Off Uest Dark-No Street Lights/Street Lights Off Wet Dark-No Street Lights/Street Lights Off Wet Dark-No Street Lights/Street Lights Off Uest Dark-No Street Lights/Street Ligh	Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Speeding	Yes No Yes Yes
	The control of the	6185 1102561.35 962223.66 2017	1 Vehicle/Party Animal	Minor or Possible Injury Other/Unknown Minor or Possible Injury Speeding Property Damage Only Other/Unknown	25-34 No D Inder 25 No Ice/Sno	Dry Dark-No Street Lights/Street Lights Off ow/Slush Dark-No Street Lights/Street Lights Off Dry Dark-Street Lights On	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Grade	Motorcycle No No Vulnerable Road User Involved Sp No Vulnerable Road User Involved Ot		Dark-No Street Lights/Street Lights Off Ice/Snow/	Not at Intersection/Driveway/Roundabout Slush Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Speeding No Risky Driving Behaviors Involved	No Yes Yes No
	The content of the	6189 1104768.31 956750.8 2014 6190 1104778.8 955514.55 2017 6191 1104765.32 954719.89 2018 6192 1104756.825 954596.8567 2015	2 Vehicles/Parties Turning 1 Vehicles/Parties Struck Object 2 Vehicles/Parties Rear-End	Property Damage Only Improper Turn/Merge Minor or Possible Injury Under Influence of Alcohol/Drugs/Medication Property Damage Only Follow Too Closely	55-64 No	Dry Daylight Wet Dark-No Street Lights/Street Lights Off	At Driveway or Intersection Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level Intersection Related but Not at Intersection Straight & Level	No Vulnerable Road User Involved Im No Vulnerable Road User Involved Ur No Vulnerable Road User Involved Fo	nder Influence of Alcohol/Drugs/Medication ollow Too Closely	Daylight Dry Dark-No Street Lights/Street Lights Off Wet Daylight Wet Daylight Wet	At Driveway or Intersection Not at Intersection/Driveway/Roundabout Intersection Related but Not at Intersection	No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication Follow Too Closely	No Yes No Yes
	The column	6193 1104799.08 953219.63 2015 6194 1104736.04 952796.9 2018 6195 1104697.567 952698.9632 2014	1 Vehicle/Party Struck Object 2 Vehicles/Parties Turning 2 Vehicles/Parties Angle	Minor or Possible Injury Apparently Asleep, Fatigued or III Property Damage Only Apparently Asleep, Fatigued or III Property Damage Only Fati to Yield		Dry Dark-Street Lights On	Not at Intersection/Driveway/Roundabout Curve & Level At Driveway or Intersection Curve & Grade At Driveway or Intersection Straight & Grade	Older Road User 65+ Ap No Vulnerable Road User Involved Dr Older Road User 65+ Ea	pparently Asleep or Fatigued river Physical Condition	Dark-Street Lights On Dry	Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Apparently Asleep, Fatigued or III Apparently Asleep, Fatigued or III Fail to Yield	Yes No No Yes
	Column C	6197 1104765.69 952994.4 2019 6198 1099956.79 951155.23 2015	2 Vehicles/Parties Pedalcyclist 2 Vehicles/Parties Angle	Minor or Possible Injury Fail to Vield Property Damage Only Fail to Vield Property Damage Only Other/Unknown		Wet Dawn/Dusk Dry Davlight	Not at Intersection Driveway/Roundabout Straight & Grade At Driveway or Intersection Straight & Level	Pedalcydist and Older Road User 65+ Di No Vulnerable Road User Involved Fa	id Not Grant R/W to Non Motorist ail to Yield	Dawn/Dusk Wet Daylight Dry Daylight Dry	Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Fail to Yield Fail to Yield	No No No Yes
Part	The content of the	6202 1076876.89 997944.59 2019	1 Vehicle/Party Struck Object	Property Damage Only Apparently Asleep, Fatigued or III Mnor or Possible Injury Office/Unknown Property Damage Only Inattention Apparently Asleep, Fatigued or III	15-44 No D	Dry Daylight Dry Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level	No Vuinerable Road User Involved No	one	Daylight Dry Dark-No Street Lights/Street Lights Off Dry Daylight Dry	Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Inattention	Yes Yes Yes Yes
Part	Column C	6205 1076982.52 997888.56 2017 6206 1063211.58 930721.12 2019	1 Vehicle/Party Struck Object	Suspected Serious Injury Apparently Asleep, Fatigued or III Minor or Possible Injury Apparently Asleep, Fatigued or III	25-34 Apparently Asleep, Fatigued or III 25-34 D 85+ Apparently Asleep, Fatigued or III 65+ D 15-5-54 No IcioSno Index 25 No IcioSno	Dry Daylight	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved	pparently Asleep or Fatigued pparently Asleep or Fatigued	Daylight Dry Daylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Slush Not at Intersection/Driveway/Roundabout Slush Not at Intersection/Driveway/Roundabout	Apparently Asleep, Fatigued or III Apparently Asleep, Fatigued or III Inattention	Yes Yes Yes Yes
	Column C	6210 1065263.54 931824.52 2015	1 Vehicle/Party Struck Object	Property Damage Only Apparently Asleep, Fatigued or III	15-44 No IceSno 15-54 Apparently Asleep, Fatigued or III 45-64 D 15-54 No W 25-34 Apparently Asleep, Fatigued or III 25-34 D	Dry Daylight Wet Dark-No Street Lights/Street Lights Off	Not at Intersection/DrivewayRoundabout Straight & Level Not at Intersection/DrivewayRoundabout Straight & Level	No Vulnerable Road User Involved No No Vulnerable Road User Involved Dr No Vulnerable Road User Involved In: No Vulnerable Road User Involved Ap	river Physical Condition	Daylight Dry Dark-No Street Lights/Street Lights Off Wet	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Apparently Asleep, Fatigued or III Inattention	Yes Yes Yes No
1	The column	6214 1069598.04 948952.19 2018	1 Vehicle/Party Struck Object	Property Damage Only Speeding Property Damage Only Inattention	15-14-0 NO	Wet Dark-No Street Lights/Street Lights Off MudDirt Dark-No Street Lights/Street Lights Off ow/Slush Daylight	Not at Intersection/Driveway/Roundabout Straight & Grade	No Vuinerable Road User Involved No No Vuinerable Road User Involved Sp No Vuinerable Road User Involved Ins	one peeding attention		d/Dirt Not at Intersection/Driveway/Roundabout	Speeding Inattention Speeding	Yes Yes No Yes
The column	Column C	6218 1067738.95 943408.66 2019 6219 1067766.74 940744.34 2017 6220 1067766.142 941495.4514 2014	2 Vehicles/Parties Struck Object 1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object		5+ No V	Wet Daylight Wet Dark-No Street Lights/Street Lights Off Dry Daylight	Not at Intersection/Driveway/Roundabout Curve at Hillcrest Not at Intersection/Driveway/Roundabout Straight at Hillcrest	No Vulnerable Road User Involved Ur No Vulnerable Road User Involved Ur No Vulnerable Road User Involved Dr	nder Influence of Alcohol/Drugs/Medication nder Influence of Alcohol/Drugs/Medication	Daylight Wet Dark-No Street Lights/Street Lights Off Wet Daylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Under Influence of Alcohol/Drugs/Medication Under Influence of Alcohol/Drugs/Medication	Yes Yes Yes Yes
Part	Column C	6222 1067837 935929.75 2018 6223 1067757.12 942352.83 2019 6224 1067779.119 940601.2597.2015	1 Vehicle/Party Animal	Property Damage Only Speeding	15-34 No Ioe/Sno	Dry Dank-No Street Lights/Street Lights Off		Older Road User 65+ No No Vulnerable Road User Involved Sp No Vulnerable Road User Involved Ap	peeding	Daylight Dry Daylight Ice/Snow/	Not at Intersection/Driveway/Roundabout	Speeding Apparently Asleep, Fatigued or III	Yes No Yes Yes
		6226 10677799.79 938921.13 2019 6227 1067776.18 941001.99 2019	1 Vehicle/Party Overturned 1 Vehicle/Party Over Embankment - No Guardrail Present	Minor or Possible Injury Apparently Asleep, Fatigued or III Minor or Possible Injury Apparently Asleep, Fatigued or III	5-64 Apparently Asleep, Fatigued or III 55-64 D		Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved Ap No Vulnerable Road User Involved Ap	pparently Asleep or Fatigued	Daylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Apparently Asleep, Fatigued or III	Yes Yes Yes
		6229 1067827.55 936976.86 2014 6230 1067829.713 93682.3988 2015 6231 1067830.68 936543.42 2018 6232 1069381.79 947990.13 2017	1 Vehicle/Party Felimas 1 Vehicle/Party Animas 1 Vehicle/Party Animas 1 Vehicle/Party Animas 1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object	Property Damage Only Other/Unknown	10-0-4 NO U.S. 55-64 NO U.S. 5	Wet Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level		ther/Unknown	Dark-No Street Lights/Street Lights Off Uet Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off Uet Dawn/Dusk Wet	Not at Intersection/Driveway/Roundabout	Inattention	Yes No Yes
Column C	Ministry March M	6235 1064241.34 931631.31 2018	1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object	Property Damage Only Distracted Driving Minor or Possible Injury Operating Defective Equipment	15-44 No D Inder 25 No D	Dry Daylight Dry Daylight	At Driveway or Intersection Curve & Level Not at Intersection/Driveway/Roundabout Curve & Grade	No Vulnerable Road User Involved Sp No Vulnerable Road User Involved Di No Vulnerable Road User Involved Oc	peeding istracted Driving perating Defective Equipment	Daylight Dry	At Driveway or Intersection Not at Intersection/Driveway/Roundabout	Speeding Distracted Driving No Risky Driving Behaviors Involved	Yes No Yes
March Marc	March Marc	6238 1059783.95 92353.64.2014 6239 1061329.69 928225.27 2014 6240 1060946.28 927710.33 2016	1 Vehicle/Party Over Embankment - No Guardrail Present	Property Damage Only Apparently Aeleep, Fasigued or III Mnor or Possible Injury Mnor or Possible Injury Distracted Driving Mnor or Possible Injury Other/Unknown Other/Unknown Other/Unknown	15-54 No Vi	Wet Dark-No Street Lights/Street Lights Off	Not at Intersection/DrivewayRoundabout Curve & Grade Not at Intersection/DrivewayRoundabout Curve & Grade	No Vulnerable Road User Involved O: No Vulnerable Road User Involved Ot	pparently Asleep or Fatigued berating Handheld Cell Phone	Daylight Day	Not at Intersection/Driveway/Roundabout	Distracted Driving	Yes Yes Yes
10	March Marc	6243 1060049.161 924128.745 2015	1 Vehicle/Party Over Embankment - No Guardrail Present	Property Damage Only Speeding Property Damage Only Under Influence of Alcohol/Drugs/Medication Property Damage Only Under Influence of Alcohol/Drugs/Medication	25-34 Under Influence of Alcohol 25-34 Vi	Dry Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Straight & Grade	No Vulnerable Road User Involved Ur No Vulnerable Road User Involved Ur	nder Influence of Alcohol/Drugs/Medication	Danie to direct Eightar direct Eightar Oil Bry	Not at Intersection/Driveway/Roundabout	Under Influence of Alcohol/Drugs/Medication	Yes Yes Yes Yes
March Marc	March Marc	6247 1059774.31 923414.774 2017	1 Vehicle/Party Struck Object 1 Vehicle/Party Animal	Minor or Possible Injury Speeding Property Damage Only Other/Unknown	25:34 Apparently Asleep, Fatigued or III 25:34 D D D D D D D D D	Dry Daylight Wet Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Curve & Grade Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Grade	No Vulnerable Road User Involved Dr	peeding one	Dark-No Street Lights/Street Lights Off Wet Dark-No Street Lights/Street Lights Off Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Speeding No Risky Driving Behaviors Involved	Yes Yes No Yes
Workstook Windows Wi	Wilson W	6252 1056863.25 918189.65 2017	2 Vehicles/Parties Struck Parked Vehicle 3 Vehicles/Parties Struck Parked Vehicle	Property Damage Only Improper Backing Property Damage Only Apparently Asleep, Fatigued or III Property Damage Only OtherUnknown	Jnder 25 No D	Dry Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Curve & Level	No Vulnerable Road User Involved Ap No Vulnerable Road User Involved Im Older Road User 65+ Ap	pparently Asleep or Fatigued pproper Backing/Passing/Turning/Parking/Signaling pparently Asleep or Fatigued one	Daylight Wet Dawn/Dusk Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Apparently Asleep, Fatigued or III	Yes Yes Yes No
1966 1975	Size Control	6255 1092586.43 988788.58 2019	1 Vehicle/Party Animal 1 Vehicle/Party Struck Object	Property Damage Only Inattention Property Damage Only Other/Unknown Property Damage Only Other/Unknown	55+ No Vi 55+ No D	Wet Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Curve & Level At Driveway or Intersection Straight & Grade	Older Road User 65+ No Older Road User 65+ No No Vulnerable Road User Involved In:	one one		Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Inattention No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	Yes No Yes No
1992.50 1992	1972 1972 1985	6260 931204.7 1024591.68 2018	1 Vehicle/Party Struck Object	Mnor or Possible Injury Apparently Asleep, Fatigued or III Property Damage Only Inattention	15-44 Apparently Asleep, Fatigued or III 35-44 Vi	Wet Daylight	At Driveway or Intersection Curve at Hillcrest Intersection Related but Not at Intersection Straight & Level	No Vulnerable Road User Involved Dr. No Vulnerable Road User Involved Dr. No Vulnerable Road User Involved Inc.	istracted Driving river Physical Condition	Daylight Wet	Not at Intersection/Driveway/Roundabout	Distracted Driving	Yes No Yes No
State Stat	1912 1912	6262 931262.35 1024581.4 2015 6263 931260.08 1024578.09 2018 6264 931260.08 1024578.09 2019	I2 Vehicles/Parties Rear-End	Property Damage Only Inattention Minor or Possible Injury Follow Too Closely Property Damage Only Distracted Driving	55+ No D	Dry Daylight Dry Daylight Dry Daylight	At Intersection and Not Related Straight & Level At Driveway or Intersection Straight & Level At Driveway or Intersection Straight & Level	No Vulnerable Road User Involved	nknown Distraction	Daylight Dry	At Driveway or Intersection At Driveway or Intersection	Distracted Driving	No No No No
\$12, \$13,000 \$10,000	\$1,000 \$	6268 931264.68 1024560.04 2015	2 Vehides/Parties Angle Struck Object 2 Vehides/Parties Rear-End 2 Vehides/Parties Rear-End	Property Damage Only Disregard Traffic Control Device/Sign Property Damage Only Operating Defective Equipment Property Damage Only Follow Too Closely	25-34 No D Juder 25 No V	Wet Daylight Dry Daylight Wet Daylight	At Driveway or Intersection Straight & Level	No Vulnerable Road User Involved Di No Vulnerable Road User Involved Op No Vulnerable Road User Involved Ur	nknown Distraction				No Yes No No
17.5 17.5	## CPT # 1911 (5) 10 10 10 10 10 10 10 10 10 10 10 10 10	6272 931260.08 1024578.09 2019 6273 931460.4 1024555.4 2016	2 Vehicles/Paties Pegie	Property Damage Only Follow Top Clorely	55+ No V J	Wet Dark-Street Lights On Dry Daylight	At Driveway or Intersection Straight & Level At Driveway or Intersection Straight & Level At Driveway or Intersection Straight & Level	Older Road User 65+ Fa No Vulnerable Road User Involved Fo	attention ail to Yield ollow Too Closely	Dark-Street Lights On Wet Daylight Dry	At Driveway or Intersection At Driveway or Intersection	Inattention Fail to Yield	Yes No No No
\$15.00 \$	C778	6274 931516.35 1024532.44 2016 6275 931552.52 1024513.82 2019 6276 931540.89 1024516.55 2017 6277 931557.7 1024511.25 2018	2 Vehides/Parties Rear-End 2 Vehides/Parties Rear-End 2 Vehides/Parties Rear-End 2 Vehides/Parties Rear-End	Property Damage Only Operating Defective Equipment Minor or Possible Injury Distracted Driving Property Damage Only Follow Too Closely Property Damage Only Other/Unknown	75-34 No D D D D D D D D D D D D D D D D D D	Dry Daylight Wet Daylight Dry Daylight Dry Daylight	Intersection Related but Not at Intersection Straight & Grade At Driveway or Intersection Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level At Driveway or Intersection Straight & Level	No Vulnerable Road User Involved Op No Vulnerable Road User Involved Ur No Vulnerable Road User Involved Fo No Vulnerable Road User Involved No	perating Defective Equipment nknown Distraction allow Too Closely one	Daylight Dry Daylight Wet Daylight Dry Daylight Dry Daylight Dry	Intersection Related but Not at Intersection At Driveway or Intersection Not at Intersection/Driveway/Roundabout At Driveway or Intersection	No Risky Driving Behaviors Involved Distracted Driving Follow Too Closely No Risky Driving Behaviors Involved	No No No No
9168.35 910848.24 91168.35 1024482.41 20115 12 Vehicles-Partes, Rea-End Popery Damage Chip Vet Vet Nembers Of Data of	628 531683.8 1024424 [2101 2] Vehicles-Planies Read Lever Involved (1974 1974 2] Vehicles-Planies Read Lever Involved (1974 2) Vehicles-Planies Read Lever Involve	6278 931552.52 1024513.82 2019 6279 931590.53 1024504.89 2017 6280 931601.39 1024502.93 2018	4 Vehicles/Parties Rear-End 2 Vehicles/Parties Rear-End 5 Vehicles/Parties 2 Vehicles/Parties 2 Vehicles/Parties	Property Damage Only Follow Too Closely Property Damage Only Other/Unknown Property Damage Only Follow Too Closely Property Damage Only Distracted Driving	15+ No	Wet Dark-Street Lights On Wet Dark-Street Lights On Dry Dark-Street Lights On Dry Daylight	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level At Driveway or Intersection Straight & Level	Older Road User 65+ Fo No Vulnerable Road User Involved Ot No Vulnerable Road User Involved Fo No Vulnerable Road User Involved Ur	ollow Too Closely ther/Unknown allow Too Closely oknown Distraction	Dark-Street Lights On Wet Dark-Street Lights On Wet Dark-Street Lights On Dry Dark-Street Lights On Dry Davisht Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Follow Too Closely No Risky Driving Behaviors Involved Follow Too Closely	No No No No
6289 93173.57 1024/72.24[2016 [2 Vehicles-Parties (Rean-End Property/Damage Chily Follow Too Closely Dry Not at Intersection Driveway/Roundabout Straight & Level No Vulnerable Road-End Property/Damage Chily Follow Too Closely Dry Not at Intersection Driveway/Roundabout Follow Too Closely No	6289 931731.57 1024472.24[2016 [2 Vehicles:Parties Rear-End Property Dawn Dusk Dry Dry Dawn Dusk Dry	6284 931683 26 1024482 41 2014	2 Vehicles/Parties Rear-End	rroperry Damage Cnly Inattention Property Damage Cnly Other Unknown Property Damage Cnly Distracted Driving Property Damage Cnly Follow Too Closely	15-44 No V	Wet Dark-Street Lights On Wet Daylight Wet Dawn/Dusk	At Driveway or Intersection Straight & Level Intersection Related but Not at Intersection Straight & Level Not at Intersection/Driveway/Roundabout Straight & Grade	Older Road User 65+ Ot No Vulnerable Road User Involved Ur No Vulnerable Road User Involved Fo	ollow Too Closely	Dawn/Dusk Wet Dark-Street Lights On Wet Daylight Wet Dawn/Dusk Wet	Intersection Related but Not at Intersection Intersection Related but Not at Intersection Not at Intersection/Driveway/Roundabout	Follow Too Closely	No No No No
1937 TOP SCHI	1.02 State	6289 931731.57 1024472.24 2016	2 Vehicles/Parties Rear-End	Property Damage Only Follow Too Closely	Index 25 No	Dry Daylight Dry Daylight Wet Daylight Dry Dawn/Dusk	Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved Fo	ollow Too Closely attention ollow Too Closely	Daylight Dry Daylight Wet Daylight Wet Dawn/Dusk Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Follow Too Closely Inattention Follow Too Closely	No No No No
6909 03100E 0E 100M90 93 POE 1 Mahinta Park Street Indian Park Indi	2000 011006 05 100M010 201006 05 100M010 05 100M0	6290 931788.9 1024458.31 2018 6291 931850.15 1024443.8 2017 6292 931839.07 1024444.6.66 2019 6293 931906.66 1024430.32 2016	2 Vehicles/Parties Rear-End 2 Vehicles/Parties 2 Vehicles/Parties 2 Vehicles/Parties Pedestrian	Property Damage Only Follow Too Closely Property Damage Only Fail to Yield Minor or Possible Injury Inattention Property Damage Only Speeding	25:34 No V 15:54 No D 15:64 No D	Wet Dawn/Dusk Dry Daylight Dry Daylight Dry Daylight Dry Dark-Street Lights On	Intersection Related but Not at Intersection Straight & Level At Driveway or Intersection Straight & Level At Driveway or Intersection Straight & Grade Not at Intersection Dissessed Report Straight & Grade Not at Intersection Dissessed Report Straight & Grade	No Vulnerable Road User Involved Fo No Vulnerable Road User Involved Fa Pedestrian In No Vulnerable Road User Involved Sp	ollow Too Closely sail to Yield sattention peeding	Dawn/Dusk Wet Daylight Dry Daylight Dry Dark-Street Lights On Dry	Intersection Related but Not at Intersection At Driveway or Intersection At Driveway or Intersection Not at Intersection/DrivewayRoundabout	Follow Too Closely Fail to Yield Inattention	No No No Yes
93188.85 102443.45.27 12 Vehicles-Paries Rear-End Poporty Camage Cuty Speeding 45.64 No Wet Daylight Intersection Related but Not at Intersection Related but Not at Intersection Placed User Involved Speeding Daylight Wet Intersection Placed but Not at Intersection Daylight Wet Intersection Placed User Involved Speeding No	6294 53188865 102444.55[2017] Ehrbitan-Parker Rear-End Properly Damage Only Speeding 45-64 No Wet Daylight Intersection Related but Not at Intersection Statistical Lear Invoked Speeding Daylight Except No Vision American Statistics of the Computation of Daylight Statistics of the Computation of Daylight Computation Statistics of	6.294 931888.65 1024434.55 2017 6.295 931960.84 1024410.13 2015 6.296 931949.03 1024420.04 2015 6.297 931991.02 1024409.95 2014	z venices/Parties Rear-End 2 Vehicles/Parties Rear-End 2 Vehicles/Parties Rear-End 2 Vehicles/Parties Sideswipe 3 Ve	Property Damage Only Speeding Minor or Possible Injury Inattention Minor or Possible Injury Follow Too Closely	15-54 No W Juder 25 No D 15-54 No W	Wet Daylight Dry Daylight Wet Daylight	merescon Related but Not at Intersection Straight & Level Not at Intersection/DrivewayRoundabout Straight & Level	No Vulnerable Road User Involved Fo	pecang satention sate	Daylight Wet Daylight Dry Daylight Wet	Intersection Related but Not at Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout		No No No Yes

	OBJECTID JURISDICTI	TION COUNTY CITY PRIMARY TRAFFICWAY	BLOCK INTERSECTING DIST FROM FT FROM FT FROM RE-POINT FEED FROM RE-POINT FT FT FROM RE-POINT FT FROM RE-POINT FT FROM RE-POINT FT FROM RE-POINT FT FROM RE-POIN	REFERENCE MILEPO	OST A SR_ONLY HISTORY SUSPENSE_IND	REPORT DATE TIME	INJURY_TYPE	NUM NUM NUM NUM NUM NUM BIKES	VEH_1_TYPE	VEH_2_TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD_SURFACE CONDITION	ROADWAY LIGHTING CONDITION	RRST_COLLISION TYPE_OBJECT_STRUCK	VEH_1 ACTION	VBL_2 ACTION		VEH_2_COMPASS DIRECTION_FROM DIRECTION_TO
The content of the	6299 State Route 6300 State Route 6301 State Route			249.77 249.77 249.77	No No No	E400760 2015-02-06 07:50 E416507 2015-04-14 13:40 E682248 2017-06-10 11:20	Possible Injury Possible Injury Possible Injury	2 0 3 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car	Not at Intersection and Not Related At Driveway At Driveway	Raining Clear or Partly Cloudy Clear or Partly Cloudy	Dry	Daylight	Entering at angle	Going Straight Ahead Making Left Turn Making Left Turn	Going Straight Ahead	East West South West East South	Vehicle Stopped Vehicle Stopped West East West East
	6303 State Route	Cialiam Port Angeles 101 Cialiam Port Angeles 101		249.77 249.77	No No					Passenger Car	At Driveway		Dry Str Dry Str	raight & Level Daylight raight & Grade Daylight	From same direction - both going straight - one stopped - rear-end Vehicle Strikes Pedaloyclist	Going Straight Ahead Making Right Turn	Stopped for Traffic	East West South Northeast	East West
	6306 State Route	Clallam Port Angeles 101		249.78 249.78	No No	E865961 2018-11-27 17:10	No Apparent Injury	0 0 2 0 0	Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Driveway Related but Not at Driveway	Raining	Wet St	raight & Level Daylight raight & Level Dark-Street Lights C	From same direction - both going straight - one stopped - rear-end From opposite direction - one stopped - head-on	Going Straight Ahead	Stopped in Roadway	East West West East	East Vehicle Stopped
	6307 State Route 6308 State Route 6309 State Route	Clallam Port Angeles 101 Clallam Port Angeles 101 Clallam Port Angeles 101		249.78 249.79 249.80	No No No	E962274 2019-09-16 08:49 E548705 2016-05-27 08:10 E804792 2018-06-04 13:50	9 Possible Eljuly	1 0 2 0 0	Passenger Car	Passenger Car	Not at Intersection and Not Related Intersection Related but Not at Intersection Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Str Dry Str	raight & Level Daylight raight & Level Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic Stopped for Traffic	West East East West West	Vehicle Stopped Vehicle Stopped Vehicle Stopped Vehicle Stopped
		Clallam Port Angeles 101								Passenger Car	Intersection Related but Not at Intersection	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Str Dry Str	raight & Level Daylight raight & Level Dark-Street Lights C	From same direction - both going straight - both moving - sideswipe Vehicle going straight hits pedestrian	Changing Lanes Going Straight Ahead		West East West East	West East
	6312 State Route 6313 State Route 6314 State Route	Cialiam Port Angeles 101 Cialiam Port Angeles 101 Cialiam Port Angeles 101		249.81	NO.	E828566 2018-08-15 10:4: 2867121 2018-08-04 13:4-	3 Possible Injury 4 No Apparent Injury	2 0 2 0 0	Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Driveway Related but Not at Driveway Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Str Dry Str	raight & Level Daylight raight & Level Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - sideswipe	Going Straight Ahead Changing Lanes	Stopped for Traffic Stopped for Traffic Going Straight Ahead	West East East West East West	Vehicle Stopped Vehicle Stopped Vehicle Stopped Vehicle Stopped East West
	6315 State Route 6316 State Route 6317 State Route	Clallam Port Angeles 101		249.82 249.83 249.83	No No No	3815579 2018-07-16 14:2 2866905 2016-11-01 12:4 3201535 2015-10-19 11:2	1 No Apparent Injury	0 0 2 0 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Truck (Flatbad,Van,etc)	Not at Intersection and Not Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Overcast	Dry Str	raight & Level Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead	Stopped at Signal or Stop Sign	East West East West Fast West	East West East West
Column	6319 State Route	Clallam Port Angeles 101		249.83 249.83	No No		8 Possible Injury 2 No Apparent Injury	1 0 2 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related		Dry Str Dry Str	raight at Hillor Dark-Street Lights C raight & Level Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped at Signal or Stop Sign Stopped at Signal or Stop Sign	East West West East	East West Vehicle Stopped
Column	6320 State Route 6321 State Route 6322 State Route	Cialiam Port Angeles 101 Cialiam Port Angeles 101 Cialiam Port Angeles 101		249.83 249.83 249.83	No No No	3741778 2017-01-15 17:5: 3741778 2017-01-15 17:5: 3797704 2019-07-29 21:4	Possible Injury Possible Injury Possible Injury	1 0 1 0 1	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related At Intersection and Related	Overcast Clear or Partly Cloudy	Dry St	raight & Grade Dark-Street Lights C raight & Grade Dark-Street Lights C raight & Grade Dark-Street Lights C	Vehicle - Pedalcyclist From opposite direction - one left turn - one straight From opposite direction - one left turn - one straight From opposite direction - one left turn - one straight	Making Left Turn Making Left Turn	Making Right Turn Going Straight Ahead Going Straight Ahead	East South East South	West South West East West East
		Cialiam Port Angeles 101 Cialiam Port Angeles 101 Cialiam Port Angeles 101		249.83 249.83 249.83	No No No		O INO Apparent injuly	0 0 2 0 0	rassenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related	Clear or Partly Cloudy Overcast Clear or Partly Cloudy		raight at Hillor Daylight raight at Hillor Dark-Street Lights C	From same direction - both going straight - one stopped - rear-end From opposite direction - one left turn - one straight	Going Straight Ahead Going Straight Ahead	Stopped at Signal or Stop Sign Making Left Turn	East West West East Fast West	East Vehicle Stopped East South South West
Column	6326 State Route 6327 State Route	Cialiam Port Angeles 101		249.86	No	3797558 2019-08-09 08:5	1 No Apparent Injury	0 0 2 0 0	Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection		Dry Str Dry Str	raight & Level Daylight raight & Grade Daylight	From Same directors both going straight both moving strainend	Going Straight Ahead	Going Straight Ahead	East Southwest East West	West East East West
The content of the	6329 State Route 6330 State Route	Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG		249.88 249.66 249.66	No No	E529509 2016-03-23 12:5- E895283 2019-02-04 08:33				Pagganger Car	At Intersection and Related	Clear or Partly Cloudy	Dry Snow/Slush	Daylight	Signal Pole	Going Straight Ahead		East West East West West	
Part	6331 State Route 6332 State Route	Clallam Port Angeles 101COPRTANG		249.68	No	3815551 2018-01-17 19:18	7 Suspected Serious Injury 8 No Apparent Injury 6 Unknown	0 0 2 0 0	Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Intersection and Related		Wet St	raight & Grade Dark-Street Lights C	Entering at angle	Making Lett Turn	Going Straight Ahead	East West West North	East West
Part		Cialiam Port Angeles 101COPRTANG Cialiam Port Angeles 101COPRTANG		249.69 249.69	No No	3815429 2018-06-26 13:00 E364058 2014-10-12 20:2-	8 No Apparent Injury 4 No Apparent Injury	0 0 2 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related		Dry Str Dry Cu	raight & Level Daylight urve & Level Dark-Street Lights C	From same direction - both going straight - one stopped - rear-end Building	Going Straight Ahead Merging (Entering Traffic)		East West West North	Vehicle Stopped Vehicle Stopped
Part	6337 State Route 6338 State Route	Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG		249.72 249.75 249.76	No No	E971209 2019-10-15 15:4: E926863 2019-06-01 12:50	7 No Apparent Injury 0 No Apparent Injury	0 0 2 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related		Dry Cu Dry Str	urve & Grade Daylight raight & Level Daylight	From same direction - both going straight - both moving - sideswipe From same direction - both going straight - both moving - rear-end	Overtaking and Passing	Going Straight Ahead Slowing	East West East West	East West East West
	6339 State Route 6340 State Route 6341 State Route	Clallam Port Angeles 101COPRTANG		249.77 249.77 249.77	No No No	286/135 2014-06-14 12:58	1 No Apparent Injury 6 No Apparent Injury 4 Unknown	0 0 2 0 0 0 0 2 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Str	raight & Level Daylight	From same direction - both going straight - both moving - sideswipe	Changing Lanes	Going Straight Ahead	East Southwest South North	North South East West East West
	6342 State Route 6343 State Route 6344 State Route	Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG		249.77 249.77 249.77	No No	3743125 2019-07-26 15:50 3815497 2018-10-30 14:30 E218978 2014-04-04 11:5	8 Possible Injury 0 Possible Injury 1 No Apparent Injury	1 0 1 0 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup Panel Truck or Vanette under 10 000 lb	At Intersection and Related At Driveway within Major Intersection	Clear or Partly Cloudy Overcast	Dry Str Dry Cu West Str	raight & Grade Daylight urve & Grade Daylight	Entering at angle	Making Left Turn Going Straight Ahead Going Straight Ahead	Going Straight Ahead	South West South North Northeast Southwest	East West Southwest Northwest
	6345 State Route 6346 State Route	Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG		249.77 249.77 249.77	No No	E360154 2014-09-27 15:30 E454844 2015-08-20 07:20	8 No Apparent Injury 6 Possible Injury	0 0 2 0 0	Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car Passenger Car	At Intersection and Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast	Dry Cu	urve & Grade Daylight urve & Grade Daylight	From same direction - one left turn - one straight From same direction - all others	Making Left Turn	Going Straight Ahead Going Straight Ahead	South West East West	West East East West
	6348 State Route			249.77 249.77 249.77		E532297 2016-04-06 21:16 E580158 2016-08-31 18:19	6 Possible Injury 9 Possible Injury	1 0 2 0 0	Ріскир, ranei i ruck or vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Motorcycle	At Intersection and Related At Intersection and Related At Driveway within Major Intersection	Clear or Partly Cloudy Clear or Partly Cloudy	Other St Dry Cu	raight & Grade Daylight urve & Grade Dark-Street Lights C	Entering at angle	Going Straight Ahead Making Left Turn	Going Straight Ahead Going Straight Ahead	South North South West	East West North South
The content of the	6351 State Route 6352 State Route	Ciallam Port Angeles 101 COPRTANG Ciallam Port Angeles 101 COPRTANG Ciallam Port Angeles 101 COPRTANG		249.77 249.77 240.77	No No No	E790198 2018-04-19 16:5- E812486 2018-06-23 09:1- E815128 2018-07-06 00-2	4 No Apparent Injury 4 No Apparent Injury 3 No Apparent Injury	0 0 2 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway within Major Intersection At Intersection and Related At Intersection and Related	Clear or Partly Cloudy	Dry Cu Dry Str	urve & Level Daylight raight & Grade Daylight Daylight	Entering at angle From same direction - one left turn - one straight	Making Left Turn	Going Straight Ahead Going Straight Ahead Going Straight Ahead	East South East South East South	East West East West East West
	6353 State Route	Ciallam Port Angeles 101 COPRTANG Ciallam Port Angeles 101 COPRTANG Ciallam Port Angeles 101 COPRTANG		249.77 249.78	No No	E949799 2019-08-06 14:0	6 No Apparent Injury 7 No Apparent Injury 4 No Apparent Injury	0 0 2 0 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related Not at Intersection and Not Related Not at Intersection and Not Related		Dry Wet Cu	Daylight unve & Grade Dark-Street Lights C	Entering at angle	Changing Lanes Changing Lanes	Making Left Turn	East West East West Fast Mothwest	South West East West
Column C	6356 State Route	Clallam Port Angeles 101 COPRTANG Clallam Port Angeles 101 COPRTANG		249.79 249.79	No No	3472680 2016-08-03 15:40 3743118 2019-05-05 08:50	6 Unknown 0 No Apparent Injury	0 0 2 0 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway At Driveway	Clear or Partly Cloudy	Dry Str Dry Str	raight & Grade Daylight raight & Level Daylight	Same direction both turning left both moving sideswipe From same direction one left turn one straight	Making Left Turn	Making Left Turn Going Straight Ahead	Vehicle Stopped West East South	Vehicle Stopped West East West
	6358 State Route 6359 State Route 6360 State Route	Cialiam Port Angeles 101COPRTANG Cialiam Port Angeles 101COPRTANG Cialiam Port Angeles 101COPRTANG		249.79 249.80 249.81	No No No	E303227 2014-01-29 05:0 E976807 2019-10-29 09:3	7 Possible Injury 5 No Apparent Injury	1 0 2 0 0 0 0 2 0 0	Раssenger Car Passenger Car	Passenger Car Truck Tractor & Semi-Trailer	At Driveway At Driveway	Raining	Wet Str Dry Str	raight & Level Daylight	r rium same direction - born going straight - one stopped - rear-end 1 From same direction - one left turn - one straight One parked-one moving	Making Right Turn	Going Straight Ahead Going Straight Ahead Legally Parked, Unoccupied	East Vehicle Stopped East South North Southwest	East West
	6361 State Route 6362 State Route 6363 State Route			249.83 249.91 249.91	No No	E389334 2015-01-04 18:50 2867091 2014-11-21 15:00 E329520 2014-05-07 16:20	5 Possible Injury 8 Possible Injury 0 Possible Injury	1 0 1 0 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related		Dry Str	raight & Level Daylight	Metal Sign Post From same direction - both going straight - both moving - rear-end	Going Straight Ahead Slowing		East West East West Fast West	East West Vehicle Stopped Vehicle Stopped
	6364 State Route 6365 State Route	Clallam Port Angeles 101COPRTANG		249.91 249.91	No No	E670413 2017-05-11 17:3	3 No Apparent Injury 7 No Apparent Injury	0 0 2 0 0	Passenger Car Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway At Driveway	Raining	Wet Str	raight & Level Daylight	Entering at angle Entering at angle	Making Left Turn	Going Straight Ahead	Southeast Northwest South North	East West East West
	6367 State Route 6368 State Route	Cialiam Port Angeles 1101 COPK TANG		249.92 249.93 249.93	No No	2867162 2016-07-30 15:00 E346764 2014-08-04 15:00	9 No Apparent Injury 7 No Apparent Injury	0 0 2 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related At Driveway	Clear or Party Cloudy Clear or Party Cloudy Clear or Party Cloudy	Dry Str	raight & Level Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Wrong Way on One-Way Street or Road	Stopped for Traffic Stopped for Traffic Going Straight Ahead	South East	East Venice Supped
	6369 State Route 6370 State Route 6371 State Route	Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG		249.94 249.94 249.94	No No No	3473175 2016-06-04 13:5-	0 No Apparent Injury 8 No Apparent Injury	0 0 2 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway At Driveway	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy			From same direction - one left turn - one straight	Making Left Turn	Going Straight Ahead	East West East South East South	South West East West East West
		Cialiam Port Angeles 101COPRTANG		249.94 249.94	No No	E378984 2014-11-26 08:59	6 No Apparent Injury 9 No Apparent Injury	0 0 2 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway Not at Intersection and Not Related		Wet St	raight & Level Daylight	From same direction - one left turn - one straight From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Going Straight Ahead Stopped for Traffic	East Southwest East West	East West Vehicle Stopped Vehicle Stopped
	6375 State Route 6376 State Route	Claliam Port Angeles 101 COPRTANG Claliam Port Angeles 101 COPRTANG Claliam Port Angeles 101 COPRTANG		249.94 249.95	No No	E848724 2018-10-10 13:03 E380619 2014-12-04 16:5-	No Apparent Injury Possible Injury	0 0 2 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Wet St	raight & Level Daylight raight & Level Dark-Street Lights C	Entering at angle From same direction - both going straight - one stopped - rear-end	Making Left Turn Going Straight Ahead	Going Straight Ahead Stopped for Traffic	South West West East	East West Vehicle Stopped Vehicle Stopped
	6377 State Route 6378 State Route 6379 State Route						5 No Apparent Injury 8 Possible Injury 5 No Apparent Injury	0 0 1 0 0 1 0 2 0 0 0 0 1 0 0	Passenger Car Passenger Car Passenger Car	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Raining Clear or Partly Cloudy	Wet Str	raight & Level Daylight	Select Cignit Pole of Base From same direction - both going straight - both moving - rear-end Tree or Stump (stationary)	Going Straight Ahead	Slowing	West East East West East West	East West
	6380 State Route 6381 State Route 6382 State Route	Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG		249.96 249.96	No No	E446058 2015-07-23 21:49 E462863 2015-09-05 10:50 E605091 2016-11-04 16:00	8 No Apparent Injury	0 0 2 0 0	Pickup.Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Related	Clear or Partly Cloudy			From same direction - all others			East West North Vehicle Backing East West	East Southwest North Vehicle Stopped Fast West
	6383 State Route 6384 State Route	Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG		249.96 249.97	No No	E726836 2017-10-24 15:59 E349532 2014-08-14 11:00	5 No Apparent Injury 0 No Apparent Injury	0 0 2 0 0	Passenger Car Not Stated	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Driveway Related but Not at Driveway	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Str Dry Str	raight & Level Daylight raight & Level Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	East West East West	East West East West
		Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG		249.97 249.98	No No	3741393 2019-03-17 09:3-	9 No Apparent Injury 4 Possible Injury	0 0 2 0 0	Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Not Related At Driveway	Snowing	Snow/Slush Str	raight & Level Dark-Street Lights C	From same direction - both going straight - both moving - sideswipe	Changing Lanes	Going Straight Ahead	East West West South	East West East West
	6388 State Route 6389 State Route 6390 State Route	Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG		250.00	No	E409801 2015-03-19 17:4	5 Suspected Serious Injury 8 Possible Injury 8 No Apparent Injury	1 0 1 0 0 2 0 3 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	Passenger Car Passenger Car	At Driveway Intersection Related but Not at Intersection Not at Intersection and Not Related	Overcast			From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic Going Straight Ahead	Vehicle Backing East East West East West	East Vehicle Stopped East West
Column C		Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG		250.01	No	2867147 2015-12-16 09:00 3741347 2017-11-08 16:30 E407558 2015-02-27 14:5	8 Possible Injury 5 No Apparent Injury 7 No Apparent Injury	0 0 2 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car				Wet Str Dry Str	raight & Level Daylight raight & Level Dusk	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic	East West East West West	East West
The column The	6394 State Route	Cialiam Port Angeles 101COPRTANG		250.03 250.03	No No		3 No Apparent Injury 8 Possible Injury	0 0 2 0 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Driveway Intersection Related but Not at Intersection	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Str Dry Str	raight & Level Daylight raight & Level Dark-Street Lights C	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead	Stopped in Roadway	North South East West	Vehicle Stopped Vehicle Stopped East West
The content of the		Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG		250.04 250.04 250.05	No No						Intersection Related but Not at Intersection At Intersection and Related		Dry St	raight& Level Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead	Stopped for Traffic	East West East West	East Vehicle Stopped Vehicle Stopped
	6399 State Route 6400 State Route 6401 State Route	Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG		250.05 250.05 250.05	No No No	2867422 2015-11-26 21:30 2986320 2016-11-02 18:54 3201293 2016-01-12 17:00	No Apparent Injury Possible Injury No Apparent Injury	0 0 1 0 0 1 0 1 1 0 0 0 2 0 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Unknown Raining	loe Str Unknown Str Wet Cu	raight & Level Dark-Street Lights C raight & Level Dark-Street Lights C urve & Grade Dark-Street Lights C	Entering at angle	Making Left Turn Making Left Turn Merging (Entering Traffic)	Going Straight Ahead	East South East North South North	East West
	6402 State Route 6403 State Route	Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG		250.05 250.05	No No	E336688 2014-06-20 20:29	9 Possible Injury	1 0 1 1 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Overcast Clear or Partly Cloudy	Dry Wet Str	Daylight raight & Level Daylight	Vehicle turning left hits pedestrian Entering at angle	Making Left Turn Going Straight Ahead		East South South North	East West
Column C		Clallam Port Angeles 101COPK IANG		250.05		E439345 2015-07-01 13:2 E456351 2015-08-28 21:00	1 No Apparent Injury 8 No Apparent Injury	0 0 2 0 0 0 0 2 0 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Truck (Flatbad, Van, etc) Passenger Car	At Intersection and Related	Clear or Partly Cloudy Raining	Wet	Daylight Dark-Street Lights C	From same direction - one left turn - one straight	Going Straight Ahead	Stopped at Signal or Stop Sign	East North East West	North Vehicle Stopped East Vehicle Stopped
		Claliam Port Angeles 101COPRTANG		250.05 250.05 250.05	No No	E722720 2017-10-01 01:50 E860573 2018-11-13 09:30	6 Unknown 5 Possible Injury	0 0 1 0 0	Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy	Dry Str	raight & Level Dark-Street Lights C	Signal Pole	Making Left Turn		East South North South	East West
	6410 State Route 6411 State Route 6412 State Route	Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG Cialiam Port Angeles 101 COPRTANG		250.05 250.05 250.07	No No No	E986885 2019-11-26 18:0 3472988 2015-01-16 07:4	No Apparent Injury Possible Injury No Apparent Injury	0 0 1 0 0 1 0 2 0 0 0 0 2 0 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car Passenger Car	At Intersection and Not Related At Intersection and Related Not at Intersection and Not Related	Raining Clear Clear or Partly Cloudy	Wet Str Dry Str Dry Str					North South East West West	East West East West
	6413 State Route 6414 State Route	Clallam Sequim 101 Clallam Sequim 101 Clallam Sequim 101		262.82 263.00 263.02	No No	E499095 2015-12-22 18:12 2727874 2015-08-14 16:00 E815395 2018-07-06 17:40	Suspected Minor Injury No Apparent Injury Possible Injury	0 0 1 1 0	Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb	Dickup Danal Truck or Vanatta under 10 000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Overcast				Going Straight Ahead Going Straight Ahead		East West West East East West	East West
	6416 State Route 6417 State Route	Clallam Sequim 101 Clallam Sequim 101		263.04	No No	E643362 2017-02-12 17:5 E829150 2018-08-09 16:2	3 No Apparent Injury 9 Possible Injury	0 0 1 0 0	Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Str Dry Str	raight & Level Dark-No Street Light raight & Level Daylight	Vehicle Strikes Deer From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead	Stopped for Traffic	East West West East	Vehicle Stopped Vehicle Stopped
Column C	6419 State Route 6420 State Route	Clallam Seguim 101		263.78	No No	E648126 2017-03-03 18:20 E802454 2018-05-18 18:40	6 No Apparent Injury 5 No Apparent Injury	0 0 2 0 0	Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining	Wet St	raight & Level Dark-No Street Ligh	One car leaving parked position	Starting From Parked Position Going Straight Ahead	Going Straight Ahead	Vehicle Stopped East West East	West East
Column C	6423 State Route			264.25	No No No	E927874 2019-05-29 16:4 E820502 2018-07-22 17:19 2766947 2014-04-12 20:00	Dead at Scene No Apparent Injury Possible Injury	1 1 2 0 0 0 0 2 0 0 1 0 2 0 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry St	raigitta Levei Dayiigitt	Profit same direction - both going straight - both moving - real-end	Going Straight Mileau		West West West East West East	West East West East West East
Column C	6424 State Route 6425 State Route 6426 State Route	Clallam Sequim 101		264.38	No No No	E974783 2019-10-24 16:46 E364886 2014-10-11 15:54 E698880 2017-07-23 16:56	6 Possible Injury 4 Suspected Minor Injury 0 Possible Injury	1 0 2 0 0 3 0 4 0 0 1 0 2 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Raining Clear or Partly Cloudy	Dry Str	raight & Level Daylight raight & Level Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Going Straight Ahead	West East West East West East	West East West East Vehicle Stopped Vehicle Stopped
Column C		Clallam Sequim 101			No No	E817256 2018-07-11 06:4 2766891 2014-08-01 22:0 2766970 2014-09 04	1 No Apparent Injury 1 Suspected Minor Injury 0 No Apparent Injury	3 0 2 0 0	Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	raight & Grade Daylight Dark-No Street Ligh	Vehicle Strikes Deer From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead		East West West East West East	West East
Column C	6430 State Route 6431 State Route	Clallam Sequim 101 Clallam Sequim 101 Clallam Sequim 101		265.80 265.80 266.00	No No	E571779 2016-08-07 23:21 2764738 2014-10-21 07:21	5 No Apparent Injury 9 No Apparent Injury	0 0 1 0 0	Passenger Car Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast	Dry Cu Wet Str	urve & Grade Dark-Street Lights C raight & Level Daylight	Price State on the restricted Roadway Ditch Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead		West East East West	
	6432 State Route 6433 State Route 6434 State Route	Clallam Sequim 101 Clallam Sequim 101 Clallam Sequim 101			No No No	E851630 2018-10-21 19:21 E747602 2017-12-08 20:23 E601017 2016-10-24 08:01	5 No Apparent Injury 3 No Apparent Injury 5 No Apparent Injury	0 0 1 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Str Dry Str Dry Str		Roadway Ditch			East West East West West East	
Second Column C	6435 State Route 6436 State Route 6437 State Route	Clallam Sequim 101		266.23	No No No	E698876 2017-07-22 19:36 E672987 2017-05-14 19:0 E577083 2016-08-06 16-6	6 Possible Injury 1 No Apparent Injury 5 No Apparent Injury	2 0 3 0 0 0 0 1 0 0 0 0 2 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Other Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10 000 %	Not at Intersection and Not Related Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Raining Clear or Partly Cloudy	Dry Cu Wet Str	urve & Level Daylight raight & Level Daylight	From same direction - both going straight - both moving - rear-end Fire started in vehicle	Going Straight Ahead Going Straight Ahead		West East West East East West	West East East South
Second Column C	6438 State Route 6439 State Route	Ciallam Sequim 101 Ciallam Sequim 101		267.04 267.05	No No	E349302 2014-08-09 06:31 E365883 2014-10-15 16:41	9 No Apparent Injury 0 No Apparent Injury	0 0 1 0 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	, , and the second of the seco	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Raining	Dry Str Wet Str	raight & Grade Daylight raight & Grade Daylight	Vehicle Strikes Deer Vehicle Strikes Deer	Going Straight Ahead		East West	-Arabit
Second Column C	6441 State Route 6442 State Route	Cialiam Sequim 101 Ciallam Sequim 101 Ciallam Sequim 101		267.05 267.06 267.10	No No	E000400 2016-06-18 12:19 E674453 2017-05-23 12:30 E941978 2019-07-15 19:00	No Apparent Injury Possible Injury No Apparent Injury	3 0 3 0 0 0 0 1 0 0	Ріскир, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry St		vernium saffixes Lieer From same direction - both going straight - one stopped - rear-end Vehicle Strikes All Other Non-Domestic Animal	Going Straight Ahead	Stopped for Traffic	East West West East	East West
Second Column C	6443 State Route 6444 State Route 6445 State Route	Clallam Sequim 101			No No No	E312433 2014-02-24 14:1: E331607 2014-05-31 15:0: E440932 2015-07-02 14-24	Suspected Minor Injury No Apparent Injury No Apparent Injury	2 0 1 0 0 0 0 2 0 0 0 0 2 0 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Motorcycle Pickup,Panel Truck or Vanette ⊨nder 10 000 №	Not at Intersection and Not Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy				Making Lett Turn	Making Left Turn Making Right Turn	West East North East North Vehirle Stonned	West North East North
Second Column C	6446 State Route 6447 State Route 6448 State Pourte	Ciallam Sequim 101 Ciallam Sequim 101 Ciallam Sequim 101		267.21 267.21	No No	E467208 2015-09-27 15:5 E561097 2016-07-02 13:14 E708490 2017-09-40	Possible Injury No Apparent Injury No Apparent Injury	3 0 2 0 0	Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Related At Intersection and Not Related At Intersection and Not Related	Clear or Partly Cloudy Overcast Clear or Partly Cloudy	Dry St	raight in Sag Daylight	From opposite direction - one left turn - one straight Vehicle Strikes Deer Vehicle verturged	Going Straight Ahead	Going Straight Ahead	West North West East West East	East West
Sept. Contract C	6449 State Route 6450 State Route	Clallam Sequim 101 Clallam Sequim 101 Clallam Sequim 101		267.21 267.27 267.30	No No	E982830 2019-11-10 01:48 E594259 2016-10-05 15:08	8 No Apparent Injury 0 Suspected Minor Injury	0 0 1 0 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Dielou Denel Tour	Not at Intersection and Not Related Not at Intersection and Not Related	Fog or Smog or Smoke Clear or Partly Cloudy	Wet Str Dry Str	raight & Level Dark-No Street Light raight & Grade Daylight	Wood Sign Post	Other* Going Straight Ahead	Caina Camilla III	East West West East	
Sept. Contract C	6452 State Route 6453 State Route			267.35	No No	E419266 2015-04-21 23:19 E364881 2014-10-10 07:3	5 Suspected Minor Injury 7 No Apparent Injury	1 0 1 0 0	Раssenger Car Passenger Car Passenger Car	гижир, Panel Truck or vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast	Dry Cu Dry Cu	urve at Hillcrest Dark-No Street Ligh urve & Level Daylight		Going Straight Ahead Going Straight Ahead	ouing straight whead	East West West East	Edition West
Sept. Contract C	6454 State Route 6455 State Route 6456 State Route	Clallam Sequim 101		267.40 267.40 267.43	No No No	E378003 2014-11-25 22:50 E602380 2016-10-27 10:00 E333409 2014-06-05 11-40	0 No Apparent Injury 5 No Apparent Injury 6 No Apparent Injury	0 0 1 0 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Overcast Clear or Partly Cloudy	Diy Ct	urve & Grade Dark-No Street Ligh urve & Grade Daylight raight & Level Daylight	: Miscellaneous Object or Debris on Road Vehicle Strikes Deer Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East West East West East	#
Sept. Contract C	6457 State Route 6458 State Route			267.43 267.43	No No	E535247 2016-04-18 04:49 E793056 2018-04-15 22:29	5 No Apparent Injury 5 Possible Injury 5 No Apparent Injury	0 0 1 0 C	Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car		At Intersection and Not Related At Intersection and Not Related	Clear or Partly Cloudy Raining	Dry Cu	urve & Grade Dark-No Street Ligh	Vehicle Strikes Deer	Going Straight Ahead		East West West East	
Sept. Contract C		Clallam Sequim 101 Clallam Sequim 101 Clallam Sequim 101		207.44	No No	E910699 2019-04-11 18:50 E536265 2016-04-15 16:20	8 No Apparent Injury 4 No Apparent Injury	0 0 2 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Overcast	Dry Ct	urve & Grade Daylight raight & Grade Daylight	Entering at angle Entering at angle	Going Straight Ahead Making Left Turn Making Left Turn	Slowing	South West South West	East West West East
Sept. Contract C	6462 State Route 6463 State Route 6464 State Route	Clallam Sequim 101		267.44	No No No	E611568 2016-11-18 12:1: E706483 2017-08-22 19:3 E548269 2016-05-30 09-0	No Apparent Injury Suspected Minor Injury No Apparent Injury	0 0 2 0 0 1 0 2 0 0 0 0 2 0 0	Truck (Flatbad, Van,etc) Passenger Car Pickup, Panel Truck or Vanette under 10.000 lh	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10.000 lb	At Intersection and Related At Intersection and Related Intersection Related but Not at Intersection	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Dry Cu	urve & Grade Daylight		Making Left Turn	Slowing Going Straight Ahead Stopped in Roadway	East West East South East West	East West West East Vehicle Stopped Vehicle Stopped
Sept. Contract C	6465 State Route 6466 State Route	Ciallam Sequim 101 Ciallam Sequim 101 Ciallam Sequim 101		267.47	No No	E378004 2014-11-25 22:50 E683523 2017-06-15 08:33	0 No Apparent Injury 7 No Apparent Injury 0 No Apparent Injury	0 0 1 0 0	Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb	Dickup Panal Truck or Vigo-No.	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Overcast	Wet Cu Dry Str	urve & Grade Dark-No Street Ligh raight & Level Daylight	Vehicle Strikes Deer Vehicle Strikes Deer	Going Straight Ahead	Going Straight About	East West West East	Wast Foot
Sept. Contract C	6468 State Route 6469 State Route	Cialiam Sequim 101 Cialiam Sequim 101 Cialiam Sequim 101		267.64 267.68 267.73	No No	E949453 2019-08-09 04:14 E568481 2016-07-26 12:19	4 No Apparent Injury 5 No Apparent Injury	0 0 1 0 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Dry Str	urve & Level Dark-No Street Ligh	Vehicle overturned	Going Straight Ahead Going Straight Ahead	Going Straight Ahead	West East East West	West East
AFT State Roade	6470 State Route 6471 State Route 6472 State Route	Ciallam Sequim 101		267.73	No No No	2767138 2016-09-16 23:4 2767152 2016-03-28 17:1	8 No Apparent Injury 0 No Apparent Injury 9 No Apparent Injury	0 0 2 0 0 0 0 1 0 0 0 0 2 0 0	Passenger Car Passenger Car Passenger Car	Passenger Car Not Stated	Not at Intersection and Not Related At Intersection and Related At Intersection and Not Related	Raining Clear or Partly Cloudy	Dry Str Wet Str	raight & Grade Dark-Street Lights C raight & Level Daylight	Signal Pole From same direction - all others	Making Right Turn Going Straight Ahead	Stopped for Traffic Overtaking and Passing	South West	South North
## Riving Exclusion 1011/1023/05 No 1011	6473 State Route 6474 State Route 6475 State Route	Clallam Sequim 101LX26305 Clallam Sequim 101LX26305 Clallam Sequim 101LX26305		0.02 0.02 0.02	No No No	3615939 2016-02-12 21:16 3759369 2017-08-10 15:59 E406910 2015-03-08 12:00	6 No Apparent Injury 9 No Apparent Injury 2 No Apparent Injury	0 0 2 0 0 0 0 2 0 0 0 0 2 0 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Str	raight & Grade Dark-Street Lights C	Entering at angle From opposite direction - one left turn - one straight Entering at angle	Making Left Turn	Making Left Turn	South North South West South North	North East North South East South
	6476 State Route 6477 State Route	Ciallam Sequim 101LX26305 Ciallam Sequim 101LX26305 Ciallam Sequim 101LX26305		0.02	No No	E734099 2017-11-10 06:30 E956395 2019-09-03 13:00	9 No Apparent Injury 3 No Apparent Injury	0 0 2 0 0	Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car Passenger Car		Clear or Partly Cloudy Clear or Partly Cloudy	Dry Str Dry Str	raight & Grade Dawn raight & Level Daylight	From opposite direction - one left turn - one straight From opposite direction - one left turn - one straight	Making Left Turn	Going Straight Ahead	South Northwest South West	North South North South

ECTID MV_DRIVER_CONTRIBUTING	MV_DRIVER_CONTRIBUTING	MV_DRIVER_CONTRIBUTING MV_DR	RIVER MV_DRIVER	R MV_DRIVER_CONTRIBUTING MV_DRIVER_CONTRIBUTING	MV_DRIVER_CONTRIBUTING	MV_DRIVER N	/_DRIVER BICYCLIST_CONTRIBUTING BICYCLIST_CONTRIBUTING CIRCUMSTANCE_1_UNIT_1 CIRCUMSTANCE_2_UNIT_1	BICYCLIST_CONTRIBUTING BICYCLIST	T BICYCLIST BICYCLIST_CONTRIBUTING BICYCLIST_CONT	BUTING BICYCLIST_CONTRIBUTING E	BICYCLIST	BICYCLIST PEDESTRIAN_CONTRIBUTING PEDESTRIAN_CO	TRIBUTING PEDESTRIAN_CONTRIBUTING PEDESTRIAN PEDESTRIAN C JUNIT_2 GENOER_UNIT_2 GENOER_UNIT_2	FIRST_IMPACT_LOCATION
	CIRCUMSTANCE_2_UNIT_1	CIRCUMSTANCE_3_UNIT_1 AG	RIVER MV_DRIVER GE GENDER T_1 UNIT_1 33 Male	CIRCUMSTANCE_1_UNIT_2 CIRCUMSTANCE_2_UNIT_2 None	CIRCUMSTANCE_3_UNIT_2	AGE_UNIT_2	CIRCUMSTANCE_1_UNIT_1 CIRCUMSTANCE_2_UNIT_1	CIRCUMSTANCE_3_UNIT_1 AGE UNIT_1	GENUER CIRCUMSTANCE_1_UNIT_2 CIRCUMSTANCE_:	UNIT_2 CIRCUMSTANCE_3_UNIT_2 A	AGE_UNIT_2	GENDER_UNIT_2 CIRCUMSTANCE_1_UNIT_2 CIRCUMSTANCE		
			39 96 Male	None None		64 M 54 Fe	nale .							Lane 2 Decreasing Miepost Lane 2 Increasing Miepost Lane 2 Increasing Miepost Lane 2 Increasing Miepost
2 Follow Too Closely 3 Inattention 4 Exceeding Reas, Safe Speed	Did Not Grant RW to Vehicle Follow Too Closely	Unknown Distraction	75 Female 27 Female			60 Fe			None	12	2	Female		Lane 2 Decreasing Milepost Intersecting Road Increasing Milepost
05 Follow Too Closely 06 Operating Defective Equipment	Exceeding Reas. Safe Speed	Unknown Distraction	27 Female 65 Female 26 Female	None None		46 M	0							Lane 2 Increasing Milepost Lane 2 Decreasing Milepost Lelf Turn Lane Increasing Milepost
007 Operating Defective Equipment 008 Follow Too Closely 009 Inattention 100 Inattention	Inattention Follow Too Closely		53 Female 30 Female 25 Male	None Driver Not Distracted None None		46 Ma 66 Fe 25 Ma	e							Lane 2 Decreasing Milepost Lane 1 Increasing Milepost Lane 2 Decreasing Milepost
Inattention	Under Influence of Drugs	Did Not Grant RW to Vehicle	29 Female 29 Female	None								None	33 Female	Lane 1 Increasing Milepost Left Turn Lane Increasing Milepost
11 Inattention 12 Follow Too Closely 13 Other Contributing Circ Not Listed 15 None			29 Female 70 Male 39 Male 71 Female	None None None		73 Mc 24 Fe 80 Fe	nale nale							Lane 2 Increasing Milepost Left Turn Lane Decreasing Milepost Lane 2 Decreasing Milepost
15 None 16 Driver Not Distracted 17 Follow Too Closely			3/ Female	None		29 Fe 53 M	nale e							Lane 1 Decreasing Milepost
18 Disregard Stop and Go Light				Unknown Distraction None None		53 M 29 Fe 62 M 23 Fe	nale							Lane 1 Decreasing Milepost Lane 2 Decreasing Milepost Lane 2 Decreasing Milepost Lane 2 Decreasing Milepost Lane 2 Increasing Milepost
119 Follow Too Closely 20 Driver Not Distracted			21 Female	Notine Driver Not Distracted Driver Not Distracted Driver Not Distracted Driver Not Distracted Notine Not Distracted Notine Noti		46 Fe 31 M	nale Driver Not Distracted e nale	28	Male					Intersecting Road Increasing Milepost Lane 2 Increasing Milepost
20	Did Not Grant RW to Vehicle Follow Too Closely		18 Male 35 Female	None None		54 Fe	nale							Lane 1 Increasing Milepost Lane 2 Decreasing Milepost Lane 2 Increasing Milepost
25 Disregard Stop and Go Light 26 Improper Turn/Merge	Did Not Grant RW to Vehicle	Unknown Distraction	35 Female 64 Female 76 Male	None None		22 Fe 68 Fe	nale ale							Lane 2 Increasing Milepost Lane 2 Decreasing Milepost Lane 1 Increasing Milepost
27 Follow Too Closely 28 Apparently Asleep or Fatigued 29 Operating Defective Equipment 30 Exceeding Reas. Safe Speed Under Influence of Alcohol	Driver Interacting with Passengers, Anim		22 Male 53 Female	Driver Not Distracted None		74 M	nale D							Lane 2 Decreasing Milepost Lane 1 Increasing Milepost In Median Increasing Milepost
30 Exceeding Reas. Safe Speed 31 Under Influence of Alcohol	Disregard Yield Sign - Flashing Yellow		22 Male 53 Female 32 Male 29 Male 57 Female	None			nale							In Median Increasing Milepost Lane 1 Increasing Milepost Increasing Other Location
33 Did Nit Grant RW to Vehicle 33 Other Contributing Circ Not Listed 34 Follow Too Closely 35 Other Contributing Circ Not Listed		materiori	44 Male 22 Female			34 Mi 61 Fe								Lane 1 honzasing Milepost Decreasing Other Location Lane 1 honzasing Milepost Lane 1 honzasing Milepost Part Right Rhoulder increasing Milepost
35 Other Contributing Circ Not Listed 36 Did Not Grant RW to Vehicle 37 Other Contributing Circ Not Listed	Inattention Improper Turn/Merge	Inattention	22 Female 83 Male 20 Male	None		31 Fe	nale							Lane 1 Increasing Milepost
36 Did Not Grant RW to Vehicle 37 Other Contributing Circ Not Listed 38 Follow Too Closely 39 Inattention			83 Male 30 Male 20 Female 54 Male	None None		28 M 28 M 58 M	0 0							Lane 1 Increasing Miepost Lane 2 Increasing Miepost Lane 2 Increasing Miepost
	Inattention		26 Female	None		40 M: 18 Fe	e e							Lane 2 Increasing Miepost Lane 1 Increasing Miepost In Nedian Increasing Miepost
None	Inattention		17 Female 65 Female 41 Male 54 Female			60 Fe 57 Fe	nale							Lane 2 Increasing Milepost Lane 1 Increasing Milepost
Improper Turn/Merge Inattention Other Contributing Circ Not Listed	Did Not Grant RW to Vehicle		62 Male 34 Male	None Driver Not Distracted None		20 M: 74 M: 30 M:	0							Lane 2 Increasing Miepost Lane 2 Increasing Miepost Lane 2 Increasing Miepost
			27 Female	None Driver Not Distracted Did Not Grant RW to Vehicle	<u> </u>	29 Ma	e e							Lane 2 Increasing Milepost
None	Inattention		84 Female 21 Female	Driver Not Distracted Did Not Grant RW to Vehicle None None		60 24 Mi 72 Mi	0							Lane 2 Increasing Miepost Lane 2 Increasing Miepost Lane 2 Increasing Miepost Lane 2 Increasing Miepost
						41 Fe 60 Fe	nale							Lane 1 Increasing Milepost Lane 2 Increasing Milepost
55 Inattention 56 Did Not Grant RW to Vehicle	Did Not Grant RW to Vehicle		25 Male 22 Female 24 Male 53 Female	None None		28 Fe	nale							Lane 2 Increasing Milepost Lane 1 Increasing Milepost Intersecting Road Decreasing Milepost
557 Improper Turn/Merge 558 None 559 Improper Turn/Merge 660 Improper Turn/Merge	Inattention		31 Male 66 Male 25 Female 59 Female	Pollow Too Closely None		66 Ma 73 Fe 57 Ma	ze ale							Lane 2 Increasing Miepost Lane 2 Increasing Miepost Lane 2 Increasing Miepost
60 Improper Turn/Merge 61 Apparently As leep or Fatigued	Inattention	Did Not Grant RW to Vehicle	59 Female 62 Female	None										Right Shoulder Increasing Milepost In Median Increasing Milepost
	Did Not Grant RW to Vehicle	Unknown Distraction	62 Female 22 Female 19 Female 63 Female	Instention Under Influence of Alcohol None None		65 Ma 36 Fe 21 Ma	u nale							Lane 2 Increasing Miepost Lane 1 Increasing Miepost Lane 2 Increasing Miepost
165 Inattention 166 Driver Interacting with Passengers, Anim 167 Follow Too Closely 168 Inattention 169 Inattention						48 M 28 Fe 23 M	D C							Lane 2 Increasing Milepost Lane 2 Increasing Milepost
68 Inattention			34 Female 34 Male	None Onne Not Detracted Onne Not Detracted Onne Not Detracted None None None None None None None None		66 M 65 M	B .							Lane 3 Increasing Miepost Lane 1 Increasing Miepost Lane 2 Increasing Miepost
10	Inattention Other Contributing Circ Not Listed	Improper Turn/Merge	89 Male 45 Female	None Driver Not Distracted		28 Ma 29 Fe 32 Fe	e nale							Lane 2 Increasing Milepost Lane 2 Increasing Milepost Lane 2 Increasing Milepost
	Distractions Outside Vehicle Unknown Distraction	Follow Too Closely	55 Male 31 Female	None None None		65 Ma	e e							Lane 2 increasing Milepost Lane 2 increasing Milepost Lane 2 increasing Milepost
13	Unknown Distraction Did Not Grant RW to Vehicle Follow Too Closely		31 Female 22 Female 28 Male	None None		68 M: 31 Fe 23 Fe	nale nale							Lane 2 Increasing Milepost Lane 2 Increasing Milepost
77 Under Influence of Alcohol 78 Follow Too Closely 79 None			28 21 Female 60 Male	None		79 Fe								In Median Increasing Milepost Lane 2 Increasing Milepost In Median Increasing Milepost
80 Other Contributing Circ Not Listed 81 Improper Backing	Did Not Grant RW to Vehicle Unknown Distraction		23 IMare	Notice		54 Fe 43 M	0							Lane 2 Increasing Miepost In Median Increasing Miepost Lane 2 Increasing Miepost Intersecting Road Decreasing Miepost
.82 Follow Too Closely .83 Follow Too Closely .84 Follow Too Closely .85 Follow Too Closely .86 Institution Too Closely .87 Cother Contributing Circ Not Listed .88 Unique Distraction			25 Male 17 Female	Unknown Distraction None None		51 Ma 35 Fe 33 Fe	nale							Lane 2 Increasing Miepost Lane 2 Increasing Miepost
85 Inattention Cther Contributing Circ Not Listed	Unknown Distraction	Follow Too Closely	24 Male 23	None None		33 Fe 28 M 32 M	e e							Lane 2 Increasing Miepost Lane 2 Increasing Miepost Lane 2 Increasing Miepost
88 Other Contributing Circ Not Listed	Unknown Distraction	Follow Too Closely	74 Male 27 Female	None Driver Not Distracted None		18 Mi 20 Mi								Lane 2 Increasing Milepost
91 Follow Too Closely	Inattention		24 Male	None		38 Fe 53 Fe	nale nale							Lane 2 Increasing Milepost Lane 2 Increasing Milepost
92 Driver Not Distracted 93 Operating Defective Equipment 94 Other Contributing Circ Not Listed	Operating Handheld Cell Phone	Follow Too Closely	18 Male 34 Male 24 Female 45 Female	None None		55 Ma 32 Fe 41 Ma	nale							Lane 2 Increasing Milepost Lane 2 Increasing Milepost Intersecting Road Increasing Milepost
96 Exceeding Reas. Safe Speed			20 Male 40 Male 59 Male	None None		69 Fe 74 M: 42 Fe	nale							Lane 1 Increasing Miepost Lane 2 Increasing Miepost Lane 1 Increasing Miepost
198 Follow Too Closely 199 Exceeding Reas. Safe Speed	Other Driver Distractions Inside Vehicle			None		52 M								Lame 1 Priceasing Milepost Lame 1 Priceasing Milepost In Median Increasing Milepost In Median Increasing Milepost Intersecting Road Decreasing Milepost
997 Follow Too Closely 998 Follow Too Closely 998 Exceeding Reas: Safe Speed 998 Only 1998 Follow Speeding S	Did Not Grant R/W to Non Motorist		45 Male 58 Female 72 Female 75 Female	None		50 Fe	nale					None	26 Female	Intersecing Road Decreasing Milepost Lane 1 Increasing Milepost Intersecing Road Decreasing Milepost
03 Disregard Stop and Go Light Operating Defective Equipment	Follow Too Closely		55 Female 18 Male	Driver Not Distracted None None None None None		18 Fe 59 Ma	nale e					TWO TO	The state of the s	Interestability related of Microsoft Interestating Interestation Interestating Interestation Interes
.05 Improper Turn/Merge .06 Follow Too Closely .07 Improper Turn/Merge	Inattention		36 Female 20 Male	None None None		59 Mi 25 Fe 57 Mi	e ale							Intersecting Road Increasing Milepost Lane 3 Increasing Milepost Lane 2 Increasing Milepost
08 Other Contributing Circ Not Listed None			73 Male	Inattention		16 Fe	nale							In Median Increasing Milepost Lane 1 Increasing Milepost Past Right Shoulder Increasing Milepost
11 Disregard Stop and Go Light 12 Follow Too Closely	Inattention		44 Male 30 Female	None None		68 Ma 61 Ma	0 0							Lane 2 Increasing Milepost Lane 2 Increasing Milepost
09 None 10 Other Contributing Circ Not Listed 11 Disregard Stop and Go Light 12 Follow Too Closely 13 None 14 Driver Interacting with Passengers, Anim 15 Expending park Stafe Stead	Enline Ton Classic		44 Male 30 Female 33 Female 58 Male	Mone								Under Influence of Alcohol Inattention	Z9 NSIE	Lane 1 Increasing Milepost
14	Glow 100 Glosel/		67 Male 33 Female	None		49 Fe	D .							Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost Lane 1 increasing Miepost
18 Follow Too Closely 19 Did Not Grant RW to Vehicle 20 Unknown Distraction			67 Male	None		50 Ma 55 Fe								Lane 1 Increasing Miepost Lane 1 Increasing Miepost
21 None 22 Exceeding Reas. Safe Speed			27 Male 26 Male	Under Influence of Alcohol Over Center Line None		25 Ma 24 Ma	0							Past Right Shoulder Increasing Milepost Lane 1 Decreasing Milepost Lane 1 increasing Milepost
23 None 24 Apparently III 25 Inattention	Exceeding Reas. Safe Speed		44 Female 74 Female 34 Male	Operating Handheld Cell Phone None None None None None		50 Ms 25 Ms 63 Ms 55 Ms	0 0							Lane 1 Increasing Milepost Lane 1 Increasing Milepost
26 Exceeding Reas. Safe Speed None			21 Male 42 Male	None										Lane 1 Increasing Miepost Lane 1 Increasing Miepost Lane 1 Decreasing Miepost
28 Exceeding Reas, Safe Speed 29 Operating Defective Equipment 30 Under Influence of Alcohol	Follow Too Closely		65 Male 20 Female 38 Female 26 Female	None		72 M	D							Lane 1 Increasing Milepost Lane 1 Increasing Milepost Past Right Shoulder Increasing Milepost Lane 1 Decreasing Milepost
31 None 32 None	lando-sino		26 Female 59 Male											Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost
31 None 22 None 33 Under Influence of Alcohol 34 None 35 Exceeding Reas. Safe Speed 5 Driver Not Distracted 37 Inatention 30 Driver Not Distracted None None 4 None	inattention		59 Male 63 Male 67 Male 44 Female	None		20 M								Lane 1 Decreazing Milepost Past Right Shoulder Decreasing Milepost Lane 1 Increasing Milepost Lane 1 Increasing Milepost Lane 1 Increasing Milepost
36 Driver Not Distracted 37 Inattention			99 Male 90 Male 70 Male 83 Male 74 Female 51 Male 62 Female 22 Male 31 Female 60 Male	None		32 M								Lane 1 Decreasing Milepost
39 None 40 None			70 Male 83 Male 74 Female											Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost
41 Exceeding Reas, Safe Speed			51 Male 62 Female	None		34 Fe								Laire I Decreasing Melpost Lane I Decreasing Melpost Lane I hereasing Melpost Lane I hereasing Melpost Past Right Shoulder Increasing Melpost
44 Did Not Grant RW to Vehicle None			22 Male 31 Female 60 Male	None Under Influence of Acohol		60 Ma 54 Ma								Lane 1 Decreasing Milepost
46 Improper Turn/Merge 47 None			79 Male 68 Female	None		54 Ma 55 Ma								Intersecting Road Decreasing Milepost Lane 1 Decreasing Milepost Lane 1 increasing Milepost
49 None Solution			53 Male 24 Female 74 Male	Other Contributing Circ Not Listed										Lane 1 Increasing Milepost Past Right Shoulder Decreasing Milepost Past Right Shoulder Decreasing Milepost Lane 1 Decreasing Milepost
43 Institution 44 Institution 45 None 45 None 46 None 46 None 47 None 48 Exceeding Reas. Sale Speed 48 Exceeding Reas. 50 None 50 Institution 50 None 50 Institution 50 None			33 Female 30 Male 69 Female	Other Contributing Circ Not Listed		35								Past Right Shoulder Decreasing Milepost
54 None None			69 Female 65 Female 54 Male 61 Female											Lane 1 Increasing Miepost Lane 1 Increasing Miepost Lane 1 Increasing Miepost
56 None 57 None			61 Female 26 Female											Lane 1 horeasing Mispost Lane 1 horeasing Mispost Lane 1 Decreasing Mispost Lane 1 Decreasing Mispost Dest Birth Schuldt Decreasing Mispost
59 Inattention Did Not Grant RW to Vehicle	Eating or Drinking		26 Female 19 Male 53 Female 20 Male	None		57 M	e e							Lane 1 Decreasing Mispost Past Right Shoulder Decreasing Miepost Past Right Shoulder Increasing Miepost Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost
61 Improper Turn/Merge 62 Exceeding Reas. Safe Speed 63 Did Not Grant PM to Mal 1			45 Male 30 Male	None None		64 Ms 24 Ms 44 Ms	0							Lane 1 Increasing Milepost Lane 1 Decreasing Milepost
64 Exceeding Reas. Safe Speed None	Distractions Outside Vehicle		45 Maie 30 Maie 23 Maie 67 Maie 56 Female	None		44 M	e sale							Lane 1 Increasing Miepost Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost
66 None 67 Over Center Line			17 Male	None		64 M	0							Lane 1 Increasing Miepost Lane 1 Increasing Miepost Lane 1 Increasing Miepost
68 Under Influence of Drugs 50 None 50 None 70 None 71 Under Influence 72 Driver Not Distracted 73 Driver Not Distracted 73 Driver Not Distracted 74 Unknown Distraction 75 Under Not Distraction 76 Unknown Distraction 77 Under Not Distraction 78 Driver Not Distraction 78 Driver Not Distraction 79 Driver Not Distraction 79 Driver Not Distraction 79 Driver Not Distracted	Follow Too Closely		62 Male	None		50 Fe 34 Ma	nale e							Lane 1 Decreasing Mlepost
71 72 Driver Not Distracted 73 Driver Not Distracted			23 Male 31 Male	None Unknown Distraction Driver Not Distracted							-			Lane 1 Decreasing Mlepost Increasing Other Location Lane 1 LX Decreasing Milepost Lane 1 LX Decreasing Milepost
74 Unknown Distraction Unknown Distraction				Oniver Not Distracted Unknown Distraction Driver Not Distracted None		28 Ma 51 Fe 23 Fe	0							Lane 1 LX Decreasing Milepost Lane 1 LX Decreasing Milepost (Prior to 2002 Impact Location Code was not lane specific) Lane 1 LX horeasing Milepost (Prior to 2002 Impact Location Code was not lane specific) Lane 1 LX horeasing Milepost (Prior to 2002 Impact Location Code was not lane specific)
76 Did Not Grant RW to Vehicle 77 Inattention			56 Male 37	None Inattention Driver Not Distracted		60 M	e e							Lane 1 LX Increasing Milepost (Prior to 2002 Impact Location Code was not lane specific)
- Dimer not orseauted		· · · · · · · · · · · · · · · · · · ·	mare			62 Fe			 	+		· · · · · · · · · · · · · · · · · · ·		Left Turn Lane LX Increasing Milepost (Prior to 2002 Impact Location Code was not lane specific)

OBJECTID WA_STATE_PLANE SOUTH_X 2010_FORWARD 6299 932047.91	WA_STATE_PLANE	COL COL COL TYPE SEVERITY FAC	HLIM	COL_AGE IMPAIRED CONDITIONS COL COL LIGHT SURFACE CONDITIONS Wet Daylight	COL COL STUI	IEXT IEXT		COL_LIGHT COL_SURFACE CONDITIONS CONDITIONS Decoded Decoded	COL_LOCATION Decoded Not at Intersection/Driveway/Roundabout		ntersection Emphasis Roadway rea_Name Area Departure
6300 932037.6- 6301 932013.9- 6302 932024.8-	1024413.2 2015 4 Vehicles/Parties Angle 1024405.04 2017 2 Vehicles/Parties Turning 1024399.21 2018 2 Vehicles/Parties Rear-End 1024396.77 2019 2 Vehicles/Parties Pedaloyolist	Mnor or Possible Injury Follow Too Closely Mnor or Possible Injury Injurgeper TurnMerge Mnor or Possible Injury Fail to Yield Mnor or Possible Injury Fail to Yield Mnor or Possible Injury Fail to Yield Mnor or Possible Injury Instituted Mnor or Possible Injury Instituted Mnor or Possible Injury Instituted	20:54 No 35:44 No 65+ No Under 25 No 65+ No	Vivi	Not at Intersection. Driveway/Roundabout Straight & Level AD Driveway or Intersection AD Driveway or Intersection Straight & Level AD Driveway or Intersection Straight & Level AD Driveway or Intersection Straight & Level AD Driveway or Intersection Straight & Grade AD Driveway or Intersection Straight & Grade AD Driveway or Intersection Straight & Grade Details before the Contraction Straight & Grade Details before Straight & Grade Straight & G		Improper Turn/Merge Di Fail to Yield Di Follow Too Closely Di	aylight Dry sylight Dry sylight Dry sylight Dry sylight Dry	At Driveway or Intersection At Driveway or Intersection Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Follow Too Closely No Risky Driving Behaviors Involved Too Closely Follow Too Closely Fatternion Foundation	No No No No
6303 932044.1; 6304 932097.4; 6305 932087.1; 6306 932074.4; 6307 932110.3; 6308 932168.5;	1024394.37 (2015) 2 Vehides/Paries Rear-End 1024379.71 (2016) 2 Vehides/Paries Rear-End 1024398.81 (2018) 2 Vehides/Paries Head-On 1024398.31 (27 (2016) 2 Vehides/Paries Rear-End 1024367.27 (2016) 2 Vehides/Paries Rear-End	Menor or Possable Injury Speeding Propenty Damage Only Follow To Olicaely Propenty Damage Only Follow To Olicaely Propenty Damage Only Operating Describe Equation Menor or Possable Injury Good Operating Describe Equation Menor or Possable Injury Follow Too Closely Menor or Possable Injury Internetion	ipment 25:34 No ipment 45:54 No 25:34 No	Dry Daylight Wet Dark-Street Lights On Wet Davight Davight Dry Daylight	Not at Intersection On-weary/Roundabout Straight & Level Not at Intersection On-weary/Roundabout Straight & Level On-weary Related but Not at Dinneway Straight & Level On-weary Related but Not at Dinneway Not at Intersection On-weary Roundabout Intersection Related but Not at Intersection Straight & Level Not at Intersection On-weary Roundabout Intersection Related but Not at Intersection Straight & Level	Older Road User 65+ No Vulnerable Road User Involved	Operating Defective Equipment D: Follow Too Closely D:	aylight Wet	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Driveway Related but Not at Driveway Not at Intersection/Driveway/Roundabout Intersection Related but Not at Intersection Not at Intersection/Driveway/Roundabout	Tollow Too Closely No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Tollow Too Closely	No No Yes No No
6310 932226.01 6311 932262.21 6312 932225.21 6313 932237.21	1024370.04 2018 2 Venides/Farines 1024352.46 2015 2 Venides/Farines 1024343.76 2018 2 Vehides/Farines 1024343.76 2018 2 Vehides/Farines 1024343.65 2018 2 Vehides/Farines 1024349.65 2018 2 Vehides/Farines 1024349.65 2018 2 Vehides/Farines 102439.31 12018 2 Vehides/Farines 102439.31 12018 2 Vehides/Farines 102439.11 12018 2 Vehides/Farines	Property Damage Cinly Inattention Minor or Possible Injury Inattention Property Damage Only Follow Too Closely Minor or Possible Injury OherUnknown	25-34 No 25-34 No 05+ No 35-44 No 05+ No 35-44 No	Day Daylight	Intersection Related but Not at Intersection Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved	Inattention	aylight Dry ark-Street Lights On Dry	Not at intersection/Driveway/Roundabout Intersection Related but Not at Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Driveway Related but Not at Driveway Not at Intersection/Driveway/Roundabout	namenaon nattention nattention	No Yes No No
6314 932285.61 6315 932279.4: 6316 932344.7! 6317 932355.5! 6318 932356.2!	1024339.13 (2018) Z Vehicles/Parties Sideswipe 1024340.3 (2018) Z Vehicles/Parties Rear-End 1024324.91 (2016) Z Vehicles/Parties Rear-End 1024320.72 (2015) Z Vehicles/Parties Rear-End 1024323.74 (2016) Z Vehicles/Parties Rear-End	Properly Damage Chiy Improper TurnMerge Properly Damage Chy OtherUnknown Properly Damage Chy OtherUnknown Properly Damage Chy Minor or Possible Injury Follow Too Closely Minor or Possible Injury Diregraf Traffic Contro	65+ No 35-44 No 65+ No 25-34 No 1 Device/Sign 65+ No	Dry Daylight	Not at Intersection/Driveway/Roundabout Straight & Level At Driveway or Intersection Straight & Level At Driveway or Intersection Straight & Level At Driveway or Intersection Straight at Hillcrest Straight at Hillcrest	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+	None D:	sylight Dry pry sylight Dry	Not at Intersection/Driveway/Roundabout At Driveway or Intersection	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Follow Too Closely No Risky Driving Behaviors Involved	Yes No No No No
6319 93235.5.6 6320 932355.0 6321 932360.1 6322 932358.44 6323 932366.8	1024320.72 (2016) 2 Vehides/Paries Rear-End 1024321.97 (2017) 2 Vehides/Paries Pedalop(sist 1024322.82 (2017) 2 Vehides/Paries Turning 1024322.6 (2019) 2 Vehides/Paries Turning 1024319.91 (2014) 2 Vehides/Paries Rear-End	Property Damage Only Follow Too Closely Minor or Possible Injury Other Unknown Minor or Possible Injury Other Unknown Minor or Possible Injury Improper Turn/Merge Property Damage Only Inattention	25:34 No 45:54 No Under 25 No 25:34 No Under 25 No	Dry Davight Wet Dark-Street Lights On Dry Dark-Street Lights On Dry Dark-Street Lights On Dry Davight Dry Davight	At Oriveway or Intersection Straight & Level At Driveway or Intersection Straight & Grade At Driveway or Intersection Straight & Grade At Driveway or Intersection Straight & Grade At Oriveway or Intersection Straight & Grade Straight at Hillierst Straight at Hillierst	Pedalcydist No Vulnerable Road User Involved	Di Other/Unknown	ark-Street Lights On Wet ark-Street Lights On Dry ark-Street Lights On Dry aylight Dry	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Follow Too Closely No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved nattention	No No No No No
6324 932355.56 6325 932333 6326 93235221 6327 932476.5 6328 932600.38	1024320.72 2016 2 Vehides/Parlies Turning 1024326.3 2017 2 Vehides/Parlies Angle 1024321.77 2018 2 Vehides/Parlies Turning 1024292.16 2019 2 Vehides/Parlies Rear-End	Properly Damage Chily Disregard Traffic Control Properly Damage Chily Disregard Traffic Control Properly Damage Chily Improper TurnMenge Properly Damage Chily Follow Too Closely Mnor or Possible Injury Amort Vacleer, Fallow	Device Sign 35-44 No Device Sign 55-64 No 55+ No 45-54 No	Wet Dark-Street Lights On	AD Driwway or Intersection Straight at Hillcrest AD Driwway or Intersection Straight & Grade At Driwway or Intersection Straight & Level Intersection Related but Not at Intersection Straight & Grade Not at Intersection-Driwway/Roundabout Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved	Disregard Traffic Control Device/Sign D: Disregard Traffic Control Device/Sign D: Improper Turn/Merge D: Follow Too Closely D:	ark-Street Lights On Wet Ark-Street Lights On Dry sylight Dry sylight Dry	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection Intersection Related but Not at Intersection	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Follow Too Closely Apparently Asleep, Fatigued or III	No No No No
6329 931378.05 6330 931385.75 6331 931380.45 6332 931237.96	1024593.18 2016 1 Vehicle/Party Struck Object 1024594.32 2019 2 Vehicles/Parties Rear-End 1024579.72 2019 1 Vehicles/Parties Struck Object 1024697.17 2018 2 Vehicles/Parties Angle Angle 2019 2019 2 Vehicles/Parties Angle 2019 201	Property Damage Cnly Operating Defective Equ Property Damage Only Speeding Suspected Serious Injury Under Influence of Alcoh Property Damage Only Fail to Yield		Dry Daylight tecSnow/Stub Buylight 25-34 Dry Dark-Street Lights On Wet Dark-Street Lights On Dry Dark-Street Lights On Dry Dark-No Street Lights-Street Lights Of	At Driveway or Intersection Unknown At Driveway or Intersection Unknown Not at Intersection-Driveway/Roundabout Curve & Grade At Driveway or Intersection Straight & Grade	No Vulnerable Road User Involved	Operating Defective Equipment D: Speeding D: Under Influence of Alcohol/Drugs/Medication D: Fail to 'Vield D:	aylight Dry aylight Ice/Snow/Slush ark-Street Lights On Dry	At Driveway or Intersection At Driveway or Intersection Not at Intersection/Driveway/Roundabout At Driveway or Intersection	No Risky Driving Behaviors Involved Speeding Under Influence of Alcohol/Drugs/Medication Fail to Yield	Yes No Yes No Yes
6333 931255.46 6334 931231.66 6335 931242.11 6336 931171.91 6337 931072.77 6338 931039.81	1024851.12 2018 1 Vehicle-Party Struck Object 1024727 93 1018 2 Vehicles-Partes Rese-Find 1024731.48 2014 1 Vehicle-Party Struck Object 102485.578.2014 2 Vehicle-Parties Turning 102485.578.2014 2 Vehicle-Parties Turning 1024864.92 3019 2 Vehicles-Parties Struck Object 1024864.95 2019 3 Vehicles-Parties Rese-Find 1026904.97 2015 2 Vehicles-Parties Rese-Find 1026904.07 2015 2 Vehicles-Parties Rese-Find 1026904.07 2015 2 Vehicles-Parties Rese-Find 1026904.07 2015 2 Vehicles-Parties Struing	Unknown Property Damage Cohy Protect To Closely Property Damage Cohy Protect To Closely Property Damage Cohy Prope	35-44 No Under 25 No 65+ No 25-34 No Under 25 No	Dry Daykight Dry Dark-Street Lights On Dry Daykight Dry Daykight	AD Direway or Interaction AD Interway or Interaction Not at Interaction Oneway/Noundabout Straight & Level Not at Interaction Oneway/Noundabout Cure & Level AD Direway or Interaction Not at Interaction Oneway/Noundabout Cure & Carde Not at Interaction Oneway/Noundabout Cure & Grade Not at Interaction Oneway/Noundabout		Follow Too Closely D: Other Contributing Circ Not Listed D: Fail to Yield D: Other Contributing Circ Not Listed D: Other Contributing Circ Not Listed D:	aylight Dry ark-Street Lights On Dry avlight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout At Driveway or Intersection	No Risky Driving Behaviors Involved Follow Too Closely No Risky Driving Behaviors Involved Fall to Yield Oo Risky Driving Behaviors Involved Follow Too Closely	No Yes No Yes
6339 930985.31 6340 930968.31 6341 930974.91 6342 930975.01 6343 930975.01	1025025.38 2014 2 Vehicles/Parties Sideswipe	Properly Damage Chily Unknown Mnor or Possible Injury Mnor or Po	45-64 No	Ury Uskjejent Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight	At Driveway or Intersection Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level At Driveway or Intersection Straight & Grade At Driveway or Intersection Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved	Fail to Yield Di Fail to Yield Di None Di	aylight Dry sylight Dry Dry Dry Dry	Not at Intersection/Driveway/Roundabout	anitention	No Yes No Yes
6344 930983.2 6345 930986.9 6346 930983.2 6347 930975.0	1025015.47 2014 2 Vehicles/Parties Angle 1025013.2014 2 Vehicles/Parties Turning 1025015.47 2015 2 Vehicles/Parties Sideswipe 1025022.67 2015 2 Vehicles/Parties Turning	Property Damage Only Fail to Yield Property Damage Only Improper Turn/Merge Minor or Possible Injury Inattention Property Damage Only Other/Unknown	Developin	Dry Daylight Wiet Daylight Dry Daylight Dry Daylight Other Daylight	AD Driweway or Intersection Curve & Grade AD Driweway or Intersection Straight & Level AD Driweway or Intersection Curve & Grade Not at Intersection-Driveway/Roundabout Curve & Grade AD Driweway or Intersection Straight & Grade Straight & Grade	No Vulnerable Road User Involved	Fail to Yield D. Improper Turn/Merge D. Inattention D. Other Contributing Circ Not Listed D.	aylight Wet aylight Dry aylight Dry aylight Other/Unknown	At Driveway or Intersection At Driveway or Intersection Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Fail to Yield Vo Risky Driving Behaviors Involved nattention Vo Risky Driving Behaviors Involved	No No No Yes No
6348 930975.01 6349 930975.01 6350 930975.01 6351 930975.01 6352 930975.01	1025022.67 2016 2 Vehicles/Parties Turning 1025022.68 2018 2 Vehicles/Parties Angle 1025022.68 2018 2 Vehicles/Parties Turning 1025022.68 2018 2 Vehicles/Parties 1025022.68 2018 2 Vehicles/Parties 1025022.68 2018 2 Vehicles/Parties 1025022.68 2 Vehicles/Parties 10	Minor or Possible Injury Minor or Possible Injury Minor or Possible Injury Ohned Unknown Property Damage Only Property Damage Only Property Damage Only Property Damage Only Minoroper TurnMerge Property Damage Only Minoroper TurnMerge Property Damage Only Minoroper TurnMerge	25:34 No 25:34 No 65+ No Under 25 No Under 25 No	Dry Dark-Street Lights On	At Driveway or Intersection Curve & Grade At Driveway or Intersection Straight & Level At Driveway or Intersection Curve & Level At Driveway or Intersection Curve & Level At Driveway or Intersection Straight & Grade At Driveway or Intersection Unknown	Motorcycle Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	None D: Improper Turn/Merge D: Improper Turn/Merge D: Improper Turn/Merge D:	sylight Dry sylight Dry sylight Dry sylight Dry sylight Dry sylight Dry	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	No Risky Driving Behaviors Involved	No No No No
6353 93093.2* 6354 930927.9* 6355 930930.5* 6356 930906.7* 6357 930912.8* 6358 93096.3*	1025052.25 2017 2 Vehicles/Parties Sideswipe 1025050.64 2017 2 Vehicles/Parties Sideswipe 1025065.93 2016 2 Vehicles/Parties Sideswipe 1025060.11 2019 2 Vehicles/Parties Turning Turning 2 Vehicles/Parties 2 Vehicles	Property Damage Only Inattention Property Damage Conly Property Damage Conly Unknown Fall to Yield Property Damage Conly Improper Turn/Merge	25:34 No Under 25 No Under 25 No 45:54 No 25:34 No	Dry Daylight Wet Dark-Street Lights On Wet Dark-Street Lights On Dry Daylight Dry Daylight	AD Driwway or Inters ection Unknown Not at IntersectionDriwway/Roundabout Curve & Grade Not at IntersectionDriwway/Roundabout Straight & Level AD Driwway or Intersection AD Driwway or Intersection Straight & Grade AD Driwway or Intersection Straight & Level	No Vulnerable Road User Involved	Fail to Yield D. Inattention D. Fail to Yield D. Improper Turn/Merge D.	ark-Street Lights On Wet ark-Street Lights On Wet sylight Dry sylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout At Driveway or Intersection At Driveway or Intersection	nattention Fail to Yield nattention Fail to Yield No Risky Driving Behaviors Involved	No Yes Yes Yes No
6358 930896.31 6359 930834.6 6360 930797.3: 6361 930705.1 6362 930351.6 6363 930351.6	1025073.83 2017 2 Vehicles/Paries Rear-End 1025147.82 2014 2 Vehicles/Paries Struck Parked Vehicle 1025137.16 2019 2 Vehicles/Paries Struck Parked Vehicle 1025137.76 2019 4 Vehicles/Paries Rear-End 1025437.66 2014 4 Vehicles/Paries Rear-End 1025437.66 2014 4 Vehicles/Paries Rear-End 1025437.62 2015 2 Vehicles/Paries Rear-End	Property Damage Orly Property Damage Orly Property Damage Orly Menor or Proschle tryony Property Damage Orly Prope	65+ No 25.34 No 25.34 No 55.64 No 55.64 No 10 10 10 10 10 10 10 1	Wet Daylight	Not at Intersection Diveway/Roundabout Straight & Level At Drivway or Intersection Straight & Grade At Drivway or Intersection Straight & Grade At Drivway or Intersection Straight & Level Not at Intersection Drivway/Roundabout Straight & Level Not at Intersection Drivway/Roundabout Straight & Level Not at Intersection Drivway/Roundabout Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved	Improper Turn/Merge D: Apparently Asleep or Fatigued D:	ark-Street Lights On Wet	Not at Intersection/Driveway/Roundabout At Driveway or Intersection At Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Apparently Asleep, Fatigued or III No Risky Driving Behaviors Involved	No No Yes Yes No
6363 930351.6: 6364 930358.4: 6365 930358.9: 6366 930287.8: 6367 930244.1:	1022437.56 2014 4 Vehicles/Paries Rear-End 1022425.92 2015 2 Ushicles/Paries Angle 1022433.9 2017 2 Vehicles/Paries 102240.01 2018 2 Vehicles/Paries 1022505.93 2 2016 2 Vehicles/Paries 1022505.93 2 2016 2 Vehicles/Paries 1022505.94 2014 2 Vehicles/Paries Angle	Minor or Possible Injury — Follow Too Closely Property Damage Chily Property Damage Chil	Under 25 No 55-64 No 65+ No 35-44 No 45-54 No	Dry Daylight	At Driveway or Intersection Straight & Level At Driveway or Intersection Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved Older Road User 65+	None D. Follow Too Closely D. Eating or Drinking Dr. Inattention Dr. Distracted Driving D. Follow Too Closely D.		Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Follow Too Closely Distracted Driving nattention Distracted Driving Follow Too Closely	No No No No
6368 930264.26 6369 930186.01 6370 930183.91 6371 930184.51 6372 930180.64	1025495.94 2014 2 Vehides/Paries Angle 1025548.75 2016 2 Vehides/Paries Angle 1025548.55 2018 2 Vehides/Paries Turning 1025548.98 2018 2 Vehides/Paries Turning 1025551.92 2014 2 Vehides/Paries Turning	Property Damage Only Inattention Property Damage Only Inattention Property Damage Chily Fail to Yield Property Damage Chily Improper Turn/Merge Property Damage Chily Improper Turn/Merge	25:34 No 55:64 No 65+ No 45:54 No 55:64 No	Dry Daylight	At Driveway or Intersection Straight & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved	Inattention D: Fail to Yield D:	ayiigiit	At Driveway or Intersection	nattention nattention Fail to Yield No Risky Driving Behaviors Involved nattention	No No No No No
6373 9301763 6374 9301763 6375 930188.21 6376 930133.22 6377 930134.99		Property Damage Only Property Damage Only Property Damage Only Property Damage Only Improper Turn/Merge Mnor or Possible Injury Inattention	55.64 No 25.34 No Under 25 No 25.34 No Under 25 No 25.34 No Under 26 No 25.34 No 25.34 No Under Influence of Alcohol	Wet Daylight	Not at Intersection. Driveway/Roundabout Straight & Level Intersection Related but Not at Intersection Straight & Level At Driveway or Intersection Straight & Level Not at Intersection. Driveway/Roundabout Straight & Level Not at Intersection. Driveway/Roundabout Straight & Level Not at Intersection. Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Inattention Di Improper Turn/Merge Di Inattention Di	aylight Dry aylight Dry ark-Street Lights On Wet	Not at Intersection/Driveway/Roundabout	Follow Too Closely nattention No Risky Driving Behaviors Involved nattention Juder Influence of Alcohol/Drugs/Medication	No No No No Vers
6378 930111.3 6379 930089.5 6380 930125.1 6381 930089.2	1025594.12014 2 Vehildes/Parties Rear-End 1025612.69 2014 1 Vehildes/Party Struck Object 1025686.24 2015 2 Vehildes/Parties Turning 1025612.24 2015 2 Vehildes/Parties Backing	Property Damage Only Minor or Possible Injury Follow Too Closely Property Damage Only Minor or Possible Injury Follow Too Closely Minor or Possible Injury Follow Too Closely	Under 25 No 55-64 No 255-34 No 255-34 No 255-34 No	26-34	Not at Intersection/Drieway/Roundabout Straight & Level At Driveway or Intersection Unknown At Driveway or Intersection Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Follow Too Closely None Di Other Contributing Circ Not Listed Improper Backing/Passing/Turning/Parking/Signaling Di	sylight Wet sylight Wet ark-Street Lights On Dry sylight Dry	Not at Intersection/Driveway/Roundabout At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Tollow Too Closely No Risky Driving Behaviors Involved Follow Too Closely	No Yes No No
6382 930113.14 6383 930108.21 6384 930067.7 6385 930070.5 6386 930070.5 6387 930027.5 6388 930025.7	1025598.33 2017 2 Vehicles/Parties Rear-End 1025627.29 2014 2 Vehicles/Parties Rear-End 1025627.67 2016 2 Vehicles/Parties Rear-End 1025627.71 2019 2 Vehicles/Parties Sideswipe	Property Damage Only Follow Too Closely Property Damage Only Follow Too Closely Property Damage Only Inattension Property Damage Only OtherUnknown	Under 25 No 25-34 No Under 25 No Under 25 No	Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight Daylight JoelSnow/Slush Dark-Street Lights On	Not at Intersection/Driveway/Roundabout Straight & Level Driveway Related but Not at Driveway At Intersection and Not Related Straight & Level At Intersection and Not Related Straight & Level	No Vulnerable Road User Involved	Follow Too Closely	aylight Dry sylight Dry sylight Dry ark-Street Lights On Ice/Snow/Slush	Not at Intersection/Driveway/Roundabout Driveway Related but Not at Driveway At Intersection and Not Related At Intersection and Not Related	Follow Too Closely Follow Too Closely nattention No Risky Driving Behaviors Involved	No No No No Yes
6388 9300257.5 6388 930025.7 6389 929894.2 6390 92999.0 6391 929855.6 6392 929874.11 6393 929786.8	1022654.14 2019 2 Vehides/Paries Turning 1022655.3 2017 1 Vehides/Party 0xer Embarkment - No 1022731.38 2015 3 Vehides/Paries 1022733.3 2016 2 Vehides/Paries 1022767.74 2015 3 Vehides/Paries 1022767.74 2015 3 Vehides/Paries Rear-End Rear-End	Minor or Possible liquy Guardrail Present Suspende Selsories liquy Minor or Possible liquy Minor or Possible liquy Property Damage Anhy Minor or Possible liquy Property Damage Anhy Minor or Possible liquy Property Damage Only Property Damage Only Only Chickwom Only Chickwom Property Damage Only Chickwom Only Chickwom Only Chickwom Only	00" NU	Dry Daylight Dry Daylight Dry Daylight Wet Daylight Dry Daylight Wet Daylight Dry Daylight Dry Dawn/Dask	A Diniveway or Interaction A Diniveway or Interaction A Diniveway or Interaction Interaction Related but Not at Interaction Straight 8.Level Interaction Related but Not at Interaction Straight 8.Level Not at Interaction Diniveway/Roundabout Straight 8.Level Not at Interaction Diniveway/Roundabout Straight 8.Level Not at Interaction Diniveway/Roundabout Straight 8.Level	No Vulnerable Road User Involved	Other Contributing Circ Not Listed Di- Inattention Di- Operating Defective Equipment Di-	aylight Dry aylight Wet Dry Dry	At Driveway or Intersection At Driveway or Intersection Intersection Related but Not at Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Jistracked Driving No Rilsky Driving Behaviors Involved nattention No Rilsky Driving Behaviors Involved Follow Too Closely No Rilsky Driving Behaviors Involved State of the State of the State of the State of the State of	Yes No No No
6393 929786.8: 6394 929796.8: 6395 929786.8: 6396 929749.3: 6397 929751.8	1025815 2015 2 Vehicles/Pariles Rear-End 1025784 66 2016 2 Vehicles/Pariles Rear-End 1025784 66 2016 2 Vehicles/Pariles Rear-End 1025815 2017 2 Vehicles/Pariles Rear-End 1025840 28 2015 2 Vehicles/Pariles Rear-End 1025840 28 2015 2 Vehicles/Pariles Rear-End	Properly Demagae Cely Properly Demagae Cely Properly Demagae Cely Properly Demagae Cely Mero or Possible Injury Mero or Possible Injury Mero or Possible Injury Properly Demagae Cely Properly Demagae	ipment Under 25 No 45-64 No Under 25 No 35-44 No	Wet Dawlight Dry Dawlight Dry Dawlight Dry Dawlight Dry Dawlight Dry Dawlight Dry Dawlight	mersection Related but Not at intersection Straight & Level At Driveway or Intersection Straight & Level Intersection Related but Not at Intersection Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Operating Detective Equipment Di Other Contributing Circ Not Listed Di Follow Too Closely Di Speeding Di	aylight Dry ark-Street Lights On Dry aylight Dry	Intersection Related but Not at Intersection At Driveway or Intersection	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Follow Too Closely Speeding	No No No No
6398 929702.2: 6399 929702.2: 6400 929702.2: 6401 929702.2:	1025877.54 2015 2 Vehicles/Parties Rear-End 1025877.54 2015 1 Vehicle/Party Struck Object 1025877.54 2016 2 Vehicles/Parties Pedestrian 1025877.54 2016 2 Vehicles/Parties Angle	Property Damage Only Property Damage Only Property Damage Only Property Damage Only Mnor or Possible Injury Fail to Yield Property Damage Only Fail to Yield	50-94 NO 65+ No 45-54 No 65-64 No 65+ No	Dry DawnOusk Dry DawnOusk losSnowSlush Dark-Street Lights On Unknown Dark-Street Lights On Wet Dark-Street Lights On	At Driveway or Intersection Straight & Level At Driveway or Intersection Curve & Grade	Older Road User 65+ No Vulnerable Road User Involved Pedestrian Older Road User 65+	Follow Too Closely D: Speeding D: Did Not Grant R/W to Non Motorist D: Fail to Yield D: D: D: D: D: D: D: D	awn/Dusk Dry ark-Street Lights On loe/Snow/Slush ark-Street Lights On Other/Unknown ark-Street Lights On Wet	At Driveway or Intersection	Follow Too Closely Speeding Fail to Yield Fail to Yield	No No Yes No No
6402 929699.41 6403 929699.41 6404 929701.41 6405 929699. 6406 929702.23	1025873.37 2014 2 Vehicles/Parties Angle 1025873.35 2015 2 Vehicles/Parties Rear-End 1025875.61 2015 2 Vehicles/Parties Angle 1025877.54 2015 2 Vehicles/Parties Turning	Minor or Possible Injury OhenUhrknown Moor or Possible Injury OhenUhrknown Moor or Possible Injury Operating Detective Equi Moor or Possible Injury Operating Detective Equi Properly Damage Cotly Improper Turn Merge Properly Damage Cotly Follow Too Closely	pment Under 25 No 65+ No 35-44 No	Dry Daylight Wiet Daylight Dry Daylight Dry Daylight Dry Daylight Wiet Dark-Street Lights On	AD Oriveway or Intersection Unknown AD Oriveway or Intersection Straight & Level AD Oriveway or Intersection Straight & Level AD Oriveway or Intersection Straight & Level AD Oriveway or Intersection Unknown AD Oriveway or Intersection Unknown Unknown	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved	Disregard Traffic Control Device/Sign Di Operating Defective Equipment Di Improper Turn/Merge Di Follow Too Closely D.	aylight Wet aylight Dry sylight Dry ark-Street Lights On Wet	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	No Risky Driving Behaviors Involved Follow Too Closely	No No No No No
6407 929698.0 6408 929700.3 6409 929702.2 6410 929702.2 6411 929702.2	1025870.74 2017 1 Vehicle/Party Struck Object	Property Damage Only Unknown Minor or Possible Injury Property Damage Only Follow To Cicsely	Under 25 No Unknown No 65+ No 65+ No 1 Device/Sign 35-44 No	Day Daylight	AD Oriveway or Inters ection Straight & Level At Intersection and Not Related Straight & Level AD Oriveway or Intersection Straight & Level	No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+	Other Contributing Circ Not Listed Disregard Traffic Control Device/Sign Disregard Traffic Control Device/Sign	ark-Street Lights On Dry sylight Dry srk-Street Lights On Wet ark-Street Lights On Dry	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Intersection and Not Related At Driveway or Intersection	No Risky Driving Behaviors Involved	No Yes No Yes No
6411 929702.21 6412 929604.91 6413 995027.7 6414 99593.2.6 6415 996017.3 6416 996106.91	100887 20 (2018) Whiteless Parkets Registration (1998) Whiteless Parkets (1998) Whiteless (1	Minor or Possible Injury Under Influence of Alcob Property Damage Only Instructed Driving Minor or Possible Injury Speeding Property Damage Only Other Unknown	25-34 No ol/Drugs/Medication 25-34 No 55-64 No 55-64 No 65+ No	Diy Dayigit	Not at Intersection.Driveway/Roundabout Straight & Level Straight & Level	Pedestrian No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	None D: Distracted Driving D: Speeding D: None D:	ark-No Street Lights/Street Lights Off Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Follow Too Closely Juder Influence of Alcohol/Drugs/Medication Joistracted Driving Speeding No Risky Driving Behaviors Involved	No No No No
6417 999376.81 6418 999420.01 6419 1000052.61 6420 1000209.61 6421 1001190.97 6422 1001716.3	1009948 28 2018 2 Whides/Partes Rear-End 1009948 28 2017 2018 2 Whides/Partes Rear-End 1009932.16 2017 2 Whides/Partes Sides wipe 1009924.26 2018 1 Whides/Party Struck (Dipied 1009976.08 2019 2 Whides/Partes Music ADD 1009976.08 2019 2 Whides/Partes Head-On 1009976.08 2019 2 Whides/Partes Rear-End 1009976.08 2019 2 Whides/Partes Rear-End 1009976.08 2019 2 Whides/Partes	Minor or Possible Injury Distracted Driving Property Damage Chily Follow Too Closely Property Damage Chily Fall to Yield Property Damage Chily Fall to Yield Property Damage Chily Distracted Driving Fatality Object/	25:34 No Under 25 No 05+ No Under 25 No 05+ No Under 25 No 25:34 No	Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved Older Road User 65+		ark-No Street Lights/Street Lights Off Wet	Not at Intersection/Driveway/Roundabout	Distracted Driving Follow Too Closely Fail to Yeld Distracted Driving No Risky Driving Behaviors Involved	No No Yes Yes Yes
6423 1002538.58 6424 1003223.68 6425 1003420.68 6426 1003824.24	100884153 2018 Z Vehides/Paries Rear-End 100881235 2014 Z Vehides/Paries Rear-End 1009768 69 2019 Z Vehides/Paries Rear-End 100976133 2014 Vehides/Paries Rear-End 1009741.51 2017 Z Vehides/Paries 2009741.51 2017 Z Vehides/Paries 2009741.51 2017 Z Vehides/Paries 2009741.51 2017 Z Vehides/Paries	Property Damage Chty Speeding Minor or Possible Injury Other Unknown Minor or Possible Injury Other Unknown Minor or Possible Injury Minor or Possible Injury Inattention Minor or Possible Injury Inattention Minor or Possible Injury Speeding	25:34 No 35:44 No 36:44 No 65* Apparently Asleep, Fatigued or III 65* Apparently Asleep, Fatigued or III 25:34 No Under 25 No No Control 10 Control 10	Dry Daylight	Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved	Speeding D: None D: Diver Physical Condition D: nattention D:	avlight Dry	Not at Intersection/Driveway/Roundabout	Speeding No Risky Driving Behaviors Involved Apparently Asleep, Fatigued or III nattention Speeding	No No No No No
6427 1006706.4' 6428 1008213.' 6429 1010534.0: 6430 1010534.0: 6431 1011475.02'	1009517.79 2018 11 Venicle/Party Animai	Property Damage Chiy Mnor of Possible Initury Mnor of Possible Initury Property Damage Chiy Property Damag	35-44 No	Dry Davight Dry Dark-No Street Lights (Street Lights Cit Dry Dark-No Street Lights Cit Dry Dark-Street Lights Cit 35-44 Dry Dark-Street Lights Cit Wet Daylight	Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Unknown	No Vulnerable Road User Involved Older Road User 65+	None D: Speeding D:	aylight Dry ark-No Street Lights/Street Lights Off Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Speeding No Risky Driving Behaviors Involved Juder Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved	No No No Yes No
6432 1011487.26 6433 1011724.36 6434 1011892.8 6435 1012132.3	1007988.45 2018 Vehicle/Party Animal 1007797.075 2017 Vehicle/Party Struck Object 1007766.04 2016 Vehicle/Party Animal 1007647.24 2017 3 Vehicle/Party Animal 1007647.24 2017 3 Vehicle/Party Fire started in vehicle 100743.89 1 2017 Vehicle/Party Fire started in vehicle 100743.89 1 2017 Vehicle/Party Fire started in vehicle 100743.89 1 2017 100743	Property Damage Only Property Damage Only Property Damage Chiy Property Damage Chiy Minor or Possible Injury Property Damage Chiy	55-64 No No No No No No No No		Not at Intersection/Univeway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Curve & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User involved	None D:	awniDusk Dry ark-No Street Lights/Street Lights Off Dry aylight Dry aylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Jinder Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved Speeding No Risky Driving Behaviors Involved	No Yes No No
6437 1015067.9 6438 1015093.8 6439 1015140.6	100559.42 2016 2 Vehicles/Parties Rear-End 100559.41 2014 1 Vehicle/Party Animal 1005492.1 2014 1 Vehicle/Party Animal 1005497.09 2016 1 Vehicle/Party Animal	Property Damage Only Inattention Property Damage Only Other/Unknown Property Damage Only Other/Unknown Property Damage Only Other/Unknown Property Damage Only Other/Unknown	05+ No 65+ No 65+ No 65+ No 65+ No	Dry Daylight Dry Daylight Wet Daylight Dry Daylight Dry Daylight	At Driveway or Intersection Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Grade	Older Road User 65+ Older Road User 65+ Older Road User 65+ Older Road User 65+	Inattention	aylight Dry aylight Dry aylight Wet aylight Wet aylight Dry	At Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	nattention No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No No No
6440 1015146.01 6441 1015162.2: 6442 10153279; 6443 1015570.0: 6444 1015771.4: 6445 1015773.6:	1005469.34 2017 7 3 Vehides-Paries Rear-End 1005131.77 2019 1 Vehide-Party Animal 1005094.82 2014 1 Vehide-Party Over Embarkment-No 1004096.1 2014 2 Vehides-Paries 1004096.1 2015 2 Vehides-Paries 100409.6 26 2015 2 Vehides-Paries Vehides-Paries Turning	Property Damage Only Fail to Yield	45-54 No 55-64 No Under 25 No 25-34 No 55-64 No	Dry	Not at Intersection/Drivewaykoundabout Straight & Grade At Driveway or Intersection Straight & Level	No Vulnerable Road User Involved Motorcycle	Construction	aylight Dry		Speeding Vo Risky/Driving Behavlors Involved nattention all to Yield Vo Risky/Driving Behavlors Involved Vo Risky/Driving Behavlors Involved Vo Risky/Driving Behavlors Involved	No Yes No No
6447 1015774.8 6448 1015771.8 6449 1015992.7 6450 1016102. 6451 1016143.3	1004802.86 (2011) 2 venidelis**narea (1004802.86 (2011) 2 venidelis**narea (1004802.86 (2017) 1 venidelis**narea (1004808.1.1 2016) 1 venidelis**narea (1004808.1.1 2017) 1 venidelis**narea (1004808.8.1 2018) 1 venidelis**narea (1004808.8.1 2018) 1 venidelis**narea (1004808.8.1 2018) 1 venidelis**narea (1004808.8.1 2016) 1 venidelis**n	Property Damage Coly Meror or Pressable type: Import TurnMerop Property Damage Coly Observablance Property Damage Coly Observablance Moror or Possable type: Property Damage Coly Observablance Moror or Possable type: Property Damage Coly Observablance Obs	05+ No 65+ No 45-54 No Under 25 No 65+ No 25-34 No	Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight Wriet Dark-No Street Lights/Street Lights Off Dry Daylight Dry Daylight	At Intersection and Not Related Straight in Sag At Intersection and Not Related Straight & Grade	Older Road User 65+ No Vulnerable Road User Involved	None D: Speeding D: None D: Inattention D:	sylight Dry sylight Dry srk-No Street Lights/Street Lights Off Wet sylight Dry	At Intersection and Not Related At Intersection and Not Related Not at Intersection/Driveway/Roundabout	ko Risky Driving Behaviors Involved Speeding ko Risky Driving Behaviors Involved ko Risky Driving Behaviors Involved nattention ko Risky Driving Behaviors Involved Risky Driving Behaviors Involved	No Yes Yes Yes No
6452 1016275.73 6453 1016411.43 6454 1016411.44 6455 1016411.44 6456 1016473.84	1004406.06 2015 1 Vehicle/Party Struck Object	Minor or Possible Injury Under Influence of Alcoh Property Damage Only Other/Linknown	col/Drugs/Medication 25-34 No Under Influence of Arcehol 65+ No 65+ No 45-54 No 155-64 No	Dry Daylight	Not at Intersection.Universely/countabout straight a Grade Not at Intersection.Oniversely/foundabout Curve at Hillcrest Not at Intersection.Oniversely/foundabout Curve & Level Not at Intersection.Oniversely/foundabout Curve & Grade Not at Intersection.Oniversely/foundabout Curve & Grade Not at Intersection.Oniversely/foundabout Straight & Level	No Vulnerable Road User Involved Older Road User 65+	Under Influence of Alcohol/Drugs/Medication Di None Di None Di None Di None	ark-No Street Lights/Street Lights Off Dry sylight Dry ark-No Street Lights/Street Lights Off Wet sylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	to Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	Yes No No No
6457 1016469.36 6458 1016469.36 6459 1016478.4	1004051.2 2016 1 Vehicle/Party Animal 1004051.2 2018 1 Vehicle/Party Overtumed 1004027.19 2019 1 Vehicle/Party Struck Object 1004051.2 2019 2 Vehicles/Parties Angle	Property Damage Cnly Minor or Possible Injury Other/Unknown Property Damage Cnly Inattention Property Damage Cnly Fail to Yield	25:34 No Under 25 No 45:54 No Under 25 No	Dry Dark-No Street Lights/Street Lights Off Wet Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off Dry Daylight	At Intersection and Not Related Curve & Grade At Intersection and Not Related Curve & Level At Intersection and Not Related Curve & Level At Driveway or Intersection Curve & Grade	No Vulnerable Road User Involved	None D. Other Contributing Circ Not Listed D. Inattention D. Fail to Yield D.	ark-No Street Lights/Street Lights Off Dry ark-No Street Lights/Street Lights Off Wet ark-No Street Lights/Street Lights Off Dry aylight Dry	At Intersection and Not Related At Intersection and Not Related At Intersection and Not Related At Driveway or Intersection	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved nattention	No Yes Yes No
6461 1016473.5: 6462 1016480.3: 6463 1016484.4: 6464 1016503.3: 6465 1016518.8:	1004023.37 (2016 2 Vehicles/Parties Angle 1004023.7 (2016 2 Vehicles/Parties Rear-End 1004025.1 (2017 2 Vehicles/Parties Turning 1003932.2 (2016 2 Vehicles/Parties Rear-End	Property Damage Only Improper TurnMerge Property Damage Only Speeding Mnor or Possible Injury Fail to Yield Property Damage Cnly Speeding Property Damage Cnly OberdUnknown	45:54 No 25:34 No Under 25 No 65+ No 55-64 No	Day Daylight	Not at Intersection/Driveway/Roundabout Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved	Improper Turn/Merge	avlight Dry sylight Dry sylight Dry sylight Dry sylight Dry Wet Wet Wet Wet	At Driveway or Intersection At Driveway or Intersection Intersection Related but Not at Intersection Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Speeding Fail to Yield Speeding No Risky Driving Behaviors Involved	No No No No
6465 1016518.8 6466 1016546.4 6467 1016763.1 6468 1016894.2 6469 1017085.5 6470 1017080.4 6471 996183.3	1002830.06 2019 1 Vehicle/Party Overturned 1002656.25 2016 2 Vehicles/Parties Animal	Property Damage Only University Officer Officer Officer Only Owner Center Line Property Damage Only Owner Center Line Property Damage Only Under Influence of Alcob Property Damage Only Oher-Unknown	65+ No Under 25 No Under 25 No Under Influence of Drugs S5-34 Under Influence of Drugs S5-64 No No No No No No No No	Dry Daylight	Not at Intersection Drieway/Roundabout Straight & Level Not at IntersectionDrieway/Roundabout Curve & Level Not at Intersection.Drieway/Roundabout Curve & Level Not at Intersection.Drieway/Roundabout Curve & Level Not at Intersection.Drieway/Roundabout Straight & Level Not at Intersection.Drieway/Roundabout Straight & Grade	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	None D: Lane Departure D: Under Influence of Alcohol/Drugs/Medication D: None D:	aylight Dry saylight Wet ark-No Street Lights/Street Lights Off Dry saylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Juder Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved	No Yes Yes No No
6472 996200.0 6473 996194.74 6474 99618 6475 996191.73	1010585.24 2016 2 Vehicles/Parties Sideswipe	Properly Damage Coly, Institutions Properly Damage Coly, Other Unknown Properly Damage Coly, Datassized Driving Properly Damage Coly, Fail to Yield Properly Damage Coly, Fail to Yield	Under 25 No 25:34 No 25:34 No 55:64 No Under 25 No	Dry Daylight Dry Dark-Street Lights On Dry Daylight Dry Daylight	At Intersection and Not Related Straight & Level At Driveway or Intersection Straight & Grade At Driveway or Intersection Straight at Hillcrest At Driveway or Intersection Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved	D: Other/Unknown	ark-Street Lights On Dry	Not at Intersection/Drieway/Roundabout At Drieway/or Intersection At Intersection and Not Related At Drieway or Intersection At Universection At Universection At Universection At Universection At Universection At Universection	nattention No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Distracted Driving Distracted Driving Distracted Driving	Yes Yes No No
6476 996186.3 6477 996188.1 6478 996182.4	1010580.69 2017 2 Vehicles/Parties Turning	Property Damage Only Fail to Yield Property Damage Only Inattention Property Damage Only OberUnknown	55-64 No 35-44 No 35-44 No	Dry Daylight Dry Dawn/Dusk Dry Daylight Dry Daylight	At Driveway or Intersection Straight & Grade	No vuinerable Road User Involved	Fail to Yield D. Inattention Dr. Other/Unknown Dr.	aylight Dry awn/Dusk Dry aylight Dry avlight Dry avlight Dry	At Driveway or Intersection At Driveway or Intersection Not at Intersection/Driveway/Roundabout	Fail to Yield nationation No Risky Driving Behaviors Involved	No No No

OBJECTID JURISDICTION	COUNTY CITY	PRIMARY BLOCK NUMBER	INTERSECTING DIST FROM FOR POOR METAPORT METAPORT SEPONT FROM FROM POOR METAPORT MET	R DATE TIME INJURY_TYPE	NUM NUM NUM NUM NUM NUM INJ FAT VEH PEDS BIKE	VEH_1_TYPE	VEH_2_TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD_SURFACE ROADWAY CHARACTERISTIC	LIGHTING CONDITION	FRST_COLLISION TYPE_OBJECT_STRUCK	VEH_1 ACTION	VEH.2 ACTION	VEH.1.COMPASS DIRECTION_TO DIRECTION_TO DIRECTION_TO
6479 State Route 6480 State Route	Clallam Sequim Clallam Sequim Clallam Sequim	101LX26305 101LX26305	0.04 No E85523 0.09 No E62908	2018-10-31 16:45 No Apparent Injury i 2017-01-05 12:40 Possible Injury 2015-12-18 16:59 No Apparent Injury	0 0 3 0	Passenger Car Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection At Intersection and Related	Overcast Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Dusk Daylight	From same direction - both going straight - one stopped - rear-end Same direction both turning left both moving rear end	Going Straight Ahead Making Left Turn	Stopped for Traffic Making Left Turn	South North Vehicle Stopped Vehicle Stopped North East North East
6481 State Route 6482 State Route	Clallam Sequim Clallam Sequim Clallam Sequim	101LX26305 101LX26305			0 0 2 0	Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related	Raining Clear or Partly Cloudy	Dry Straight & Level	Dusk	Same direction both turning left both moving sideswipe From opposite direction - one left turn - one straight	Making Left Turn Making Left Turn	Making Left Turn Going Straight Ahead	South East South East North East North East South North
6482 State Route 6483 State Route 6484 State Route 6485 State Route 6486 State Route 6487 State Route	Clallam Sequim Clallam Sequim	101LX26305 101LX26305	0.11 No E40022 0.11 No E77036	2017-08-12 09:41 No Apparent Injury 2015-02-06 19:48 No Apparent Injury 2018-02-18 07:40 Possible Injury	0 0 2 0	D Passenger Car D Passenger Car D Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Raining Snowing	Wet Straight & Level Snow/Slush Straight & Grade	Dark-Street Lights On Daylight	Entering at angle From opposite direction - one left turn - one straight Signal Pole	Going Straight Ahead Making Left Turn Making Left Turn	Making Left Turn Going Straight Ahead	North Southeast South North West North
6486 State Route 6487 State Route	Clallam Sequim Clallam Sequim	101LX26456 101LX26456	0.00 No E42856	2015-05-24 11:00 No Apparent Injury 2018-04-03 14:24 Possible Injury	1 0 1 0 0 0 2 0 1 0 2 0	Passenger Car	Passenger Car Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight at Hillor	Daylight Daylight	Entering at angle From opposite direction - one left turn - one straight	Going Straight Ahead Making Left Turn	Going Straight Ahead Going Straight Ahead	North South East West North East South North
6487 State Route 6488 State Route 6489 State Route 6490 State Route	Clallam Sequim Clallam Sequim	101LX26456 101LX26456	0.06 No 272798 0.08 No 272787	2017-05-15 15:16 Possible Injury	1 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Motorcycle	Intersection Related but Not at Intersection At Intersection and Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level Wet Straight & Grade	Daylight Daylight	Bridge Rail - Face Entering at angle	Making Left Turn Making Left Turn	Going Straight Ahead	West North West North South North
6490 State Route 6491 State Route	Clallam Sequim Clallam Sequim Clallam Sequim	101LX26456 101LX26456	0.08 No 272787 0.08 No 375936	2015-09-15 10:51 No Apparent Injury 2017-08-09 16:03 Suspected Minor Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Motorcycle		Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight at Hillor	Daylight	Entering at angle Entering at angle	Making Left Turn Making Right Turn	Going Straight Ahead Going Straight Ahead	West North South North West North North South
6491 State Route 6492 State Route 6493 State Route 6494 State Route	Clallam Sequim Clallam Sequim	101LX26456 101LX26456	0.08 No E39123 0.08 No E75418	2019-09-10 IO3 Suspendin Minor Injury 2017-08-09 IO3 Suspendin Minor Injury 2017-08-09 IO3 Suspendin Minor Injury 2015-01-10 20-42 No Apparent Injury 2016-01-10 20-42 No Apparent Injury 2016-10-20 IO3 Suspendin Minor Injury 2016-10-20 IO3 No Apparent Injury 2016-10-10 IO3 No Apparent Injury 2016-01-10 IO3 No IO3	1 0 2 0	D Pickup,Panel Truck or Vanette under 10,000 lb D Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Related	Other Raining Clear or Partly Cloudy	Wet Straight & Grade Wet Straight & Grade Dry Straight & Grade	Dark-Street Lights On	Wood Sign Post Entering at angle From same direction - both going straight - one stopped - rear-end	Making Right Turn Making Left Turn Going Straight Ahead	Going Straight Ahead	South West South S
6495 State Route 6496 State Route	Clallam Sequim Clallam Sequim	101LX26610 101P126272	0.00 No E36588	2014-10-15 17:00 No Apparent Injury 2016-10-15 16:55 No Apparent Injury	0 0 2 0	Passenger Car D Pickup Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related Not at Intersection and Not Related	Overcast Raining	Wet Straight & Grade	Daylight	From opposite direction - all others From same direction - both going straight - one stopped - rear-end	Making U-Turn Going Straight Ahead	Going Straight Ahead Slowing	West West East West West East West East
6495 State Route 6496 State Route 6497 State Route 6498 State Route	Clallam Sequim Clallam Sequim	101P126272 101P126272	0.20 No E71914 0.25 No E37234	2017-09-29 09:45 Suspected Serious Injury 2014-10-14 00:00 No Apparent Injury	1 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car			Overcast	Dry Curve & Grade Wet Curve & Grade	Daylight Daylight Dark-Street Lights On	Wood Sign Post Wood Sign Post	Going Straight Ahead Going Straight Ahead		West East West East
6499 State Route 6500 State Route	Clallam Sequim Clallam Sequim Clallam Sequim	101P126272 101P126272	0.27 No E93849 0.30 No E62639	2019-07-09 11:09 No Apparent Injury 2016-12-27 11:50 Possible Injury	1 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Grade Dry Straight & Grade		From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing Stopped for Traffic Starting in Traffic Lane	West East West East West East Vehicle Stopped Vehicle Stopped
6501 State Route 6502 State Route	Clallam Sequim	101P126272 101P126272	0.32 No E75063	2017-12-21 12:12 No Apparent Injury				Intersection Related but Not at Intersection	Clear or Partly Cloudy	Dry Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Starting in Traffic Lane Starting in Traffic Lane Stopped for Traffic	Starting in Traffic Lane Going Straight Ahead	West East West East Vehicle Stopped Vehicle Stopped West West
6503 State Route 6504 State Route 6505 State Route 6506 State Route	Clallam Sequim Clallam Sequim Clallam Sequim Clallam Sequim	101P126272 101P126426 101P126426	0.34 NO 27/0/00 0.29 No 304666	2017-11-01 16.15 Possible filluly	0 0 2 0	Passenger Car Dickup Panel Truck or Vanette under 10,000 lb.	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight at Hillor Dry Straight & Grade	Daylight Daylight	Vehicle turning right hits pedestrian From same direction - one left turn - one straight From same direction - one left turn - one straight	Making Right Turn Going Straight Ahead Going Straight Ahead	Making Left Turn Stopped at Signal or Stop Sign	West South West North West East Waste East Wastel Stronged Wishide Stronged
	Clallam Sequim Clallam Sequim	101Q126664 101R126488	0.04 No E75154 0.04 No E75154 0.031 No 375932	2017-12-23 19:10 No Apparent Injury 2018-08-19 13:12 No Apparent Injury	0 0 1 0	Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade Dry Curve & Grade	Daylight Daylight Dark-Street Lights On Daylight	From same direction - both going straight - one stopped - rear-end Roadway Ditch From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Making Left Turn Making Right Turn	Stopped at Signal or Stop Sign	West East Vehicle Stopped Vehicle Stopped North East Sast North Fast North
6508 State Route 6509 State Route 6510 State Route 6511 State Route	Clallam Sequim Clallam Sequim		0.32 No 272798			Passenger Car	Passenger Car	At Intersection and Related	Clear or Partly Cloudy Overcast	Dry Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead	Stopped at Signal or Stop Sign Stopped at Signal or Stop Sign	West East West East East North East South
6510 State Route 6511 State Route	Clallam Sequim Clallam Sequim	101R126488 101R126488	0.32 No E56916 0.32 No E56916	2016-05-24 14:10 No Apparent Injury 2016-05-24 13:55 No Apparent Injury 2016-05-24 13:55 No Apparent Injury	0 0 2 0 0 0 2 0	D Passenger Car D Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Overcast Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Grade Dry Straight & Grade Dry Straight & Grade	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Slowing Slowing	Stopped at Signal or Stop Sign Stopped at Signal or Stop Sign	East West Vehicle Stopped Vehicle Stopped East West Vehicle Stopped Vehicle Stopped
6512 State Route 6513 State Route 6514 State Route 6514 State Route 6515 State Route	Clallam Sequim Clallam Sequim Clallam Sequim	101R126488 101R126647	0.32 No E82768 0.32 No 272797	2018-08-11 12:20 Possible Injury 2016-06-22 20:11 No Apparent Injury	2 0 2 0 0 0 1 0	D Pickup,Panel Truck or Vanette under 10,000 lb D Passenger Car	Pickup,Panel Truck or Vanette under 10,000 ib	At Intersection and Related Not at Intersection and Not Related	Overcast Overcast	Dry Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead	Stopped at Signal or Stop Sign	East West Vehicle Stopped Vehicle Stopped East West
6514 State Route 6515 State Route	Clallam Sequim Clallam Sequim	101S126272 101S126419	0.18 No E91796 0.03 No E30555	2019-05-05 21:35 No Apparent Injury 2014-02-07 20:56 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Other Curve & Grade	Daylight Dark-No Street Lights Dark-Street Lights On	Vehicle Strikes Deer Vehicle overturned	Going Straight Ahead Going Straight Ahead		East West East West
6516 State Route 6517 State Route 6518 State Route	Clallam Clallam	101	249.90 No 343099 249.90 No E47682	2018-01-03 08:57 Possible Injury 2015-10-29 08:55 No Apparent Injury 2017-10-28 19:37 No Apparent Injury 2016-10-20 16:32 No Apparent Injury	0 0 2 0	Pickup, Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Intersection Related but Not at Intersection	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Daylight Daylight Daylight Dark-Street Lights On	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead	Stopped for Traffic Stopped for Traffic	East West Vehicle Stopped Vehicle Stopped East West East Vehicle Stopped East West East White
	Clallam	101	249.91 No E59904 No E61003						Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade	Daylight	From same direction - both going straight - both moving - rear-end Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing	East West East West
6521 State Route 6522 State Route	Cialiam Cialiam Cialiam	101	249.92 NO E51030 249.93 No 320200 250.00 No E50826	2015-01-30 09:00 Unknown 2016-01-21 08:00 No Apparent Injury 2019-02-04 15:03 Possible Injury	0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup Panel Truck or Vanette under 10,000 lb Dickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level Wet Straight in Sag Ice Straight & Grade	Daylight	Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic	West East East Vehicle Stopped Vehicle Stopped
6523 State Route 6524 State Route	Clallam Clallam	101							Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end From opposite direction - both moving - head-on From same direction - both going straight - both moving - rear-end		Going Straight Ahead Going Straight Ahead	East West East East West East West
6524 State Route 6525 State Route 6526 State Route 6527 State Route 6529 State Route	Cialiam Cialiam	101	250.05 No 347316 250.05 No E98155	2014-08-09 11:53 Possible Injury 2019-11-12 08:55 No Apparent Injury 2014-06-01 10:47 No Apparent Injury 2016-04-01 07:03 No Apparent Injury	1 0 2 0 0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Grade Wet Straight & Grade	Daylight Daylight	From same direction - both going straight - both moving - rear-end Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead	Going Straight Ahead	East West East West East West
	Ciallam Ciallam	101	250.07 No 272930 250.07 No 320154	2014-06-01 10:47 No Apparent Injury 2016-04-01 07:03 No Apparent Injury					Clear or Partly Cloudy Overcast Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Grade	Daylight Dawn	From same direction - both going straight - both moving - rear-end Vehicle Strikes Deer	Going Straight Ahead	Slowing	East West East West
6529 State Route 6530 State Route 6531 State Route	Ciallam Ciallam	101	250.07 No E38816 250.07 No E4939 260.07 Ma	2014-12-29 12:07 No Apparent Injury 1 2015-06-01 17:28 No Apparent Injury 2015-05-29 15:50 Unknown	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Pickup Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Overcast Clear or Partly Cloudy	Dry Straight & Grade Dry Straight in Sag Dry Straight & Grade	Daylight	Entering at angle Vehicle Strikes Deer Guardrail - Face	Going Straight Ahead Going Straight Ahead Other*	Going Straight Ahead	East West South West
6532 State Route 6533 State Route	Clallam Clallam	101	250.07 No E45668 250.07 No E45688	2015-08-29 19:30 Possible Injury 2016-03-17 18-52 No Apparent Injury				At Intersection and Not Related At Intersection and Not Related At Intersection and Not Related	Raining Clear or Partly Cloudy	Wet Straight & Grade Dry Straight & Grade Dry Straight & Grade	Daylight	Juardraii - Face Vehicle Strikes Deer Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead		West East West East
6532 State Route 6533 State Route 6534 State Route 6535 State Route	Ciallam Ciallam	101	250.07 No E5/25/2 250.07 No E5/31/2 250.07 No E5/31/2	2016-06-03 11:36 Possible Injury 2016-07-13 12:30 No Apparent Injury	1 0 2 0	Passenger Car D Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Related At Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Grade Dry Straight & Grade	Daylight Daylight	From same direction - both going straight - one stopped - rear-end Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead	Stopped for Traffic	West East Vehicle Stopped West East Vehicle Stopped
6536 State Route	Clallam Clallam	101			0 0 1 1	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup Panel Truck or Vanette under 10.000 lb	At Intersection and Related	Clear or Partly Cloudy Raining	Dry Straight in Sag Wet Straight & Grade	Daylight Dusk	Vehicle going straight hits pedestrian Entering at angle	Other* Changing Lanes	Making Right Turn	North South East West North West
6537 State Route 6538 State Route 6539 State Route 6540 State Route	Cialiam Cialiam	101	250.09 No E38057 250.10 No E70646	2014-12-03 17:08 No Apparent Injury 2017-08-30 11:50 No Apparent Injury 2014-01-30 12:27 No Apparent Injury	0 0 1 0	Passenger Car Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Grade	Dark-Street Lights On Daylight Daylight	Mscollaneous Object or Debris on Road From same directon - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing Stopped for Traffic	East West East West
6540 State Route 6541 State Route 6542 State Poute	Cialiam Cialiam	101	250.15 INo IE54655	2016-05-22 18:17 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	NOT ALT INTERSECTION AND NOT Related	Raining Clear or Partly Cloudy	Dry Straight & Grade	Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end Cable Burger	Going Straight Ahead	Stopped for Traffic Slowing	East Vest Vehicle Slopped East West East West Study
6543 State Route 6544 State Route	Clallam Clallam Clallam	101	250.15 No 885571 250.19 No E93057 250.20 No 276639	2018-10-20 08:37 Unknown 2019-06-14 17:00 Possible Injury	1 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Raining	Dry Straight & Level Dry Straight & Grade Wet Straight & Grade	Daylight Daylight	Jable Barner From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic Stopped for Traffic	South North West East West Wehicle Stopped Vehicle Backing Vehicle Stopped Vehicle Backing Vehicle Stopped
6545 State Route 6546 State Route	Clallam Clallam	101	250.20 No 276694	2014-01-30 14:30 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb		Raining Clear or Partly Cloudy	Wet Straight & Grade	Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end	Slowing Going Straight Ahead	Stopped for Traffic Starting in Traffic Lane	East West Vehicle Backing Vehicle Stopped West East Vehicle Stopped Vehicle Stopped
6546 State Route 6547 State Route 6548 State Route	Clallam Clallam Clallam	101	250.21 No E50925 250.25 No E42905	2015-05-27 14:54 Possible Injury	3 0 2 0	D Passenger Car D Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Not at Intersection and Not Related At Intersection and Related	Raining Clear or Partly Cloudy	Dry Straight & Grade Wet Straight & Grade Dry Straight & Level	Daylight Daylight	From same direction - both going straight - both moving - rear-end Entering at angle	Going Straight Ahead Making Left Turn	Going Straight Ahead Going Straight Ahead	East West East West North East East West
6549 State Route 6550 State Route 6551 State Route 6552 State Route	Clallam Clallam	101	250.25 No E55588	2016-06-16 12:40 No Apparent Injury	0 0 2 0	Passenger Car	Passenger Car Passenger Car	At Driveway At Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight	Entering at angle From same direction - both going straight - both moving - rear-end	Making Left Turn Going Straight Ahead Making Left Turn	Going Straight Ahead	South West East South West East West East
6551 State Route 6552 State Route	Clallam Clallam	101	250.25 No E58009 250.26 No E35980	2016-09-01 16:37 No Apparent Injury 2014-09-23 16:37 No Apparent Injury	0 0 2 0	Passenger Car D Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway Intersection Related but Not at Intersection		Dry Straight & Grade Wet Straight & Level Dry Straight & Level	Daylight Daylight	Entering at angle From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Going Straight Ahead Slowing	South West West East West East West East
6553 State Route 6554 State Route 6555 State Route 6556 State Route	Clallam Clallam	101	250.26 No E4408 250.26 No E67114	2015-07-17 16:00 No Apparent Injury 2017-05-01 15:20 No Apparent Injury	0 0 2 0	D Pickup,Panel Truck or Vanette under 10,000 lb D Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Driveway	Clear or Partly Cloudy Overcast	Dry Straight at Hillor Dry Straight & Level	Daylight	From opposite direction - one left turn - one straight Entering at angle	Making Left Turn Going Straight Ahead	Going Straight Ahead Making Right Turn	East South West East West East South East
6556 State Route 6557 State Route	Clallam	101		2017-05-01 15:20 No Apparent Injury 2018-11-30 19:15 No Apparent Injury 2018-11-17 03:17 No Apparent Injury 2016-10-04 07:43 No Apparent Injury	0 0 2 0 0 0 2 0 0 0 1 0 0 0 2 0	D Passenger Car D Passenger Car D Dickup Panel Truck or Vanette under 10 000 lb.	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight at Hillor Dry Straight & Level	Dark-Street Lights On	From same direction - both going straight - both moving - rear-end Tree or Stump (stationary) From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing	West East Wort East Wort East
6558 State Route 6559 State Route	Clallam Clallam	101	250.28 No E99131 250.20 No E90131 250.30 No E59428	2019-02-25 13:58 No Apparent Injury 2019-02-25 13:58 No Apparent Injury 2016-10-10 14:21 No Apparent Injury	0 0 3 0	D Pickup,Panel Truck or Vanette under 10,000 lb D Passenger Car	Passenger Car Passenger Car Pickup.Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Level	Daylight	From same direction - both going straight - oes topped - rear-end From same direction - both going straight - oes topped - rear-end From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Stopped for Traffic Slowing	West East Vehicle Stopped Vehicle Stopped West East West East East
6560 State Route 6561 State Route	Clallam Clallam	101	250.30 No E78042 250.30 No E87334	2019-02-26 13:58 No Apparent Injury 2016-10-10 14:21 No Apparent Injury 2016-10-10 14:21 No Apparent Injury 2018-03-16 12:23 No Apparent Injury 2018-12-17 14:19 No Apparent Injury 2019-07-22 17:20 No Apparent Injury	0 0 2 0	Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Raining	Dry Straight & Level Wet Straight & Grade	Daylight Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing Stopped for Traffic	West East West East West East Vehicle Stopped Vehicle Stopped
6562 State Route 6563 State Route	Clallam Clallam	101	250.30 No E94458 250.31 No E87474	2019-07-22 17:20 No Apparent Injury 2018-12-12 16:38 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast	Dry Straight & Level Dry Straight & Level	Daylight Dark-Street Lights On	From same direction - both going straight - both moving - sideswipe From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end		Slowing Stopped for Traffic Stopped for Traffic	West East West East West East Vehicle Stopped Vehicle Stopped
6564 State Route 6565 State Route	Clallam Clallam	101	250.32 No E97977 250.33 No E64932	2019-11-07 14:45 Possible Injury 2017-03-01 11:37 No Apparent Injury	2 0 2 0 0 0 2 0	D Pickup,Panel Truck or Vanette under 10,000 lb Truck (Flatbad,Van,etc)	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection At Driveway	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight	From opposite direction - one left turn - one straight	Going Straight Ahead Going Straight Ahead Making Left Turn	Going Straight Ahead	West East Vehide Stopped Vehicle Stopped West East Vehide Stopped Vehide Stopped East South West East
6566 State Route 6567 State Route 6568 State Route 6569 State Route	Clallam Clallam	101	250.34 No E4044 250.36 No E83020	2015-03-01 09:48 Suspected Minor Injury 2018-08-11 14:50 No Apparent Injury 2018-12-13 17:12 No Apparent Injury	0 0 2 0	D Pickup,Panel Truck or Vanette under 10,000 lb D Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Call Pickup Panel Truck or Vanette under 10,000 lb Passenger Car Pickup Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection	Clear or Partly Cloudy Overcast	Dry Straight & Level Dry Straight & Level Met Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - rear-end From opposite direction - one left turn - one right turn	Going Straight Ahead Going Straight Ahead Making Right Turn Going Straight Ahead	Changing Lanes Slowing	West East West East West East West East Worth Wart South Mart
6569 State Route 6570 State Route	Clallam Clallam	101	250.36 No E95923 250.37 No E85060	2019-08-28 19:10 No Apparent Injury 2018-10-09 15:27 Suspected Minor Injury	0 0 2 0	Passenger Car D Pickup Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Grade	Daylight Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead	Starting in Traffic Lane Stopped at Signal or Stop Sign	East West Vehicle Stopped West West East West Vehicle Stopped
6571 State Route 6572 State Route	Clallam Clallam	101	250.38 No E34705 250.38 No E35036	2014-08-05 19:21 No Apparent Injury 2014-08-19 20:25 Possible Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level Dry Straight & Grade	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Starting in Traffic Lane	Slowing Stopped at Signal or Stop Sign	West East West East West East West Vehicle Stopped
6573 State Route 6574 State Route	Cialiam Cialiam	101	250.38 No E35731 250.38 No E35731 250.38 No E38720	2014-08-25 16:51 No Apparent Injury 2014-12-24 20:52 No Apparent Injury	0 0 2 0	D Passenger Car D Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Dark-Street Lights On	From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Stopped for Traffic Making Left Turn	East West East Vehicle Stopped West East East South
6574 State Route 6575 State Route 6576 State Route 6577 State Route	Clallam Clallam	101	250.38 No E42081 250.38 No E45681	2015-04-24 16:00 No Apparent Injury 2015-08-31 07:02 Possible Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 b Pickup Panel Truck or Vanette under 10,000 b Pickup Panel Truck or Vanette under 10,000 b Passenger Car Pickup Panel Truck or Vanette under 10,000 b Pickup Panel Truck or Vanette under 10,000 b Pickup Panel Truck or Vanette under 10,000 b Pickup Panel Truck or Vanette under 10,000 b Passenger Car Pickup Panel Truck or Vanette under 10,000 b Pickup Panel Truck or Vanette under 10,000 b Pickup Panel Truck or Vanette under 10,000 b	At Intersection and Related At Intersection and Related	Overcast Overcast	Dry Straight & Level Wet Straight & Level	Dark-No Street Lights Daylight	From same direction - both going straight - both moving - sideswipe From opposite direction - all others From same direction - both going straight - one stopped - rear-end	Changing Lanes Making Left Turn	Going Straight Ahead Stopped at Signal or Stop Sign	South North South North South West West Vehicle Stopped
6579 State Route 6579 State Route	Clallam Clallam	101	250.38 No E48648 250.38 No E52549	2016-03-11 15:35 No Apparent Injury 2016-03-11 15:35 No Apparent Injury 2016-05-28 09-45 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Overcast Overcast	Dry Straight & Level	Daylight	rom same direction - ooin going straight - one stopped - rear-end From same direction - one right turn - one straight From opposite direction - all others	Going Straight Ahead	Stopped at Signal or Stop Sign Making Right Turn Stopped at Signal or Stop Sign	West East Vehicle Stopped Vehicle Stopped East West East North West North Vehicle Stopped Vehicle Stopped
6580 State Route 6581 State Route	Clallam Clallam	101	20.38 No E50496	2017-05-11 09:33 Possible Injury 2017-09-07 14:10 No Apparent Injury	1 0 1 1	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Raining Fog or Smog or Smoke	Wet Straight & Level Wet Straight & Level Dry Straight & Level	Daylight Daylight	Vehicle turning left hits pedestrian From same direction - both going straight - both moving - rear-end	Making Left Turn Making Left Turn Going Straight Ahead	Stopped at Signal or Stop Sign Slowing	South West East West
6582 State Route 6583 State Route	Clallam Clallam	101	250.38 No E85468 250.38 No E87154	2018-10-19 11:09 No Apparent Injury 2018-12-11 13:20 Possible Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Raining	Dry Straight & Level Wet Straight & Level	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end	Starting in Traffic Lane Starting in Traffic Lane Going Straight Ahead Going Straight Ahead		Vehicle Stopped East Vehicle Stopped Vehicle Stopped Vehicle Stopped East West East
6582 State Route 6583 State Route 6584 State Route 6585 State Route 6586 State Route	Clallam Clallam	101	250.38 No E89352 250.38 No E89352	2019-02-14 19:14 No Apparent Injury 2019-03-03 19:31 No Apparent Injury 2019-09-11 06:40 No Apparent Injury	0 0 2 0 0 0 2 0 0 0 2 0	D Pickup,Panel Truck or Vanette under 10,000 lb D Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Raining Clear or Partly Cloudy	Dry Straight & Level	Dark-Street Lights On	Entering at angle Entering at angle	Going Straight Ahead Going Straight Ahead Starting in Traffic Lane	Making Left Turn Making Left Turn Starting in Traffic Lane	East West North East East West North East
6587 State Route 6588 State Route	Clallam	101	250.39 No E7603	2017-09-20 11:00 No Apparent Injury	0 0 2 0	D Passenger Car D Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level West Straight & Conde	Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead	Going Straight Ahead	West East West East West East West East
6589 State Route 6590 State Route	Clallam Clallam	101	250.42 No E75045 250.43 No E58477	2017-12-19 16:47 Possible Injury 2016-08-19 13:07 No Apparent Injury	2 0 2 0	D Pickup,Panel Truck or Vanette under 10,000 lb D Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Passenger Car	Intersection Related but Not at Intersection Intersection Related but Not at Intersection	Overcast Clear or Partly Cloudy	Wet Straight & Grade Wet Straight & Level Dry Straight & Grade	Dark-Street Lights On Davlight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead	Slowing Slowing Stopped for Traffic	East West East West East West Vehicle Stopped Vehicle Stopped
6591 State Route 6592 State Route	Clallam Clallam	101	250.44 No 298621 250.44 No E58812	2014-08-18 11:30 No Apparent Injury 2016-09-19 17:24 No Apparent Injury	0 0 2 0	Pickup Panel Truck or Vanette under 10 000 lb	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy Other	Dry Straight & Grade	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead	Stopped for Traffic Stopped for Traffic	East West Vehicle Stopped East West East Vehicle Stopped
6593 State Route 6594 State Route	Clallam Clallam Clallam	101	250.46 No E44128 250.46 No E52135	2015-06-28 02:00 Possible Injury 2016-03-03 10:17 No Apparent Injury	1 0 2 0 0 0 2 0	D Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Motorcycle Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast	wet Straight & Level	Dark-Street Lights On Daylight	From same direction - both going straight - both moving - sideswipe From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead	Going Straight Ahead Slowing	West East West East East West East West
State Route	Clallam	101	250.47 No E56008 250.48 No E6605 270.48 No E6605	2016-07-01 13:53 Possible Injury 2017-01-27 17:50 No Apparent Injury 2018-11-03 13:05 No Apparent Injury	0 0 2 0 0 0 4 0	Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight in Sag Dry Curve & Grade	Daylight Dark-Street Lights On	Entering at angle From same direction - both going straight - both moving - rear-end Tree or Sturm (stationand)	Making Left Turn Going Straight Ahead Other*	Going Straight Ahead Slowing	recriii East West East West East West Fast North
6598 State Route 6599 State Route	Cialiam Cialiam Cialiam Cialiam	101	250.48 No E86665 250.50 No E43287 250.50 No E98737	2018-11-03 13:05 No Apparent Injury 2015-06-09 10:10 No Apparent Injury 2019-11-26 10:12 No Apparent Injury	0 0 2 0	D Pickup,Panel Truck or Vanette under 10,000 lb D Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Intersection Related but Not at Intersection Not at Intersection and Not Related	Clear or Partly Cloudy Overcast	Dry Curve & Level Dry Curve & Grade Dry Straight & Level	Daylight	Tree or Stump (stationary) From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - sideswipe	Other* Going Straight Ahead Other*	Stopped for Traffic Going Straight Ahead	East North East West East Vehicle Stopped East West East West
6599 State Route 6600 State Route 6601 State Route 6602 State Route	Clallam Clallam Clallam	101	200.53 No E3/349 250.54 No E3/332 250.54 No E3/332	2015-06-12 (22-45 No Apparent Injury 2015-06-12 (22-45 No Apparent Injury 2014-11-07 20:30 No Apparent Injury 2016-11-09 18:25 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade	Dark-Street Lights On	Guardrail - Face	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East West East
6602 State Route 6603 State Route	Clallam	101						Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level	Dark-No Street Lights Dark-Street Lights On Dusk	From same direction - both going straight - one stopped - rear-end		Stopped in Roadway	East West West East Vehicle Stopped Vehicle Stopped
6604 State Route 6605 State Route	Ciallam Ciallam	101	250.60 No 276696 250.60 No E48054	2014-06-13 07:55 No Apparent Injury 2015-11-09 13:20 Possible Injury	0 0 3 0	D Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Grade Other Straight & Grade Dry Curve & Grade	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic Going Straight Ahead	East West Vehicle Backing Vehicle Stopped East West East West
6002 Solds Route 6003 Solds Route 6004 Solds Route 6005 Solds Route 6005 Solds Route 6005 Solds Route 6006 Solds Route 6007 Solds Route 6007 Solds Route 6007 Solds Route 6008 Solds Route 6009 Solds Route 6010 Solds Route 6011 Solds Route 6013 Solds Route 6014 Solds Route 6015 Solds Route 6016 Solds Route 6016 Solds Route	Cialiam Cialiam Cialiam	101	250.60 No E73113 250.60 No E73515 250.60 No E75555	2018-01-03 17:05 No Apparent Injury 2018-01-03 17:05 No Apparent Injury 2018-11-03 21:00 No Apparent Injury	0 0 1 0	Passenger Car Pickup Panel Trick or Vanette :de- 40 000 "	гилир, ranei truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast		Dark-Street Lights On Dark-No Street Lights	From same direction - both going straight - both moving - rear-end Vehicle Strikes Deer Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing	East West East West
6609 State Route 6610 State Route	Cialiam Cialiam Cialiam	101	250.60 No E85685	2018-11-03 21:00 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related	Raining Clear or Partly Cloudy Raining	Wet Straight in Sag Dry Straight & Grade Wet Curve in Sag	Daylight Dawn	Vehicle Strikes Deer From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Other*	West East West East West East
6611 State Route 6612 State Route	Clallam	101		2017-03-17 12:20 No Apparent Injury 2015-05-14 12:05 Possible Injury	0 0 3 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Passenger Car		Overcast Clear or Partly Cloudy	Dry Straight in Sag Dry Straight & Level	Daylight Daylight	From same direction - both going straight - both moving - sideswipe Entering at anole	Overtaking and Passing Going Straight Ahead	Going Straight Ahead Making Left Turn	East West East West West East South West
6613 State Route 6614 State Route	Clallam Clallam	101	250.64 No E30737 250.64 No E42536	2014-02-12 13:19 No Apparent Injury 2015-05-15 21:35 Possible Injury 2015-06-04 07:51 Possible Injury	2 0 2 0 0 0 2 0 1 0 2 0 1 0 2 0	Passenger Car D Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Grade	Daylight Dark-Street Lights On	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead	Stopped at Signal or Stop Sign	East West Vehicle Stopped Vehicle Stopped East West East West
6615 State Route 6616 State Route	Cialiam Cialiam Cialiam	101	250.64 No E43123 250.64 No E41323	2015-06-04 07:51 Possible Injury 2015-07-06 11:47 Possible Injury	1 0 2 0	Passenger Car D Pickup,Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10.000 lb	At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Grade	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Stopped at Signal or Stop Sign Going Straight Ahead	Going Straight Ahead Going Straight Ahead Stopped at Signal or Stop Sign	East Vehicle Stopped East West East West Vehicle Stopped Vehicle Stopped
6618 State Route	Ciallam Ciallam	101	250.64 No E44345	2015-08-19 14:14 No Apparent Injury 2015-08-19 14:14 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Raining	Dry Straight & Level Dry Straight & Grade Wet Straight & Grade	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Starting in Traffic Lane	Stopped at Signal or Stop Sign Stopped at Signal or Stop Sign	East West Vehicle Stopped Vehicle Stopped East West Vehicle Stopped Vehicle Stopped West Vehicle Stopped Vehicle Stopped West Stopped Vehicle Stopped
6620 State Route 6621 State Poute	Cialiam Cialiam Cialiam	101	Z5U.54 NO E68U59	2017-06-07 13:13 INO Apparent injury	0 0 2 0	Passenger Car Dickup Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy	Dry Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end		Stopped at Signal or Stop Sign Stopped at Signal or Stop Sign	West East Vehicle Stopped Vehicle Stopped East West East Vehicle Stopped Webicle Stopped Vehicle Stopped Webicle Stopped Vehicle Stopped Webicle Stopped Vehicle Stopped
6622 State Route 6623 State Route	Clallam Clallam Clallam	101	250.64 No E93519		0 0 2 0 1 0 4 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb.	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Overcast Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - all others Entering at angle From same direction - both going straight - one stopped - sideswipe	Backing Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped at Signal or Stop Sign Making Left Turn Stopped at Signal or Stop Sign	Vehicle Stopped Vehicle Backing Vehicle Stopped Vehicle Stopped East West North East East West East West
6624 State Route 6625 State Route	Ciallam Ciallam	101	250.64 No E94766	2019-08-06 08:34 No Apparent Injury	0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10.000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Grade Dry Straight & Level	Daylight	From opposite direction - one left turn - one straight	Going Straight Ahead Going Straight Ahead	Making Left Turn Stopped at Signal or Stop Sign Stopped at Signal or Stop Sign	West East East South East West East Vehicle Stopped
6620 State Route 6621 State Route 6621 State Route 6622 State Route 6623 State Route 6623 State Route 6623 State Route 6623 State Route 6624 State Route 6625 State Route 6625 State Route 6626 State Route	Cialiam Cialiam Cialiam	101	250.67 No E94197 250.69 No E33014	2018-07-19 17:01 Possible Injury 1 2019-07-13 16:57 No Apparent Injury 2014-05-21 14:02 Possible Injury	0 0 3 0	D Passenger Car D Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Grade Dry Straight & Level	Daylight	From same direction - both goling straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	East West East Vehicle Stopped East West Vehicle Stopped Vehicle Stopped East West East West
6628 State Route 6629 State Route 6630 State Route 6631 State Route	Cialiam Cialiam Cialiam Cialiam	101					Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb		Raining Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Grade	Dark-Street Lights On	From same direction - both going straight - both moving - rear-end Entering at angle		Slowing Left Turn	West East West East East West North East Court West North East
6631 State Route		101	250.75 No E50925 250.75 No E50925 250.75 No E50927	2015-11-21 13:50 No Apparent Injury 1 2016-01-23 14:31 No Apparent Injury 2016-09-01 16:55 No Apparent Injury 2019-09-13 05:53 Surported Sprious Injury	0 0 2 0					Wet Straight & Level	Daylight	Entering at angle From same direction - both going straight - one stopped - sideswipe	Going Straight Ahead Making Left Turn Going Straight Ahead Going Straight Ahead	Going Straight Ahead Stopped at Signal or Stop Sign Going Straight Ahead	South West East West West East Vehicle Stopped Vehicle Stopped South North West East
6633 State Route 6634 State Route	Ciallam Ciallam Ciallam	101	250.75 No E96371 250.76 No E67298 250.78 No E74142	2017-05-10 12:47 No Apparent Injury 2017-11-30 07:50 No Apparent Injury	0 0 2 0 0 0 2 0	Passenger Car Dickup Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Intersection Related but Not at Intersection Not at Intersection and Not Palated	Clear or Partly Cloudy Raining	Dry Curve & Level Dry Straight & Level	Dark-Street Lights On Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead	Going Straight Ahead Slowing Slowing	South North West East East West East West East West East West
6635 State Route 6636 State Route	Cialiam Cialiam Cialiam	101 101 101	250.78 No E74142 250.80 No E65283 250.80 No E833749	2017-03-18 18:05 No Apparent Injury 2018-09-13 10:00 No Apparent Injury	0 0 2 0 0 0 1 0	Passenger Car D Pickup,Panel Truck or Vanette under 10 000 lb	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level Dry Curve & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - sideswipe Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	West East West East West East
6831 Salat Route 6832 Salat Route 6833 Salat Route 6834 Salat Route 6834 Salat Route 6834 Salat Route 6836 Salat Route 6836 Salat Route 6836 Salat Route 6837 Salat Route 6837 Salat Route 6837 Salat Route 6837 Salat Route 6838 Salat Route 6843 Salat Route 6844 Salat Route 6844 Salat Route	Cialiam Cialiam	101	250.83 No 276897 250.85 No E76897	2014-01-15 08:05 No Apparent Injury 2018-02-13 09:00 Possible Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Driveway	Overcast Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - both moving - rear-end	Changing Lanes Making Right Turn	Going Straight Ahead Going Straight Ahead	West East West East South East West East
6639 State Route 6640 State Route	Clallam Clallam	101	250.86 No E59426 250.88 No E87361	2016-10-08 13:43 No Apparent Injury 2018-12-17 17:32 No Apparent Injury	0 0 2 0 0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Intersection and Related		Wet Straight & Level Wet Straight & Level	Daylight Dark-Street Lights On	From same direction - both going straight - both moving - rear-end Entering at angle	Going Straight Ahead Making Left Turn	Slowing Going Straight Ahead	West East West East North West East West
6641 State Route 6642 State Route	Clallam Clallam Clallam	101	250.89 No E59478 250.90 No E80430	2016-10-02 19:22 Suspected Minor Injury 2018-05-25 17:26 No Apparent Injury	1 0 1 1 0 0 3 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pedup Parel Trutor or Vaneteu under 10,000 B Pedup Parel Trutor or Vaneteu under 10,000 B Passenger Car Passenger Car Passenger Car Pedup Parel Trutor or Vaneteu under 10,000 B	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Dark-Street Lights On Daylight	Vehicle going straight hits pedestrian From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead	Stopped at Signal or Stop Sign	West East Vehicle Stopped Vehicle Stopped
6644 State Route	Clallam	101	250.90 No E89960 250.91 No E74919	2019-03-01 16:35 Possible Injury 2017-12-11 15:29 No Apparent Injury	2 0 2 0 0 0 2 0	Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Starting in Traffic Lane	Stopped at Signal or Stop Sign	West East West East West East Vehicle Stopped Vehicle Stopped
6645 State Route 6646 State Route 6647 State Route 6648 State Route	Clallam Clallam Clallam Clallam	101	250.93 No E71765 250.96 No E31296 250.96 No E31296	2017-06-20 21:01 No Apparent Injury 2014-03-07 16:24 No Apparent Injury 2017-02-16 14:05 No Apparent Injury	0 0 2 0	Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast	Dry Straight & Level Dry Straight & Grade Dry Straight & Level	Daylight	From same direction - both going straight - both moving - sideswipe	Going Straight Ahead	Going Straight Ahead Slowing	west East West East West East West East West East West East
6648 State Route		101	250.96 No E631206	2017-12-22 15:25 No Apparent Injury 2019-09-21 13:25 No Apparent Injury	0 0 3 0	Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Intersection Related but Not at Intersection Not at Intersection and Not Related	Overcast Overcast Overcast	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic Stopped for Traffic	West East West East East West East Webide Stopped W
6650 State Route 6651 State Route	Cialiam Cialiam Cialiam	101	220.96 No E96372 250.97 No E76322 250.97 No E76322 250.98 No E50001	2018-04-03 21:26 No Apparent Injury 2016-01-12 13:30 Possible Injury	0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Intersection Related but Not at Intersection	Overcast Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Dark-Street Lights On Daylight	From same direction -outry going straight - both moving - rear-end From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end	Going Straight Ahead Starting in Traffic Lane Going Straight Ahead	Slowing Stopped for Traffic	West Venice Supplied Venice Supplied Venice Supplied Venice Supplied Venice Supplied Venice Supplied Venice Stopped
6652 State Route 6653 State Route	Clallam	101	250.99 No E20296	2014-01-16 15:44 Possible louny	1 0 4 0	Truck (Flathad Van etc)	Dickup Panel Truck or Vanette under 10 000 lb	Dringway Related but Not at Dringway	Overcast				Going Straight Anead	Slowing Stopped for Traffic	West East West East East East West Vehicle Stopped Vehicle Stopped
0649 Sate Route	Clallam Clallam	101	250.99 No E41417 251.00 No E29869	2015-03-31 13:43 No Apparent Injury 2014-01-04 18:38 No Apparent Injury	0 0 2 0	Passenger Car Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	At Driveway Driveway Related but Not at Driveway	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Level	Daylight Dark-No Street Lights	Entering at angle Form same direction - both going straight - both moving - sideswipe Forn same direction - both going straight - both moving - rear-end	Making Left Turn Changing Lanes Going Straight Ahead	Going Straight Ahead Going Straight Ahead	South West West East
6656 State Route 6657 State Route 6658 State Route	Clallam Clallam	101	251.00 No E47957 251.00 No E58453 251.00 No E58453	2010-11-00 17:25 Possible Injury 2016-09-12 17:48 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	At Driveway	Overcast Clear or Partly Cloudy	Dry Straight & Level Dry Curve & Level	Daylight	From same direction - both going straight - both moving - rear-end From same direction - one right turn - one straight Vehicle Strikes Deer	Going Straight Ahead Overtaking and Passing Going Straight Ahead	Slowing Making Right Turn	East West East West West East West East West East West South
State Route			D1.00 NO E87637		VI VI II VI		+		Raining	au algnt & Level	_aux oweer Lights Un	second would will	Coming Greenglitt PRICED		pages

OBJECTID MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_1_UNIT_1	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE 2_UNIT_1	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_3_UNIT_1 MV_DRIVER AGE GENDE UNIT_1 UNIT_1		MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_2_UNIT_2	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_3_UNIT_2 MV_DRIVER GENDER UNIT_2	BICYCLIST_CONTRIBUTING CIRCUMSTANCE_1_UNIT_1 BICYCLIST_CONTRIBUTING CIRCUMSTANCE_2_UNIT_1	BICYCLIST_CONTRIBUTING CIRCUMSTANCE_3_UNIT_1 BICYCLIST AGE UNIT_1	BICYCLIST BICYCLIST_CONTRIBUTING CROMSTANCE 1 UNIT 2	BICYCLIST_CONTRIBUTING BICYCLIST_CC CIRCUMSTANCE_2_UNIT_2 CIRCUMSTANC	ONTRIBUTING BICYCLIST BICYCLIST PEDEST CE_3_UNIT_2 GENDER_UNIT_2 CIRCUI	RIAN_CONTRIBUTING PEDESTRIAN_CONTRIBUTING	PEDESTRIAN_CONTRIBUTING PEDESTRIAN CIRCUMSTANCE_3_UNIT_2 AGE_UNIT_2	PEDESTRIAN	FIRST_IMPACT_LOCATION City_St_County_Rd & Misc_2010_forward_St
	CIRCUMSTANCE_Z_UNIT_1	CIRCUMSTANCE_3_UNIT_1 AGE GENDEF UNIT_1 UNIT_1 27 Male	None	CIRCUMSTANCE_2_UNIT_2	GROUMSTANCE_3_UNIT_2 AGE_UNIT_2 UNIT_2 23 Female	CIRCUMSTANCE_1_UNIT_1 CIRCUMSTANCE_2_UNIT_1	CIRCUMISTANCE_3_UNIT_1 UNIT_1	UNIT_1 CIRCUMSTANCE_1_UNIT_2	CIRCUMSTANCE_Z_UNIT_Z CIRCUMSTANC	CE_3_UNIT_2 AGE_UNIT_2 GENDER_UNIT_2 CIRCUI	ISTANCE_1_UNIT_2 CIRCUMSTANCE_Z_UNIT_2	CIRCUMSTANCE_3_UNIT_2 AGE_UNIT_2		ane 2 LX Decreasing Mileopst
6479 Follow Too Closely 6480 Follow Too Closely 6481 Direct Not Distracted 6482 Did Not Grant RW to White 6483 Did Not Grant RW to White 6483 Did Not Grant RW to White 6485 Exceding Reas: Safe Speed 6486 None 6487 Instantion	Improper Turn/Merge	18 Male 65 Female	Driver Not Distracted Driver Not Distracted		47 Male 47 Male 22 Male									.eft Turn Lane LX Increasing Milepost (Prior to 2002 Impact Location Code was not lane specific) .eft Turn Lane LX Increasing Milepost (Prior to 2002 Impact Location Code was not lane specific)
6483 Did Not Grant RW to Vehicle		70 Female 73 Female 63 Male	Driver Not Distracted		22 Maie 28 Female 31 Female									.ane 1 LX Decreasing Milepost . ane 1 LX Decreasing Milepost (Prior to 2002 Impact Location Code was not lane specific) . ane 1 LX Decreasing Milepost .
6485 Exceeding Reas. Safe Speed 6486 None		63 Male 55 Male 58 Female	Other Contributing Circ Not Listed											.ane 1 LX Decreasing Milepost Past Right Shoulder LX Decreasing Milepost .ane 1 LX Increasing Milepost (Prior to 2002 Impact Location Code was not lane specific)
6487 Inattention 6488 Inattention 6489 Did Not Grant RW to Vehicle 6490 Did Not Grant RW to Vehicle	Inattention	18 Male 84 Female 61 Female 76 Female	None		20 Waie 34 Maie									ane 1 LX Decreasing Milepost Right Shoulder LX Decreasing Milepost ane 1 LX Decreasing Milepost ane 1 LX Decreasing Milepost
6490 Did Not Grant RW to Vehicle 6491 Inattention	Inattention	21 Female	None		18 Male 40 Male									
6491 hattendom (NV o Vinico) 6492 Under Influence of Alcohol 6493 Did Not Grant RW to Vehicle 6493 None 6494 None 6495 None 6495 Instantion 6496 Instantion 6497 Exceding Reas: Safe Speed 6499 Under Influence of Alcohol 6499 Under Influence of Alcohol		70 Male 20 Female 60 Male 88 Female	None None		71 Male 60 Male									James Lick Rocaldens, Mitopourty and Local Replace Solution Code was not lane specific) and Right Rhoulder LX flucrosasting Milepost (Phorito 2002 Impact Location Code was not lane specific) and 1 LX horeasting Milepost (Phorito 2002 Impact Location Code was not lane specific) and 1 LX horeasting Milepost (Phorito 2002 Impact Location Code was not lane specific) and 1 LX Decreasing Milepost (Phorito 2002 Impact Location Code was not lane specific)
6495 Did Not Grant RW to Vehicle 6496 Inattention	Exceeding Reas. Safe Speed	86 Female 40 Female	None None		94 Male 63 Male									ane 1 LX increasing Milepost (Prior to 2002 Impact Location Code was not lane specific) ane 1 Off Ramp increasing Milepost Side of Mainline assit at 6thoulder Off Ramp increasing Milepost Side of Mainline assit at 6thoulder Off Ramp increasing Milepost Side of Mainline
6497 Exceeding Reas, Safe Speed 6498 Under Influence of Alcohol	Operating Handheld Cell Phone	40 Female 67 Female 31 Male	Ness		50 Family									Past Left Shoulder Off Ramp Increasing Milepost Side of Mainline Past Right Shoulder Off Ramp Increasing Milepost Side of Mainline
osse Uniter Brueffur of Natural 489 Follow Too Classely 6800 Distractions Ostate Vehicle 6800 Distractions Ostate Vehicle 6802 Inatention 6802 None 6804 Follow Too Classely 6806 Exceeding Ress. Sale Speed 6806 Exceeding Ress. Sale Speed	Follow Too Closely Follow Too Closely	50 Maie 651 Maie 69 Fernale 64 Maie	None None		58 Female 65 Female 59 Female									ane 1 Off Ramp Increasing Milepost Side of Mainline ane 2 Off Ramp Increasing Milepost Side of Mainline elf Turn Lanc Off Mamp Increasing Milepost Side of Valuniline ane 2 Off Ramp Increasing Milepost Side of Mainline
6502 None 6503 Inattention			Inattention	Follow Too Closely	59 Female							4		
6504 Follow Too Closely 6505 Inattention 6506 Expeeding Reas, Safe Speed	Follow Too Closely	23 Male 87 Male 87 Male	None None		48 Male 86 Male									ane 2 Off Ramp Increasing Miepost Side of Mainline ane 2 Off Ramp Increasing Miepost Side of Mainline aust Left Shoulder On Ramp Increasing Miepost Side of Mainline
6507 Unknown Distraction 6508 Inattention 6509 Follow Too Closely 6510 Follow Too Closely 6511 Follow Too Closely		23 Maie 42 Main	None None		47 Maie 59 Maie									ane 1 Off Ramp Decreasing Milepost Side of Mainline ane 1 Off Ramp Decreasing Milepost Side of Mainline
6510 Follow Too Closely 6511 Follow Too Closely		33 Female 73 Female 73 Female	None None None		74 Maie 64 Maie 64 Maie									ane 1 Off Ramp Decreasing Miepost Side of Mainline ane 1 Off Ramp Decreasing Miepost Side of Mainline ane 1 Off Ramp Decreasing Miepost Side of Mainline
6512 Instention 6514 Instention 6514 Instention 6513 None 6514 Driver Not Distracted 6515 Exceeding Reas, Safe Speed 6515 Exceeding Reas, Safe Speed 6516 Control of C	Exceeding Reas. Safe Speed				30 Male									ane 1 Off Ramp Decreasing Milepost Side of Mainline.
6514 Driver Not Distracted 6515 Exceeding Reas. Safe Speed		71 Male 33 Male 16 Female	Ness		50 Female									ane 1 Off Ramp Decreasing Mitopost Side of Mainline ane 1 On Ramp Decreasing Mitopost Side of Mainline Past Left Shoulder On Ramp Decreasing Mitopost Side of Mainline
6516 Follow Too Closely 6517 Unknown Distraction 6518 Follow Too Closely 6519 Follow Too Closely	Follow Too Closely	27 Female 33 Male 21 Male	None Other Contributing Circ Not Listed None		40 Female 35 Male									.eft Turn Lane Decreasing Miepost .ane 2 Decreasing Miepost .ane 2 Decreasing Miepost .ane 2 Decreasing Miepost
6519 Follow Too Closely 6520 Driver Not Distracted		63 Male			62 Male									ane 1 Increasing Milepost
6520 Driver Not Distracted 6521 Exceeding Reas. Safe Speed 6522 Exceeding Reas. Safe Speed 6523 Exceeding Reas. Safe Speed	Driver Interacting with Passengers, Anim	65 Female 26 Male 31 Male	None None		31 Female 57 Female									.ane 1 Increasing Miepost .ane 2 Decreasing Miepost .ane 1 increasing Miepost
		55 Female 31 Male	None None		63 Male 56 Male								1 1	Leve 3 Decreasing Milepost ane 1 Decreasing Milepost ane 1 Decreasing Milepost ane 1 Decreasing Milepost Are 1 Decreasing Milepost
6526 None 6527 Follow Too Closely 6528 None		31 Male 83 Female 55 Male 69 Male	None		44 Male									ane 1 Decreasing Milepost
6528 None 6529 Disregard Stop Sign - Flashing Red 6530 None 6531 Exceeding Reas. Safe Speed		59 Male 49 Male 67 Male	None		30 Male								1	ane 2 Decreasing Mispost Past Right Shoulder Decreasing Mispost ane 2 Decreasing Mispost
6531 Exceeding Reas. Safe Speed 6532 Driver Not Distracted		56 Male 52 Female												.ane 2 Decreasing Miepost Hight Shoulder Decreasing Miepost .ane 1 Increasing Miepost
6534 Inattention 6535 None	Did Not Grant RW to Vehicle	16 Male 71 Female 63 Female	Driver Not Distracted		35 Male									ane 2 Increasing Milepost ane 1 Decreasing Milepost ane 1 Increasing Milepost net secting Road Decreasing Milepost
6531 December Not Destaction 6532 Other Not Distraction 6532 Other Not Distraction 6533 December Not Distraction 6533 December Not Distraction 6536 Septimental Not December Not Destaction 6536 Instruction 6537 Oct Not Grant RNW behicle 6538 None 6539 Exceeding Ress. 5546 Speed 6540 Follow Too Closely 6549 Follow Too Closely		74 Female 20 Male	None		27 Female					Inattention		3		
6538 None 6539 Exceeding Reas, Safe Speed 6540 Follow Too Closely		74 Maie 75 Female 68 Female	Driver Not Distracted None		19 Male 23 Female									ane 1 Decreasing Miepost ane 2 Decreasing Miepost ane 2 Decreasing Miepost
6541 Follow Too Closely 6542 Other Contributing Circ Not Listed		41 Female	None		23 Female 28 Male									ane 1 Decreasing Milepost
6543 Exceeding Reas. Safe Speed 6544 Exceeding Reas. Safe Speed	Follow Too Closely	27 Male 44 Male	None None		44 Female 57 Female									ane 2 Increasing Milepost ane 2 Decreasing Milepost
5840 Potovi too Ucizely 5641 Potovi Too Outsely 5642 Other Contributing Clirt Not Listed 5643 Exceloing Ress. Sale Speed 5644 Exceloing Ress. Sale Speed 5646 Exceloing Ress. Sale Speed 5646 Instantion 5647 Exceloing Ress. Sale Speed 5648 Instruction 5647 Exceloing Ress. Sale Speed 5648 Improper Turniberge 5649 Improper Turniberge		40 Female 74 Male 44 Male	None None		51 Female 38 Female 73 Male									.ane 2 Decreasing Miepost .ane 2 Increasing Miepost .ane 2 Decreasing Miepost
6549 Did Not Grant RW to Vehicle	Did Not Grant RW to Vehicle	30 Female 29 Female	None		78 Male 44 Female									ane 2 Decreasing Milepost eft Turn Lane Decreasing Milepost
6549 Did Not Grant RW to Vehicle 6550 Follow Too Closely 6551 Did Not Grant RW to Vehicle 6552 Exceeding Reas. Safe Speed 6553 Inattention 6554 None		65 Male 33 Male 18 Female	None Other Contributing Circ Not Listed		66 Male 68 Male 55 Female									.ane 1 Increasing Milepost .eft Turn Lane Increasing Milepost .ane 2 Increasing Milepost
6553 Inattention 6554 None 6555 Follow Too Closely	Did Not Grant RW to Vehicle	60 Female 30 Male	None Improper Turn/Merge	Other Contributing Circ Not Listed	31 Female Did Not Grant RW to Vehicle									ane 1 Degreasing Mileopst
6556 Expeeding Stated Speed Limit	Exceeding Reas. Safe Speed	29 Male 37 Male	None		26 Female									.ane 2 Increasing Milepost .ane 2 Decreasing Milepost .ans Right Shoulder Increasing Milepost
6557 Exceeding Reas. Safe Speed 6558 Distractions Outside Vehicle 6559 Exceeding Reas. Safe Speed 6560 Follow Too Closely 6561 Follow Too Closely		25 Male 61 Male 77 Female	Improper Turn/Merge None None None None None None		37 Female 44 Female 65 Female									ane 1 Increasing Milepost ane 1 Increasing Milepost ane 1 Increasing Milepost
6560 Follow Too Closely 6561 Follow Too Closely		42 Female	None		28 Female 59 Female									ane 1 Increasing Miepost .ane 2 Increasing Miepost .ane 1 Increasing Miepost
6562 Under Influence of Alcohol 6563 Exceeding Reas. Safe Speed Inattention 6564 Did Not Grant RW to Vehicle	Follow Too Closely	56 Male 46 Male	None None		33 Female 46 Male									ane 2 Increasing Miepost
6565 Did Not Grant RW to Vehicle 6566 Follow Too Closely		50 Female 73 Male	None None		34 Female 19 Female 52 Male								1 1	.ane 2 Increasing Miepost .ane 2 Increasing Miepost .ane 2 Increasing Miepost
6566 Follow Too Closely 6567 Inattention 6568 Other Contributing Circ Not Listed 6569 Apparently III	Exceeding Reas. Safe Speed	73 Indiae 62 Male 71 Female 56 Male	None None		31 Maie 17 Maie 74 Female									ane 2 Increasing Milepost ane 2 Decreasing Milepost ane 1 Decreasing Milepost
6570 Under Influence of Alcohol 6571 Follow Too Closely		llow Too Closely 64 Male 18 Male	None None		74 Female 40 Male 54 Female									ane 1 Increasing Milepost ane 1 Increasing Milepost
6570 Under Influence of Alcohol 6571 Follow Too Closely 6572 Instention 6573 Instention	Other Driver Distractions Inside Vehicle Follow Too Closely	17 Male 60 Female	Driver Not Distracted Driver Not Distracted		51 Female 75 Female									ane 1 Increasing Milepost ane 1 Decreasing Milepost
6574 Disregard Stop and Go Light 6575 Did Not Grant RW to Vehicle		24 Male 23 Female 19 Female	None None		36 Female 68 Male 31 Female									.ane 2 Increasing Miepost ntersecting Road Increasing Miepost .ane 2 Decreasing Miepost
6576 Exceeding Kest. Salts Speed 6577 Operating Defective Equipment 6579 Exceeding Stated Speed Limit 6599 Exceeding Stated Speed Limit 6590 Other Contributing Circ Not Listed 6581 Follow Too Closely 6582 Inattention 6585		41 Male 16 Female	None None		46 Male									anne 1 Decreasing Milepost ane 1 Decreasing Milepost
6579 Exceeding Stated Speed Limit 6580 Other Contributing Circ Not Listed			None		71 Male 69 Male					None		7	l l	ane 1 Decreasing Miepost ane 2 Decreasing Miepost ane 2 Decreasing Miepost
6581 Follow Too Closely 6582 Inattention 6583 None			None Driver Not Distracted Other Contributing Circ Not Listed None	Exceeding Reas. Safe Speed	69 Male 28 Male 41 Male									ane 2 Decreasing Miepost ane 1 Decreasing Miepost ane 1 Increasing Miepost
6592 hattention 6593 Non- 6593 Non- 6594 Disregard Stop Sign - Flashing Red 6595 Disregard Stop and Go Light 6596 Unknown Distraction 6597 Other Contributing Circ Not Listed 6598 Exceeding Stated Speed Limit 6599 Glow Too Closely	Driver Interacting with Passengers, Anim	36 Male 27 Male	None None		41 Male 17 Male 40 Male 49 Female									ane 2 Decreasing Milepost ane 1 Decreasing Milepost
6586 Unknown Distraction 6587 Other Contributing Circ Not Listed	Driver Advention Audio or Entertainment	22 Male 33 Male	None None		/U Male									ane 2 Decreasing Milepost ane 1 Increasing Milepost
	Exceeding Reas. Safe Speed	30 Female 77 Male 19 Male	None None		23 Female 29 Female 37 Male									.ane 2 brerasing Miepost .ane 1 Decreasing Miepost .ane 1 Decreasing Miepost
6591 Follow Too Closely 6592 Under Influence of Alcohol 6593 Under Influence of Alcohol 6594 Exceeding Reas. Safe Speed		17 Male 56 Male	None None Other Contributing Circ Not Listed None		21 Female									ane 1 Decreasing Miepost ane 2 Decreasing Miepost ane 1 Decreasing Miepost ane 1 Increasing Miepost ane 2 Decreasing Miepost
6594 Exceeding Reas. Safe Speed 6595 Improper Turn/Merge	Other Contributing Circ Not Listed	39 Male 16 Female 69 Male	None None		43 Female 33 Male									ane 1 Increasing Miepost ane 2 Decreasing Miepost eft Turn Lane Decreasing Miepost
6596 Other Contributing Circ Not Listed 6597 Other Contributing Circ Not Listed		69 Maie 99 Maie 23 Maie 23 Female 44 Female	None		75 Male									ane 1 Decreasing Milepost Past Right Shoulder Decreasing Milepost
6599 Other Contributing Circ Not Listed	Follow Too Closely Apparently III				58 Maie 21 Female									ane 1 Decreasing Miepost ane 1 Decreasing Miepost
6601 None 6602 None		56 Female 42 Female												Past Right Shoulder Increasing Milepost ane 2 Increasing Milepost ane 2 Decreasing Milepost
6603 Under Influence of Alcohol 6604 Exceeding Reas. Safe Speed	Distractions Outside Vehicle	65 Maie 25 Maie 21 Maie 52 Female	None None		27 Maie 22 Female									ane 2 Increasing Milepost ane 1 Decreasing Milepost
5654. Exceeding Reas. Sails Speed 5856 in Speed Turn-Morage 5856. The Contributing Dir Not Listed 5857. Chec Contributing Dir Not Listed 5857. Chec Consibuting Dir Not Listed 5859. Other Contributing Circ Not Listed 5859. Other Contributing Circ Not Listed 6850. Under Hulmann of Alcohol 6851. None 6850. When Hulmann of Alcohol 6851. Exceeding Reas. Saile Speed 6850. Chec Contributing Circ Not Listed 6850. Chec Chec Chec Chec Chec Chec Chec Chec	Over Center Line Follow Too Closely	52 Female 60 Female	None		22 Female 58 Female 29 Female									ane 1 Decreasing Milepost ane 2 Decreasing Milepost ane 2 Increasing Milepost
6608 None 6609 Inattention	Exceeding Reas. Safe Speed	60 Female 84 Female 18 Male	None		35 Male									ane 2 Increasing Miepost ane 2 Increasing Miepost ane 2 Cereasing Miepost ane 1 Cereasing Miepost ane 2 Increasing Miepost
6611 Improper Passing 6612 Disregard Stop and Go Linbt		18 Male 61 Male Fernale 67 Fernale	None None		35 Male 40 Female 24 Female 17 Female									.ane 2 horeasing Miepost .ane 2 Decreasing Miepost .ane 1 horeasing Miepost
6612 Disregard Stop and Go Light 6613 Instention 6614 Under Influence of Alcohol 6615 Driver Not Distracted	Follow Too Closely	17 Male 48 Male	None None Driver Interacting with Passengers, Anim		17 Female 50 Male 23 Male 66 Female									ane 1 Rensaing Milepost ane 2 Decreasing Milepost ane 2 Decreasing Milepost ane 2 Decreasing Milepost Decreasing Milepost Decreasing Milepost Decreasing Milepost
6615 Driver Not Distracted 6616 Follow Too Closely	Enline Top Clore-t-	47 Female 17 Female	Driver Interacting with Passengers, Anim None		66 Female 40 Female									ane z Decreasing Miepost
9819 - Driver Work Dissourch State 6617 - Palor Too Clissely 6617 - Inatiention 6618 - Inder Influence of Alcohol 6619 - Inder Influence of Alcohol 6619 - Palor Title Crossely 6620 - Follow Too Clissely 6620 - Follow Too Clissely 6620 - Solew Too Clissely 6621 - Instendion 6620 - Dissourch Too Clissely 6621 - Other Contributing Clie Not Listed 6623 - Other Contributing Clie Not Listed 6634 - Streamy Ston and Go Light	Follow Too Closely Exceeding Reas. Safe Speed	17 Female 19 Male 64 Female 61 Female	None None		40 Female 61 Male 33 Female 48 Female									.ane 2 Decreasing Mlepost .ane 2 Decreasing Mlepost .ane 2 Decreasing Mlepost
6620 Follow Too Closely 6621 Inattention	Improper Backing				52 Male 57 Male 57 Male 66 Male 47 Female									ane 2 Decreasing Milepost
6622 Disregard Stop and Go Light 6623 Other Contributing Circ Not Listed 6624 Disregard Stop and Go Light		75 Male 80 Female 52 Male 44 Male	INOTIC		66 Male 47 Female									ane 2 knreasing Miepost ane 1 Decreasing Miepost ane 1 Decreasing Miepost ane 1 Decreasing Miepost ane 2 Decreasing Miepost
6625 Inattention 6626 Operating Handheld Cell Phone	Follow Too Closely Did Not Grant RW to Vehicle	44 Maie 28 Female 26 Female	None None Driver Not Distracted None		83 Male 47 Female 39 Female 70 Male									ane 2 Increasing Milepost ane 2 Decreasing Milepost ane 1 Decreasing Milepost ane 2 Decreasing Milepost
6622 Direct Confrouncy Life Not Listed 6624 Direct Confrouncy Life Not Listed 6625 Instended Sop and Go Light 6626 Special plandhold Cell Phone 6627 Follow Too Closely 6629 Follow Too Closely 6629 None 6630 Did Not Grant RW to Vehicle 6631 Over Center Line 6631 Over Center Line 6632 Did Not Grant RW to Vehicle		26 Female 58 Female	None None Other Contribution Circ N		52 Female									
6630 Did Not Grant RW to Vehicle 6631 Over Center Line		83 Male	None Other Contributing Circ Not Listed None None		64 Male 44 Female									ane 2 Decreasing Miepost ane 2 Decreasing Miepost elt Turn Lane Increasing Miepost
6632 Did Not Grant RW to Vehicle 6633 Inattention	Unknown Distraction Follow Too Closely	34 Male 18 Female	Driver Not Distracted Driver Not Distracted		30 Male 24 Female									ane 2 Increasing Milepost ane 1 Decreasing Milepost
6631 Over Center Line 6632 De Me Committ Was Vehicle 6532 De Me Committ Was Vehicle 6534 Per Service Medical Control 6535 Perpentive Medical Control 6536 Perpentive Medical Control 6537 Perpentive Medical Control 6538 Perpentive Medical Control 6539 Perpentive Medical Control 6539 Perpentive Medical Control 6539 Perpentive Medical Control 6540 Perpentive Medical Control 6541 Perpentive Medical Control 6542 Perpentive Medical Control 6543 Perpentive Medical Control 6543 Perpentive Medical Control 6544 Descriptions Medical Control 6545 Perpentive Medical Control 6545 Perpentive Medical Control 6546 Perpentive Medical Control 6547 Perpentive Medical Control 6548 Perpentive Medical Control 6549 Pe		45 Male 56 Male	None None		32 Male 33 Male									ane 2 Decreasing Miepost ane 2 Increasing Miepost ane 1 Increasing Miepost
6637 Apparently III 6638 Did Not Grant RW to Vehicle			None None		52 Male 42 Male									ane 2 Increasing Milepost
6639 Exceeding Reas. Safe Speed 6640 Inattention	Did Not Grant RW to Vehicle	85 Male 20 Female 56 Female	None None		42 Male 60 Female 23 Female					Lader hills	Calculation Vision		Male	ane 1 Increasing Milepost ane 1 Increasing Milepost ane 2 Decreasing Milepost
6642 Under Influence of Alcohol 6643 Inattention	Exceeding Reas. Safe Speed	80 26 Female 62 Male	Driver Not Distracted None Driver Not Distracted		63 Female 34 Female 73 Male 16 Male					Under Influ	ence of Alcohol Failure to Use Xwalk	4		and 1 Increasing Milepost and 2 Increasing Milepost and 1 Increasing Milepost and 1 Increasing Milepost and 2 Increasing Milepost
6644 Inattention 6645 Other Contributing Circ Not Listed		Male	Driver Not Distracted		73 Male 16 Male									ane 1 Increasing Milepost
6649 Other Contributing Circ Not Listed 6646 Other Contributing Circ Not Listed 6646 Follow Too Clostely 6647 Exceeding Ress. Sale Speed 6648 Instention 6649 Exceeding Ress. Sale Speed 6650 Follow Too Clostely 6651 Follow Too Clostely 6652 Exceeding Ress. Sale Speed 6655 Follow Too Clostely 6655 Sale Sale Sale Sale Speed 6655 Sale Sale Sale Sale Sale Sale Sale Sale	Exceeding Reas. Safe Speed	32 Female 75 Male	None Exceeding Reas . Safe Speed None		16 Male 31 Male 68 Male 63 Female								 	ane 2 Increasing Miepost ane 2 Increasing Miepost ane 1 Increasing Miepost
6649 Exceeding Reas. Safe Speed 6650 Follow Too Closely		55 Male 26 Female	None None		63 Female 39 Male 66 Male 61 Female									ane 1 Increasing Milepost
6651 Follow Too Closely 6652 Follow Too Closely		55 Male 25 Female 67 Male 65 Female	None None											ane 1 Decreasing Miepost ane 2 Increasing Miepost ane 1 Increasing Miepost
6653 Exceeding Reas. Safe Speed 6654 Did Not Grant RW to Vehicle 6655 Inattention 6656 Inattention 6657 Other Contributing Circ Not Listed 6658 None	Did Not Grant RW to Vehicle	53 Male	None None		47 Female 31 Female									ane 1 Decreasing Miepost ane 1 Increasing Miepost ane 2 Decreasing Miepost
6656 Inattention 6657 Other Contributing Circ Not Listed	Did Not Grant RW to Vehicle Exceeding Reas. Safe Speed	19 Male Male	None None		51 Female 62 Female 45 Female									.ane 2 Decreasing Milepost .ane 1 Increasing Milepost .ane 1 Increasing Milepost .ane 1 Increasing Milepost .ane 1 Increasing Milepost
6658 None		Male 26 Female	1	1										ane 1 Increasing Milepost

OBJECTID WA. STATE. PLANE SOUTH. Y 2010_FORWARD COMPARD COMPARED C		COL COL_HUM COI SEVERITY FACTOR AG	CONDITION	COL_LIGHT CONDITIONS	COL LOCATION (COL STUDY GEOMETRY AREA	COL COL TYPE SEVERITY TEXT	COL_HUMAN COL FACTOR_Decoded MVIW	COL_LIGHT COL_SUR CONDITIONS CONDITI Decoded Decode		COL_RISKY In BEHAVIOR A	Intersection Emphasis Ro Area_Name Area De
6480 996172.17 1010215.63 2013 6481 996180.42 1010135.92 2011	18 3 Vehicles/Parties Rear-End 17 2 Vehicles/Parties Rear-End 5 2 Vehicles/Parties Sideswipe 6 2 Vehicles/Parties Turning	Property Damage Only Follow Too Closely 25-34 Minor or Possible Injury Follow Too Closely Under Property Damage Only 65+ 65+ Property Damage Only Fail to Yield 65+	No Dry	Dawn/Dusk Daylight Dark-Street Lights On Dawn/Dusk	At Driveway or Intersection Straig At Driveway or Intersection Unkn		o Vulnerable Road User Involved FC o Vulnerable Road User Involved FC Ider Road User 65+ Ider Road User 65+ FF	follow Too Closely follow Too Closely bler/Unknown fail to Yield	Dawn/Dusk Dry Daylight Dry Dark-Street Lights On Dry Dawn/Dusk Dry	Intersection Related but Not at Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Follow Too Closely Follow Too Closely No Risky Driving Behaviors Involved Fail to Yield	No No Yes
6483 996169.78 1010131.53 2013	77 2 Vehicles/Paries Veh	Property Damage Only	No Dry No Dry No Wet No Loc/Snow/Stute	Daylight Dark-Street Lights On h Daylight	At Driveway or Intersection Unkn	nown Oil ght & Level No ght & Grade No	Ider Road User 65+ 0 Vulnerable Road User Involved Fa 0 Vulnerable Road User Involved Sp	ail to Yield	Dawn/Dusk Dry Daylight Dry Dark-Street Lights On Wet Daylight Ice/Snow/	At Driveway or Intersection	No Risky Driving Behaviors Involved Fail to Yield	No No Yes
6487 1004133.73 1009977.53 2011	18 2 Vehicles/Parties Turning	Minor or Possible Injury Inattention Under Minor or Possible Injury Inattention 65+	No Dry 25 No Dry	Daylight Daylight Daylight	At Driveway or Intersection Straig At Driveway or Intersection Straig Intersection Related but Not at Intersection Straig	ght & Level No obt at Hillcrest No	o Vulnerable Road User Involved No o Vulnerable Road User Involved Inc	lone nattention nattention	Daylight Dry Daylight Dry	At Driveway or Intersection At Driveway or Intersection	Speeding No Risky Driving Behaviors Involved Inattention Inattention	No No Yes
6491 1004118.45 1009569.37 201	17 1 Vehide,Party Struck Object 15 2 Vehide,Parties Angle 15 2 Vehide,Parties Angle 17 2 Vehides,Parties Angle 18 19 19 19 19 19 19 19	Property Damage Only Fail to Yield 55-64 Property Damage Only Fail to Yield 65+	No Wet No Wet No Dry 25 No Dry	Daylight Daylight Daylight Dark-Street Lights On	At Driveway or Intersection Straig At Driveway or Intersection Straig At Driveway or Intersection Straig		lotorcycle In:	all to Yield ail to Yield nattention	Daylight Wet Daylight Dry Daylight Dry	At Driveway or Intersection	Fail to Yield Fail to Yield Inattention	No No
6491 1004118.45 1009569.37 [201] 6492 1004131.61 1009570.49 2011 6493 1004118.45 1009569.37 [201] 6494 1012179.72 1008130.22 [201] 6495 1012189.8 1008141.75 [201]	1 Vehicle/Party Struck Object	Property Damage Only Other/Unknown 55-64	No Wet No Dry	Daylight Daylight	At Driveway or Intersection Straig	ght & Grade Oil ght & Grade No ght & Grade No ght & Grade No	o Vulnerable Road User Involved Fa o Vulnerable Road User Involved No	ail to Yield Ione	Dark-Street Lights On Wet Daylight Wet Daylight Daylight Dry Daylight Wet	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Under Influence of Alcohol/Drugs/Medication Fail to Yield No Risky Driving Behaviors Involved	Yes No No
6496 995169.2 1010510.83 2016 6497 995494.67 1010389.46 2013 6498 995735.83 1010273.35 2016	14 2 Vehicles:Parties Sideswipe 16 2 Vehicles:Parties Rear-End 7 1 Vehicle:Party Struck Object 14 1 Vehicle:Party Struck Object 14 1 Vehicle:Party Struck Object 15 16 17 17 17 17 17 17 17	Property Damage Only Fail to Yield 65+ Property Damage Only Inattention 35-44 Suspected Serious Injury Speeding 65+ Property Damage Only Under Influence of Alcohol/Drugs./Medication 25-34		Daylight Daylight Daylight Dark-Street Lights On	Not at Intersection/Driveway/Roundabout Straig Not at Intersection/Driveway/Roundabout Curve	ght & Grade No e & Grade Oi	o Vulnerable Road User Involved In		Daylight Wet Daylight Dry Dark-Street Lights On Wet	At Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Fail to Yield Inattention Speeding Under Influence of Alcohol/Drugs/Medication	No Yes Yes
6499 995829.22 1010240.26 2019 6500 995968.52 1010173.05 2010 6501 996066.47 1010146.89 2013	9 2 Vehicles/Parties Rear-End	Property Damage Only Follow Too Closely 45:54 Mnor or Possible Injury Distracted Driving 65+ Property Damage Only Inattention 65+	No Dry No Dry	Daylight Daylight Daylight	Intersection Related but Not at Intersection Straig Not at Intersection/Driveway/Roundabout Straig Intersection Related but Not at Intersection Straig	ght & Level No ght & Grade Oil	o Vulnerable Road User Involved	follow Too Closely Distractions Outside Vehicle nattention	Daylight Dry Daylight Dry Daylight Dry	Intersection Related but Not at Intersection Not at Intersection/Driveway/Roundabout Intersection Related but Not at Intersection	Follow Too Closely Distracted Driving Inattention	No No No
6502 996135.84 1010133.07 2011 6503 996166.67 1010128.59 2011	18 2 Vehicles/Parties Rear-End 17 2 Vehicles/Parties Pedestrian 14 2 Vehicles/Parties Turning 17 2 Vehicles/Parties Rear-End	Property Damage Only Oher/Unknown 55-64 Minor or Possible Injury Inattention 65+ Property Damage Only Follow Too Closely Under	No Dry No Dry 25 No Dry	Daylight Dark-Street Lights On Daylight	Intersection Related but Not at Intersection Straig At Driveway or Intersection Straig At Driveway or Intersection Straig	ght & Level No oht & Grade Pe	o Vulnerable Road User Involved Ni edestrian and Older Road User 65+ In o Vulnerable Road User Involved Fc Ider Road User 65+ In	lone nattention	Daylight Dry Dark-Street Lights On Dry Daylight Dry	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	No Risky Driving Behaviors Involved Inattention Follow Too Closely	No No No
6506 1012212.07 1007506.21 2013 6507 1004168.26 1009975.34 2011	17 1 Vehicle/Party Struck Object 18 2 Vehicles/Parties Rear-End	Property Damage Only Inattention 65+ Property Damage Only Speeding 65+ Property Damage Only Distracted Driving Under	No Dry No Dry 25 No Dry	Daylight Dark-Street Lights On Daylight	Intersection Related but Not at Intersection Curve At Driveway or Intersection Curve	e & Grade Oi e & Grade No	ider Road User 65+ Sp o Vulnerable Road User Involved User	nattention Speeding Unknown Distraction	Daylight Dry Dark-Street Lights On Dry Daylight Dry	At Driveway or Intersection Intersection Related but Not at Intersection At Driveway or Intersection	Inattention Speeding Distracted Driving	No Yes No
6508 1004135.32 1009977.48 201: 6509 1004135.48 1009979.91 201: 6510 1004135.52 1009980.45 201: 6511 1004141.12 1009977.01 201:	17 2 Vehicles/Parties Rear-End 6 2 Vehicles/Parties Rear-End 6 2 Vehicles/Parties Rear-End 6 2 Vehicles/Parties Rear-End 6 2 Vehicles/Parties Rear-End	Property Damage Only Inattention 35-44 Property Damage Chly Follow Too Closely 25-34 Property Damage Chly Follow Too Closely 65+ Property Damage Only Follow Too Closely 65+	No Dry	Daylight Daylight Daylight Daylight	At Driveway or Intersection Straig At Driveway or Intersection Straig	ght & Grade No ght & Grade Oil	o Vuinerable Road User Involved In: o Vuinerable Road User Involved F-c Ider Road User 65+ F-c Ider Road User 65+ F-c	follow Too Closely	Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry	At Driveway or Intersection	Inattention Follow Too Closely Follow Too Closely Follow Too Closely	No No No
6512 1004135.32 1009977.48 2011	2 Vehicles 2 V	Property Damage Only Other/Unknown 95-34 Property Damage Only Other/Unknown 95-34	25 No Dry	Daylight Daylight Daylight Dark-No Street Lights/Street Lights Off Dark-Street Lights On	At Driveway or Intersection Straig	ght & Level No		nattention	Daylight Dry Daylight Wet Dark-No Street Lights/Street Lights Off Dry Dark-Street Lights On Other/Unix	At Universary or intersection	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No No
6516 932669.51 1024251.33 2011	18 2 Vehicles/Parties Rear-End	Minor or Possible Injury Follow Too Closely 25-34	No Dry	Daylight	Not at Intersection/Driveway/Roundabout Straig	ght & Level No	o Vulnerable Road User Involved Fo	ollow Too Closely		nown Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Speeding Follow Too Closely Distracted Driving	Yes No No
6516 932669.51 1024251.33.201 6517 932691.63 1024234.54.201 6518 932685.99 1024244.85.201 6519 932741.16 1024231.44.201 6520 932790.68 102420.34.201	15 2 Vehicles/Parties Rear-End 17 2 Vehicles/Parties Rear-End 16 2 Vehicles/Parties Rear-End 16 1 Vehicle/Party Animal	Property Damage Only Follow Too Closely 25-34 Property Damage Only Other/Unknown 55-64	NO DITY	Daylight Dark-Street Lights On Daylight Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Straig Not at Intersection/Driveway/Roundabout Straig Not at Intersection/Driveway/Roundabout Straig	ght & Level No ght & Grade No ght & Grade No	o Vulnerable Road User Involved Fo o Vulnerable Road User Involved Fo	ollow Too Closely Other/Unknown	Dark-No Street Lights/Street Lights Off Dry	Intersection Related but Not at Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Follow Too Closely Follow Too Closely No Risky Driving Behaviors Involved	No No No
6521 932840.28 1024208.72 2019 6522 933188.55 1024127.54 2019 6523 933187.02 1024127.52 2019	15 1 Vehicle/Party Animal	Unknown Speeding 55+ Property Damage Only Speeding 25-34 Minor or Possible Injury Speeding 25-34	No Dry No V/tet No Ice/Snow/Slus	Daylight Daylight h Daylight	Not at Intersection/Driveway/Roundabout Straig Not at Intersection/Driveway/Roundabout Straig Not at Intersection/Driveway/Roundabout Straig	ght & Level Oil ght in Sag No ght & Grade No	Ider Road User 65+ Sp O Vulnerable Road User Involved Sp O Vulnerable Road User Involved Sp	peeding peeding	Daylight Dry Daylight Wet Daylight Ice/Snow/	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Slush Not at Intersection/Driveway/Roundabout	Speeding Speeding Speeding	No No Yes
6525 933434.86 1024069.48 201- 6526 933434.84 1024069.48 2011	6 3 Whicles/Parties Rear-End 4 2 Whicles/Parties Rear-End 1 Vehicles/Party Animal	Property Damage Only Speeding 55-64 Minor or Possible Injury Follow Too Closely 25-34 Property Damage Only Other/Unknown 65+	No Dry No Wet	Daylight Daylight	Not at Intersection/Driveway/Roundabout Straig Not at Intersection/Driveway/Roundabout Straig	ght & Grade No ght & Grade Oil	o Vulnerable Road User Involved Fo Ider Road User 65+ No	ollow Too Closely Ione	Daylight Dry Daylight Dry Daylight Wet	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Speeding Follow Too Closely No Risky Driving Behaviors Involved	No No No
6528 933533.96 1024046.27 2011	4 2 Vehicles/Parties Rear-End	Property Damage Only Follow Too Closely 55-64 Property Damage Only Other/Unknown 55-64 Property Damage Only Disregard Traffic Control Device/Sign 45-54 Property Damage Only Other/Unknown 65-	No Dry No Dry	Daylight Dawn/Dusk Daylight Daylight	At Intersection and Not Related Straig At Driveway or Intersection Straig	ght & Grade No	o Vulnerable Road User Involved No	lone	Daylight Dry Dawn/Dusk Dry Daylight Dry Daylight Dry	At Driveway or Intersection At Intersection and Not Related At Driveway or Intersection At Intersection and Not Related	Follow Too Closely No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No No
6531 933541.29 1024064.19 2019 6532 933533.96 1024046.27 2019 6533 933533.96 1024046.27 2019	15 1 Vehicle/Party Struck Object 5 1 Vehicle/Party Animal 6 1 Vehicle/Party Animal	Unknown Speeding 55-64 Minor or Possible Injury Other/Unknown 45-54 Property Damage Only Other/Unknown Under	No Dry No Wet 25 No Dry	Daylight Daylight Daylight	At Driveway or Intersection Straig At Intersection and Not Related Straig	ght & Grade No	o Vuinerable Road User Involved Sp o Vuinerable Road User Involved Of	Other/Unknown	Daylight Dry Daylight Wet Daylight Up Dayl	At Driveway or Intersection At Intersection and Not Related At Intersection and Not Related	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	Yes No No
6533 933533.96 1024046.27 2011 6534 933527.35 1024049.22 2011 6535 93353.96 1024046.27 2014 6536 93353.96 1024046.27 2014	16 2 Vehicles/Parties Rear-End 16 1 Vehicles/Party Animal 16 2 Vehicles/Parties Pedestrian	Minor or Possible Injury Inattention 65+ Property Damage Only Other/Unknown 55-64 Minor or Possible Injury Inattention 65+	No Dry No Dry No Dry No Dry	Daylight Daylight Daylight	At Driveway or Intersection Straig At Intersection and Not Related Straig	ght & Grade Oil ght & Grade No	Ider Road User 65+	attention one sattention	Daylight Dry Daylight Dry Daylight Dry Daylight Dry	At Driveway or Intersection At Intersection and Not Related At Driveway or Intersection	Inattention No Risky Driving Behaviors Involved Inattention	No No No
6537 933533 96 1024046 27 201	17 2 Vehicles/Parties Angle 4 1 Vehicle/Party Struck Object or Debris On Road 17 2 Vehicles/Parties Rac Find 14 3 Vehicles/Parties Rear-End	Property Damage Only Fail to Yield Under Property Damage Only Other/Unknown 65+ Property Damage Only Speeding 65+ Property Damage Only Follow Too Closely 65+	No Dry No Dry	Dawn/Dusk Dark-Street Lights On Daylight	At Driveway or Intersection Straig Not at Intersection/Driveway/Roundabout Straig Not at Intersection/Driveway/Roundabout Straig		o Vulnerable Road User Involved F3 Ider Road User 65+ Ni Ider Road User 65+ Sp	ail to Yield lone speeding	Dawn/Dusk Wet Dark-Street Lights On Dry Daylight Dry	At Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Fail to Yield No Risky Driving Behaviors Involved Speeding	No No No
6541 933954.23 1023955.46 2016	16 2 Vehicles/Parties Rear-End	Property Damage Only Follow Too Closely 35-44		Daylight Daylight Daylight	Not at Intersection/Driveway/Roundabout Straig Not at Intersection/Driveway/Roundabout Straig Not at Intersection/Driveway/Roundabout Straig	ght & Level No	Ider Road User 65+	ollow Too Closely	Daylight Dry	Not at Intersection/Driveway/Roundabout	Follow Too Closely Follow Too Closely No Risky Driving Behaviors Involved	No No Yes
6543 934184.56 1023905.76 2019 6544 934216.82 1023898.73 2019 6555 934216.82 1023898.73 2019 6566 934216.82 1023898.73 2019	18 1 VehicleParty Struck Object	Unknown Other/Unknown Unknown Minor or Possible Injury Speeding 25-34 Minor or Possible Injury Speeding 35-44 Property Damage Only Speeding 35-44 Demonstry Damage Only Speeding 35-44	No Dry No Wet No Wet	Daylight Daylight Daylight Daylight	Not at Intersection/Driveway/Roundabout Straig	ght & Grade No ght & Grade No	o Vulnerable Road User Involved Sp o Vulnerable Road User Involved Sp	speeding	Daylight Wet	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Intersection/Driveway/Roundabout Intersection Polisted but Not at Intersection	Speeding Speeding Speeding	No No
6548 934455.65 1023844.6 2011	9 3 Vehicles/Parties Rear-End 16 2 Vehicles/Parties Rear-End 15 2 Vehicles/Parties Angle 16 2 Vehicles/Parties Angle 16 2 Vehicles/Parties Angle	Property Damage Only Inattention 65+ Property Damage Only Speeding 35-44 Minor or Possible Injury Improper Turn/Merge 25-34 Property Damage Only Fail to Yield 25-34		Daylight	At Driveway or Intersection Straig	ght & Grade Oil ght & Grade No ght & Level No ght & Level No	o Vulnerable Road User Involved Sp o Vulnerable Road User Involved Im	nproper Lurn/werge	Daylight Dry Daylight Wet Daylight Dry Daylight Dry Daylight Dry Daylight Dry	Intersection Related but Not at Intersection Not at Intersection/Diveway/Roundabout At Driveway or Intersection At Driveway or Intersection	Inamention Speeding No Risky Driving Behaviors Involved Fail to Yield	No No No
6549 934479.51 1023841.97 201 6550 934472.51 102382.22 201 6551 934500.94 102383.37 201 6552 934531.94 1023830.7 201	16 3 Vehicles/Parties Rear-End 16 2 Vehicles/Parties Angle 17 3 Vehicles/Parties Angle 18 3 Vehicles/Parties Rear-End 19 19 19 19 19 19 19	Property Damage Only	No Dry	Daylight Daylight Daylight Daylight	At Intersection and Not Related Straig At Driveway or Intersection Straig	ght & Grade Oi ght & Level No	ider Road User 65+ Fc o Vulnerable Road User Involved Fz	all to Yield	Daylight Dry Daylight Dry Daylight Wet Daylight Dry	At Intersection and Not Related At Driveway or Intersection Intersection Related but Not at Intersection	Fail to Yield Follow Too Closely Fail to Yield Speeding	No No No
6553 934531.94 1023830.69 2019 6554 934548.68 1023828.37 2019 6555 934591.9 1023816.96 2010	5	Property Damage Only Inattention 55-64 Property Damage Only Other/Unknown 25-34 Property Damage Only Follow Too Closely 25-34	No Dry No Dry	Daylight Daylight Dark-Street Lights On	At Driveway or Intersection Straig At Driveway or Intersection Straig Driveway Related but Not at Driveway Straig	ght at Hillcrest No ght & Level No ght & Level No	o Vuinerable Road User Involved In o Vuinerable Road User Involved Ni o Vuinerable Road User Involved Fc	nattention	Daylight Dry Daylight Dry Dark-Street Lights On Dry	At Driveway or Intersection At Driveway or Intersection Driveway Related but Not at Driveway	Inattention No Risky Driving Behaviors Involved Follow Too Closely	No No No
6556 934636.69 1023808.17 2010 6557 934625.76 1023785.11 2010 6558 934636.71 1023808.16 2010	18 1 Vehicle/Party Struck Object 16 2 Vehicles/Parties Rear-End 19 3 Vehicles/Parties Rear-End	Property Damage Only Speeding 35-44 Property Damage Only Speeding 25-34 Property Damage Only Distracted Driving 55-64	No Dry	Dark-Street Lights On Daylight Daylight	Not at Intersection/Driveway/Roundabout Straig Intersection Related but Not at Intersection Straig	ght at Hillcrest No ght & Level No ght & Grade No	o Vulnerable Road User Involved Sp o Vulnerable Road User Involved Sp o Vulnerable Road User Involved Di	Speeding Distractions Outside Vehicle	Dark-Street Lights On Dry Daylight Dry Daylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Intersection Related but Not at Intersection	Speeding Speeding Distracted Driving	Yes No No
6560 934741.52 1023785.63 2011 6561 934762.43 1023783.9 2011	18 2 Vehicles/Parties Rear-End 18 2 Vehicles/Parties Rear-End	Property Damage Only Speeding 65+ Property Damage Only Follow Too Closely 25-34 Property Damage Only Follow Too Closely 35-44	No Dry No Dry No Wet	Daylight Daylight Daylight	Not at Intersection/Driveway/Roundabout Straig Not at Intersection/Driveway/Roundabout Straig Not at Intersection/Driveway/Roundabout Straig	ght & Level Oil ght & Level No ght & Grade No	ider Road User 65+ Sp o Vulnerable Road User Involved Fo o Vulnerable Road User Involved Fo	ollow Too Closely ollow Too Closely	Daylight Dry Daylight Dry Daylight Wet	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Speeding Follow Too Closely Follow Too Closely	No No No
6563 934793.89 1023774.37 2011	9 2 Vehicles/Parties Sideswipe 8 4 Vehicles/Parties Rear-End 9 2 Vehicles/Parties Rear-End	Properly Damage Only Under Influence of Alcohol/Drugs/Medication 55-64 Properly Damage Only Speeding 45-54 Minor or Possible Injury Institution 25-34	No Dry	Daylight Dark-Street Lights On Daylight	Not at Intersection/Driveway/Roundabout Straig Intersection Related but Not at Intersection Straig	ght & Level No ght & Level No ght & Level No	o Vulnerable Road User Involved Sp o Vulnerable Road User Involved In	nattention	Daylight Dry Dark-Street Lights On Dry Daylight Dry	Not at Intersection/Driveway/Roundabout Intersection Related but Not at Intersection	Under Influence of Alcohol/Drugs/Medication Speeding Inattention	Yes No No
6566 934942.33 1023743.81 2011	15 2 Vehides Paries Rear-End	Property Damage Only Fail to Yield 45-54	No	Daylight Daylight Daylight Dark-Street Lights On	Intersection Related but Not at Intersection Straig	ght & Level No		ollow Too Closely	Daylight Dry Daylight Dry Daylight Dry Dark-Street Lights On Wet Daylight Dry	At Driveway or Intersection Driveway Related but Not at Driveway Intersection Related but Not at Intersection Intersection Related but Not at Intersection	Fall of the Control o	No No No
6570 935108.85 1023703.95 2011	9	Property Damage Only Apparently Asleep, Fatigued or III 55-64 Minor or Possible Injury Under Influence of Alcohol/Drugs/Medication 55-64 Property Damage Only Follow Too Closely Under	Apparently Asleep, Fatigued or III 55-64 Dry Under Influence of Alcohol 55-64 Dry ES No Dry	Daylight Daylight Daylight	At Driveway or Intersection Straig	ght & Level No	o Vulnerable Road User Involved Di o Vulnerable Road User Involved Us	Onver Physical Condition Under Influence of Alcohol/Drugs/Medication	Daylight Dry	At Driveway or Intersection	Apparently Asleep, Fatigued or III Under Influence of Alcohol/Drugs/Medication Follow Too Closely	No No No
6572 935160.74 1023695.5 201- 6573 935160.74 1023695.5 201-	14 2 Vehicles/Parties Rear-End	Minor or Possible Injury Inattention Under Property Damage Only Inattention 55-64 Property Damage Only Disregard Traffic Control Device/Sign Under	25 No Dry No Dry	Daylight Daylight Dark-Street Lights On	At Driveway or Intersection Straig At Driveway or Intersection Straig At Driveway or Intersection Straig	ght & Level No ght & Grade No ght & Level No	o Vulnerable Road User Involved In: o Vulnerable Road User Involved In:	nattention	Daylight Dry Daylight Dry	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Inattention Inattention No Risky Driving Behaviors Involved	No No No
6575 935160.74 1023695.5 2011 6576 935160.74 1023695.5 2011 6577 935160.74 1023695.5 2011	15. 2 Vehicles/Parties Sideswipe 15. 2 Vehicles/Parties Sideswipe 15. 2 Vehicles/Parties Rear-End	Property Damage Only Fail to Yield Under Mnor or Possible Injury Speeding Under Property Damage Only Operating Defective Equipment 35-44	25 No Dry 25 No Wet No Wet	Dark-No Street Lights/Street Lights Off Daylight Dark-Street Lights On	At Driveway or Intersection Straig At Driveway or Intersection Straig	ght & Level No ght & Level No	o Vuinerable Road User Involved Fa o Vuinerable Road User Involved Sp	ail to Yield	Dark-No Street Lights/Street Lights Off Dry Daylight Wet Dark-Street Lights On Wet	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Fall to Yield Speeding No Risky Driving Behaviors Involved	Yes Yes No
6579 935160.74 1023695.5 2016 6580 935158.38 1023697.9 2013	6 2 Vehicles/Paries Turning 6 3 Vehicles/Paries Sideswipe 17 2 Vehicles/Paries Pedestrian	Property Damage Only Follow Too Closely Under Property Damage Only Speeding 35-44 Minor or Possible Injury Other/Unknown 25-34	No Wet No Wet	Daylight Daylight Daylight	At Driveway or Intersection Straig At Driveway or Intersection Straig	ght & Level No ght & Level Pe		ollow Too Closely Speeding		At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Follow Too Closely Speeding No Risky Driving Behaviors Involved Follow Too Closely	No Yes No
6582 935160.74 1023695.5 2011	17 2 Vehicles/Parties Rear-End 8 2 Vehicles/Parties Rear-End 8 2 Vehicles/Parties Rear-End 9 2 Vehicles/Parties Angle 9 2 Vehicles/Parties Angle	Property Damage Only Follow Too Closely 25-34 Property Damage Only Inattention 35-44 Minor or Possible Injury Other/Unknown 35-44	No Dry No Dry No Wet	Daylight Daylight Daylight Daylight Dark-Street Lights On	At Driveway or Intersection Straig At Driveway or Intersection Straig	ght & Level No	o Vulnerable Road User Involved In- o Vulnerable Road User Involved No	nattention Jone	Daylight Dry Daylight Wet	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Inattention	No No No
6585 935160.74 1023695.48 2019 6586 935164.18 1023697.56 2019	9	Property Damage Only Disregard Traffic Control Device/Sign 35-44 Property Damage Only Disregard Traffic Control Device/Sign 25-34 Property Damage Only Distracted Driving Under Property Damage Only Ofter/Unknown 25-34		Dark-Street Lights On Dawn/Dusk Daylight	At Driveway or Intersection Straig At Driveway or Intersection Straig	ght & Level No ght & Level No	o Vulnerable Road User Involved Ur	Disregard Traffic Control Device/Sign Unknown Distraction	Dark-Street Lights On Dry Dawn/Dusk Dry	At Driveway or Intersection At Driveway or Intersection	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Distracted Driving No Risky Driving Behaviors Involved	No No No
6588 935350.07 1023654.21 2019 6589 935360.18 1023652.01 2019 6590 935420.39 1023639.39 2019	15 2 Vehicles/Parties Rear-End	Property Damage Only Speeding 25-34 Minor or Possible Injury Follow Too Closely 65+ Property Damage Only Inattention Under	No Wet No Wet	Daylight Dark-Street Lights On Daylight	Not at Intersection/Driveway/Roundabout Straig Intersection Related but Not at Intersection Straig Intersection Related but Not at Intersection Straig	ght & Grade No ght & Level Oil ght & Grade No		Speeding follow Too Closely	Daylight Wet Dark-Street Lights On Wet Daylight Dry	Not at Intersection/Driveway/Roundabout Intersection Related but Not at Intersection Intersection Related but Not at Intersection	Speeding Follow Too Closely Inattention	No No No
6591 935460.51 1023630.01 201- 6592 935465.27 1023625.49 201- 6593 935560.65 1023609.87 2019- 6594 935562.64 1023608.52 2011-	4 2 Vehicles/Parties Rear-End	Property Damage Only Follow Top Closely Under	25 No Dry	Daylight Daylight Dark-Street Lights On Daylight	Not at Intersection/Driveway/Roundabout Straig Intersection Related but Not at Intersection Straig	ght & Level Mc	btorcycle Us o Vulnerable Road User Involved Us	ollow Too Closely	Daylight Dry Daylight Dry Dark-Street Lights On Dry	Not at Intersection/Driveway/Roundabout Intersection Related but Not at Intersection Not at Intersection/Driveway/Roundabout	Follow Too Closely Under Influence of Alcohol/Drugs/Medication Under Influence of Alcohol/Drugs/Medication	No No Yes
6595 935620.3 1023598.67 2016	16 2 Vehicles/Parties Angle	Property Damage Only Speeding Under Minor or Possible Injury Improper Turn/Merge 65+ Property Damage Only Oher/Unknown 65+ Property Damage Only Oher/Unknown Under	15 No Wet No Dry No Dry	Daylight Dark-Street Lights On	At Driveway or Intersection Straig Not at Intersection/Driveway/Roundabout Curve	e & Grade Ol	Ider Road User 65+ Im Ider Road User 65+ Or	peeding nproper Turn/Merge Other Contributing Circ Not Listed	Daylight Wet Daylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout At Driveway or Intersection Not at Intersection/Driveway/Roundabout	Speeding No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No
6599 935761.24 1023580.81 2011	2 Vehides Paris Rear-End 1 Vehides Paris Struck Object 15 2 Vehides Paris Rear-End 19 2 Vehides Paris Rear-End 19 2 Vehides Paris Rear-End 10 Vehides Paris Rear-End 10 Vehides Paris Rear-End 11 Vehides Paris Rear-End 12 Vehides Paris Rear-End 13 Vehides Paris Rear-End 14 Vehides Paris Rear-End 15 Vehides Paris Rear-End 16 Vehides Paris Rear-End 17 Vehides Paris Rear-End 18 Vehides Paris Rear-End 18 Vehides Paris Paris Paris 18 Vehides Paris Paris Paris 18 Vehides Paris Paris 18 Vehides Paris Paris 18 Vehides	Property Damage Only Inamention 25-34 Property Damage Only Other/Unknown 45-54	No Dry No Dry	Daylight Daylight Daylight Dark-Street Lights On	Intersection Related but Not at Intersection Curve Not at Intersection/Driveway/Roundabout Strain	ght & Level No	o Vulnerable Road User Involved In: o Vulnerable Road User Involved Or o Vulnerable Road User Involved III	nattention Other Contributing Circ Not Listed	Daylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Intersection Related but Not at Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Inattention No Risky Driving Behaviors Involved Linder Influence of Micobol/Drugs/Medication	Yes No Yes
6603 936017.94 1023562.73 2019	15 1 Vehicle/Party Struck Object	Property Damage Only Under Influence of Alcohol/Drugs/Medication 55-64 Property Damage Only Other/Unknown 55-64 Property Damage Only Other/Unknown 35-44 Suspected Senious Injury Under Influence of Alcohol/Drugs/Medication 65-	No Dry No Dry	Dark-Street Lights On Dark-No Street Lights/Street Lights Off Dark-Street Lights On Dawn/Dusk		ght & Level No e & Grade No	o Vulnerable Road User Involved Ni o Vulnerable Road User Involved Ni	lone lone	Dark-Street Lights On Dry Dark-No Street Lights Off Dry Dark-Street Lights On Dry Dark-Street Lights On Dry Dawn/Dusk Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication	No No No
6604 936273.1 1023570.79 201- 6605 936275.24 1023573.8 2011 6606 936271.77 1023570.69 2011	14 3 Vehicles/Parties Rear-End 15 2 Vehicles/Parties Rear-End 17 2 Vehicles/Parties Rear-End	Property Damage Only Speeding 25:34 Minor or Possible Injury Other/Unknown Under Property Damage Only Inattention 45:54	No Wet 25 No Other	Daylight Daylight Daylight	Not at Intersection/Driveway/Roundabout Straig Not at Intersection/Driveway/Roundabout Straig	ght & Grade No ght & Grade No e & Grade No		Speeding Other Contributing Circ Not Listed	Daylight Wet Daylight Other/Unix Daylight Dhy	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout nown Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved	No No No
6607 936271.77 1023570.69 2010 6608 936273.61 1023569.18 2010	18 1 Vehicle/Party Animal 18 1 Vehicle/Party Animal	Property Damage Only Other/Unknown 55-64 Property Damage Only Other/Unknown 65+	No Dry No Vet	Dark-Street Lights On Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Straig Not at Intersection/Driveway/Roundabout Straig	ght & Grade No ght in Sag Oil	Ider Road User 65+ No	Other/Unknown None	Dark-No Street Lights Of Unit Off West	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Intersection Related but Not at Intersection	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Inattention	No No No
6611 936419.22 1023585.13 201 6612 936476.71 1023590.38 201	15 2 Vehidesi/Parties Rear-End 18 2 Vehidesi/Parties Rear-End 17 3 Vehidesi/Parties Sideswipe 15 2 Vehidesi/Parties Angle	Property Damage Only Speeding 55-64 Property Damage Only Improper Passing Under Minor or Possible Injury Discrepard Traffic Control Device/Sign 85+	No Dry	Daylight	Not at Intersection/Driveway/Roundabout Straig At Driveway or Intersection Straig	ght in Sag No ght & Level Oi	o Vulnerable Road User Involved Im Ider Road User 65+ Di	peeding mproper Backing/Passing/Turning/Parking/Signaling Disregard Traffic Control Device/Sign	Daylight Dry Daylight Dry	Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Speeding No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No Yes No
6613 936476.71 1023590.38 2014 6614 936471.52 1023589.87 2011 6615 936476.71 1023590.38 2011	14 2 Vehicles/Parties Rear-End 15 2 Vehicles/Parties Rear-End 15 2 Vehicles/Parties Rear-End	Minor or Possible Injury Other/Unknown 45-54	25 No Dry Under influence of Acohol 45-54 Dry No Dry	Daylight Dark-Street Lights On Daylight	At Intersection and Not Related Straig At Driveway or Intersection Straig	ght & Grade No	o Vulnerable Road User Involved Or	Other/Unknown	Daylight Dry Dark-Street Lights On Dry Daylight Dry	At Driveway or Intersection	Inattention Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved	No No No
6617 936471.64 1023592.74 2019 6618 936476.71 1023590.38 2019 6619 936475.51 1023590.18 2019	15 3 Vehicles/Parties Rear-End 15 2 Vehicles/Parties Rear-End 15 2 Vehicles/Parties Rear-End 16 2 Vehicles/Parties Rear-End	Minor or Possible Injury Follow Too Closely Under Property Damage Cnly Inattention Under Property Damage Only Under Influence of Alcohol/Drugs/Medication 55-64 Minor or Possible Injury Follow Too Closely 55-64	25 No Dry 25 No Dry Under Influence of Alcohol 55-64 Dry No Wet	Daylight	At Driveway or Intersection Straig	ght & Level No ght & Grade No		ollow Too Closely nattention	Daylight Dry Daylight Dry		Follow Too Closely Inattention Under Influence of Alcohol/Drugs/Medication Follow Too Closely	No No No
6520 936476.71 1023590.38 2011 6621 936476.71 1023590.38 2011 6621 936476.71 1023590.38 2011 6622 936476.71 1023590.38 2011 6623 936476.71 1023590.38 2011	77 2 Vehicles Parties Rear-End	Property Damage Only Follow Too Closely 55-64 Property Damage Only Follow Too Closely 55-64 Property Damage Only Institution 65+ Property Damage Only Institution 55-64 Minor or Possible Injury Other Unknown 45-54	No Dry No Dry	Daylight Daylight Daylight	At Driveway or Intersection Straig At Driveway or Intersection Straig	ght & Level No oht & Grade Oil	o Vulnerable Road User Involved Fo	ollow Too Closely	Daylight Dry	At Driveway or Intersection	Inattention No Risky Driving Behaviors Involved	No No No
		Property Damage Only Disregard Traffic Control Device/Sign 35-44	No Dry No Dry	Daylight Daylight	At Driveway or Intersection Straig At Driveway or Intersection Straig	ght & Grade No	o Vulnerable Road User Involved Di o Vulnerable Road User Involved In-	Disregard Traffic Control Device/Sign	Daylight Dry	At Driveway or Intersection	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Inattention	Yes No No
6628 936763.08 1023624.44 2011	18 2 Vehicles/Parties Rear-End 9 3 Vehicles/Parties Rear-End 14 2 Vehicles/Parties Rear-End 15 2 Vehicles/Parties Rear-End	Mnor or Possible Injury Inattention 25-34 Property Damage Only Distracted Driving 25-34 Mnor or Possible Injury Follow Too Closely 25-34 Property Damage Only Follow Too Closely 55-64	No Dry No Wet	Daylight Daylight Daylight Daylight Dark-Street Lights On	Not at Intersection/Driveway/Roundabout Straig Not at Intersection/Driveway/Roundabout Straig	ght & Grade No ght & Level No ght & Grade No	o Vuinerable Road User Involved Or o Vuinerable Road User Involved Fc o Vuinerable Road User Involved Fc	Follow Too Closely Follow Too Closely		At Driveway or Intersection Intersection Related but Not at Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Distracted Driving Follow Too Closely Follow Too Closely	No No No
6629 937052.94 1023654.29 2019 6630 937066.53 1023656.56 2019 6631 937065.98 1023660.51 2019	15 2 Vehicles/Parties Angle	Property Damage Only Fail to Yield 65+ Property Damage Only Over Center Line 65+	25 No Dry No Wet No Wet	Daylight Daylight Daylight	At Driveway or Intersection Straig At Driveway or Intersection Straig At Driveway or Intersection Straig	ght & Level No ght & Level Oi ght & Level Oi	o Vulnerable Road User Involved Ni Ider Road User 65+ Fa Ider Road User 65+ La	lone ail to Yield ane Departure	Daylight Dry Daylight Wet Daylight Wet	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	No Risky Driving Behaviors Involved Fail to Yield No Risky Driving Behaviors Involved	No No Yes
6632 937056.24 1023658.28 2011 6633 937105.78 1023655.75 2011	19 2 venices/Parties /Angle 17 2 Vehicles/Parties Rear-End	Property Damage Only Institution Under	No Wet	Dark-Street Lights On Daylight Daylight	Intersection Related but Not at Intersection Straig	ght & Level No	o Vulnerable Road User Involved Fa o Vulnerable Road User Involved In- o Vulnerable Road User Involved Fo	ail to Yield	Dark-Street Lights On Dry Daylight Dry	At Driveway or Intersection Intersection Related but Not at Intersection	Inattention Follow Too Closely	No No
6637 937458.45 1023649.78 2014		Property Damage Only Other/Unknown 45-54	Apparently Asleep, Fatigued or III	Daylight Daylight Daylight Daylight	Not at Intersection/Driveway/Roundabout Straig Not at Intersection/Driveway/Roundabout Straig	ght & Level No ght & Level No	o Vulnerable Road User Involved Ni o Vulnerable Road User Involved Di o Vulnerable Road User Involved Di	Inver Physical Condition		Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Apparently Asleep, Fatigued or III No Risky Driving Behawors Involved Aparently Asleep, Fatigued or III Fail to Yield	No No
6638 937691.19 1023643.79 2011 6639 937632.75 1023643.21 2011 6640 937713.41 1023640.62 2011 6641 937790.81 1023637.32 2011	18 2 Vehicles/Parties Angle 6 2 Vehicles/Parties Rear-End 18 2 Vehicles/Parties Angle 16 2 Vehicles/Parties Pedestrian	Property Damage Only Speeding Under Property Damage Only Inattention 55-64 Minor or Possible Injury Under Influence of Alcohol/Druss/Medication 65+	No Wet	Daylight Dark-Street Lights On Dark-Street Lights On	Not at Intersection/Driveway/Roundabout Straig At Driveway or Intersection Straig Not at Intersection/Driveway/Roundabout Straig	ght & Level No ght & Level Pe	o Vulnerable Road User Involved Sş o Vulnerable Road User Involved Inc	Speeding	Daylight Dry Daylight Wet Dark-Street Lights On Wet Dark-Street Lights On Dry	At Driveway or Intersection Not at Intersection/Driveway/Roundabout At Driveway or Intersection Not at Intersection/Driveway/Roundabout	Speeding Inattention	No No No
6642 937843.54 1023635.38 2011 6643 937869.09 1023634.37 2011 6644 937876.1 1023632.11 2011	18 3 Vehicles/Parties Rear-End 19 2 Vehicles/Parties Rear-End 17 2 Vehicles/Parties Rear-End	Property Damage Only Under Influence of Alcohol/Drugs/Medication 25:34 Mnor or Possible Injury Inattention 55:64 Property Damage Only Inattention 25:34	Under Influence of Alcohol 25-34 Dry	Daylight Daylight Daylight	Not at Intersection/Driveway/Roundabout Straig Not at Intersection/Driveway/Roundabout Straig Intersection Related but Not at Intersection Straig	ght & Level No ght & Level No ght & Level No	o Vuinerable Road User Involved Us o Vuinerable Road User Involved In o Vuinerable Road User Involved In	Inder Influence of Alcohol/Drugs/Medication nattention nattention	Daylight Dry Daylight Dry Daylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Intersection Related but Not at Intersection	Under Influence of Alcohol/Drugs/Medication Inattention Inattention	No No No
6645 938026.5 1023627.93 2011	17 2 Vehicles/Parlies Sideswipe 14 2 Vehicles/Parlies Rear-End 17 3 Vehicles/Parlies Rear-End 17 3 Vehicles/Parlies Rear-End	Property Damage Only Other/Unknown Under Property Damage Only Follow Too Closely 25-34 Property Damage Only Speeding 65+ Property Damage Only Inattention 65-	15 No Dry No Dry No Dry No Dry	Daylight Daylight Daylight	Not at Intersection/Driveway/Roundabout Straig Not at Intersection/Driveway/Roundabout Straig Not at Intersection/Driveway/Roundabout Straig	ght & Level No ght & Grade No	o Vulnerable Road User Involved Or o Vulnerable Road User Involved Fo	Other Contributing Circ Not Listed	Daylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Intersection Related but Not at Intersection	No Risky Driving Behaviors Involved Follow Too Closely Speeding	Yes No No
6649 938160.68 1023623.6 2011	17 3 Vehicles/Parties Rear-End	Property Damage Only Instalention 65+ Property Damage Only Speeding 55-64-6 Property Damage Only Follow Too Closely 25-34-8 Minor or Possible Injury Follow Too Closely 65+ Property Damage Only Follow Too Closely 65-	No Dry No Dry No Dry No Dry	Daylight Dark-Street Lights On	Not at Intersection/Driveway/Roundabout Straig Not at Intersection/Driveway/Roundabout Straig	ght & Level No	o Vulnerable Road User Involved Sp	speeding	Daylight Dry	Not at Intersection/Driveway/Roundabout	Inattention Speeding Follow Too Closely	No No
6651 938264.95 1023619.9 2010 6652 938268.97 1023595.53 2010		Minor or Possible Injury Follow Too Closely 65+ Property Damage Only Follow Too Closely 65+ Minor or Possible Injury Speeding 45-54	No Wet No Dry	Daylight Daylight Daylight	Not at Intersection/Driveway/Roundabout Straig Driveway Related but Not at Driveway Straig	ght & Level Oil ght & Level No	o Vulnerable Road User Involved Sp	ollow Too Closely follow Too Closely speeding	Daylight Dry Daylight Wet Daylight Dry	Not at Intersection/Driveway/Roundabout Intersection Related but Not at Intersection Not at Intersection/Driveway/Roundabout Driveway Related but Not at Driveway	Follow Too Closely Follow Too Closely Speeding	No No No
6653 938317.84 1023617.96 201-		Describe Demons Only College									Cail to Male	
6654 938317.6 1023617.97 2019 6655 938370.37 1023616.03 201- 6656 938370.25 1023616.04 2019	15	Property Damage Only Fail to Yield 25-34 Property Damage Only Inattention 65+	25 No Dry	Daylight Dark-No Street Lights/Street Lights Off Dawn/Dusk Daylight Dark-Street Lights On	Not at Intersection/Driveway/Roundabout Straig	ght & Level No	Ider Road User 65+ In: o Vulnerable Road User Involved In:	nattention	Daylight Dry Dark-No Street Lights/Street Lights Off Dry Darwn/Dusk Dry Daylight Dry Dark-Street Lights On Wet	At Driveway or Intersection Driveway Related but Not at Driveway Not at Intersection/Driveway/Roundabout At Driveway or Intersection Not at Intersection/Driveway/Roundabout	Fail to Yield Inattention Inattention No Risky Driving Behaviors Involved	No Yes No

OBJECTID JURISDICTION	COUNTY CITY	PRIMARY BLOCK TRAFFICWAY NUMBER	INTERSECTING COMP. DIR REFERENCE FROM REPOINT REFERENCE FROM REPOINT REPOINT REPOINT REPOINT	MILEPOST A SR_ONLY HISTORY SUSPENSE_IND	DATE TIME MOST_SEVERE INJURY_TYPE	NUM NUM NUM NUM NUM NUM FAT VEH PEDS BIR	M VEH_1_TYPE	VEH_2_TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD_SURFACE ROADWAY CHARACTERISTIC	LIGHTING CONDITION	FRST_COLLISION TYPE_OBJECT_STRUCK	VEH_1 ACTION	VEH_2 ACTION	VEH_1_COMPASS VEH_1_COMPASS VEH_2_COMPASS VEH_2_COMPASS DIRECTION_FROM DIRECTION_TO DIRECTION_TO
6659 State Route 6660 State Route	Ciallam 1 Ciallam 1	101		251.01 No E507541 251.01 No E805086	2016-01-15 19:18 No Apparent Injury 2018-06-05 14:10 No Apparent Injury	0 0 2 0	0 Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Truck Tractor & Semi-Trailer	At Intersection and Related	Overcast Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end From opposite direction - one left turn - one straight	Going Straight Ahead Making Left Turn	Slowing Going Straight Ahead	East West East West West North East West
6662 State Route 6663 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101			2016-08-09 16:25 No Apparent Injury 2017-01-18 17:44 Possible Injury	0 0 2 0	Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Not Related Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Raining	Dry Straight & Level	Daylight	From same direction - both going straight - both moving - sideswipe From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end	Changing Lanes Going Straight Ahead Going Straight Ahead	Going Straight Ahead Stopped for Traffic Slowing	East West East Webide Stopped Webide Stopped West East West East West East
6662 State Route 6663 State Route 6664 State Route 6665 State Route 6666 State Route 6666 State Route	Clallam 1 Clallam 1	101		251.02 No E930575 251.02 No E996341	2019-06-15 01:40 No Apparent Injury 2019-12-19 18:10 No Apparent Injury	0 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	 Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb 	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Intersection Related but Not at Intersection	Clear or Partly Cloudy Raining	Dry Straight & Level Wet Straight & Level	Dark-Street Lights On Dark-Street Lights On	Other Objects From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead	Slowing	South North West East West East
6666 State Route 6667 State Route	Cialiam 1 Cialiam 1	101		251.04 No E934861 251.05 No E977763			Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection Intersection Related but Not at Intersection	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead	Stopped at Signal or Stop Sign Stopped for Traffic	West East East Vehicle Stopped West East Vehicle Stopped Vehicle Stopped
6667 State Route 6668 State Route 6669 State Route 6670 State Route	Cialiam 1 Cialiam 1	101		251.06 No 2766886 251.06 No 2766938		0 0 2 0 0 0 2 0 0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Raining	Dry Curve & Level Wet Straight & Level	Daylight Daylight	From same direction - both going straight - one stopped - rear-end Entering at angle	Going Straight Ahead	Stopped at Signal or Stop Sign Making Left Turn	East West East West East West North East
6670 State Route 6671 State Route 6672 State Route	Clallam 1 Clallam 1 Clallam 1	101		251.06 No 3759328	2017-08-16 15:50 No Apparent Injury	0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Dark-Street Lights On Daylight	Roadway Ditch From same direction - both going straight - one stopped - rear-end	Making Right Turn Going Straight Ahead	Stopped at Signal or Stop Sign	West South South North South North
6672 State Route 6673 State Route 6674 State Poute	Cialiam 1 Cialiam 1	101		251.06 No E313793 251.06 No E337458 251.06 No E34967	2014-03-11 15:47 No Apparent Injury 2014-06-11 15:43 Possible Injury 2014-07-13 15:12 No Apparent Injury	1 0 2 0	O Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	Passenger Car Passenger Car Passenger Car	At Driveway within Major Intersection At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Curve & Grade Dry Straight & Level	Daylight	Entering at angle From opposite direction - one left turn - one straight From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Making Left Turn Making Left Turn Going Straight Ahead	Going Straight Ahead Going Straight Ahead Stopped at Signal or Stop Sign	South West West East West North East West West Marie East West North East Webide Stronged Webide Stronged
6675 State Route 6676 State Route	Cialiam 1 Cialiam 1	101		251.06 No E392193	2015-01-12 16:34 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car	At Intersection and Related Driveway Related but Not at Driveway	Overcast Clear or Partly Cloudy	Dry Straight & Level	Dusk	Street Light rection - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	West South West East West East
6675 State Route 6676 State Route 6677 State Route 6678 State Route	Cialiam 1 Cialiam 1	101		251.06 No E476980 251.06 No E507539	2015-10-31 17:08 No Apparent Injury	0 0 2 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level	Daylight Dark-No Street Lights Dark-Street Lights On	Entering at angle Vehicle turning left hits pedestrian	Going Straight Ahead Making Left Turn	Going Straight Ahead	East West North South North East
6679 State Route 6680 State Route 6681 State Route 6682 State Route	Cialiam 1 Cialiam 1	101				1 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end	Going Straight Ahead Stopped at Signal or Stop Sign	Slowing Starting in Traffic Lane Going Straight Ahead	West East West East Vehicle Stopped West East
6681 State Route 6682 State Route	Cialiam 1 Cialiam 1	101		251.06 No E687830 251.06 No E707301	2017-06-29 14:02 Possible Injury 2016-04-29 12:02 Possible Injury					Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Going Straight Ahead Stopped at Signal or Stop Sign	East West West East West East Vehicle Stopped Vehicle Stopped
6683 State Route 6684 State Route 6685 State Route 6686 State Route	Cialiam 1 Cialiam 1	101		251.06 No E751023 251.06 No E88535 251.06 No E948526 251.06 No E974963	2019-01-25 11:24 No Apparent Injury 2019-01-05 14:13 No Apparent Injury	0 0 2 0	Pickup, Panel Truck or Vinaette under 10,000 lb Passenger Car O Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car O Passenger Car O Passenger Car O Dober	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Not Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Level	Dark-Street Lights On Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Slowing	Stopped at Signal or Stop Sign	East West South Vehicle Stopped
	Clallam 1 Clallam 1	101		251.06 No E974963 251.06 No E995069	2019-10-20 17:20 No Apparent Injury 2019-12-18 09:56 No Apparent Injury	0 0 2 0	Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Overcast Overcast	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - both moving - rear-end Entering at angle From same direction - both going straight - one stopped - rear-end	Changing Lanes Going Straight Ahead Going Straight Ahead	Going Straight Ahead Stopped at Signal or Stop Sign	West East North South West East Vehicle Stopped Vehicle Stopped
6687 State Route 6688 State Route 6689 State Route 6690 State Route 6691 State Route	Cialiam 1 Cialiam 1	101							Intersection Related but Not at Intersection	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade	Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Stopped at Signal or Stop Sign Slowing	East West Vehicle Stopped Vehicle Stopped East West East West
6690 State Route 6691 State Route	Cialiam 1 Cialiam 1	101		251.09 No E331507 251.11 No E863234	2017-10-27 11:48 No Apparent Injury 2014-06-01 00:05 No Apparent Injury 2018-11-09 06:48 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Curve & Level Dry Curve & Level Curve & Level	Dark-Street Lights On Dawn	Street Light Pole or Base Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East West East
6692 State Route 6693 State Route 6694 State Route 6695 State Route	Cialiam 1 Cialiam 1	101		251.12 No E625726 251.12 No E634305	2016-12-23 16:36 No Apparent Injury 2017-01-13 21:05 No Apparent Injury 2018-08-03 17:20 Suspected Minor Injury 2017-04-09 16:04 Suspected Minor Injury 2017-04-09 17:00 Suspected Minor Injury 2017-04-09 17:00 Suspected Minor Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level Dry Curve & Level	Dark-No Street Lights	Entering at angle From opposite direction - all others	Making Left Turn	Going Straight Ahead Going Straight Ahead	South West West East West East West
6695 State Route	Cialiam 1 Cialiam 1	101		251.12 No E825266 251.14 No 3501120	2018-08-03 17:20 Suspected Minor Injury 2017-04-09 16:04 Suspected Minor Injury 2014-09-09 17:39 Suspected Minor Injury	1 0 3 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		At Intersection and Related At Driveway Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Grade Dry Curve & Level	Daylight Daylight	Pedalcyclist Strikes Moving Vehicle Entering at angle	Making Right Turn Making Left Turn Going Straight Ahead	Going Straight Ahead	South East South West West East
6696 State Route 6697 State Route 6698 State Route	Cialiam 1 Cialiam 1	101		251.14 No E761218 251.15 No E722345	2018-01-22 10:06 No Apparent Injury 2017-09-29 17:30 No Apparent Injury	0 0 4 0	O Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Passenger Car Passenger Car	Passenger Car Pickup Panel Truck or Vanette under 10 000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Other Curve & Level Dry Straight & Level	Daylight Daylight Dark-Street Lights On	Venues oversomes From same direction - both going straight - both moving - sideswipe From same direction - both going straight - both moving - rear-end	Changing Lanes	Going Straight Ahead	West East West East Fact West Fact West Wast
6699 State Route	Cialiam 1 Cialiam 1	101		251.15 No E866664 251.16 No E508964	2018-11-17 06:04 No Apparent Injury 2016-01-22 18:00 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast	Dry Curve & Level Wet Straight & Level	Dark-Street Lights On Dark-Street Lights On	Vehicle Strikes Deer From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stooped for Traffic	West East Vehicle Stopped Vehicle Stopped
6701 State Route 6702 State Route	Cialiam 1 Cialiam 1	101		251.16 No E793644 251.20 No E718496	2018-04-27 07:20 No Apparent Injury 2017-08-26 13:31 Possible Injury 2015-10-26 15:08 Suspected Minor Injury 2017-10-20 17:00 No Apparent Injury	0 0 2 0	Passenger Car Truck (Flatbad, Van,etc) Pickup, Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Intersection Related but Not at Intersection	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - both mowing - sideswipe From same direction - both going straight - one stopped - rear-end	Changing Lanes Going Straight Ahead Making Left Turn	Going Straight Ahead Stopped for Traffic	West East West East East West Vehicle Stopped Vehicle Stopped
6703 State Route 6704 State Route	Cialiam 1 Cialiam 1	101		251.22 No E481131 251.22 No E728553	2015-10-26 15:08 Suspected Minor Injury 2017-10-20 17:00 No Apparent Injury	3 0 2 0 0 0 2 0		Motorcycle Description Co.	At Universary	Clear or Partly Cloudy Clear or Partly Cloudy			From opposite direction - one left turn - one straight From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Going Straight Ahead Slowing	East South West East West East West East
6704 State Route 6705 State Route 6706 State Route 6707 State Route 6709 State Route	Cialiam 1 Cialiam 1	101		251.26 No E568910 251.28 No E881658	2016-07-24 15:53 Possible Injury 2018-12-27 19:50 Suspected Serious Injur	1 0 2 0 ry 1 0 2 0	Passenger Car Passenger Car Passenger Car Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Driveway	Clear or Partly Cloudy Overcast	Dry Straight & Level Dry Straight & Level	Daylight Dark-Street Lights On	From same direction - both going straight - both moving - rear-end From opposite direction - one left turn - one straight	Going Straight Ahead Making Left Turn Making Left Turn	Slowing Going Straight Ahead	West East West East East South West East Court Meat East
	Cialiam 1	101		251.28 No E959235 251.29 No E864157 251.20 No 2766048	2018-11-21 18:20 Suspected Minor Injury 2014-04-24 09-25 No. Accessed Minor Injury	1 0 2 0	Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Menate and a 40 000	At Driveway Not at Intersection and Not Related Intersection Related but Not at Intersection	Other	Wet Straight & Level	Dark-Street Lights On Davidet	Entering at angle Vehicle going straight hits pedestrian Form symp difference, both pages straight, both making a reasonal		Going Straight Ahead	East West East
6709 State Route 6710 State Route 6711 State Route	Cialiam 1 Cialiam 1	101		251.31 No E778224 251.31 No E94824	2018-03-07 12:47 No Apparent Injury 2018-03-07 12:47 No Apparent Injury 2018-07-06 22:00 Suspected Minor Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb	Truck Tractor & Semi-Trailer	Intersection Related but Not at Intersection At Driveway At Intersection and Related	Clear or Partly Cloudy Overcast Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Coul-	Daylight Daylight Dark-Street Lights On	From same direction - both going straight - both moving - rear-end Entering at angle Whicle coing straight hits pedestrian	Changing Lanes Merging (Entering Traffic) Going Straight Ahead	Going Straight Ahead Going Straight Ahead	North West East West East West
6712 State Route 6713 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101		251.32 No E711836 251.32 No E818490	2017-09-13 06:46 No Apparent Injury 2018-07-05 14:20 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Truck Tractor & Semi-Trailer Truck Tractor & Semi-Trailer	At Driveway	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Level Dry Straight & Level	Dawn	Vehicle going straight hits pedestrian Entering at angle Entering at angle	Making Right Turn Making Left Turn	Changing Lanes Going Straight Ahead	North West East West South West East West
6712 State Route 6713 State Route 6714 State Route 6715 State Route	Cialiam 1 Cialiam 1	101		251.34 No E311367 251.35 No E930574	2014-02-26 11:43 Suspected Minor Injury 2019-06-14 23:15 No Apparent Injury 2016-05-28 11:55 No Apparent Injury 2019-04-12 17:27 Possible hims	1 0 1 0			Not at Intersection and Not Related At Driveway	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Grade	Daylight Dark-Street Lights On	Roadway Ditch Entering at angle	Going Straight Ahead Making Right Turn	Going Straight Ahead Stopped for Traffic	East West South East West East
	Cialiam 1 Cialiam 1	101		251.40 No E548266 251.42 No E970097	2016-05-28 11:55 No Apparent Injury 2019-10-12 17:27 Possible Injury	0 0 2 0			Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast	Dry Straight at Hillor Dry Straight & Grade	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end	Slowing Going Straight Ahead	Stopped for Traffic Going Straight Ahead	East West Vehicle Stopped Vehicle Stopped West East West East
6717 State Route 6718 State Route 6719 State Route 6720 State Route	Cialiam 1 Cialiam 1	101		251.43 No E825269 251.45 No E809504	2018-08-06 10:50 Suspected Minor Injury 2018-06-12 14:25 No Apparent Injury	1 0 4 0	O Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Distor Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Grade Dry Straight & Grade	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end	Going Straight Ahead Slowing Changing Lanes	Stopped for Traffic Slowing Going Straight Ahead	West East Vehicle Stopped Vehicle Stopped West East West East
6720 State Route 6721 State Route	Cialiam 1 Cialiam 1	101		251.47 No 2986209 251.47 No E347290 251.48 No E856778	2014-05-04 16:03 No Apparent Injury	3 0 3 0	O Processor Cos	Dielos Desel Tools as Venette onder 10 000 lb	Intersection Deleted but Not related	Clear or Partly Cloudy Clear or Partly Cloudy Raining	Dry Straight & Grade Dry Straight & Grade	Daylight	From same direction - both going straight - both moving - sideswipe From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing	East West East West East West West West
6721 State Route 6722 State Route 6723 State Route 6724 State Route 6724 State Route	Clallam 1 Clallam 1 Clallam 1	101		251.50 No E889442 251.51 No F766967	2019-02-03 17:26 Possible Injury 2017-12-24 17:19 Possible Injury	1 0 2 0	Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Intersection Related but Not at Intersection At Intersection and Related	Snowing Snowing	Snow/Slush Straight & Grade Snow/Slush Straight & Grade	Daylight Dark-Street Lights On Dark-Street Lights On Dark-Street Lights On	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Stopped for Traffic Going Straight Ahead Slowing	Going Straight Ahead Stopped for Traffic Stopped at Signal or Stop Sign	Vehicle Stopped Vehicle Stopped West East West East West East West East Vehicle Stopped Vehicle Stopped
	Cialiam 1 Cialiam 1	101		251.51 No E935585 251.51 No E976833	2019-06-24 00:12 Possible Injury 2019-10-29 17:05 Possible Injury	1 0 2 0 1 0 2 0 0 0 2 0	O Passenger Car O Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car Passenger Car	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade	Dark-Street Lights On	From same direction - both going straight - one stopped - sideswipe From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead	Stopped at Signal or Stop Sign Stopped at Signal or Stop Sign	West East Vehicle Stopped Vehicle Stopped West East Vehicle Stopped Vehicle Stopped
6726 State Route 6727 State Route 6728 State Route	Clallam 1 Clallam 1 Clallam 1	101		251.52 No E445342 251.52 No E706484	2015-07-10 09:55 No Apparent Injury 2017-08-22 17:20 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car Pickup Panel Truck or Vanette under 10 000 lb	At Intersection and Related At Intersection and Related	Overcast Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Grade Dry Straight & Grade Dry Straight & Grade	Daylight Daylight	From same direction - both going straight - both moving - sideswipe From opposite direction - one left turn - one right turn	Other* Making Right Turn	Going Straight Ahead Making Left Turn	West East West East North West South West
6729 State Route 6730 State Route	Cialiam 1 Cialiam 1	101 101				0 0 2 0	0 Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related	Overcast	Dry Straight & Grade Wet		From same direction - one right turn - one straight From same direction - both going straight - one stopped - rear-end	Starting in Traffic Lane	Starting in Traffic Lane Slowing	North South North South West East West East
6729 State Route 6730 State Route 6731 State Route 6732 State Route	Cialiam 1 Cialiam 1	101 101		251.53 No E373323 251.53 No E481130	2014-11-10 15:58 No Apparent Injury	0 0 2 0	0 Passenger Car 0 Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Raining	Dry Straight & Level Wet Straight & Grade	Daylight Dark-Street Lights On	Entering at angle From same direction - both going straight - one stopped - rear-end	Stopped at Signal or Stop Sign Going Straight Ahead Going Straight Ahead	Slowing Making Left Turn Stopped at Signal or Stop Sign	East West North East West East West Vehicle Stopped
6733 State Route 6734 State Route	Cialiam 1 Cialiam 1	101 101				1 0 2 0	0 Plassenger Car 0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Raining Clear or Partly Cloudy	Dry Straight & Grade Wet Straight & Grade	Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Slowing Going Straight Ahead	Stopped at Signal or Stop Sign Stopped at Signal or Stop Sign	West East Vehicle Stopped Vehicle Stopped West East Vehicle Stopped Vehicle Stopped
6735 State Route 6736 State Route	Cialiam 1 Cialiam 1	101		251.53 No E540331 251.53 No E544685	2016-03-14 15:58 Possible Injury 2016-05-02 08:40 No Apparent Injury 2016-05-15 10:31 Possible Injury 2016-06-13 11:21 Possible Injury	2 0 3 0	0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Overcast	Dry Straight & Grade Dry Straight & Grade	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end	Stopped at Signal or Stop Sign Changing Lanes	Going Straight Ahead Slowing	Vehicle Stopped Vehicle Stopped West East West East West East
6737 State Route 6738 State Route 6739 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101		251.53 No E601813 251.53 No E60479	2016-10-28 15:55 Possible Injury 2016-10-28 21:57 Possible Injury	1 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb	At Intersection and Not Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Grade Dry Straight & Grade	Daylight Dark-Street Lights On	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Starting in Traffic Lane Going Straight Ahead	Stopped at Signal or Stop Sign Starting in Traffic Lane Stopped at Signal or Stop Sign	West East Least Vehicle Stopped West East West East West East Vehicle Stopped Vehicle Stopped
6738 State Route 6739 State Route 6740 State Route 6741 State Route 6742 State Route	Cialiam 1 Cialiam 1	101		251.53 No E605439 251.53 No E625729	2016-11-04 10:46 No Apparent Injury 2016-12-14 16:26 No Apparent Injury	0 0 2 0	O Pickup, Panel Truck or Vanette under 10,000 ib Passenger Car Pickup, Panel Truck or Vanette under 10,000 ib Passenger Car Dirkup, Panel Truck or Vanette under 10,000 ib	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Grade	Dayiigiit	From same direction - both going straight - one stopped - rear-end Entering at angle From same direction - both going straight - both moving - rear-end	Making Right Turn Going Straight Ahead	Stopped at Signal or Stop Sign Going Straight Ahead Slowing	West East West East West East
6742 State Route 6743 State Route 6744 State Route	Cialiam 1 Cialiam 1	101 101		251.53 No E636509 251.53 No E718486	2017-01-23 19:00 No Apparent Injury 2017-07-04 19:35 Suspected Minor Injury	0 0 2 0 2 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy	Dry Straight & Grade	Dark-Street Lights On	From same direction - both going straight - one stopped - rear-end Entering at angle	Going Straight Ahead	Stopped at Signal or Stop Sign Stopped at Signal or Stop Sign	West East Vehicle Stopped Vehicle Stopped North West Vehide Stopped Vehide Stopped West East North South
6745 State Route	Cialiam 1 Cialiam 1	101		251.53 No E718488 251.53 No E796944	2017-08-01 09:49 Suspected Minor Injury 2018-04-27 16:44 No Apparent Injury	1 0 2 0 0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Grade Dry Curve & Grade	Daylight	Entering at angle From same direction - all others	Making Right Turn Going Straight Ahead Backing	Stopped at Signal or Stop Sign	South Vehicle Backing Vehicle Stopped Vehicle Stopped
6746 State Route 6747 State Route 6748 State Route 6749 State Route	Cialiam 1 Cialiam 1	101		251.53 No E807603 251.53 No E817826 251.53 No E888502	2018-06-08 16:57 No Apparent Injury 2018-07-15 18:46 Possible Injury 2019-01-29 16:35 Possible Injury	1 0 2 0	Pickup Panel Truck or Vanelle under 10,000 b Passenger CE Pickup Panel Truck or Vanelle under 10,000 b Passenger CB Passenger CB Pickup Panel Truck or Vanelle under 10,000 b Pickup Panel Truck or Vanelle under 10,000 b Truck & Trailer Pickup Panel Truck or Vanelle under 10,000 b	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Grade Dry Straight & Grade Dry Straight & Grade Dry Straight & Grade	Daylight Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead Stopped at Signal or Stop Sign Stopped for Traffic	North South North South East West East Vehicle Stopped East West Vehicle Stopped Vehicle Stopped
6749 State Route 6750 State Route	Cialiam 1 Cialiam 1	101		251.53 No E909417 251.53 No E947981	2019-04-09 10:45 Possible Injury 2019-08-07 11:37 No Apparent Injury	4 0 3 0	Pickup,Panel Truck or Vanette under 10,000 lb Truck & Trailer	Passenger Car Truck (Flatbad Van.etc)	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Grade	Daylight	Entering at angle From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Starting in Traffic Lane Going Straight Ahead	Going Straight Ahead Stopped at Signal or Stop Sign	South North West East West East Vehicle Stopped Vehicle Stopped
6751 State Route 6752 State Route	Cialiam 1 Cialiam 1	101		251.54 No E961328 251.55 No E349304	2019-08-28 17:10 No Apparent Injury 2019-08-28 17:10 No Apparent Injury 2014-08-08 14:25 Possible Injury 2019-08-31 18:21 No Apparent Injury 2018-08-23 14:53 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection At Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Grade	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Starting in Traffic Lane Going Straight Ahead Going Straight Ahead	Stopped for Traffic Stopped at Signal or Stop Sign	East West Vehicle Stopped Vehicle Stopped East West Vehicle Stopped Vehicle Stopped
6753 State Route 6754 State Route	Cialiam 1 Cialiam 1	101		251.55 No E956907 251.56 No E831440	2019-08-31 18:21 No Apparent Injury 2018-08-23 14:53 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Intersection Related but Not at Intersection Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end		Stopped for Traffic Going Straight Ahead	East West Vehicle Stopped Vehicle Stopped East West East West
6755 State Route 6756 State Route	Cialiam 1 Cialiam 1	101		251.57 No E951113 251.62 No 2764776	2019-08-03 12:41 No Apparent Injury 2014-07-09 15:50 Suspected Minor Injury	0 0 1 0 5 0 3 0	O Pickup, Panel Truck or Vanette under 10,000 lb O Pickup, Panel Truck or Vanette under 10,000 lb O District Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Intersection Related but Not at Intersection	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Curve & Grade	Daylight Daylight	Vehicle Strikes Deer From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead	Stopped for Traffic	East West East West Vehicle Backing Vehicle Stopped
6758 State Route 6759 State Route	Cialiam 1 Cialiam 1	101								Clear or Partly Cloudy Raining Overcast	Dry Straight & Grade Wet Curve & Level Dry Curve & Grade	Daylight	Vehicle Strikes Deer From same direction - both going straight - both moving - sideswipe Entering at annie	Going Straight Ahead Changing Lanes Going Straight Ahead	Going Straight Ahead Going Straight Ahead	West East West East West South North West Fast
6760 State Route 6761 State Route	Cialiam 1 Cialiam 1	101 101		251.68 No E434944 251.68 No E508963	2015-03-29 17:20 Suspected Serious Injur 2015-06-17 16:00 Possible Injury 2016-01-21 23:42 No Apparent Injury	1 0 2 0	Pickup Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Driveway within Major Intersection At Intersection and Not Related At Intersection and Not Related	Clear or Partly Cloudy Raining	Dry Curve & Grade Dry Curve & Grade Wet Curve & Grade	Daylight Dark-Street Lights On	Entering at angle From same direction - both going straight - both moving - sideswipe One parked-one moving	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead Going Straight Ahead Legally Parked, Unoccupied	East West East West West East
6762 State Route 6763 State Route	Cialiam 1 Cialiam 1	101 101		251.68 No E571537 251.68 No E604476	2016-07-23 17:45 No Apparent Injury 2016-10-18 17:00 No Apparent Injury	0 0 2 0	0 Passenger Car 0 Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway within Major Intersection At Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade	Daylight Daylight	Entering at angle From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Making Left Turn Slowing	North South East South West East West East
6762 State Route 6763 State Route 6764 State Route 6765 State Route 6765 State Route 6766 State Route	Cialiam 1 Cialiam 1	101		251.68 No E687832 251.68 No E715030	2017-06-22 09:45 No Apparent Injury 2017-07-28 10:46 No Apparent Injury	0 0 2 0	Passenger Car Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Pickup Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car Passenger Car	At Intersection and Related At Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade Dry Curve & Grade	Daylight Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - sideswipe	Going Straight Ahead Going Straight Ahead Changing Lanes	Slowing Going Straight Ahead	East West East West West East West East
6766 State Route 6767 State Route 6768 State Route	Cialiam 1 Cialiam 1	101		251.68 No E945765	2019-07-30 08:52 Possible Injury	2 0 2 0	Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Rickup Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway within Major Intersection At Driveway within Major Intersection	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade Dry Curve & Grade	Daylight	Entering at angle From opposite direction - one left turn - one straight	Making Right Turn Making Left Turn	Going Straight Ahead Going Straight Ahead	South East West East East South West East
6769 State Route 6770 State Route	Cialiam 1 Cialiam 1	101		251.69 No E928354 251.77 No E634304	2019-06-08 18:40 Possible Injury 2017-01-13 21:00 No Apparent Injury	1 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup Panel Truck or Vanette under 10.000 lb	At Driveway within Major Intersection Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade Dry Straight & Grade	Dark-Street Lights On Daylight Dark-No Street Lights	From same direction - both going straight - both moving - sideswipe	Going Straight Ahead Making Left Turn Overtaking and Passing	Going Straight Ahead Going Straight Ahead	West West East West East West East
6771 State Route 6772 State Route	Cialiam 1 Cialiam 1	101 101		251.78 No E650149 251.78 No E915482	2017-03-05 20:40 No Apparent Injury 2019-04-27 13:04 No Apparent Injury	0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Wet Curve & Grade Dry Curve & Grade	Dark-No Street Lights Daylight	Over Embankment - No Guardrail Present Strikes or Was Struck by a Part of Another Vehicle (Not from Load)	Going Straight Ahead Going Straight Ahead	Going Straight Ahead	West East East West East
6769 Sate Route 6770 Sate Route 6771 Sate Route 6771 Sate Route 6772 Sate Route 6773 Sate Route 6773 Sate Route 6773 Sate Route 6773 Sate Route 6775 Sate Route 6775 Sate Route 6777 Sate Route 6777 Sate Route 6777 Sate Route 6777 Sate Route	Cialiam 1 Cialiam 1 Cialiam 1 Cialiam 1	101		251.79 No E438854 251.80 No E373031	2015-06-15 18:10 No Apparent Injury 2014-11-07 11:52 No Apparent Injury	0 0 5 0	Procupy-raties Hock or Valentee under 10,000 is Passenger Car Pickup Panel Truck or Valentee under 10,000 is Pickup Panel Truck or Valentee under 10,000 is Plexup Panel Truck or Valentee under 10,000 is	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade Dry Curve & Grade	Daylight Daylight	Over Embankment - No Guardraii Present Selikes or Was Struck by a Part of Another Vehicle (Not from Load) Rock Bank or Ledge From same direction - all others Making Struck Description	Going Straight Ahead Making U-Turn Going Straight Ahead	Going Straight Ahead	East West West East
6776 State Route 6777 State Route	Cialiam	101		251.88 No E975256	2019-10-26 10:26 No Apparent Injury	0 0 1 0	0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy				Going Straight Ahead Going Straight Ahead		West East West Fast
6778 State Route 6779 State Pouts	Claliam	101		251.90 No E766328	2014-03-28 14:45 Suspected Minor Injury 2015-04-07 09:45 No Apparent Injury 2018-02-03 18:00 No Apparent Injury 2019-04-24 21:39 No Apparent Injury	0 0 1 0	Passenger Car Pickup Panel Truck or Vaneth and 40 000 "	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Raining Clear or Partly Cloudy	Wet Curve & Grade Dry Curve & Conde	Daylight Dark-No Street Lights Dark-No Street Lights	Roadway Ditch Whicle Strikes Deer From same direction - both going straight - both moving - rear-end Weblick overhunger	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing	West East West East
6780 State Route 6781 State Route	Claliam 1 Claliam Claliam	101			2019-04-24 (21:39 No Apparent Injury 2016-07-29 16:10 No Apparent Injury 2016-08-14 02:10 No Apparent Injury			Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Parity Cloudy Clear or Parity Cloudy Clear or Parity Cloudy Clear or Parity Cloudy	Dry Curve & Grade Dry Straight & Grade	Daylight Daylight	Venicie overtiffred From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - rear-end Roadway Ditta	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing Going Straight Ahead	West East West East East West East West
6782 State Route 6783 State Route	Clallam 1	101							Not at Intersection and Not Related Not at Intersection and Not Related	Overcast	Wet Curve & Grade	Dark-No Street Lights	From same direction - both going straight - both moving - sideswipe		Going Straight Ahead	West East East West East West
6784 State Route 6785 State Route	Cialiam 1 Cialiam 1	101		252.00 No E338119 252.03 No E475768	2014-06-27 15:02 Possible Injury 2015-10-27 21:05 No Apparent Injury	1 0 1 0	0 Motorcycle 0 Pickup Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Grade Dry Curve & Grade	Daylight Dark-No Street Lights	Vehicle overturned Vehicle Strikes Derr Culvert and/or ofher Appurtenance in Ditch	Going Straight Ahead Going Straight Ahead Going Straight Ahead		East West East West
6787 State Route	Cialiam 1 Cialiam 1	101		252.03 No E869731 252.09 No E529990 252.09 No E731124	2016-03-30 14:00 Suspected Minor Injury 2017-10-27 00-12 Suspected Minor Injury	1 0 1 0	Motorcycle Pickup, Panel Truck or Vanette under 10,000 lb Truck Tractor & Semi-Trailer		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy				Going Straight Anead		West East
6789 State Route 6790 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101		252.10 No E786321 252.11 No E36940	2018-04-04 21:26 No Apparent Injury 2014-09-19 20:30 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Truck Tractor & Semi-Trailer Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Raining Clear or Partly Cloudy	Wet Curve & Grade Dry Curve in Sec	Dark-Street Lights On Dark-Street Lights On Dark-No Street Lights	Earth Bank or Ledge Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East East West
6791 State Route 6792 State Route	Cialiam 1 Cialiam 1	101		252.12 No E635420 252.12 No E651601	2017-01-19 13:11 Suspected Serious Injury 2017-03-12 08:50 No Apparent Injury	ry 2 0 2 0	0 Passenger Car 0 Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10 000 lb.	At Intersection and Not Related At Intersection and Related	Raining Clear or Partly Cloudy Clear or Partly Cloudy Overcast			Earth Bank or Ledge Vehicle Strikes Deer From opposite direction - both moving - head-on From same direction - both going straight - both moving - sideswipe	Going Straight Ahead Other*	Going Straight Ahead Going Straight Ahead	East West West East East West West Faxt
6793 State Route 6794 State Route	Clallam 1 Clallam 1	101		252.13 No E401374 252.13 No E455868	2015-02-17 11:30 No Apparent Injury 2015-08-20 14:00 No Apparent Injury	0 0 2 0	o Prickup-Parier Hock of National United Hock of Description of Semi-Trailer O Passenger Car O Passenger Car O Passenger Car O Passenger Car O Pickup-Panel Truck or Vanette under 10,000 ib	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10.000 lb	At Intersection and Related At Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve in Sag Dry Curve & Grade	Daylight Daylight	Fifth saint unite, on Fourigning salagin. Fouri moving "sales imperation for the fifth of the fi	Slowing Going Straight Ahead	Going Straight Ahead Slowing	South North West East West East West East
6795 State Route 6796 State Route	Clallam 1	101						Passenger Car		Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade Dry Curve & Grade	Daylight Daylight	Entering at angle Vehicle Strikes Deer	Making Left Turn	Going Straight Ahead	South West East West East
6797 State Route 6798 State Route	Clallam	101		252.15 No E400829 252.21 No E472950	2015-02-12 17:24 Unknown 2015-10-10 14:10 No Apparent Injury 2019-02-22 18:44 No Apparent Injury 2019-09-09 09:36 No Apparent Injury	0 0 1 0	0 Passenger Car 0 Motorcycle		Not at Intersection and Not Related Not at Intersection and Not Related	Overcast	Wet Curve & Level Wet Curve in Sag	Dark-No Street Lights Daylight Dark-No Street Lights	Vehicle Strikes Deer Miscollaneous Object or Debris on Road Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East West East
6799 State Route 6800 State Route	Cialiam 1	101		252.22 No E896586 252.25 No E961327	2019-09-09 09:36 No Apparent Injury 2019-09-09 09:36 No Apparent Injury	0 0 1 0	0 Passenger Car	Distance Densel To 19 19 19 19 19 19 19 19 19 19 19 19 19	Not at Intersection and Not Related Intersection Related but Not at Intersection	Clear or Partly Cloudy Overcast				Making Right Turn	Making Ly 6 W	west East North West
6801 State Route 6802 State Route	Cialiam 1 Cialiam 1	101		252.26 No E350371 252.26 No E453283 252.26 No E453285	2013-03-03-03-03-03-03-03-03-03-03-03-03-03	1 0 2 0	2 Motorcycle	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Overcast	Dry Curve & Grade Dry Curve & Grade	Daylight Daylight	From opposite direction - one left turn - one straight Vehicle - Pedaloyclist		Making Left Turn	West East West North
6804 State Route 6805 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101		252.26 No E526280 252.26 No E911268	2016-03-13 20:13 Suspected Minor Injury 2019-04-13 11:38 Possible Injury	2 0 3 0	Passenger Car Pickup Panel Truck or Vanette under 10 000 lb.	Passenger Car Pickup Panel Truck or Vanette under 10 000 lb		Raining Overcast	Wet Curve & Grade	Dark-Street Lights On	From opposite direction - both moving - head-on	Going Straight Ahead Going Straight Ahead Going Straight Ahead Making Right Turn	Going Straight Ahead Going Straight Ahead	West East West North West East West
6806 State Route 6807 State Route	Claliam 1 Claliam 1 Claliam 1	101		252.27 No E923828 252.29 No 3202162	2019-05-24 17:00 No Apparent Injury 2019-01-13 00:55 No Apparent Injury	0 0 2 0	Plassenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast	Dry Curve & Grade Wet Curve & Grade	Daylight Dark-Street Lights On	Entering at angle From same direction - both going straight - both moving - rear-end Guide Post	Changing Lanes Going Straight Ahead	Going Straight Ahead Going Straight Ahead	East West East West East West
6808 State Route 6809 State Route 6810 State Route 6811 State Route	Cialiam 1 Cialiam 1 Cialiam 1 Cialiam 1	101		252.30 No E463263 252.30 No E475285	2015-09-18 15:48 No Apparent Injury 2015-10-24 13:28 No Apparent Injury	0 0 1 0	0 Passenger Car 0 Pickup,Panel Truck or Vanette under 10,000 lb		March 1 Committee of the Committee of	Clear or Partly Cloudy Raining	Dry Curve & Grade Wet Curve & Grade	Daylight Daylight	Guardrail - Face From same direction - all others	Going Straight Ahead	Going Straight Ahead	East West East West East West East
6810 State Route 6811 State Route		101		252.30 No E475285 252.32 No E378002 252.33 No E813012	2014-11-21 02:05 Suspected Minor Injury 2018-06-21 17:00 Dead on Arrival	2 0 2 0	U Passenger Car O Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Motorcycle O Passenger Car O Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Dry Curve & Grade Dry Curve & Grade	Dark-No Street Lights Daylight	Guardrai - Face From same direction - all others From opposite direction - both going straight - sideswipe Vehicle overtured From opposite direction - both going straight - sideswipe Vehicle overtured From opposite direction - all others Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead Going Straight Ahead Going Straight Ahead	West East East West East West East
6812 State Route 6813 State Route	Cialiam 1 Cialiam 1	101		252.35 No E936007 252.36 No E804734 252.37 No E999039	2018-06-03 07:49 No Apparent Injury	3 0 2 0	0 Passenger Car	Prokup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast Clear or Partly Cloudy	Dry Straight & Grade	Daylight Daylight	From opposite direction - all others Vehicle Strikes Deer Mond Size Deer	Going Straight Ahead Going Straight Ahead	Going Straight Ahead	East West East East West East
6815 State Route 6816 State Poute	Cialiam 1 Cialiam 1 Cialiam 1	101 101 101		252.37 No E999028 252.44 No E861015 252.56 No E931296	2018-11-01 13:45 No Apparent Injury 2019-06-15 16:27 No Apparent Injury	0 0 2 0	O Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast Clear or Partly Cloudy	Wet Curve & Grade Dry Straight & Conde	Daylight Daylight	Vehicle Strikes Deer Wood Sign Pout From same direction - both going straight - both moving - sideswipe From same direction - both going straight - both moving - sideswipe Vehicle Strikes Deer	Going Straight Ahead Changing Lanes Changing Lanes	Going Straight Ahead Going Straight Ahead	East West East West Fact Ward
6817 State Route 6818 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101		252.56 No E482473 252.62 No E605036	2015-10-23 12:52 No Apparent Injury 2016-10-09 18:56 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	The state of the s	Not at intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Grade Wet Straight & Grade	Daylight Dark-No Street Linhte	Vehicle Strikes Deer Earth Bank or Ledge	Changing Lanes Going Straight Ahead Changing Lanes	and analysis remaind	West East Yeest West East
6819 State Route 6820 State Route	Cialiam 1 Cialiam 1	101		252.63 No 2766897 252.65 No E539345	2015-05-24 18:32 No Apparent Injury 2016-03-11 20:30 No Apparent Injury	0 0 1 0	Pickup, Parel Truck or Vaneste under 10,000 to Plackup Parel Truck or Vaneste under 10,000 to Passenger Car Plackup Parel Truck or Vaneste under 10,000 to Passenger Car Plackup Parel Truck or Vaneste under 10,000 to Motorcycle Plackup Parel Truck or Vaneste under 10,000 to Plackup Parel Truck or Vaneste under 10,000 to Passenger Car Passenger Car Passenger Car Plackup Parel Truck or Vaneste under 10,000 to Plackup Parel Truck or Vaneste under 10,000 to Plackup Parel Truck or Vaneste under 10,000 to		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast	Dry Curve & Grade Wet Curve & Grade	Dark-No Street Lights Daylight Dark-No Street Lights	Vehicle Strikes Deer Vehicle Strikes Deer	Changing Lanes Going Straight Ahead Going Straight Ahead		West East East West
6821 State Route 6822 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101		252.73 No E536584 252.78 No E364880	2016-04-18 13:13 No Apparent Injury 2014-10-04 15:30 No Apparent Injury	0 0 1 0	Motorcycle Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade Dry Curve & Grade	Daylight Daylight	Vehicle overturned From same direction - both going straight - both moving - sideswipe	Going Straight Ahead Changing Lanes	Going Straight Ahead Going Straight Ahead	East West West East West East
6823 State Route 6824 State Route	Cialiam 1	101		252.80 No E394213 252.80 No E897845	2015-01-16 09:45 No Apparent Injury 2019-02-23 05:20 No Apparent Injury	0 0 2 0	0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Overcast	Wet Curve & Grade Dry Curve & Grade	Daylight Dark-No Street Lights	Vehicle overturned From same direction - both going straight - both moving - sideswipe From same direction - all others Guardrail - Face Guardrail - Face	Making U-Tum Going Straight Ahead	Going Straight Ahead	East East West West East
6825 State Route 6826 State Route 6827 State Route 6828 State Route	Clallam 1	101		252.81 No 3501108 252.91 No E693362	2016-04-15 06:03 No Apparent Injury 2017-07-18 22:02 Possible Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		At Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve in Sag Dry Curve & Level	Dawn Dark-No Street Lights	Concrete Barrier/Jersey Barrier - Face Guardrail - Face	Going Straight Ahead		East West East West
6827 State Route 6828 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101		252.92 No E698889 252.93 No 3202406	2017-07-24 16:18 No Apparent Injury 2016-11-16 15:00 No Apparent Injury	0 0 1 0	0 Passenger Car 0 Bus or Motor Stage 0 Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade Curve at Hillcrest	Dark-No Street Lights Daylight	Concrete Barner Jessey Barrier - Face Goardrail - Face Goardrail - Face Goardrail - Leading End From same direction - both going straight - one stopped - rear-end Taxer of Some Intersect.	Going Straight Ahead Making Right Turn Stopped for Traffic	Slowing	North West Vehicle Stopped Vehicle Stopped North South
6830 State Route 6831 State Route	Claliam 1 Claliam 1 Claliam 1	101		252.93 No E416019 252.93 No E727026 252.93 No E807604	2015-04-07 08:05 No Apparent Injury 2017-10-14 12:08 No Apparent Injury 2018-06-12 10:36 No Apparent Injury	0 0 1 0	Truck Tractor & Semi-Trailer Pickup Panel Truck or Vanette under 10 000 %	Pickup Panel Truck or Vanette under 10 000 %	At Intersection and Related At Intersection and Related At Intersection and Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy				Making Right Turn Making Right Turn Going Straight Ahead	Stopped at Signal or Stop Sign	East North North West North East Vehicle Stopped Vehicle Stopped
6832 State Route 6833 State Route	Cialiam 1	101		252.93 No E807604 252.96 No 3202022 253.00 No E652032	2019-05-15 16:19 No Apparent Injury 2017-03-15 07:45 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10 000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Overcast			Crash Cushions - Impact Attenuators From same direction - both going straight - one stopped - rear-end Vehicle Strikes Deer From same direction - both going straight - both moving - sideswipe	Making Right Turn Going Straight Ahead Going Straight Ahead Going Straight Ahead	Changing Lanes	East West East West East
6834 State Route 6835 State Route	Cialiam 1 Cialiam 1	101		253.00 No E761219 253.00 No E990422	2018-01-22 15:17 No Apparent Injury 2019-11-30 23:15 No Apparent Injury	0 0 2 0	O Passenger Car O Polsop Panel Truck or Vanente under 10,000 ib. O Polsop Panel Truck or Vanente under 10,000 ib. O Polsop Panel Truck or Vanente under 10,000 ib. O Passenger Car O Truck (Flathad Van.erb) O Plassenger Car O Passenger Car O Passenger Car O Passenger Car O Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Curve & Grade	Daylight Dark-Street Lights On	From same direction - both going attaight - both moving - sideswipe From same direction - both going attaight - both moving - sideswipe From same direction - both going attaight - both moving - sideswipe From same direction - both going attaight - both moving - sideswipe Vehicle Strikes Deer	Changing Lanes	Going Straight Ahead Going Straight Ahead	West East West East East West East West
6836 State Route 6837 State Route 6838 State Route	Cialiam 1 Cialiam 1	101		253.08 No E546950 253.08 No E725244	2016-05-25 07:36 No Apparent Injury 2017-05-25 22:10 No Apparent Injury	0 0 1 0	0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related At Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Grade	Daylight Dark-Street Lights On Daylight	Vehicle Strikes Deer Linear Curb	Changing Lanes Going Straight Ahead Going Straight Ahead Making Right Turn		West East West East
5535 State Route	ualiam 1	101	+	203.10 No E587635	2010-03-01 15:00 No Apparent Injury	U 0 2 0	ograssenger Car	r-assenger Car	rx intersection and Related	Overcast	ury Straight & Level	Daylight	Emering as angle	waxing Kignt Turn	Going Straight Ahead	ouus East West East

MV_DRIVER_CONTRIBUTING ORCUMSTANCE_1_UNT_1 Fallow Toe Closely Joi MAD Grant RVI to Vehicle Joi MAD Grant RVI to Vehicle LI LI LI LI LI LI LI LI LI L												
	G MV_DRIVER_CONTRIBUTING MV_DRIVER_CONTRIBUTING AGE CIRCUMSTANCE_2_UNIT_1 CIRCUMSTANCE_3_UNIT_1 INIT_1 INIT_1	ER MV_DRIVER	MV_DRIVER_CONTRIBUTING MV_DRIVER_CONTRIBUTING MV_DRIVER_CONTRIBUTING	MV_DRIVER	MV_DRIVER BICYCLIST_CONTRIBUTING BICYCLIST_CONTRIBUTING	BICYCLIST_CONTRIBUTING CIRCUMSTANCE_3_UNIT_1 BICYCLIST AGE UNIT_1	T BICYCLIST BICYCLIST_CONTRIBUTING BICYCLIST_CONTRIBUTING	BICYCLIST_CONTRIBUTING BICYCL	CLIST B	BICYCLIST PEDESTRIAN_CONTRIBUTING PEDESTRIAN_CONTRIBUTING	PEDESTRIAN_CONTRIBUTING PEDESTRIAN PEDESTRIAN CIRCUMSTANCE_3_UNIT_2 AGE_UNIT_2 GENDER_UNIT_2	FRST_IMPACT_LOCATION City_StCounty_Rd_&_Misc_2010_forwardSt
59 Follow Too Closely	GEI_1	GENDER UNIT_1		AGE_UNIT_2	GENDER UNIT_1 CIRCUMSTANCE_1_UNIT_1 CIRCUMSTANCE_2_UNIT_1	CIRCUMSTANCE_3_UNIT_1 AGE UNIT_1	GENDER UNIT_1 CIRCUMSTANCE_1_UNIT_2 CIRCUMSTANCE_2_UNIT_2	CIRCUMSTANCE_3_UNIT_2 AGE_UN	JNIT_2 GEN	IDER_UNIT_2 CIRCUMSTANCE_1_UNIT_2 CIRCUMSTANCE_2_UNIT_2	CIRCUMSTANCE_3_UNIT_2 AGE_UNIT_2 GENDER_UNIT_2	City_St_County_Rd_&_Misc_2010_forward,_St
	44	40 Male 86 Female 58 Female	None None	28	Male Male						L	Lane 2 Decreasing Milepost
Did Not Grant RW to Vehicle	56	58 Female	None	49	Male						L L	Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost
2 Inattention 3 Exceeding Reas, Safe Speed	Exceeding Reas. Safe Speed 2:	23 Male 31 Female	None None	23 43	Female Female						L L	Lane 1 Increasing Milepost Lane 1 Increasing Milepost
4 Under Influence of Alcohol 5 Exceeding Pear Safe Speed	21	26 Male 62 Male	None								F F	Past Right Shoulder Decreasing Milepost Lane 1 Increasing Milepost
6 Follow Too Closely	21	26 Male 62 Male 26 Female	None	53	Male Male						L	Lane 2 Increasing Miepost
7 Inattention 8 Follow Too Closely	Follow Too Closely 3:	33 Male 63 Male 41 Male	None None									Lane 1 Increasing Milepost Lane 1 Decreasing Milepost
Disregard Stop and Go Light Expending Pear Safe Speed	4	41 Male	None	49	Male Male						L	Lane 1 Decreasing Milepost Past Right Shoulder Increasing Milepost
71 None	2	24 Male	Follow Too Closely	73	Female	,						
171 None 172 Other Contributing Circ Not Listed 173 Disregard Stop and Go Light 174 Under Influence of Alcohol 175 Improve Turn/Merce	42	77 Female	Follow Too Closely None	34 24	Female Female Male						L L	Lane 2 Increasing Miepost Lane 1 Decreasing Miepost Lane 2 Increasing Miepost
74 Under Influence of Alcohol	Exceeding Reas. Safe Speed 56	50 Male	None								L	Lane 2 Increasing Milepost Past Right Shoulder Increasing Milepost
In Improper Turn/Merge Follow Too Closely Disregard Stop and Go Light Other Contributing Circ Not Listed	1	72 Female 17 Female 36 Female	None	48 53	Male							
77 Disregard Stop and Go Light 78 Other Contributing Circ Not Listed		36 Female	None	53	ale					Inattention	35 Female L	Lane 2 Decreasing Milepost Lane 1 Increasing Milepost
ouer common gric Not Listed long to Mone Other Contributing Circ Not Listed Other Contributing Circ Not Listed Follow Too Closely Aparently Asleen or Estimated	Exceeding Reas. Safe Speed 55	57 Female	None Other Contribution Circ Not Listed	18	Male Female						L L	Lane 2 Increasing Milepost
Other Contributing Circ Not Listed	77	77 Female	Other Contributing Circ Not Listed None None	59	Female Female Female							Lane 2 Increasing Milepost Lane 2 Decreasing Milepost
33 Apparently As leep or Fatigued	3	31 Male	None	32	emaie						E F	Lane 2 Increasing Milepost Past Right Shoulder Increasing Milepost
33 Apparently As leep or Fatigued 34 Inattention 35 Other Contributing Circ Not Listed 36 Exceeding Reas, Safe Speed	Inattention 88	85 Male 24 Male	Notes Date of Grant RW to Veholae Notes No	62 18	Jale Female						L L	Lane 1 Decreasing Milepost Lane 2 Increasing Milepost
36 Exceeding Reas. Safe Speed	Distractions Outside Vehicle 4	47 Female	None	51	Female Male Male	1						Lane 1 Increasing Milepost
86 Exceeding Reas. Safe Speed 37 Inattention 88 Follow Too Closely 99 Exceeding Reas. Safe Speed 100 Apparently Asleep or Fatigued 100 None 21 Did Not Grant RIW to Vehicle	Exceeding Reas. Safe Speed 2:	34 Male	None None	30	Male						L L	Lane 2 Increasing Milepost Lane 1 Decreasing Milepost
Speeding Reas. Safe Speed Apparently As leep or Fatigued	21	26 Male 17 Female	None	61	Female						L L	Lane 1 Decreasing Milepost Past Right Shoulder Increasing Milepost
None None	44	42 Male										Lane 1 Increasing Milepost
None None	Other Contributing Circ Not Listed 11	64 Female 18 Male	None None	74 41	Female Male						L L	Lane 1 Increasing Milepost Lane 1 Decreasing Milepost Intersecting Road Increasing Milepost
Mone Did Not Grant RW to Vehicle	Other Driver Distractions Inside Vehicle 44	18 Male 34 Female 44 Male	None		Female		Did Not Grant RW to Vehicle	18	Male		l l	Intersecting Road Increasing Milepost Lane 1 Increasing Milepost
6 Inattention	Exceeding Reas, Sale Speed 5.	bz Male										Lane 2 Decreasing Milepost
6 Inattention 7 Other Contributing Circ Not Listed 8 Distractions Outside Vehicle 9 Driver Not Distracted	21	26 Female 51 Female	None	60	Female Female						L	Lane 1 Increasing Miepost Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost
Driver Not Distracted Exceeding Reas. Safe Speed				36	Female		<u> </u>	<u> </u>	_+-			Lane 1 Decreasing Milepost Lane 1 Increasing Milepost
1 Did Not Grant RW to Vehicle	Expanding Page Safe Spand	44 Male	None Driver Not Distracted	36	Asie Male							
Exceeding Reas. Safe Speed Did Not Grant RW to Vehicle Inattention Did Not Grant RW to Vehicle	Exceeding Reas. Safe Speed 2:	67 Male	Note Dinzer Not Distracted Dinzer Not Dinzer Not Dinzer Note Dinzer	36 40 36								Lane 2 Decreasing Milepost Lane 1 Increasing Milepost
14 Inattention 15 Follow Too Closely 16 Under Influence of Alcohol 17 Dish Core Of Management (1997)	Distractions Outside Vehicle Follow Too Closely 20	20 Male 18 Female	None None	60	Female		+ +		$ \parallel$		L L	Lane 1 Increasing Milepost
16 Under Influence of Alcohol 17 Did Not Grant RW to Vehicle	Did Not Grant RW to Vehicle	28 Female	None None	55	Male Male Male							Lane 1 Increasing Milepost Lane 2 Increasing Milepost
	8	32 Male	TAU III	67	MIN					Other Contributing Circ Not Listed Inattention	18 Male	Lane 1 Increasing Milepost Lane 2 Decreasing Milepost
18 None 19 Did Not Grant RW to Vehicle 10 Inattention 11 None	Did Not Grant RW to Vehicle 8:	68 Male 82 Female	None Driver Not Distracted	57 57	ale Male				_+-			Lane 2 Increasing Milepost
1 None 2 Did Not Grant DW to Vehicle	44	49 Male	Did Not Great PW to Vakida							None	53 Male	Lane 2 Decreasing Milepost Lane 2 Decreasing Milepost Lane 1 Decreasing Milepost
3 Did Not Grant RW to Vehicle	4:	83 Male	None None	65 49	Male Male							Lane 1 Decreasing Milepost Lane 2 Decreasing Milepost
4 Apparently III 5 Did Not Grant RW to Vehicle	5.	57 Male 19 Female	None		Female		 		-+-			Past Right Shoulder Decreasing Mlepost Lane 1 Increasing Mlepost
6 Inattention	Follow Too Closely 33 Follow Too Closely 14	35 Male 16 Male	None None	53	Male							Lane 1 Decreasing Milepost
10 Instantion 10 Instantion 11 None 12 Did Not Grant RW to Vehicle 12 Did Not Grant RW to Vehicle 14 Apparently 18 Did Not Grant RW to Vehicle 14 Apparently 18 Did Not Grant RW to Vehicle 16 Instantion 17 Operating Hands-Free Cell Phone 18 Exceeding Reas. Sale Speed 19 Follow Too Closely 19 19 19 19 19 19 19 1	11 11 11 11 11 11 11 11 11 11 11 11 11	23 Male	Notes									Lane 2 Increasing Milepost Lane 1 Increasing Milepost
19 Follow Too Closely 20 Other Contributing Circ Not Listed	21 R	20 Male 84 Male	None None	65 29	Fornale Maile Maile				$-\mathbb{F}$			Lane 1 Increasing Milepost Lane 1 Decreasing Milepost
21 Follow Too Closely	15	19 Male	None	45	Asie							Lane 1 Decreasing Milepost
None Exceeding Reas, Safe Speed	7.2	24 Female	None	65 41	Male Male Female							Lane 2 Increasing Milepost Left Turn Lane Increasing Milepost Left Turn Lane Increasing Milepost
24 Other Contributing Circ Not Listed 25 Exceeding Reas, Safe Sneed	Unknown Distraction	26 Male	None None	21	remaie				$-\mathbb{F}$			Left Turn Lane Increasing Milepost Lane 2 Increasing Milepost
26 Exceeding Reas. Safe Speed	11	18 Female 27 Male 60 Female	None None	61	Male							_ane 2 Increasing Milepost
Cher Contributing Circ Not Listed Hollow Too Closely Hollow Too Closely None Sand Speed Hollow Too Closely Exceeding Reas. Safe Speed Hollow Too Circ Not Listed Exceeding Reas. Safe Speed Exceeding Reas. Safe Speed Exceeding Reas. Safe Speed Exceeding Reas. Safe Speed Too Cher Contributing Circ Not Listed More Contributing Circ Not Listed Hollow Too Cher Contributing Circ Not Listed More Contributing Circ Not Listed Hollow Circ Safe Speed Hollow Circ Safe S	21	60 Female	None								L	Lane 2 Increasing Miepost Lane 3 Increasing Miepost Lane 2 Decreasing Miepost
		21 Male	None Follow Too Closely Inattention Distractions Outside Vehicle	62	Female				$ \parallel$		li li	Intersecting Road Decreasing Milepost Intersecting Road Decreasing Milepost
None Inattention Other Contributing Circ Not Listed	Distractions Outside Vehicle 24	24 Male	Politor too closely illiamentori Distractions dusing venture Driver Not Distracted	39	Male Female							Lane 1 Decreasing Milepost
32 Other Contributing Circ Not Listed 33 Driver Interacting with Passengers, Anim	inim 4	49 Male		46 30	Female Female						L L	Lane 3 Increasing Milepost Lane 2 Increasing Milepost
Other Contributing Circ Not Listed Driver Interacting with Passengers, Anir Cother Contributing Circ Not Listed	Inim Follow Too Closely 51	49 Male 58 Female 34 Male 76 Female	None None	53	Fornale Maile Male		+ +					Lane 3 Increasing Milepost Lane 3 Increasing Milepost Lane 2 Increasing Milepost
Other Contributing Circ Not Listed	71	76 Female	None Days No Districted	64	Asie							ane 2 Increasing Milepost
	6.	24 Female	Doner No. Detracted No.	62	Female Female						L	Lett Turn Lane Increasing Milepost Lane 3 Decreasing Milepost
39 Operating Defective Equipment 40 Did Not Grant RW to Vehicle	22	28 Male 74 Male	Driver Not Distracted None	61 40	Male Male				$\neg \top$			Lane 2 Increasing Miepost Lane 1 Decreasing Miepost Left Turn Lane Increasing Miepost Left Turn Lane Increasing Miepost
Follow Too Closely	22	22 Male	None	29	Male							_eftTurn Lane Increasing Milepost
43 Exceeding Reas, Safe Speed	172 172 173 173	72 Female 22 Male	Driver Not Distracted	60 68	Female Male							
43 Exceeding Reas. Safe Speed 44 Disregard Stop and Go Light 45 Inattention	Inattention 51 Improper Backing 49	58 Male 49 Male	Oriner Not Dis tracted Driver Not Dis tracted Driver Not Dis tracted Driver Not Distracted	35 26	Maie Maie Maie						L	Left Turn Lane Increasing Milopost Lane 3 Increasing Milopost Innesecing Road Decreasing Milepost
46 Follow Too Closely	Follow Top Cloraby	70 Female	None	59	Male							Intersecting Road Decreasing Milepost
16 Follow Too Closely 17 Inattention 18 Other Driver Distractions Inside Vehicle 19 Disregard Stop and Go Light	Follow Too Closely 7: cle Distractions Outside Vehicle Follow Too Closely 7:	73 Female 72 Female 57 Female	None	55 44	Female Male							Lane 2 Decreasing Miepost Lane 1 Decreasing Miepost Lane 3 Increasing Miepost
19 Disregard Stop and Go Light	Exceeding Reas. Safe Speed 3:	57 Female 37 Female	None None	81 50	fale Male						L. L.	Lane 3 Increasing Milepost Lane 2 Increasing Milepost
Disregate sup and GO Eight Disregate Sup and GO Eight Inattention Apparently As leep or Fatigued Follow Too Closely None	2	21 Male	None None	44	Asie						L.	ane 1 Decreasing Milepost
53 Follow Too Closely	66	21 Male 41 Male 66 Male	None	70	Male Male Male						L L	Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost Lane 2 Decreasing Miepost
Sellow 180 Closery Hone Ho			Follow Too Closely		Male						L	Lane 3 Decreasing Milepost Lane 1 Decreasing Milepost
56 Follow Too Closely	Inattention 79	63 Male 75 Male 71 Female	None		Female						L	Lane 1 Decreasing Milepost Lane 1 Increasing Milepost
58 Did Not Grant RW to Vehicle			None	66	Female							
50 Other Contributing Circ Not Listed Apparently Asleep or Fatigued	Inattention Did Not Grant RW to Vehicle 6:	63 Female 21 Female	None None	45 51	Male Female						L	Lane 2 Increasing Milepost Lane 1 Decreasing Milepost Past Right Shoulder Decreasing Milepost
Inattention			D. M. C DWG M. C.								F	Past Right Shoulder Decreasing Milepost Intersecting Road Decreasing Milepost
53 Follow Too Closely	33	33 Female	None	38	dale							Lane 2 Increasing Milepost
Follow Too Closely Inattention	Did Not Grant RW to Vehicle 7'	71 Female	None Driver Not Distracted	35 61	emale Pemale						L L	Lane 3 Decreasing Milepost Lane 2 Increasing Milepost
11 Inatiention	8.8	83 Female 64 Female	Del Nati Casta RIVI to Vehicle None None Done Hot Distanced Jones Hot Distanced None None None None	31 61	Maile Maile Fermale Fermale Mule Mule Mule Mule Mule Mule Mule Mu		+ -					Lane 1 increasing Milepost Lane 1 increasing Milepost
inattention	6-	22 Male 77 Male		01							F	Past Right Shoulder Increasing Milepost
mproper rum/Merge Under Influence of Alcohol	Other Contributing Circ Not Listed 18	18 Male	None	20 45	AND .		 		- 1			
71 Operating Defective Equipment 72 Operating Defective Equipment	33	35 Male	None	.49	Male .							Lane 2 Increasing Milepost Lane 2 Increasing Milepost
73 Apparently Asleep or Fatigued		73 Male										Lane 2 Increasing Milepost Lane 2 Increasing Milepost
	2	73 Male 24 Male	1		Maie						F L	Laine z increasing wiepors Laine z increasing Miepors Laine 1 Chreasing Miepors Past Right Shoulder Increasing Miepost Laine 1 Decreasing Miepost Past Right Shoulder Decreasing Miepost
75 None	4	35 Male 73 Male 24 Male 65 Female 44 Female		64								Land 2 Romaning Milepost Land 2 Romaning Milepost Land 2 Romaning Milepost Land 2 Romaning Milepost Land 1 Romaning Milepost Land 1 Romaning Milepost Land 1 Romaning Milepost Land 1 Romaning Milepost Land 2 Romaning Milepost
75 None Distractions Outside Vehicle 77 None	4	44 Female										Law 2 z Romaking Milegoot Para Bloth Shouled in Forestand Milegoot Laws 1 Romaking Milegoot Laws 1 Romaking Milegoot Laws 2 Romaking Milegoot Laws 2 Romaking Milegoot Laws 2 Romaking Milegoot Laws 2 Romaking Milegoot Milegoot Laws 2 Romaking Milegoot Mileg
Dist Mold Stant RVV to Webside Manteriolo	44 42 22	30 Female 42 Male 23 Male	None	64 69	Male Female							Land 2 Jordistang Malipoot All Part Right Shoulder Increasing Malipoot Land 1 Decreasing Malipoot Land 1 Decreasing Malipoot Land 2 Decreasing Malipoot Land 1 Decreasing Malipoot
	44 42 22	30 Female 42 Male 23 Male	None	64 69	Mole							Land a American Angles Commission (Milegoet Land 1 December 1 December 1 December 2 Dece
10 Exceeding Reas. Safe Speed 11 Follow Too Closely	2 2 2 3 3 3 3 5 2 2 2 2 3 3 3 3 3 3 3 3	30 Female 42 Male 23 Male	None	64 69	Mole							Land a American Angles Commission (Milegoet Land 1 December 1 December 1 December 2 Dece
10 Exceeding Reas. Safe Speed 11 Follow Too Closely	2 2 2 3 3 3 3 5 2 2 2 2 3 3 3 3 3 3 3 3	44 remaie 30 Female 42 Male 23 Male 20 Male 34 Female 24 Female 41 Male 53 Female	None None None None None	69 68 82 21	Mole Female Mole Mole Female							Land 2 inchasting Millipools All Part Right Shoulder increasing Millipools Land 1 Decreasing Millipool Land 2 Decreasing Millipool Land 1 Decreasing Millipool Land 2 Decreasing Millipool Land 3 Decreasing Millipool Land 2 Decreasing Millipool Land 3 Decreasing Millipool Land 4 Dec
Exceeding Reas. Safe Speed Follow Too Closely Under Influence of Alcohol Other Contributing Circ Not Listed Other Contributing Circ Not Listed None	4.4 2.2 2.3 2.4 2.5 3.6 4.6 5.6 6.6	44 remaie 30 Female 42 Male 23 Male 20 Male 34 Female 24 Female 41 Male 53 Female	None None None None None	69 68 82 21	Maio Fornale Maio Malo Malo							Land 2 inchasting Millipools All Part Right Shoulder increasing Millipools Land 1 Decreasing Millipool Land 2 Decreasing Millipool Land 1 Decreasing Millipool Land 2 Decreasing Millipool Land 3 Decreasing Millipool Land 2 Decreasing Millipool Land 3 Decreasing Millipool Land 4 Dec
Exceeding Reas. Safe Speed Follow Too Closely Under Influence of Alcohol Other Contributing Circ Not Listed Other Contributing Circ Not Listed None	4.4 2.2 2.3 2.4 2.5 3.6 4.6 5.6 6.6	44 Female 42 Male 42 Male 23 Male 20 Male 34 Female 44 Female 45 Female 47 Male 47 Male 47 Female 47 Male 48 Female 49 Female 41 Male 41 Female 41 Male 42 Female 43 Female 44 Male 45 Female 46 Female 47 Female	None None None None None None None None None	69 68 82 21	Mole Female Mole Mole Female							Land a Zendeskerg Melipolit Land Televisiskerg Melipolit Land 1 Decreasing Melipolit Land 1 Decreasing Melipolit Land 2 Decreasing Melipolit Land 1 Romaniang Melipolit Land 2 Decreasing Melipolit Land 1 Decreasing Melipolit Land 1 Decreasing Melipolit Land 1 Decreasing Melipolit Land 1 Land Land Land Land Land Land Land Land
Exceeding Reas. Safe Speed Follow Too Closely Under Influence of Alcohol Other Contributing Circ Not Listed Other Contributing Circ Not Listed None	4.4 2.2 2.3 2.4 2.5 3.6 4.6 5.6 6.6	44 Female 42 Male 42 Male 23 Male 20 Male 34 Female 44 Female 45 Female 47 Male 47 Male 47 Female 47 Male 48 Female 49 Female 41 Male 41 Female 41 Male 42 Female 43 Female 44 Male 45 Female 46 Female 47 Female	None None None None None None None None None	64 69 68 82 21	Mole Female Mole Mole Female Female							Land a Zendesking Milegoot Amerikan (1997) Am
Exceeding Reas. Safe Speed Follow Too Closely Under Influence of Alcohol Other Contributing Circ Not Listed Other Contributing Circ Not Listed None	4.4 2.2 2.3 2.4 2.5 3.6 4.6 5.6 6.6	44 Female 42 Male 42 Male 23 Male 20 Male 34 Female 44 Female 45 Female 47 Male 47 Male 47 Female 47 Male 48 Female 49 Female 41 Male 41 Female 41 Male 42 Female 43 Female 44 Male 45 Female 46 Female 47 Female	None None None None None None None None None	64 69 68 82 21	Malair Fernale Malair Malair Fernale							Land a Zendesking Milegoot Amerikan (1997) Am
Exceeding Reas. Safe Speed Follow Too Closely Under Influence of Alcohol Other Contributing Circ Not Listed Other Contributing Circ Not Listed None	4.4 2.2 2.3 2.4 2.5 3.6 4.6 5.6 6.6	44 Female 42 Male 42 Male 23 Male 20 Male 24 Female 44 Female 44 Female 44 Female 44 Female 41 Male 53 Female 41 Male 54 Female 42 Male 43 Male 54 Female 44 Male 54 Female 54 Female 54 Male 54 Male 54 Male 54 Male 54 Male 55 Male 56 Male	None	64 69 68 82 21 21	Malair Fernala Malair Fernala Malair Fernala							Land a American September 1 Land 1 December 1 Land 2 December 1 Land 2 December 1 Land 2 December 1 Land 1 Land 1
Exceeding Reas. Safe Speed Follow Too Closely Under Influence of Alcohol Other Contributing Circ Not Listed Other Contributing Circ Not Listed None	4.4 2.2 2.3 2.4 2.5 3.6 4.6 5.6 6.6	44 Female 42 Male 42 Male 23 Male 20 Male 24 Female 44 Female 44 Female 44 Female 44 Female 41 Male 53 Female 41 Male 54 Female 42 Male 43 Male 54 Female 44 Male 54 Female 54 Female 54 Male 54 Male 54 Male 54 Male 54 Male 55 Male 56 Male	None	64 69 68 82 21 21	Malair Fernala Malair Fernala Malair Fernala							Land a American September 1 Land 1 December 1 Land 2 December 1 Land 2 December 1 Land 2 December 1 Land 1 Land 1
Exceeding Reas. Safe Speed Follow Too Closely Under Influence of Alcohol Other Contributing Circ Not Listed Other Contributing Circ Not Listed None	4.4 2.2 2.3 2.4 2.5 3.6 4.6 5.6 6.6	44 Female 42 Male 30 Female 42 Male 23 Male 20 Male 30 Female 42 Male 20 Male 31 Male 42 Female 44 Female 47 Female 47 Female 47 Female 47 Female 47 Female 48 Male 48 Male 49 Male 40 Male 40 Male 40 Male 41 Male 42 Female 43 Female 44 Male 45 Male 46 Male 47 Male 48 Female 48 Female 48 Female 49 Male 49 Male 40 Male	None	64 69 68 82 21 21	Malaire Fermale Malaire Mal							Land a Jenestania Milepost Land 1 Personaling Milepost Land 1 Dermaining Milepost Land 1 Milepost Land 2 Dermaining Milepost Land 2 Dermaining Milepost Land 1 Dermaining Milepost Land 2 Land 2 Mile
Executing Pleas, Sale Speed Executing Pleas, Sale Speed Under Influence of Alcohol Under Combination Chi RU Listed Son Combination Chi RU Listed None Combination Chi RU Listed None Account of Alcohol None Apparently Revised Ruthol None Son Combination Chi Ruthol Son Combination Child Son	1	44 Female 42 Male 30 Female 42 Male 23 Male 20 Male 30 Female 42 Male 20 Male 31 Male 42 Female 44 Female 47 Female 47 Female 47 Female 47 Female 47 Female 48 Male 48 Male 49 Male 40 Male 40 Male 40 Male 41 Male 42 Female 43 Female 44 Male 45 Male 46 Male 47 Male 48 Female 48 Female 48 Female 49 Male 49 Male 40 Male	None	64 69 69 68 82 21 21 69 67 67 59	Malaire Fermale Malaire Mal							Land a Zendesking Milegoot Amerika Standard Milegoot Amerika Milegoot Am
Executing Places, Sale Speed Fattor Too Control Places Under Enterton of Archive Under Enterton of Archive Control Places Control Places None	1	44 Female 42 Male 30 Female 42 Male 23 Male 20 Male 30 Female 42 Male 20 Male 31 Male 42 Female 44 Female 47 Female 47 Female 47 Female 47 Female 47 Female 48 Male 48 Male 49 Male 40 Male 40 Male 40 Male 41 Male 42 Female 43 Female 44 Male 45 Male 46 Male 47 Male 48 Female 48 Female 48 Female 49 Male 49 Male 40 Male	None	64 69 68 82 2 21 21 69 67 67 59 31 31	Malaire Fermale Malaire Mal							Land a Zendesking Milegoot Amerika Standard Milegoot Amerika Milegoot Am
Executing Places, Sale Speed Fattor Too Control Places Under Enterton of Archive Under Enterton of Archive Control Places Control Places None	1	44 Female 43 Female 44 Female 45 Female 46 Female 47 Male 48 Female 48 Female 49 Male 40 Male 40 Male 40 Male 40 Male 41 Male 51 Female 41 Male 52 Male 64 Male 64 Male 64 Male 65 Female 65 Female 66 Female 66 Female 66 Female	None	64 69 68 82 21 21 69 67 67 59 31 32	Malair Fernale Malair Fernale Malair Fernale Malair Fernale Malair Fernale Fernale Fernale							Land 2 Americaning Milepools Land 1 December 1 Land 1 December 2 Land 1 December 3 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 2 Land 3 Land 1 Land 3 Land 1 Land 3 Land 4 Land 3 Land 4 Land
Executing Places, Sale Speed Fattor Too Control Places Under Enterton of Archive Under Enterton of Archive Control Places Control Places None	1	44 Female 30 Female 31 Female 32 Main 32 Female 33 Female 34 Female 34 Female 35 Female 36 Female 36 Female 37 Female 38 Female 38 Female 38 Female 38 Female 39 Main 30 Main 40 Main 41 Main 42 Main 43 Main 44 Main 45 Main 46 Main 47 Main 48 Main 48 Main 48 Main 48 Female	None	64 69 68 82 21 21 69 67 67 59 31 32	Malair Formale Malair Formale Malair Formale Malair Formale Malair Formale Formale Formale							Land 2 Americaning Milepools Land 1 December 1 Land 1 December 2 Land 1 December 3 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land 1 Land
Executing Places, Sale Speed Fattor Too Control Places Under Enterton of Archive Under Enterton of Archive Control Places Control Places None	1	34, Femile 39, Femile 39, Femile 39, Femile 31, Femile	None None None None None None None None	64 69 68 82 21 21 69 67, 59 31 11 23	Male Female Male Male Male Male Male Male Male M							Land 2 Informating Welgoot Jan 1 Dermaning Welgoot Lann 1 Ment Commaning Welgoot Lann 1 Ment Lann 1
Execution, Ress. Sale Speed Feltow Too Continues of Arobot Under Information of Arobot Under Information of Arobot Other Scottenson of Arobot Arobot None Arobot Ar	1	3.0 Jemila 3.1 J	Nome	64 69 68 82 21 21 69 67 67 77 77	Moles Fernale Moles Mol		None					Land 2 Americaning Weighbors Land 1 December 1 December 2 December 3 Decembe
Executing Places, Sale Speed Fattor Too Control Places Under Enterton of Archive Under Enterton of Archive Control Places Control Places None	1	34, Fernite 35, Fernite 36, Fernite 37, Male 48, Fernite 48, Fernite 38, Fernite 39, Fernite 30, Ferni	None None None None None None None None	64 69 68 82 21 21 69 67 67 77 77	Male Female Male Male Male Male Male Male Male M		None					Land 2 increasing Mispoot Land 1 Containing Mispoot Land 2 Containing Mispoot Land 1 Containing Mispoot Land 1 Containing Mispoot Land 1 Containing Mispoot Land 1 Containing Mispoot Land 2 Londaining Mispoot Land 2 Containing
Executing Places, Sale Speed Fattor Too Control Places Under Enterton of Archive Under Enterton of Archive Control Places Control Places None	1	34, Fernite 35, Fernite 36, Fernite 37, Male 48, Fernite 48, Fernite 38, Fernite 39, Fernite 30, Ferni	None None None None None None None None	64 69 68 82 2 1 21 69 69 67 77 59 31 23 23 24 48	Maise Fornale Mole Mole Mole Mole Mole Mole Mole Mole		None					Land 2 Americaning Weighoot American To Decreasing Weighoot Lane 1 Decreasing Weighoot Lane 1 Decreasing Weighoot Lane 1 Decreasing Weighoot American Lane 1 Decreasing Weighoot Lane 1 Memoricaning Weighoot Lane 2 Memoricaning Weighoot Paul Right Shoulder bereasing Weighoot Lane 1 Memoricaning Weighoot Paul Right Shoulder bereasing Weighoot Lane 1 Memoricaning Weighoot Lane 2 Memori
Execution, Ress. Sale Speed Feltow Too Continues of Arobot Under Information of Arobot Under Information of Arobot Other Scottenson of Arobot Arobot None Arobot Ar	1	34, Fernite 35, Fernite 36, Fernite 37, Male 48, Fernite 48, Fernite 38, Fernite 39, Fernite 30, Ferni	None None None None None None None None	64 69 69 68 82 2 2 1 21 69 69 67 77 59 31 23 30 45 45 45 45 45 45 45 45 45 45 45 45 45	Abbie Fernale Moles Moles Moles Moles Moles Fernale Fernale Moles Fernale Fernale Moles Fernale Fernale Fernale Fernale Moles Fernale		None					Land 2 Americaning Weighoot American To Decreasing Weighoot Lane 1 Decreasing Weighoot Lane 1 Decreasing Weighoot Lane 1 Decreasing Weighoot American Lane 1 Decreasing Weighoot Lane 1 Memoricaning Weighoot Lane 2 Memoricaning Weighoot Paul Right Shoulder bereasing Weighoot Lane 1 Memoricaning Weighoot Paul Right Shoulder bereasing Weighoot Lane 1 Memoricaning Weighoot Lane 2 Memori
Executing Places, Sale Speed Fattor Too Control Places Under Enterton of Archive Under Enterton of Archive Control Places Control Places None	1	3-0 Femina	None	64 69 68 82 21 21 31 31 23 30 51 48 88 32 27 77	Maise Fornale Mole Mole Mole Mole Mole Mole Mole Mole		None	60				Land 2 Membershop Milegoot Land 1 Dermaking Milegoot Land 1 Dermaking Milegoot Land 1 Dermaking Milegoot Land 1 Dermaking Milegoot Land 1 Membershop Milegoot Land 2 Membershop Milegoot Land 1 Membershop Milegoot Land 2
Executing Places, Sale Speed Fattor Too Control Places Under Enterton of Archive Under Enterton of Archive Control Places Control Places None	1	3-0 Femina	None	64 69 69 62 21 21 69 69 69 9 9 9 9 9 23 23 23 23 23 23 23 23 24 44 45 46 46 46 46 46 46 46 46 46 46 46 46 46	Males Fernale Moles Mole		None	60	Male			Land of American State (1997) Land of Lorentaring Milegoot Land 1 Decreasing Milegoot Land 1 Decreasing Milegoot Land 1 Decreasing Milegoot Land 1 Lorentaring Milegoot Land 2 Lorentaring Milegoot Land 3 Lorentaring Milegoot Land 1 Lorentaring Milegoot Land 1 Lorentaring Milegoot Land 1 Lorentaring Milegoot Land 2 Lorentaring Milegoot Land 2 Lorentaring Milegoot Land 2 Lorentaring Milegoot Land 3 Lorentaring Milegoot Land 2 Lorentari
Executing Places, Sale Speed Fattor Too Control Places Under Enterton of Archive Under Enterton of Archive Control Places Control Places None	1	3-0 Femina	None	64 69 69 62 21 21 69 69 69 9 9 9 9 9 23 23 23 23 23 23 23 23 24 44 45 46 46 46 46 46 46 46 46 46 46 46 46 46	Males Fernale Moles Mole		None	60	Male			Land of American State (1997) Land of Lorentaring Milegoot Land 1 Decreasing Milegoot Land 1 Decreasing Milegoot Land 1 Decreasing Milegoot Land 1 Lorentaring Milegoot Land 2 Lorentaring Milegoot Land 3 Lorentaring Milegoot Land 1 Lorentaring Milegoot Land 1 Lorentaring Milegoot Land 1 Lorentaring Milegoot Land 2 Lorentaring Milegoot Land 2 Lorentaring Milegoot Land 2 Lorentaring Milegoot Land 3 Lorentaring Milegoot Land 2 Lorentari
Execution, Ress. Sale Speed Feltow Too Continues of Arobot Under Information of Arobot Under Information of Arobot Other Scottenson of Arobot Arobot None Arobot Ar	1		None None None None None None None None	64 69 69 62 21 21 69 69 69 9 9 9 9 9 23 23 23 23 23 23 23 23 24 44 45 46 46 46 46 46 46 46 46 46 46 46 46 46	Makie Fernate Molie Mo		None	60	Male			Land 2 Interesting Weighbors 2
Executing Reas. Sate Speed Fiction Too Conding Reas. Sate Speed Fiction Too Conding Reas. Under Influence of Airchol Under Influence of Airchol Owner Constitution, Circ Nat Land Owner Constitution, Circ Nat Land None Land Land Land Land Land Land Land Land	1		None None None None None None None None	64 69 69 62 21 21 69 69 69 9 9 9 9 9 23 23 23 23 23 23 23 23 24 44 45 46 46 46 46 46 46 46 46 46 46 46 46 46	Males Fernale Moles Mole		None	60	Male			Land 2 Americaning Mispoot Land 1 Commission Mispoot Land 2 Commission Mispoot Land 2 Commission Mispoot Land 2 Commission Mispoot Land 2 Commission Mispoot Land 1 Commission Mispoot Land 2 Commission
Execution, Ress. Sale Speed Feltow Too Continues of Arobot Under Information of Arobot Under Information of Arobot Other Scottenson of Arobot Arobot None Arobot Ar	1	3-0 Femina	None None None None None None None None	64 69 69 62 21 21 69 69 69 9 9 9 9 9 23 23 23 23 23 23 23 23 24 44 45 46 46 46 46 46 46 46 46 46 46 46 46 46	Makin Fernale Makin Maki		None	60	Male			Land 2 Americaning Weighoot Land 1 Decreasing Weighoot Land 1 Mericaning Weighoot Land 2 Mericaning Weighoot Land 1 Mericaning Weighoot Land 2 Mericaning Weighoot Land 1 Mericaning Weighoot L
Executing Reas. Sate Speed Fiction Too Conding Reas. Sate Speed Fiction Too Conding Reas. Under Influence of Airchol Under Influence of Airchol Owner Constitution, Circ Nat Land Owner Constitution, Circ Nat Land None Land Land Land Land Land Land Land Land	1	3-0 emiles 3-0 emiles 3-1 emiles	None	60 60 60 60 60 60 60 60 60 60 60 60 60 6	Mobile Fernale Mobile Mobile Mobile Fernale Mobile Fernale Mobile Fernale Mobile Fernale Mobile Fernale		None	60	Male			Land 2 Americaning Milepoot Land 1 December 1 December 2 December 3 December
Execution, Ress. Sale Speed Feltow Too Continues of Arobot Under Information of Arobot Under Information of Arobot Other Scottenson of Arobot Arobot None Arobot Ar	1	3-0 emilied 3-0 emilied 3-1 emilied 3-	None	60 60 60 60 60 60 60 60 60 60 60 60 60 6	Makin Fernale Makin Makin Makin Makin Makin Fernale Makin Fernale Makin Mak		None	60	Male			Land 2 Americaning Welspoort Land 1 Decreasing Welspoort Land 1 Americaning Welspoort Land 1 Memoricaning Welspoort Land 2 Decreasing Welspoort Land
Executing Reas. Sate Speed Fiction Too Conding Reas. Sate Speed Fiction Too Conding Reas. Under Influence of Airchol Under Influence of Airchol Owner Constitution, Circ Nat Land Owner Constitution, Circ Nat Land None Land Land Land Land Land Land Land Land	1	34 Jemis - 1 - 1 - 1 - 1 - 1 - 1 - 1 - 1 - 1 -	None None None None None None None None	60 60 60 60 60 60 60 60 60 60 60 60 60 6	Mobile Fernale Mobile Mobile Mobile Fernale Mobile Fernale Mobile Fernale Mobile Fernale Mobile Fernale		None	60	Male			Land 2 Americaning Weighoot Land 1 Decreasing Weighoot Land 1 Decreasing Weighoot Land 1 Decreasing Mepoet Land 1 Decreasing Mepoet Land 1 Decreasing Mepoet Land 1 Memority Memority Land 2 Memority Memority Land 1 Memority Land 2 Mem
Exceeding Reas, Sale Speed Follow Too Cloring, Reas, Sale Speed Follow Too Cloring, Chi No. Linder Influence of Alsohol Under Influence of Alsohol Under Influence of Alsohol None Linder Influence of Alsohol Legaranthi III Apparenthy III Apparenth III Apparenthy	1	34 Jemis - 1 - 1 - 1 - 1 - 1 - 1 - 1 - 1 - 1 -	None None None None None None None None	60 60 60 60 60 60 60 60 60 60 60 60 60 6	Mobile Fernale Mobile Mobile Mobile Fernale Mobile Fernale Mobile Fernale Mobile Fernale Mobile Fernale		None	60	Male			Land 2 Americaning Weighoot Land 1 Decreasing Weighoot Land 1 Decreasing Weighoot Land 1 Decreasing Mepoet Land 1 Decreasing Mepoet Land 1 Decreasing Mepoet Land 1 Memority Memority Land 2 Memority Memority Land 1 Memority Land 2 Mem
Exceeding Reas, Sale Speed Follow Too Cloring, Reas, Sale Speed Follow Too Cloring, Chi No. Linder Influence of Alsohol Under Influence of Alsohol Under Influence of Alsohol None Linder Influence of Alsohol Legaranthi III Apparenthy III Apparenth III Apparenthy	1	34 Jemis - 1 - 1 - 1 - 1 - 1 - 1 - 1 - 1 - 1 -	None None None None None None None None	60 60 60 60 60 60 60 60 60 60 60 60 60 6	Mobile Fernale Mobile Mobile Mobile Fernale Mobile Fernale Mobile Fernale Mobile Fernale Mobile Fernale		None	60	Male			Land 2 Americaning Milepools Land 1 Decreasing Milepool Land 2 Decreasing Milepool Land 1 Decreasing Milepool Land 2 Decreasing Milepool Land 1 Decreasing Milepool Land 2 Decreasing Milepool Land 1 Decreasing Milepool Land 1 Decreasing Milepool Land 2 Decreasing Milepool Land 3 Decreasing Milepool Land 4 Decreasing Milepool Land 4 Decreasing Milepool Land 4 Decreasing Milepool Land 5 Decreasing Milepool Land 5 Decreasing Milepool Land 6 Decreasing Milepool Land 1 Decreasing Milepool Land 2 Decreasing Milepool Land 1 Decreasing Milepool Land 2 Decreasing Milepool Land 3 Decreasing Milepool Land 3 Decrea
Executing Places, Sale Speed Fattor Too Control Places Tool Too Too Too Too Too Too Too Too	1	34 Jemis - 1 - 1 - 1 - 1 - 1 - 1 - 1 - 1 - 1 -	None None None None None None None None	60 60 60 60 60 60 60 60 60 60 60 60 60 6	Mobile Fernale Mobile Mobile Mobile Fernale Mobile Fernale Mobile Fernale Mobile Fernale Mobile Fernale		None	60	Male			Land 2 Americaning Milepools Land 1 Decreasing Milepool Land 2 Decreasing Milepool Land 1 Decreasing Milepool Land 2 Decreasing Milepool Land 1 Decreasing Milepool Land 2 Decreasing Milepool Land 1 Decreasing Milepool Land 1 Decreasing Milepool Land 2 Decreasing Milepool Land 3 Decreasing Milepool Land 4 Decreasing Milepool Land 4 Decreasing Milepool Land 4 Decreasing Milepool Land 5 Decreasing Milepool Land 5 Decreasing Milepool Land 6 Decreasing Milepool Land 1 Decreasing Milepool Land 2 Decreasing Milepool Land 1 Decreasing Milepool Land 2 Decreasing Milepool Land 3 Decreasing Milepool Land 3 Decrea
Benedering Rases, Sale Speed Felore To Control of the State of Total On The State On The State of Total On The State On The State of Total On The State o	1	3-50 Femnile 3-50 Femnile 3-50 Femnile 3-50 Miles 3-50	None None None None None None None None	60 60 60 60 60 60 60 60 60 60 60 60 60 6	Mobile Fernale Mobile Mobile Mobile Fernale Mobile Fernale Mobile Fernale Mobile Fernale Mobile Fernale		None	60	Maio			Land 2 Membrashing Milipolat Land 1 Decreasing Milipolat Land 1 Membrashing Milipolat Land 2 Membrashing Milipolat Land 2 Membrashing Milipolat Land 2 Membrashing Milipolat Land 2 Decreasing Milipolat Land 2 Decreasing Milipolat Land 2 Decreasing Milipolat Land 2 Decreasing Milipolat Land 1 Decreasing Milipolat Land 2 Decreasing Milipolat Land 1 Decreasing Milipolat Land 2 Lecensary Milipolat Land 1 Decreasing Milipolat Land 2 Lecensary Milipolat Land 2 Lecensary Milipolat Land 2 Lecensary Milipolat Land 2 Lecensary Milipolat Land 3 Lecensary Milipolat Land 1 Decreasing Milipolat Land 1 Decreasing Milipolat Land 2 Lecensary Milipolat Land 3 Lecensary Milipolat Land 3 Lecensary Mi
Benedering Rases, Sale Speed Felore To Control of the State of Total On The State On The State of Total On The State On The State of Total On The State o	1	3-50 Femnile 3-50 Femnile 3-50 Femnile 3-50 Miles 3-50	None None None None None None None None	60 60 60 60 60 60 60 60 60 60 60 60 60 6	Mobile Fernale Mobile Mobile Mobile Fernale Mobile Mobile Mobile Mobile Fernale Mobile Fernale Fernale Mobile Fernale Fernale Mobile Mobile Mobile Fernale Fernale Fernale Mobile		None	60	Maio			Land 2 Interesting Weighbors Land 1 Decreasing Meighbors Land 1 Lonesang Meighbors Land 1 Lonesang Meighbors Land 1 Lonesang Meighbors Land 1 Lonesang Meighbors Land 2 Decreasing Meighbors Land 2 Decreasing Meighbors Land 2 Lonesang Meighbors Land 3 Lonesang Meighbors Land 3 Lonesang Meighbors Land 2 Lonesang Meighbors Land 3 Lonesang Meighbors Land 3 Lonesang Meighbors Land 4 Lonesang Meighbors Land 4 Lonesang Meighbors Land 1 Lonesang Meighbors Land 2 Lonesang Meighbors Land 1 Lonesang Meighbors Land 2 Lonesang Meighbors Land 2 Lonesang Meighbors Land 1 Lonesang Meighbors Land 2 Lonesang Meighbors Land 1 Lonesang Meighbors Land
Benedering Rases, Sale Speed Felore To Control of the State of Total On The State On The State of Total On The State On The State of Total On The State o	1	3-50 Femnile 3-50 Femnile 3-50 Femnile 3-50 Miles 3-50	None None None None None None None None	564 66 66 66 66 66 66 66 66 66 66 66 66 6	Makie Fernale Makie Makie Makie Makie Fernale Makie		None	60	Male			Land 2 Interesting Weighbors Land 1 Decreasing Meighbors Land 1 Lonesang Meighbors Land 1 Lonesang Meighbors Land 1 Lonesang Meighbors Land 1 Lonesang Meighbors Land 2 Decreasing Meighbors Land 2 Decreasing Meighbors Land 2 Lonesang Meighbors Land 3 Lonesang Meighbors Land 3 Lonesang Meighbors Land 2 Lonesang Meighbors Land 3 Lonesang Meighbors Land 3 Lonesang Meighbors Land 4 Lonesang Meighbors Land 4 Lonesang Meighbors Land 1 Lonesang Meighbors Land 2 Lonesang Meighbors Land 1 Lonesang Meighbors Land 2 Lonesang Meighbors Land 2 Lonesang Meighbors Land 1 Lonesang Meighbors Land 2 Lonesang Meighbors Land 1 Lonesang Meighbors Land
Exceeding Reas, Sale Speed Follow Too Cloring, Reas, Sale Speed Follow Too Cloring, Chi No. Londor Scheduler, Chi No. Londor Combination, Chi No. Londor Combination, Chi No. Londor Combination, Chi No. Londor Tellemon Ad Alborol Agent Market Sale Sale Sale Sale Sale Represently Allow Alborol Agentary Market Represently Allow of Alborol Represently Allow Represently Representl	4 4 2 2 2 2 2 2 2 2	3-0 Jemila 3-0 Jemila 3-1 J	None None None None None None None None	566 566 567 577 577 577 578 567 567 567 567 567 567 567 567 567 567	Abilia Fernate Male Male Fernate		None	60	Maio			Land 2 Americaning Milegoot Land 1 Decreasing Milegoot Land 1 Decreasing Milegoot Land 1 Decreasing Milegoot Land 1 Decreasing Milegoot Land 1 Milegoot Land 2 Milegoot Land 1 Milegoot Land 1 Milegoot Land 1 Milegoot Land 1 Milegoot Land 2 Milegoot Land 1 Milegoot Land 1 Milegoot Land 1 Milegoot Land 2 Milegoot Land 1 Milegoot Land 2 Milegoot Land 1 Milegoot Land 1 Milegoot Land 1 Milegoot Land 2 Milegoot Land 1 Milegoot Land 2 Milegoot Land 2 Milegoot Land 2 Milegoot Land 2 Milegoot Land 1 Milegoot Land 1 Milegoot Land 2 Milegoot Land 1 Milegoot Land 2 Milegoot L

OBJECTID WA STATE PLANE WA STATE PLANE SOUTH Y 2010_FORWARD 2010_FORWARD YEA		COL. COL.HUM SEVERITY FACTOR	co	COL COL_LIGHT SURFACE CONDITIONS ONDITIONS	COL LOCATION	COL STUDY AREA	IEXT IEXT	Y COL_HUMAN COI FACTOR_Decoded MVIII	Decoded Dec	TIONS Decoded Decoded		Intersection Area_Name Emphasis Area Depa
6660 938411.65 1023617.89 2018 6661 938422.94 1023614.1 2018	6 2 Vehicles:/Parties Rear-End 8 2 Vehicles:/Parties Turning 8 2 Vehicles:/Parties Sideswipe 6 2 Vehicles:/Parties Rear-End	Property Damage Only Follow Too Closely Property Damage Only Fail to Yield Property Damage Only Fail to Yield Property Damage Only Inattention	35:44 No	Dry Dark-Street Lights On Dry Daylight Dry Daylight Dry Daylight	At Intersection and Not Related At Driveway or Intersection At Intersection and Not Related Not at Intersection/Driveway/Roundabout	Straight & Level Straight & Level Straight & Level Straight & Level	No Vuinerable Road User Involved	Follow Too Closely Fail to Yield Fail to Yield Inattention	Dark-Street Lights On C Daylight C Daylight C Daylight C Daylight C	y At Intersection and Not Related y At Driveway or Intersection y At Intersection and Not Related y Not at Intersection/Driveway/Roundabout	Follow Too Closely Fail to Yield Fail to Yield Inattention	No No Yes No
6662 938485.67 1023613.3 2016 6663 938467.81 1023613.82 2017 6664 938474.69 1022813.44 2019 6665 938479.33 1023612.96 2016 6666 938579 1023612.42 1019 6667 1023612.86 2018 1023612.86 1023612.86	7 4 Vehicles/Parties Rear-End 9 1 Vehicle/Party Struck Object 9 2 Vehicles/Parties Rear-End 9 3 Vehicles/Parties Rear-End	Minor or Possible Injury Speeding Properly Damage Only Under Influence of Alcohol/Drugs/Medication Properly Damage Only Speeding Property Damage Only Follow Too Closely	25-34 Under Influence of Alcohol 25-34 55-64 No 25-34	Wet Dark-Street Lights On Dry Dark-Street Lights On Wet Dark-Street Lights On Dark-Street Lights On	At Driveway or Intersection	Straight & Level Straight & Level Straight & Level Straight & Level	No Vulnerable Road User Involved	Under Influence of Alcohol/Drugs/Medication Speeding	Dark-Street Lights On V Dark-Street Lights On D	et At Driveway or Intersection y Not at Intersection/Driveway/Roundabout tel Intersection Related but Not at Intersection y Intersection Related but Not at Intersection	Speeding Under Influence of Alcohol/Drugs/Medication Speeding Filler, Tan Cheek	No Yes No
6667 938629.89 1023611.12 2019	9 3 Verhides/Parties Rear-End 4 2 Verhides/Parties Rear-End 4 2 Verhides/Parties Rear-End 4 2 Verhides/Parties Rear-End 6 1 Verhides/Parties Angle 6 1 Verhides/Party Struck Object	Property Damage Only Speeding Property Damage Only Speeding	25-34 No	Dry Daylight Dry Daylight Dry Daylight Vet Daylight	Intersection Related but Not at Intersection At Driveway or Intersection At Driveway or Intersection	Straight & Level Curve & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Inattention Follow Too Closely Disregard Traffic Control Device/Sign	Daylight	y intersection Related but Not at Intersection y At Driveway or Intersection et At Driveway or Intersection y At Driveway or Intersection y At Driveway or Intersection	Inattention Follow Too Closely No Risky Driving Behaviors Involved	No No No
6671 938685.94 1023607.98 2017	7 12 Vehicles/Parties Rear-End		Under 25 No	Wet Daylight Dry Dark-Street Lights On Dry Daylight Dry Daylight	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+	None	Daylight E	ry At Driveway or Intersection ry At Driveway or Intersection	Speeding No Risky Driving Behaviors Involved	Yes No No
6675 938688.63 1023609.9 2015	5 2 Vehicles/Parties Struck Object	Property Damage Only Other/Unknown Minor or Possible Injury Dis regard Traffic Control Device/Sign Property Damage Only Property Damage Only Improper Turnal Only Imprope	55+ No	Dry Daylight Dry Daylight Dry Dawn/Dusk Dry Dawlight	At Driveway or Intersection	Curve & Grade Straight & Level Straight & Level Straight & Level	Older Road User 65+	Improper Turn/Merge	Dawn/Dusk D		No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved Follow Too Closely	No No Yes
6678 938681.53 1023609.55 2016 6679 938681.53 1023609.55 2016	5 2 Vehioles/Parties Rean-End 5 2 Vehioles/Parties Angle 6 2 Vehioles/Parties Pedestrian 6 2 Vehioles/Parties Rean-End	Property Damage Only Follow Too Closely Property Damage Only Disregard Traffic Control Device/Sign Mnor or Possible Injury Property Damage Only Inattention	35-44 No 35-44 No	Dry Daylight Wet Dark-No Street Lights/Street Lights Off Dry Dark-Street Lights On Dry Daylight	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Straight & Level Straight & Level Straight & Level	No Vulnerable Road User Involved Pedestrian No Vulnerable Road User Involved	Follow Too Closely Disregard Traffic Control Device/Sign Other Contributing Circ Not Listed Inattention	Daylight Dark-No Street Lights/Street Lights Off V Dark-No Street Lights On Dark-Street Lights On Daylight D	y At Driveway or Intersection	Follow Too Closely No Risky Driving Behaviors Involved Inattention Inattention	No No No
6680 938681.53 1023610.17 2016 6681 938681.53 1023609.54 2017 6682 938677.03 1023608.94 2016	6 2 Vehicles/Parties Rear-End 7 2 Vehicles/Parties Rear-End 6 2 Vehicles/Parties Rear-End 6 2 Vehicles/Parties Rear-End	Minor or Possible Injury Often/Unknown Minor or Possible Injury Often/Unknown Minor or Possible Injury Follow Too Closely	55+ No 55-64 No	Dry Daylight Dry Daylight Dry Daylight	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Straight & Level Straight & Level Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved	None Other Contributing Circ Not Listed Follow Too Closely	Daylight D Daylight D Daylight D Daylight D Dark-Street Lights On D	ny At Driveway or Intersection ny At Driveway or Intersection ny At Driveway or Intersection	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Follow Too Closely	No No No
6684 938681.53 1023609.55 2019 6685 938678.31 1023608.91 2019	7 1 Vehide/Party Struck Object 9 2 Vehides/Parties Rear-End 9 2 Vehides/Parties Rear-End 9 2 Vehides/Parties Angle	Suspected Serious Injury Property Damage Only Speeding	25:34 Apparently Asleep, Fatigued or III 25:34 S5+ No No No No No No No No	Dry Dark-Street Lights On Dry Daylight Dry Daylight Dry Daylight Dry Daylight	At Intersection and Not Related At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Curve & Level Curve & Level Straight & Level Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Apparently As leep or Fatigued Inattention Other Contributing Circ Not Listed Speeding	Daylight Daylight	ny At Intersection and Not Related At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Apparently Asleep, Fatigued or III Inatiention No Risky Driving Behaviors Involved Speeding	No No No
6687 938681.53 1023609.55 2019 6688 938744.9 1023611.44 2018	9 2 Vehicles/Parties Rear-End 8 2 Vehicles/Parties Rear-End	Property Damage Only Inattention Property Damage Only Follow Too Closely	25-34 No 25-34 No	Dry Daylight Dry Daylight	At Driveway or Intersection At Driveway or Intersection	Straight & Level Curve & Grade Straight & Level Curve & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Inattention Follow Too Closely	Daylight C	ry At Driveway or Intersection	Inattention Follow Too Closely Speeding	No No No
6692 938984.48 1023666.41 2016	7 2 Vehicles/Parties Rear-End 1 1 Vehicle/Party Struck Object 8 1 Vehicle/Party Animal 6 2 Vehicles/Parties Angle 2 Vehicles/Parties Angle 3 Vehicles/Parties Angle	Property Damage Only Speeding Property Damage Only Apparently Asleep, Fatigued or III Property Damage Only Other/Unknown Property Damage Only Fail to Yield	Junder 25 Apparently Asleep, Fatigued or III Under 25 35-44 No 55-64 No Junder 25 Under 25 Under 25 Under 26	Dry Daylight Dry Dark-Street Lights On Dry Dawn/Dusk Wet Dawn/Dusk	Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Speeding Apparently Asleep or Fatigued None Fail to Yield		y Intersection Related but Not at Intersection y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout et At Driveway or Intersection	Apparently Asleep, Fatigued or III No Risky Driving Behaviors Involved Fail to Yield	Yes No No
6692 938984.48 1023666.41 [2016 6693 938996.71 1023669.29 [2016 6694 938991.28 1023625.1 [2018 6695 939091.88 1023717.95 [2017 6696 939078.75 1023708.4 [2017	7 2 Vehicles:/Parties Sideswipe 8 2 Vehicles:/Parties Pedaloculist 7 3 Vehicles:/Parties Angle 4 1 Vehicle:/Party Overturned	Property Damage Only Under Influence of Alcohol/Drugs/Medication Minor or Possible Injury Minor or Possible Injury Fail to Yield Minor or Possible Injury Inattention	25-34 No 35-44 No	Dry Dark-No Street Lights/Street Lights Off Dry Daylight Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout	Curve & Level Straight & Level Straight & Grade Curve & Level	No Vulnerable Road User Involved Pedalcyclist No Vulnerable Road User Involved Motorcycle	Under Influence of Alcohol/Drugs/Medication None Fail to Yield Inattention	Daylight C	ny Not at Intersection/Driveway/Roundabout y At Driveway or Intersection y At Driveway or Intersection ny Not at Intersection/Driveway/Roundabout	Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved Fail to Yield Inattention	No No Yes
6696 939078.76 1023708.44 2014 6697 939105.84 1023721.82 2018 6698 939138.07 1023739.19 2017 6699 939117.44 1023724.12 2018	4 I Vehicle.Party Overtumed 8 4 Vehicles.Parties Sideswipe 7 2 Vehicles.Parties Rear-End 8 I Vehicle.Party Animal	Property Damage Only Other/Unknown Property Damage Only Distracted Driving Property Damage Only Other/Unknown	25-34 No 45-54 No	Other Daylight Dry Daylight Dry Dark-Street Lights On	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Curve & Level Straight & Level Curve & Level	No Vuinerable Road User Involved No Vuinerable Road User Involved No Vuinerable Road User Involved	Inattention Other Contributing Circ Not Listed Distractions Outside Vehicle Other/Unknown	Daylight Other/L Daylight D Dark-Street Lights On D	nknown Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Distracted Driving No Risky Driving Behaviors Involved	Yes No No
6700 939176.48 1023764.69 2016 6701 939181.85 1023765.3 2018 6702 939365.11 1023879.12 2017	6 2 Vehicles/Parties Rear-End 8 2 Vehicles/Parties Sideswipe 7 2 Vehicles/Parties Rear-End 5 2 Vehicles/Parties Turning	Property Damage Only Speeding Property Damage Only Fail to Yield Minor or Possible Injury Inattention Minor or Possible Injury Fail to Yield	25-34 No	Wet Dark-Street Lights On Dry Daylight Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Intersection Related but Not at Intersection At Driveway or Intersection	Straight & Level Straight & Level Straight & Level Straight & Level	No Vuinerable Road User Involved No Vuinerable Road User Involved No Vuinerable Road User Involved Motorcycle and Older Road User 65+	Speeding Fail to Yield Inattention Fail to Yield	Dark-Street Lights On V	et Not at Intersection/Driveway/Roundabout	Speeding Fail to Yield Inattention Fail to Yield	No Yes No
6704 939452.78 1023934.05 2017 6705 939633.15 1024046.39 2016 6706 939722.32 1024097.1 2018	7 2 Vehides/Parties Rear-End 6 2 Vehides/Parties Rear-End 8 2 Vehides/Parties Turning	Property Damage Only Inattention	Under 25 No Under 25 No	Dry Daylight Dry Daylight Dry Daylight Dry Dark-Street Lights On	Not at Intersection Driveway/Roundahout	Straight & Level Straight & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Inattention Follow Too Closely Under Influence of Alcohol/Drugs/Medication	Daylight C Daylight C Dark-Street Lights On C	y Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Inattention Follow Too Closely Under Influence of Alcohol/Drugs/Medication	No No No
6708 939768.04 1024130.41 2018	9 2 Vehioles/Paries Angle 8 2 Vehioles/Paries Pedestrian 2 Vehioles/Paries Rest-End 8 2 Vehioles/Paries Angle	Minor or Possible Injury Other/Unknown Property Damage Only Fail to Yield	55+ No 25-34 No	Dry Daylight Wet Dark-Street Lights On Dry Daylight	At Driveway or Intersection Not at Intersection/Driveway/Roundabout Intersection Related but Not at Intersection	Straight at Hillcrest Straight & Level Straight & Level	Older Road User 65+ Pedestrian Older Road User 65+	None Fail to Yield None	Daylight C Dark-Street Lights On V Daylight C	y At Driveway or Intersection y At Driveway or Intersection At Driveway or Intersection et Not at Intersection/Driveway/Roundabout y Intersection Related but Not at Intersection	Fail to Yield No Risky Driving Behaviors Involved Fail to Yield	No No No
6711 939858.15 1024186.54 2018	8 2 Vehicles/Parties Pedestrian 7 2 Vehicles/Parties Angle 8 2 Vehicles/Parties Angle	Property Damage Only Institention Minor or Possible Injury Other/Unknown Property Damage Only Fail to Yield Property Damage Only Fail to Yield	55+ No	Dry Daylight Dry Dark-Street Lights On Dry Dawn/Dusk Dry Daylight	At Driveway or Intersection At Driveway or Intersection	Straight & Level	Older Road User 65+ Pedestrian No Vulnerable Road User Involved Older Road User 65+	None Fail to Yield	Daylight Dark-Street Lights On Dawn/Dusk D	ry At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Inattention No Risky Driving Behaviors Involved Fail to Yield Fail to Yield	No No No
6714 939996.39 1024272.64 2014 6715 940016.16 1024284.99 2019 6716 940263.8 1024440.93 2016	4 1 Vehicle/Party Struck Object 9 2 Vehicles/Parties Nagle 6 2 Vehicles/Parties Rear-End	Minor or Possible Injury Apparently Asleep, Fatigued or III Property Damage Only Fail to Yield Property Damage Only Inattention	Apparently Asleep, Fatigued or III	Dry Daylight Dry Dark-Street Lights On Dry Daylight	Not at Intersection/Driveway/Roundabout At Driveway or Intersection Not at Intersection/Driveway/Roundabout	Straight & Grade Straight & Grade Straight at Hillcrest	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Driver Physical Condition Fail to Yield Inattention	Daylight C Dark-Street Lights On C Daylight C	ry Not at Intersection/Driveway/Roundabout ry At Driveway or Intersection ry Not at Intersection/Driveway/Roundabout	Apparently Asleep, Fatigued or III Fail to Yield Inattention	Yes No No
6717 940373.52 1024512.45 2019	9 4 Vehicles/Parties Rear-End 8 4 Vehicles/Parties Rear-End 8 2 Vehicles/Parties Rear-End 4 2 Vehicles/Parties Stdeswipe	Minor or Possible Injury Distracted Driving Minor or Possible Injury Speeding Property Damage Only Fillow Too Closely Property Damage Only Other/Unknown	Inder 25 No Index	Dry Daylight Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Intersection Related but Not at Intersection Not at Intersection/Driveway/Roundabout	Straight & Grade Straight & Grade Straight & Grade Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	Operating Hands-Free Cell Phone Speeding Follow Too Closely Other Contribution Circ Net Listed		Not at Intersection/Driveway/Roundabout y Intersection Related but Not at Intersection y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout	Distracted Driving Speeding Follow Too Closely No Risko Driving Rehaviors Insphere	No No No
6720 940578.72 1024639.23 [2014] 6721 940578.72 1024639.23 [2014] 6722 940632.4 1024673.19 [2018] 6723 940712.51 1024723.47 [2017] 6724 940770.92 1024751.77 [2017]	4 2 Vehicles-Parties Sideswipe 4 2 Vehicles-Parties Rear-End 8 2 Vehicles-Parties Rear-End 9 2 Vehicles-Parties Rear-End 7 2 Vehicles-Parties Rear-End	Properly Damage Only Oher/Unknown Properly Damage Only Follow Too Closely Minor or Possible Injury Oher/Unknown Minor or Possible Injury Oher/Unknown Minor or Possible Injury Oher/Unknown	55+ NO	Dry Daylight Dry Daylight Wet Dark-Street Lights On (Snow/Slush Dark-Street Lights On		Straight & Grade Straight & Grade Straight & Grade Straight & Grade	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved	Follow Too Closely None	Daylight	y Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout et Intersection Related but Not at Intersection w/Slush Intersection Related but Not at Intersection w/Slush At Driveway or Intersection	No Risky Driving Behaviors Involved Follow Too Closely No Risky Driving Behaviors Involved Speeding	No No No
6725 940772.61 1024758.76 2019	2 Vehicles/Parties Rear-End 2 Vehicles/Parties Rear-End 2 Vehicles/Parties Sideswipe 2 Vehicles/Parties Rear-End 2 Vehicles/Parties Rear-End 2 Vehicles/Parties Sideswipe	Minor or Possible Injury Oberdininown Minor or Possible Injury Speeding Minor or Possible Injury Speeding Minor or Possible Injury Speeding Property Damage Only Other Unknown	25-34 No loss 18-34 No loss 18	/Snow/Slush Dark-Street Lights On Dry Dark-Street Lights On	At Driveway or Intersection At Driveway or Intersection	Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Speeding Other Contributing Circ Not Listed Speeding Speeding Other Contributing Circ Not Listed	Dark-Street Lights On E	ry At Driveway or Intersection ry At Driveway or Intersection	Speeding Speeding Speeding	No Yes No
6727 940818.47 1024799.96 2015 6728 940823 1024791.11 2017 6729 940826.17 1024797.76 2015	7 2 Vehicles/Parties Turning 5 2 Vehicles/Parties Turning	Property Damage Only Fail to Yield Property Damage Only Inattention	25:34 No 55:64 No Jnder 25 No	Dry Daylight Dry Daylight	At Driveway or Intersection At Driveway or Intersection	Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	hattenton	Daylight L	ry At Driveway or Intersection ry At Driveway or Intersection ry At Driveway or Intersection	Speeding No Risky Driving Behaviors Involved Fail to Yfeld Inattention	Yes No No
6732 940826.66 1024802.84 2014	6 2 Vehicles/Parties Rear-End 4 2 Vehicles/Parties Angle 4 2 Vehicles/Parties Rear-End 5 2 Vehicles/Parties Rear-End	Property Damage Only Property Damage Only Mnor or Possible Injury Mnor or Possible Injury Distracted Driving	Under 25 No 45-54 No	Vet Daylight Dry Daylight Wet Dark-Street Lights On Dry Daylight	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Unknown Straight & Level Straight & Grade Straight & Grade	No Vuinerable Road User Involved	Inattention Other Contributing Circ Not Listed	Dark-Street Lights On V	et At Driveway or Intersection y At Driveway or Intersection et At Driveway or Intersection y At Driveway or Intersection	No Risky Driving Behaviors Involved Inattention No Risky Driving Behaviors Involved Distracted Driving	No No No
6734 940825.64 1024796.1 2016 6735 940848.57 1024808.44 2016 6736 940826.68 1024795.1 2016	6 2 Vehicles/Parties Rear-End 6 2 Vehicles/Parties Rear-End 6 3 Vehicles/Parties Rear-End 6 3 Vehicles/Parties Rear-End	Minor or Possible Injury Distracted Driving Minor or Possible Injury Distracted Driving Property Damage Only Other/Unknown Minor or Possible Injury Other/Unknown	15-54-1 No 15-54-1 No 15-54-1 No	Wet Daylight Dry Daylight Dry Daylight Dry Daylight	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Straight & Grade Straight & Grade Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+		Daylight V Daylight C Daylight C	et At Driveway or Intersection by At Driveway or Intersection by At Driveway or Intersection by At Driveway or Intersection	Distracted Driving No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No No
	6 2 Vehides/Parties Rear-End 6 2 Vehides/Parties	Minor or Possible Injury Minor or Possible Injury Minor or Possible Injury Property Damage Cnly Property Damage Cnly Fail to Yield	Under 25 No 25-34 No	Dry Daylight Dry Daylight Dry Dark-Street Lights On	At Intersection and Not Related At Driveway or Intersection	Straight & Grade Straight & Grade Straight & Grade Straight & Grade	No Vuinerable Road User Involved No Vuinerable Road User Involved No Vuinerable Road User Involved Older Road User 65+	Under Influence of Alcohol/Drugs/Medication Follow Too Closely Operating Defective Equipment Fail to Yield	Daylight E Daylight C Dark-Street Lights On E	ry At Intersection and Not Related ry At Driveway or Intersection ry At Driveway or Intersection	Under Influence of Alcohol/Drugs/Medication Follow Too Closely No Risky Driving Behaviors Involved	No No No
6740 940848.57 1024808.44 2016 6741 940821.99 1024802.97 2016 6742 940826.92 1024795.08 2017 6743 940827.83 1024795.04 2017	7 2 Vehicles/Parties Rear-End	Property Damage Only Follow Too Closely Property Damage Only Disregard Traffic Control Device/Sign	90+ NO NO NO NO NO NO NO NO	Dry Daylight Dry Dawn/Dusk Dry Dark-Street Lights On Dry Daylight	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Straight & Grade Straight & Grade Straight & Grade	No Vulnerable Road User Involved Older Road User 65+	Follow Too Closely Disregard Traffic Control Device/Sign	Daylight	y At Driveway or Intersection	Fall to Yield Follow Too Closely No Risky Driving Behaviors Involved Speeding	No No No
6745 940830.28 1024796.28 2018 6746 940831.53 1024795.66 2018	7 2 Vehioles:/Paries Aegle	Minor or Possible Injury Minor or Possible Injury Disregard Traffic Control Device/Sign Property Damage Only Property Damage Only Follow Too Closely	45-54 No	Dry Daylight Dry Daylight Wet Daylight	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Straight & Grade Curve & Grade Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+	Speeding Disregard Traffic Control Device/Sign Inattention Follow Too Closely	Daylight C Daylight C Daylight C Daylight V	y At Driveway or Intersection y At Driveway or Intersection	Speeding No Risky Driving Behaviors Involved Inattention Follow Too Closely	No No No
6747 940847.66 1024807.9 2018 6748 940827.33 1024795.84 2019 6749 940847.05 1024805.09 2019	8 2 Vehicles/Parties Rear-End 9 3 Vehicles/Parties Rear-End 9 3 Vehicles/Parties Pargle Pargle	Minor or Possible Injury Inattention Minor or Possible Injury Distracted Driving Minor or Possible Injury Disregard Traffic Control Device/Sign	55+ No 55+ No 55-64 No	Dry Daylight Dry Daylight Dry Daylight	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Straight & Grade Straight & Grade Straight & Grade	Older Road User 65+ Older Road User 65+ No Vulnerable Road User Involved	Inattention Distracted Driving Disregard Traffic Control Device/Sign	Daylight C Daylight C Daylight C	ny At Driveway or Intersection ny At Driveway or Intersection ny At Driveway or Intersection	Inattention Distracted Driving No Risky Driving Behaviors Involved	No No No
6750 940826.97 1024794.9] 2019 6751 94087.94 1024840.57 2019 6752 940940.13 1024863.01 2014 6753 940948.67 1024836.56 2019	9 2 Vehicles/Parties Rear-End 9 2 Vehicles/Parties Rear-End 4 3 Vehicles/Parties Rear-End 9 2 Vehicles/Parties Rear-End	Property Damage Only Inattention Property Damage Only Inattention Mnor or Possible Injury Apparently Asleep, Fatigued or III Property Damage Only Follow Too Closely	95-44 NO	Dry Daylight Dry Daylight Dry Daylight Dry Daylight	At Driveway or Intersection Intersection Related but Not at Intersection At Intersection and Not Related Intersection Related but Not at Intersection	Straight & Grade	No Vuinerable Road User Involved No Vuinerable Road User Involved No Vuinerable Road User Involved Older Road User 65+	Inattention Inattention Apparently Asleep or Fatigued Follow Too Closely	Daylight E	y At Driveway or Intersection y Intersection Related but Not at Intersection y At Intersection and Not Related y Intersection Related but Not at Intersection	Inattention Apparently Asleep, Fatigued or III Follow Too Closely	No No No
6755 941030.32 1024916.77 2019 6756 941261.21 1025051.03 2014	8 2 Vehicles/Parties Rear-End 9 1 Vehicles/Parties Animal 4 3 Vehicles/Parties Rear-End	Property Damage Only Other/Unknown Property Damage Only Other/Unknown Minor or Possible Injury Follow Too Closely	15-54 No 55-64 No 55+ No	Dry Daylight Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Intersection Related but Not at Intersection	Straight & Level Straight & Grade Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	None Other/Unknown Follow Too Closely	Daylight C Daylight C Daylight C	y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Follow Too Closely	No No No
6758 941515.21 1025130.23 2019	9 1 Vehicle/Party / Animal 9 2 Vehicle/Parties Sideswipe 5 2 Vehicles/Parties / Rogle 5 2 Vehicles/Parties Sideswipe	Property Damage Only Other/Unknown Property Damage Only Fail to Yield		Dry Daylight Wet Daylight Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Intersection Related but Not at Intersection At Driveway or Intersection At Intersection and Not Related	Straight & Grade Curve & Level Curve & Grade Curve & Grade	Older Road User 65+	Fail to Yield		y Not at Intersection/Driveway/Roundabout et Intersection Related but Not at Intersection y At Driveway or Intersection y At Intersection and Not Related et At Intersection and Not Related	No Risky Driving Behaviors Involved Fail to Yield No Risky Driving Behaviors Involved Apparently Asleep, Fatigued or III	No Yes No
6761 941565.34 1025132.47 2016	6 3 Vehicles/Parties Struck Parked Vehicle	Suspected Serious Injury OtherUnknown Minor or Possible Injury Apparently Asleep, Fatigued or III Property Damage Only Inattention Property Damage Only OtherUnknown Property Damage Only Foliow Too Closely	Under 25 No	Dry Daylight Wet Dark-Street Lights On Dry Daylight Dry Daylight	At Intersection and Not Related At Intersection and Not Related At Driveway or Intersection At Intersection and Not Related	Curve & Grade	No Vulnerable Road User Involved	Other/Unknown Follow Top Closely		y At Intersection and Not Related by At Driveway or Intersection by At Intersection and Not Related At Intersection and Not Related	No Risky Driving Behaviors Involved	Yes No No
6/66 941583.42 1025132.56 2018	6 2 Vehicles-Paries Angle 2 Vehicles-Paries Rear-End 7 2 Vehicles-Paries Rear-End 7 2 Vehicles-Paries Rear-End 7 2 Vehicles-Paries Rear-End 8 2 Vehicles-Paries 8 2 Vehicles-Paries Regie 9 2 Vehicles-Paries Regie	Property Damage Only Osterofluoren Property Damage Only Follow Too Closely Property Damage Only Follow Too Closely Property Damage Only Inattention Property Damage Only Fail to Yield	Inder 25 No 55+ No 55+ No	Dry Daylight Dry Daylight Dry Daylight	At Driveway or Intersection At Intersection and Not Related At Driveway or Intersection	Curve & Grade Curve & Grade Curve & Grade	No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+	Fall to Yield	Daylight E Daylight E Daylight C	ny At Driveway or Intersection At Intersection and Not Related by At Driveway or Intersection	Follow Too Closely Follow Too Closely Inattention Fail to Yield	No Yes No
6768 941563.71 1025132.41 2019 6769 941603.96 1025133.89 2019 6770 942006.85 1024993.26 2017	9 2 Vehicles/Parties Turning 9 1 Vehicle/Party Struck Object 9 2 Vehicles/Parties Angle 7 2 Vehicles/Parties Sideswine	Minor or Possible Injury Fail to Yield Property Damage Only Institution Minor or Possible Injury Improper Turn/Merge Property Damage Only Under Influence of Alcohol/Drugs/Medication	15-64 No	Dry Daylight Wet Dark-Street Lights On Dry Daylight Dry Dark-No Street Lights/Street Lights Off	At Driveway or Intersection At Intersection and Not Related At Driveway or Intersection Not at Intersection Driveway/Roundahout	Curve & Grade Curve & Grade Curve & Grade Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved	Fail to Yield Inattention Improper Turn/Merge Under Influence of Airobol/Drugs/Medication	Daykight C Dark-Street Lights On V Daykight C Dark-No Street Lights/Street Lights Off C	At Driveway or Intersection et At Intersection and Not Related by At Driveway or Intersection by Not at Intersection Driveway Roundahout	Fail to Yield Inattention No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication	No Yes No Yes
6770 94206.85 1024993.26 [207] 6771 94206.41 1024993.26 [207] 6772 942053.06 1024961.99 [2019] 6773 942088.15 1024920.15 [2015] 6774 942126.29 1024882.77 [2014]	7 1 Vehicle/Party Over Embankment - No Guardrail Present 9 2 Vehicles/Parties Strikes or Was Struck by Part or Object of Another Vehicle 5 Vehicles/Parties Struck Object 4 2 Vehicles/Parties Turning	Property Damage Only Operating Defective Equipment Property Damage Only Operating Defective Equipment			Not at Intersection/DrivewayRoundabout Not at Intersection/DrivewayRoundabout	Curve & Grade Curve & Grade Curve & Grade	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved	Operating Defective Equipment Operating Defective Equipment Apparently Asleep or Fatigued	Dark-No Street Lights/Street Lights Off V Daylight D	y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Apparently Asleep, Fatigued or III	Yes No Yes
6775 942285.14 1024497.92 2019	9 1 Vehicle/Party Animal	Property Damage Only Apparently Acleep, Fatigued or III Property Damage Only Inproper Turn/Merge Property Damage Only OtherUnknown Minor or Possible Injury Distracted Driving	55+ No	Dry Daylight Dry Daylight Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Curve & Grade Curve & Grade Curve & Grade	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Apparently Asleep or Fatigued Improper Backing/Passing/Turning/Parking/Signaling None Distractions Outside Vehicle	Daylight C Daylight C	ny Not at Intersection/Driveway/Roundabout ny Not at Intersection/Driveway/Roundabout ny Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Distracted Driving	No No Yes
6779 942288.13 1024391.91 2019	1 Nehide Party Struck Object	Property Damage Cnly Speeding Speeding Speeding	15-44 No	Dry Daylight Wet Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off Dry Daylight	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Curve & Grade	No Vuinerable Road User Involved	None Follow Too Closely Under Influence of Alcohol/Drugs/Medication Speeding	Daylight C Dark-No Street Lights/Street Lights Off V Dark-No Street Lights/Street Lights Off C Daylight C	Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout by Not at Intersection/Driveway/Roundabout by Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Follow Too Closely Under Influence of Alcohol/Drugs/Medication Speeding	No No Yes
	9 2 Vehicles/Parties Rear-End 6 2 Vehicles/Parties Rear-End 6 1 Vehicle/Parties Rear-End 6 1 Vehicle/Parties Sideswipe	Property Damage Only Speeding Property Damage Only Follow Too Closely Property Damage Only Under Influence of Alcohol/Drugs/Medication Property Damage Only Other/Unknown		Dry Daylight Dry Dark-No Street Lights/Street Lights Off Wet Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Curve & Grade Straight & Grade Straight & Grade Curve & Grade	No Vulnerable Road User Involved	Follow Too Closely Under Influence of Alcohol/Drugs/Medication Other Contributing Circ Not Listed	Daylight C Dark-No Street Lights/Street Lights Off C Dark-No Street Lights/Street Lights Off V	y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout	Follow Too Closely Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved	No No Yes Yes
6784 942138.14 1023881.9 2014 6785 942091.8 1023729.4 2015 6786 942098.76 1023755.45 2018	4 1 Vehida/Party Overturned 5 1 Vehida/Party Animal 8 1 Vehida/Party Struck Object	Minor or Possible Injury Other/Unknown Property Damage Only Other/Unknown Property Damage Only Under Influence of Alcohol/Drugs/Medication	15-54 No Street No Under 25 Under Influence of Alcohol Under 25 Under Influence Ontoloo Under 25 Under Influence Ontoloo Unde	Wet Daylight Dry Dark-No Street Lights/Street Lights Off Wet Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Straight & Grade Curve & Grade Curve & Grade	Motorcycle Older Road User 65+ No Vulnerable Road User Involved	Under Influence of Alcohol/Drugs/Medication	Daylight V Dark-No Street Lights/Street Lights Off D Dark-No Street Lights/Street Lights Off V	et Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication	Yes No Yes
6788 942071.87 1023434.66 2017	6 1 Vehicle/Party Struck Object 7 1 Vehicle/Party Struck Object 8 1 Vehicle/Party Struck Object 4 1 Vehicle/Party Animal	Minor or Possible Injury Apparently Asleep, Fatigued or III Minor or Possible Injury Under Influence of Alcohol/Drugs/Medication Property Damage Only Apparently Asleep, Fatigued or III Property Damage Only Other/Unknown	Under 25 Under Influence of Alcohol Under 25 55-64 Apparently Asleep, Fatigued or III 55-64	Dry Daylight Dry Dark-Street Lights On Wet Dark-Street Lights On Dry Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout		Older Road User 65+	Driver Physical Condition Under Influence of Alcohol/Drugs/Medication Apparently As keep or Fatigued None	Daykight	y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout	Apparently /sleep, Fatigued or III Under Influence of Alcohol/Drugs/Medication Apparently /sleep, Fatigued or III No Risky Driving Behaviors Involved	Yes Yes Yes No
6792 942111.07 1023267 2017	Tvenicur-ary	Suspected Senous Injury Distracted Driving Property Damage Only Other/Linknown	25:34 No	Wet Daylight Wet Daylight Dry Daylight	At Intersection and Not Related At Driveway or Intersection At Driveway or Intersection	Curve in Sag Curve & Grade Curve in Sag	No Vulnerable Road User Involved Older Road User 85+	Operating Handheld Cell Phone Other Contributing Circ Not Listed Operating Defective Equipment Follow Too Closely	Daylight V Daylight V Daylight C	et At Intersection and Not Related et At Driveway or Intersection by At Driveway or Intersection	No Risky Driving Benaviors involved Distracted Driving No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Follow Too Closely	Yes Yes No
6794 942131.78 1023209.95 2015 6795 942131.78 1023209.95 2016 6796 942129.8 1023213.85 2017 6797 942189.96 1023147.49 2015	6 2 Vehicles/Parties Angle	Property Damage Only Property Damage Only Property Damage Only Follow Too Closely Property Damage Only Property Damage Only Property Damage Only Other Unknown Description	Under 25 No	Dry Daylight Dry Daylight Dry Daylight	At Driveway or Intersection	Curve & Grade Curve & Grade Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved	Fail to Yield	Daylight	ry At Driveway or Intersection ry At Intersection and Not Related	Fail to Yield No Risky Driving Behaviors Involved	No No No
6796 942129.8 1023213.85 2017 6797 942183.96 1023117.49 2015 6798 942377.69 102290.77 2015 6799 942419.7 1022877.78 2015 6800 94254.97 1022872.38 2019	7 1 Vehicle-Party Animal 1 1 Vehicle-Party Animal 5 1 Vehicle-Party Animal 9 1 Vehicle-Party Animal 9 1 Vehicle-Party Animal 9 1 Vehicle-Party Animal 9 1 Vehicle-Party Struck Coject	Unknown Other/Unknown Property Damage Only Other/Unknown Property Damage Only Other/Unknown Property Damage Only Improper Turn/Merge	15-54 No 15-44 No 15-54 No 15-54 No 15-54 No 15-54 No	Wet Dark-No Street Lights/Street Lights Off Wet Daylight Dry Dark-No Street Lights/Street Lights Off Wet Daylight	Not at Intersection/DrivewayRoundabout Not at Intersection/DrivewayRoundabout Not at Intersection/DrivewayRoundabout Intersection Related but Not at Intersection	Curve & Level Curve in Sag Curve & Grade Curve in Sag	No Vulnerable Road User Involved Motorcycle Older Road User 65+ No Vulnerable Road User Involved	None None None Improper Turn/Merge	Daylight V Dark-No Street Lights/Street Lights Off D Daylight V	et Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout et Intersection Related but Not at Intersection	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No No Var
6800 942540.97 1022812.38 2019 6801 942620.68 1022785.48 2014 6802 942620.68 1022785.48 2014 6803 942602.57 1022785.86 2015	9 1 Vehicle-Party Struck Object 4 2 Vehicles-Parties Turning 5 3 Vehicles-Parties Pedalcyclist 5 1 Vehicle-Party Overturned	Property Damage Only Improper Turn/Merge Minor or Possible Injury Other/Unknown Minor or Possible Injury Other/Unknown Property Damage Only Speeding	35-44 No 25-34 No	Dry Daylight Dry Daylight Dry Daylight Dry Daylight	At Driveway or Intersection At Driveway or Intersection At Intersection and Not Related	Curve & Grade Curve & Grade Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved Motorcycle and Pedalcyclist Motorcycle	Other/Unknown Other Contributing Circ Not Listed	Daylight	ry At Driveway or Intersection ry At Driveway or Intersection ry At Intersection and Not Related	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Speeding	No No Yes
6804 942605.97 1022788.54 2016 6805 942604.87 1022789.59 2019 6806 942653.13 1022777.12 2019	6 3 Vehicles/Parties Head-On 9 9 2 Vehicles/Parties Rear-End 9 1 Vehicle/Party Struck Object	Minor or Possible Injury Minor or Possible Injury Minor or Possible Injury Property Damage Chly Property Damage Chly Distracted Driving		Wet Dark-Street Lights On Dry Daylight Dry Daylight Wet Dark-Street Lights On	At Intersection and Not Related	Curve & Grade Curve in Sag Curve & Grade Curve & Grade	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Improper Turn/Merge	Dark-Street Lights On V Daylight C Daylight C Dark-Street Lights On V	et At Intersection and Not Related y At Driveway or Intersection y Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout	Apparently Asleep, Fatigued or III No Risky Driving Behaviors Involved Fail to Yell Distracted Driving	Yes No No
6808 942828.92 1022754.75 2015	9 T Ventices-Party Struck Coject 5 1 Vehicles-Party Struck Coject 5 2 Vehicles-Parties Sides-wipe 4 2 Vehicles-Parties Sides-wipe 8 2 Vehicles-Parties Sides-wipe 8 2 Vehicles-Parties Overnamed	Property Damage Only Other/Unknown Property Damage Only Speeding Minor or Possible Injury Other/Unknown	55+ No 55-64 No 25-34 No	Dry Daylight Wet Daylight Dry Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout	Curve & Grade Curve & Grade Curve & Grade	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Other Contributing Circ Not Listed Speeding		Not at Intersection/Driveway/Roundabout ty Not at Intersection/Driveway/Roundabout ty Not at Intersection/Driveway/Roundabout ty Not at Intersection/Driveway/Roundabout ty Not at Intersection/Driveway/Roundabout	Distraced Driving No Risky Driving Behaviors Involved Speeding No Risky Driving Behaviors Involved	Yes Yes Yes
6812 943040.12 1022780.37 2019 6813 943095.82 1022797.39 2018	9 2 Vehicles/Parties Sideswipe 8 1 Vehicle/Party Animal	Fatality Over Center Line Minor or Possible Injury Inattention Property Damage Only OtherUnknown	25-34 No 35-44 No	Dry Daylight Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Curve & Grade Curve & Grade Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved	Lane Departure Inattention None	Daylight C	y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Inattention No Risky Driving Behaviors Involved	Yes Yes No
6814 943142.47 1022816.17 2019 6815 94342.5 1023017.62 2018 6816 943816.27 1023481.07 2019 6817 943945.52 1023634.71 2015	9 1 Vehicle/Party Struck Object 8 2 Vehicles/Parties Sideswipe 9 2 Vehicles/Parties Sideswipe 5 1 Vehicle/Party Animal	Property Damage Only Inattention Property Damage Only Speeding Property Damage Only Other/Unknown Property Damage Only Other/Unknown	25-34 No	Dry Daylight	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Curve & Grade Curve & Grade Straight & Grade Straight & Grade	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+	Inattention Speeding Other Contributing Circ Not Listed None		y Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout	Inattention Speeding No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	Yes Yes Yes
6818 944035.63 1023738.97 2016 6819 944110.4 1023831.65 2015 6820 944200.37 1023902.13 2016	6 1 Vehida/Party Struck Object 5 1 Vehida/Party Animal 6 1 Vehida/Party Animal	Property Damage Only Under Influence of Alcohol/Drugs/Medication Property Damage Only Other/Unknown Property Damage Only Other/Unknown	Under 25	Wet Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Straight & Grade Curve & Grade Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Under Influence of Alcohol/Drugs/Medication None None	Dark-No Street Lights/Street Lights Off V	et Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout	Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	Yes No No
6821 944600.1 1024022.59 2016	6 1 Vehicle,Party Overtumed 4 2 Vehicles:Parties Sideswipe 5 2 Vehicles:Parties Turning 9 1 Vehicle:Party Struck Object	Property Damage Only Other/Unknown Property Damage Only Inattention Property Damage Only Improper Turn/Merge Property Damage Only Speeding	25-34 No 55+ No	Dry Daylight Dry Daylight Wet Daylight Dry Dark-No Street Lights/Street Lights Off	Not at Intersection/DrivewayRoundabout Not at Intersection/DrivewayRoundabout	Curve & Grade Curve & Grade Curve & Grade	Motorcycle	Other Contributing Circ Not Listed Inattention Improper Backing/Passing/Turning/Parking/Signaling Speeding	Daylight E		No Risky Driving Behaviors Involved Inattention No Risky Driving Behaviors Involved	Yes Yes No
6824 94499.82 1023929.76 2012 6825 944993.69 1023997.7 2016 6826 945040.13 102389.52 2017 6827 945090.89 1023845.1 2017 6828 945126.04 1023811.88 2016	9 1 Vehicle-Party Suruk Object 6 1 Vehicle-Party Struck Object 7 1 Vehicle-Party Struck Object 7 1 Vehicle-Party Struck Object 6 2 Vehicles-Party Struck Object 6 2 Vehicles-Partes Rean-End	Property Damage Only Speeding Property Damage Only Agenety Aeleep, Fasigued or III Minor or Possible Injury OtherUnknown Property Damage Only Impropert TurnMerge Other	Under 25 Apparently Asleep, Fatigued or III Under 25 25-34 No	Dry Dark-No Street Lights/Street Lights Off Dry Dawn/Dusk Dry Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off	Not at Intersection/DrivewayRoundabout At Intersection and Not Related	Curve & Grade Curve in Sag Curve & Level Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved	Speeding Apparently Asleep or Fatiqued Other Contributing Circ Not Listed Improper Turn/Marge Other/Unknown	Dark-No Street Lights/Street Lights Off Dawn/Dusk Dawn/Dusk Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lig	ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout ry At Intersection and Not Related ry At Driveway or Intersection	Speeding Apparently Asleep, Fatigued or III No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	Yes Yes Yes
		Property Damage Only Improper Turn/Merge	55-64 No Under 25 No	Dry Daylight Dry Daylight Dry Daylight	At Driveway or Intersection At Driveway or Intersection	Curve at Hillcrest Straight & Grade	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+	Improper Turn/Merge Improper Turn/Merge Improper Turn/Merge Instension	Daylight E Daylight E	ry At Driveway or Intersection ry At Driveway or Intersection	No Risky Driving Behaviors Involved	No Yes Yes
6831 945126.96 1023810.52 2018 6832 945228.75 1023695.73 2019 6833 945248.19 1023494.55 2017	7 1 Vehicle Party Struck Object 8 2 Vehicles Parties Rear-End 9 1 Vehicle Party Arimal 7 2 Vehicles Parties Sideswipe	Property Damage Only Improper Turn/Merge Property Damage Only Inattention Property Damage Only OherUnknown Property Damage Only OherUnknown Other Oher Oher Oher Oher Oher Oher Oher O	55-64 No	Dry Daylight Uet Daylight Wet Daylight	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Curve & Level Straight & Grade Straight & Level Straight & Grade	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Other/Unknown None	Daylight C Daylight C Daylight C Daylight C Daylight V	y At Driveway or Intersection y At Driveway or Intersection y Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout	Inattention No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No Yes
6833 945348.19 1023494.56 2017 6834 945341.63 1023494.57 2018 6835 945364.46 1023534.52 2018 6836 945629.97 1023164.22 2016 6837 94564.49 1023154.18 2017	8 2 Vehicles/Parties Sideswipe 9 2 Vehicles/Parties Sideswipe 6 1 Vehicle/Party Animal 7 1 Vehicle/Party Struck Object	Property Damage Only Property Damage Only Under Influence of Alcohol/Drugs/Medication Property Damage Only Other/Unknown Under Influence of Alcohol/Drugs/Medication Under Influence of Alcohol/Drugs/Medication Under Influence of Alcohol/Drugs/Medication	25:34 No 25:34 Under Influence of Alcohol 25:34 Under Influence of Alcohol 25:34 Signature of Alcohol 25:34 No 25:34 Under Influence of Alcohol 25:34 Under Influence	Dry Daylight Dry Dark-Street Lights On Dry Daylight Dry Daylight Dry Dark-Street Lights On	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Straight & Grade Curve & Grade Straight & Grade Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved	None	Daylight C Dark-Street Lights On C Daylight C Dark-Street Lights On C	ny Not at Intersection/Driveway/Roundabout ny Not at Intersection/Driveway/Roundabout ny Not at Intersection/Driveway/Roundabout ny Mintersection and Not Related	No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication	Yes Yes No
	7 1 Vehicle/Party Struck Object 6 2 Vehicles/Parties Angle	Property Damage Only Under Influence of Alcohol/Drugs/Medication Property Damage Only Fail to Yield	55+ No 25-34	Dry Dark-Street Lights On Dry Daylight	At Intersection and Not Related At Driveway or Intersection	Straight & Level	No Vulnerable Road User Involved Older Road User 65+	Under Influence of Alcohol/Drugs/Medication Fail to Yield	Dark-Street Lights On Daylight D	ry At Intersection and Not Related ry At Driveway or Intersection	Under Influence of Alcohol/Drugs/Medication Fail to Yield	Yes No

OBJECTID JURISDICTION	COUNTY CITY	PRIMARY BLOCK TRAFFICWAY NUMBER	INTERSECTING FROM ML_or FROM FT FROM F	MILEPOST A SR_ONLY HISTORY SUSPENSE_IND REPORT NUMBER	DATE IME INJURY_TYPE	NUM NUM NUM NUM NUM NUM NUM PEDS BIKI	ES VEH_1_TTPE	VEH_2_TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD_SURFACE ROADWAY CHARACTERISTIC	C CONDITION	RRST_COLLISION TYPE_OBJECT_STRUCK	VEH ACTI	H_1 VEH_2 FION ACTION	VEH_1_COMPASS DIRECTION_FROM DIRECTI	
6839 State Route 6840 State Route 6841 State Route 6842 State Route 6842 State Route 6843 State Route 6844 State Route 6844 State Route 6845 State Route 6846 State Route 6846 State Route	Clallam 1 Clallam 1	101 101		253.10 No E717655 : 253.10 No E914500 :	2017-06-14 17:41 Possible Injury 2019-04-21 02:17 Possible Injury 2019-04-27 22:47 No Apparent Injury 2019-05-08 07:57 No Apparent Injury	1 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		At Intersection and Not Related At Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Earl Dark-Street Lights On Con	n Bank or Ledge crete Barrier/Jersey Barrier - Face testic animal other (cat, dog, etc)	Going Straight Ahead Going Straight Ahead Going Straight Ahead		ast West /est East	
6841 State Route 6842 State Route	Cialiam 1	101		253.11 No E915006 2 253.11 No E919212 2	2019-04-27 22:47 No Apparent Injury 2019-05-08 07:57 No Apparent Injury 2016-06-27 17:35 No Apparent Injury	0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Intersection and Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade	Daylight From	estic animal other (cat, dog, etc) n same direction - both going straight - one stopped - rear-end cle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped at Signal or Stop Sign	ast West South	Vehicle Stopped
6844 State Route 6845 State Route	Clallam 1 Clallam 1	101		253.18 No 3615857 2 253.20 No E546557 2	2017-05-31 02:21 No Apparent Injury 2016-05-18 21:35 No Apparent Injury 2019-03-30 12:30 No Apparent Injury	0 0 1 0	Passenger Car Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Grade	Dark-No Street Lights Veh Dark-Street Lights On Veh	cle Strikes Deer cle Strikes Deer	Going Straight Ahead Going Straight Ahead		ehicle Backing West ast West	
6846 State Route 6847 State Route	Cialiam 1 Cialiam 1	101					Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Veh Daylight Veh	cle Strikes Deer	Going Straight Ahead Going Straight Ahead		/est East /est East	
6847 State Route 6848 State Route 6849 State Route 6850 State Route	Cialiam 1 Cialiam 1	101 101		253.40 No E604477 2 253.44 No E860921 2 253.45 No E450948 2	2016-11-02 18:13 No Apparent Injury 2018-11-14 17:30 No Apparent Injury 2015-09-08 16:40 Possible Injury	0 0 1 0	0 Passenger Car 0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Intersection and Not Related Not at Intersection and Not Related	Raining Overcast Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level Dry Straight & Grade	Dark-No Street Lights Veh Dark-Street Lights On Woo Daylight From	cle Strikes Deer d Sign Post	Going Straight Ahead Going Straight Ahead	Going Straight Ahead	ast West /est East	West
6851 State Route 6852 State Route	Clallam 1 Clallam 1	101		253.45 No E627745 : 253.46 No E617102 :	2016-12-26 06:38 No Apparent Injury 2016-12-06 07:45 No Apparent Injury	0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Treating and Track of Faircine Group Toyoto to	At Intersection and Not Related Not at Intersection and Not Related	Overcast Overcast	Snow/Slush Straight & Level Snow/Slush Straight & Level	Dark-Street Lights On Meta Daylight Roa	il Sign Post	Changing Lanes Going Straight Ahead Overtaking and Passing	Comg disagn zerosa	/ost East /ost East	
6854 State Route	Cialiam 1 Cialiam 1	101		253.70 No E552660 2 253.70 No E901476 2	2016-12-26 06:38 No Apparent Injury 2016-12-06 07:45 No Apparent Injury 2016-06-08 13:42 No Apparent Injury 2019-03-12 09:14 No Apparent Injury	0 0 1 0	0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Overcast		Daylight Roa Daylight Veh Daylight Veh		Going Straight Ahead Going Straight Ahead		/est East /est East	
6855 State Route 6856 State Route 6857 State Route 6858 State Route	Cialiam 1	101 101					O Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger C		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Raining Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Wet Straight & Grade Dry Straight & Level	Daylight Veh Dark-Street Lights On Miss	cle Strikes Deer ellaneous Object or Debris on Road	Going Straight Ahead Going Straight Ahead		ast West fest East	
6858 State Route 6859 State Route	Cialiam 1	101		253.79 No E921849 : 253.82 No E634309 :			Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Dark-No Street Lights Veh Dark-Street Lights On Roa	Use Stakes Deer dis Stakes Deer distances Chiject or Debris on Road de Strikes Deer	Going Straight Ahead Going Straight Ahead Other*		est East West	
6859 State Route 6860 State Route 6861 State Route 6862 State Route	Cialiam 1 Cialiam 1	101		253.88 B No E440137 2 253.88 No E441578 2	2017-01-13 21:00 Possible Injury 2015-07-04 08:40 No Apparent Injury 2015-07-09 07:52 No Apparent Injury 2017-12-30 16:50 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb		At Intersection and Not Related At Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Veh Daylight Veh Dark-Street Lights On Fen	cle Strikes Deer cle Strikes Deer	Going Straight Ahead Going Straight Ahead		/est East /est East	
6862 State Route 6863 State Route	Cialiam 1	101		253.88 No E754041 2 253.88 B No E799818 2	2017-12-30 16:50 No Apparent Injury 2018-05-13 16:55 No Apparent Injury	0 0 1 0	0 Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Not Related	Raining Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level	Dark-Street Lights On Fen Daylight Ente	ce ring at angle	Going Straight Ahead Changing Lanes	Making Left Turn	ast West /est East North	East
6863 State Route 6864 State Route 6865 State Route 6866 State Route	Cialiam 1 Cialiam 1	101 101 101		253.90 No E718493 2 253.90 No E972577 2	2017-05-28 19:58 Suspected Minor Injury 2019-10-19 20:30 No Apparent Injury	1 0 1 0	O Pickup Panel Truck or Vanette under 10,000 lb O Pickup Panel Truck or Vanette under 10,000 lb Passenger Car O Pickup Panel Truck or Vanette under 10,000 lb O Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade Dry Straight & Level	Daylight Ente Dark-Street Lights On Meta Daylight Veh Dark-Street Lights On Veh	ii sign Post cle Strikes Deer cle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead		rest East ast West fest East	
6867 State Route 6868 State Route	Cialiam 1 Cialiam 1	101		254.00 No E861569 2 254.03 No E832283 2	2018-11-15 23:40 No Apparent Injury 2018-08-25 21:00 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy	Dry Straight & Level	Dark-Street Lights On Veh	cle Strikes Deer	Going Straight Ahead Going Straight Ahead		/est East /est East	
6867 State Route 6868 State Route 6869 State Route 6870 State Route 6871 State Route	Cialiam 1 Cialiam 1	101		254.07 No E379722 2 254.10 No E746579 2	2014-12-01 11:29 No Apparent Injury 2017-12-11 08:14 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Wet Straight & Level	Daylight Veh Daylight Eart Daylight Roa	cle Strikes Deer h Bank or Ledge	Going Straight Ahead Changing Lanes Going Straight Ahead		/est East ast West	
6872 State Route 6873 State Route	Cialiam 1	101		254.26 No E490081 2 254.40 No F825267	2016-07-04 12:04 Possible injury 2015-11-01 01:05 Unknown 2018-07-29 23:42 No Apparent Injury	0 0 1 0	0 Passenger Car 0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy	Wet	Dark-Street Lights On Fen	oway poor	Other* Going Straight Ahead		ast west /est East ast West	
6872 State Route 6873 State Route 6874 State Route 6875 State Route 6875 Cate Route	Cialiam 1 Cialiam 1	101		254.45 No E560081 2 254.49 No E438750 2	2016-07-04 05:56 Possible Injury 2015-06-26 23:00 No Apparent Injury	1 0 1 0	Passenger Car Passenger Tark Packup Parel Truck or Vanette under 10,000 lb Pickup Parel Truck or Vanette under 10,000 lb Pickup Parel Truck or Vanette under 10,000 lb Pickup Parel Truck or Vanette under 10,000 lb Passenger Car Pass		At Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Level Dry Curve & Grade	Dawn Roa Dark-Street Lights On Veh	dway Ditch cle Strikes Deer	Going Straight Ahead Going Straight Ahead		ast West ast West	
6876 State Route 6877 State Route 6878 State Route 6879 State Route	Cialiam 1 Cialiam 1	101 101		254.51 No E499990 : 254.51 No E772152 :	2015-12-28 02:20 No Apparent Injury 2018-01-25 02:40 Unknown	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb		At Intersection and Not Related At Intersection and Related Not at Intersection and Not Related	Raining Raining Clear or Partly Cloudy	Wet Straight & Level Wet Straight & Level	Dark-Street Lights On Roa Dark-Street Lights On Roa	dway Ditch dway Ditch de Strikes Deer n same direction - both going straight - both moving - sideswipe	Going Straight Ahead Making Right Turn Going Straight Ahead		ast West /est South	
6878 State Route 6879 State Route	Cialiam 1	101 101		254.53 No E903674 2 254.58 No E379031 2 254.59 No E361575	2019-03-18 09:30 No Apparent Injury 2014-11-22 19:20 No Apparent Injury 2014-09-22 17:34 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast	Dry Straight & Level Dry Straight & Grade Dry Straight & Grade	Daylight Veh Dark-Street Lights On From	cle Strikes Deer n same direction - both going straight - both moving - sideswipe n same direction - both going straight - both moving - sideswipe	Changing Lanes	Going Straight Ahead Going Straight Ahead	/est East ast West East	West
6880 State Route 6881 State Route 6882 State Route 6883 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101 101		254.59 No E361575 2 254.60 No E388954 2 254.61 No E380255 2	2014-12-29 23:39 No Apparent Injury 2014-11-29 13:33 Suspected Minor Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Overcast	Dry Straight & Level loe Straight & Level	Dark-No Street Lights Veh Daylight Sno	n same direction - both going straight - both moving - sides wipe cle Strikes Deer M Bank cle Strikes Deer	Changing Lanes Going Straight Ahead Going Straight Ahead Going Straight Ahead		/est East /est East	
6883 State Route 6884 State Route	Cialiam 1 Cialiam 1	101 101		254.69 No E851959 2 254.70 No 2766971 2	2018-10-22 07:47 No Apparent Injury 2017-03-29 16:00 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Dickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast	Dry Straight & Grade	Daylight From	n same direction - both going straight - both moving - sideswipe	Changing Lanes	Going Straight Ahead	ast West /est East West	East
State Stat	Cialiam 1 Cialiam 1	101		254.70 No E380261 2 254.70 No E380571 2 254.71 No E978180	2014-12-03 17:53 No Apparent Injury 2014-12-03 17:53 No Apparent Injury 2019-11-01 07:33 No Apparent Injury	0 0 1 0 0 0 1 0	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Passenger Car 0 Passenger Car 0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Motoroxie		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	loe Straight & Level Wet Straight & Level Dry Straight & Level	Daylight Earl Dark-No Street Lights Veh	h Bank or Ledge cle Strikes Deer cle Strikes Deer	Going Straight Ahead Going Straight Ahead		rest East /est East /est East	
6888 State Route 6889 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101		254.80 No 2766896 2 254.80 No E447197 2	2015-04-08 15:17 No Apparent Injury 2015-07-23 14:00 Suspected Minor Injury	0 0 1 0	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Motorcycle		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level	Daylight Veh	cle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead		/est East East	
6890 State Route 6891 State Route	Clallam	101		254.80 No E932517 2 254.81 No E938584 2	2015-07-23 14:00 Suspected Minor Injury 2019-06-09 14:00 No Apparent Injury 2019-07-05 04:05 Dead at Scene	0 0 1 0	Motorcycle Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear	Dry Straight & Grade Dry Straight & Level Dry Straight & Level	Dark-No Street Lights Veh	cle Strikes Deer cle Strikes Deer cle overturned	Going Straight Ahead		East East	+==
6892 State Route 6893 State Route 6894 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101 101 101		254.81 No E946359 2	2018-07-17 07:30 Possible Injury	1 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy Raining	Wet Straight & Level Dry Straight & Level	Daylight Earl Daylight Veh	h Bank or Ledge cle Strikes Deer cle Strikes Deer	Changing Lanes Going Straight Ahead		rest East ast West /est East	#
6892 State Route 6893 State Route 6894 State Route 6895 State Route 6895 State Route 6896 State Route	Cialiam 1	101		254.89 No E511377 : 254.90 No E982829 :	2018-10-28 19:40 No Apparent Injury 2016-01-29 12:50 No Apparent Injury 2019-11-08 16:35 Possible Injury	0 0 2 0 2 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight From Dusk Veh	n same direction - both going straight - both moving - sideswipe cle Strikes Deer	Going Straight Ahead Changing Lanes Going Straight Ahead	Going Straight Ahead	ast West East	West
6897 State Route 6898 State Route 6899 State Route 6990 State Route	Clallam 1 Clallam 1	101		254.91 No E674450 2 254.94 No E544688 2	2017-05-19 08:40 No Apparent Injury 2016-05-12 10:14 Suspected Minor Injury	0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level		cle Strikes Deer	Going Straight Ahead		/est East /est East	
6900 State Route 6901 State Route	Cialiam 1 Cialiam 1	101		255.06 No E304299 2 255.07 No E325020	2014-01-31 07:50 No Apparent Injury 2014-01-31 07:50 No Apparent Injury 2014-06-14 15:47 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy Overcast	Dry Straight & Level Wet Straight & Level Wet Straight & Grade Dry Straight & Grade	Daylight From	n Bank or Ledge n same direction - both going straight - both moving - rear-end n same direction - both going straight - one stopped - rear-end cle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Other* Stopped in Roadway	/est East West	Vehicle Stopped
	Cialiam 1 Cialiam 1	101		255.10 No E910473 : 255.10 No E930573 :	2019-03-28 11:34 No Apparent Injury 2019-06-13 21:51 Possible Injury	0 0 2 0	O Pickup, Parel Truck or Visentis under 10,000 b Pickscaper Car O Pickscaper Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level		cie Strikes Deer le direction both turning right both moving rear end te	Making Right Turn Making Left Turn Going Straight Ahead	Making Right Turn	iorth West North North North	West
6904 State Route 6905 State Route	Cialiam 1	101		255.11 No 2766943 : 255.11 No 3202418 :	2014-01-31 07:50 Suspected Serious Injury 2018-05-20 17:21 No Apparent Injury	3 0 2 0 0 0 2 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy	Dry Straight & Level	Daylight From	ring at angle n same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Going Straight Ahead Stopped at Signal or Stop Sign	orth South West outh North Vehicle Stopped	Vehicle Stopped
6906 State Route 6907 State Route 6908 State Route	Cialiam 1 Cialiam 1	101 101 101		255.11 No 3615726 2 255.11 No 3615878 2 255.11 No E400250	2016-01-30 16:55 No Apparent Injury 2015-02-04 16:59 Possible Injury	1 0 2 0 0 0 2 0 2 0 2 0	U Pickup,Panel Truck or Vanette under 10,000 lb 0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Related	Overcast Clear or Partly Cloudy Raining	Dry Straight & Level Dry Straight & Level	Dawn From Daylight From	n same direction - one right turn - one straight n same direction - both going straight - one stopped - rear-end n opposite direction - one left turn - one straight	Going Straight Ahead Stopped at Signal or Stop S Making Left Turn	Stopped for Traffic Sign Going Straight Ahead Going Straight Ahead	outs North Vehicle Backing lorth South North /est North	Vehicle Stopped South West
6909 State Route 6910 State Route	Cialiam 1	101		255.11 No E490664 2 255.11 No E504037 2	2015-11-23 09:45 No Apparent Injury 2016-01-06 16:58 No Apparent Injury	0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Overcast	Dry Curve & Grade	Daylight Meta	Il Sign Post	Going Straight Ahead	Going Straight Ahead	/est West lortheast East East	West
6909 State Route 6910 State Route 6911 State Route 6912 State Route	Cialiam 1 Cialiam 1	101		255.11 No E523876 2 255.11 No E611570 2	2016-01-06 16:58 No Apparent Injury 2016-03-11 07:50 No Apparent Injury 2016-11-18 14:56 Possible Injury	0 0 2 0	O Pickup, Panel Truck or Vanette under 10,000 lb Panel Panel Care	Passenger Car Passenger Car	At Intersection and Related At Intersection and Related	Overcast Clear or Partly Cloudy Overcast	Dry Straight & Level Dry Straight at Hillor Dry Straight & Level	Daylight From Daylight Enter	ring at angle n same direction - both going straight - one stopped - rear-end ring at angle	Making Left Turn Going Straight Ahead Making Left Turn	Stopped at Signal or Stop Sign Going Straight Ahead	orth South Vehicle Stopped outh West West	Vehicle Stopped East
6913 State Route 6914 State Route 6915 State Route 6916 State Route 6916 State Route	Cialiam 1	101 101		255.11 No E636510 2 255.11 No E640113 2	2017-02-05 14:05 No Apparent Injury	0 0 2 0	0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related	Clear or Partly Cloudy Overcast	Dry Straight & Level Wet Straight & Level	Daylight Ente	ring at angle ring at angle	Making Left Turn Making Left Turn	Going Straight Ahead Going Straight Ahead	orth East West outh West West	East East
6916 State Route 6917 State Route	Cialiam 1	101		255.11 No E658006 2 255.11 No E660548 2	2017-03-30 17:56 Suspected Minor Injury 2017-03-30 11:34 Possible Injury	0 0 2 0 1 0 1 0 2 0 2 0	0 Motorcycle 0 Pickup,Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10.000 lb	Intersection Related but Not at Intersection At Intersection and Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Level Dry Straight & Level	Daylight Veh	n same direction - both going straight - one stopped - rear-end cle overturned n opposite direction - one left turn - one straight	Slowing Slowing Making Left Turn	Stopped at Signal or Stop Sign Going Straight Ahead	wenter supped ast West Fast /est North East	Vehicle Stopped West
6918 State Route 6919 State Route	Cialiam 1 Cialiam 1	101 101		255.11 No E737811 2 255.11 No E810605 2	2017-11-14 14:56 No Apparent Injury 2018-06-15 14:25 Possible Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup Panel Truck or Vanette under 10 000 lb	At Intersection and Related	Raining Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level	Daylight Ente Daylight Ente	ring at angle ring at angle	Starting From Parked Posi	ition Going Straight Ahead Going Straight Ahead	outh North West outh North West	East East
6917 State Route 6918 State Route 6919 State Route 6919 State Route 6920 State Route 6921 State Route 6921 State Route 6921 State Route 6922 State Route 6923 State Route 6923 State Route 6924 State Route 6925 State Route 6925 State Route 6926 State Route 6926 State Route	Cialiam 1 Cialiam 1	101		255.11 No E853573 2 255.11 No E931770 2	2017-11-14 14:56 No Apparent Injury 2018-06-15 14:25 Possible Injury 2018-06-15 15:45 No Apparent Injury 2018-06-18 15:55 No Apparent Injury 2019-07-12 15:24 Possible Injury 2019-07-12 15:24 Possible Injury	0 0 2 0	Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Packup Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight From Daylight From	n opposite direction - all otners n same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped at Signal or Stop Sign	ast West West orth South Vehicle Stopped	West Vehicle Stopped
6922 State Route 6923 State Route	Cialiam 1 Cialiam 1	101 101		255.11 No E948239 2 255.28 No E711778 2 256.28 No E826111	2019-07-12 15:24 Possible Injury 2017-08-19 14:30 No Apparent Injury 2018-08-04 13:15 Possible Injury 2018-08-04 10:05 No Apparent Injury	3 0 2 0 0 0 2 0	0 Passenger Car 0 Passenger Car 0 Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb		Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Ente	ring at angle	Making Left Turn Making Left Turn Making Left Turn	Going Straight Ahead Going Straight Ahead Going Straight Ahead	outh West West /est North East /ort North East	East West
6925 State Route 6926 State Route	Cialiam 1	101		255.28 No E888136 2 255.31 No E775220 2	2019-01-18 19:30 No Apparent Injury 2018-02-28 16:00 No Apparent Injury	0 0 1 0	0 Passenger Car		At Intersection and Related Not at Intersection and Not Related	Overcast	Dry Straight & Level Dry Curve & Level	Dark-Street Lights On Meta	a Sign Post as Struck by Object from the Load of Another Vehicle	Making Left Turn Going Straight Ahead	Going Straight Ahead	/est North /est East West	East
6926 State Route 6927 State Route 6928 State Route 6929 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101		255.38 No E378212 : 255.39 No E305259 :	2014-11-15 01:58 No Apparent Injury 2014-01-31 17:50 No Apparent Injury	0 0 1 0	Pickup Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Driveway	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Curve & Level	Dark-No Street Lights Cab Dark-No Street Lights From	es or Was Struck by Object from the Load of Another Vehicle 6 Barner s asme direction - all others r Objects	Going Straight Ahead Backing Making Right Turn	Going Straight Ahead	lorth South outh Vehicle Stopped West	East
6929 State Route 6930 State Route 6931 State Poute	Clallam 1 Clallam 1	101 101		255.39 No E923495 2 255.44 No E742835 2 266.66 No 2601116	2019-05-18 12:57 No Apparent Injury 2017-12-01 17:25 No Apparent Injury 2016-11-17 04:33 Surported Minor Injury	0 0 1 0	O Passenger Car Passenger Car Passenger Car Passenger Car Passenger Car Pickup Panel Truck or Vanette under 10,000 ib Pickup Panel Truck or Vanette under 10,000 ib Pickup Panel Truck or Vanette under 10,000 ib Rickup Panel Truck or Vanette under 10,000 ib	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy Raining Clear or Partly Cloudy	Dry Straight & Level Wet Straight & Level Dry Straight & Level	Dark-No Street Lights From	r Objects s ame direction - both going straight - both moving - sideswipe s same direction - both going straight - both moving - rear-end	Going Straight Ahead	Changing Lanes Going Straight Ahead	/est South ast West East fort East West West	West
6930 State Route 6931 State Route 6932 State Route 6933 State Route	Cialiam 1	101		255.61 No E969505 2 255.74 No E924760 2	2019-10-09 17:15 No Apparent Injury 2019-05-24 06:15 No Apparent Injury	0 0 1 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Truck - Double Trailer Combinatoria	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight Met: Daylight Veh	al Sign Post	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Comg disagn zerosa	ast West	
6934 State Route 6935 State Route 6936 State Route 6937 State Route 6937 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101 101		255.75 No E359807 2 255.76 No 2766944 2	2014-09-22 17:28 No Apparent Injury 2014-02-09 11:30 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight Fire Daylight Mail Dark-No Street Lights Veh	started in vehicle	Going Straight Ahead Going Straight Ahead		ast West /est East	
6936 State Route 6937 State Route	Clallam	101		255.90 No E680574 2 255.94 No E546205 2	2017-06-12 21:52 No Apparent Injury 2016-05-20 09:26 Suspected Minor Injury	1 0 2 0	Pickup Parel Truck or Vanette under 10,000 lb Pickup Parel Truck or Vanette under 10,000 lb Passenger Car Pickup Parel Truck or Vanette under 10,000 lb Passenger Car Pickup Parel Truck or Vanette under 10,000 lb Truck (Flatbad Van etc) Mobbroycle Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Driveway	Clear or Partly Cloudy	Dry Straight & Grade	Daylight From	n same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead Merging (Entering Traffic)	Going Straight Ahead	ast West outh West East	West
6938 State Route 6939 State Route 6940 State Route 6941 State Route	Clallam 1 Clallam 1	101		256.00 No E601811 : 256.10 No E538073 :	2016-10-28 23:55 No Apparent Injury 2016-04-25 13:22 No Apparent Injury	0 0 1 0	0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Grade	Dark-No Street Lights Miss Daylight Fire	dway Ditch Billaneous Chipet or Debris on Road stanted in vehicle cle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead		/est East	
6941 State Route 6942 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101		256.10 No E674452 : 256.15 No E428515 :	2017-05-22 09:36 No Apparent Injury 2015-05-24 20:15 Suspected Minor Injury	1 0 1 0	0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight in Sag Dry Straight & Grade	Daylight Veh Daylight Gua	cle Strikes Deer rdrail - Through, Over or Under	Going Straight Ahead		ast West	
6942 State Route 6943 State Route 6944 State Route 6945 State Route 6946 State Route	Cialiam 1 Cialiam 1	101 101		256.19 No E996342 2 256.20 No E379036 2 256.24 No E733146	2019-12-20 18:35 No Apparent Injury 2014-11-29 13:40 No Apparent Injury 2017-11-05 23:30 No Apparent Injury	0 0 2 0	O Passenger Car O Passenger Car O Passenger Car O Pickup Panel Truck or Vanette under 10,000 lb Passenger Car O Pickup Panel Truck or Vanette under 10,000 lb Dickup Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level Snow/Slush Straight & Level Other Curve & Level	Dark-No Street Lights From Daylight Brid Dark-Street Lights On Other	rdrail - Through, Over or Under same direction - both going straight - both moving - rear-end ge Rail - Face gr Cbjects	Going Straight Ahead Going Straight Ahead	Going Straight Ahead	/est East West /est East	East
6946 State Route 6947 State Route 6948 State Route	Clallam 1 Clallam 1	101		256.29 No E987884 2 256.34 No E461164 2	2019-11-09 11:10 No Apparent Injury 2015-09-10 14:50 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Dry Straight in Sag	Daylight From	n same direction - both going straight - both moving - sideswipe	Going Straight Ahead Going Straight Ahead	Changing Lanes	/est East East West East	West
6948 State Route 6949 State Route	Cialiam 1 Cialiam 1	101		256.40 No 2766960 2 256.40 No 3202389 2	2014-05-08 04:20 Possible Injury 2014-02-25 16:30 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Dark-Street Lights On Woo	d Sign Post n same direction - both going straight - one stopped - rear-end n opposite direction - one left tum - one straight	Going Straight Ahead Going Straight Ahead Making Left Turn	Stopped at Signal or Stop Sign	/est East ehicle Backing Vehicle Stopped South	North
6948 Saste Route 6949 Saste Route 6950 Saste Route 6950 Saste Route 6951 Saste Route 6952 Saste Route 6952 Saste Route 6953 Saste Route 6954 Saste Route 6954 Saste Route 6955 Saste Route 6956 Saste Route 6956 Saste Route 6956 Saste Route 6957 Saste Route 6957 Saste Route 6958 Saste Route	Cialiam 1	101		256.40 No E562729 2 256.40 No E672319 2	2014-12-30 17:28 No Apparent Injury 2016-07-10 15:00 No Apparent Injury 2017-04-30 10:23 No Apparent Injury	0 0 2 0	Updatesinger Lut. Plakup Panel Track or Variette under 10,000 lb. Piktup Panel Track or Variette under 10,000 lb. Piktup Panel Track or Variette under 10,000 lb. Piktup Panel Track or Variette under 10,000 lb. Plakup Panel Track or Variette under 10,000 lb. Plakup Panel Track or Variette under 10,000 lb. Piktup Panel Track or Variette under 10,000 lb. Piktup Panel Track or Variette under 10,000 lb. Passenger Car	Passenger Car Passenger Car	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight at Hillor	Daylight From	n opposite direction - one left turn - one straight n opposite direction - one left turn - one straight ring at angle	Making Lett Turn Making Lett Turn Making Lett Turn	Going Straight Ahead Going Straight Ahead Going Straight Ahead	/est North East /est North East lorth East West	West Fast
6953 State Route 6954 State Route	Cialiam 1 Cialiam 1 Cialiam 1 Cialiam 1 Cialiam 1	101		256.40 No E746575 2 256.40 No E923371 2	2017-12-05 13:28 Possible Injury 2019-05-21 09:19 No Apparent Injury	1 0 2 0	0 Passenger Car 0 Pickup,Panel Truck or Vanette under 10,000 lb	Truck (Flatbad, Van, etc) Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Overcast	Dry Straight & Level Dry Straight & Level	Daylight Enter Daylight From	ring at angle n opposite direction - one left turn - one straight	Going Straight Ahead Making Left Turn	Going Straight Ahead Going Straight Ahead	outh North East ast South West	West East
6955 State Route 6956 State Route	Cialiam	101 101		256.40 No E925430 : 256.40 No E996077 :	2019-05-25 15:23 Suspected Minor Injury 2019-12-09 13:33 Possible Injury	1 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Passenger Car	At Intersection and Related At Intersection and Related Not at Intersection and Not Related	Raining Overcast	Wet Straight & Level Dry Straight & Level Dry Straight & Conde	Daylight Ente	ring at angle	Making Left Turn Going Straight Ahead	Going Straight Ahead Going Straight Ahead	outh West West orth South West	East East
6957 State Route 6958 State Route 6959 State Route	Cialiam 1 Cialiam 1 Cialiam 1 Cialiam 1	101		256.50 No E903645 : 256.60 No E477318 :	2017-04-06 19:49 No Apparent Injury 2019-03-07 18:50 No Apparent Injury 2015-10-29 11:10 No Apparent Injury	0 0 2 0	Passenger Car Passenger Car Pickup Panel Truck or Vanette under 10 000 lb.	Passenger Car	Not at intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Snowing Overcast	Snow/Slush Straight & Level Dry Straight & Level	Dark-Street Lights On From Daylight Veh	ring at angle d Sign Post n same direction - both going straight - both moving - rear-end cle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	ast west /est East West	East
6960 State Route 6961 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101		256.70 No E775979 2 256.80 No E445026 2	2018-02-28 07:34 No Apparent Injury 2015-07-21 12:30 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy			Ne Colland School P Pole n same direction - both going straight - one stopped - rear-end n same direction - both going straight - both moving - sideswipe	Going Straight Ahead	Stopped in Roadway Going Straight Ahead	ast West Vehicle Stopped	Vehicle Stopped
6962 State Route 6963 State Route 6964 State Route	Clallam	101 101							Not at Intersection and Not Related	Overcast				Changing Lanes Slowing	Going Straight Ahead	ast West East ast West Fart	West
6965 State Route 6966 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101		256.83 No E932600 2 256.87 No E740631 2 256.90 No 2986217 2	2017-11-26 20:25 Suspected Serious Injury 2015-06-23 14:50 No Apparent Injury	3 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Not at Intersection and Not Related At Driveway Not at Intersection and Not Related	Clear or Partly Cloudy Overcast Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level	Dark-No Street Lights Enter Daylight From	cle Strikes Deer ring at rangle n same direction - both going straight - both moving - rear-end cle Strikes Deer	Going Straight Ahead Making Left Turn Going Straight Ahead	Going Straight Ahead Slowing	outh West West /est East West	East East
6968 State Route	Cialiam 1 Cialiam 1	101		256.90 No E974775 : 256.97 No E553761 :	2019-10-23 06:55 No Apparent Injury 2016-06-12 15:08 No Apparent Injury	0 0 1 0	O Passenger Car O Pickup Panel Truck or Vanette under 10,000 lb O Passenger Car	Passenger Car	Not at Intersection and Not Related At Driveway	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight From	n same direction - both going straight - both moving - sideswipe	Changing Lanes	Going Straight Ahead	/est East West	East
6969 State Route 6970 State Route 6971 State Pouts	Cialiam 1	101 101		257.09 No E367679 2 257.13 No 3615774 2 257.13 No E422039 2	2014-10-21 14:59 Suspected Serious Injury 2018-03-31 17:45 No Apparent Injury 2015-05-01 13:46 No Apparent Injury	3 0 2 0 0 0 2 0	U Pickup,Panel Truck or Vanette under 10,000 lb 0 Truck (Flatbad,Van,etc)	Proxup,Panel Truck or Vanette under 10,000 lb	At Intersection and Not Related At Intersection and Not Related At Intersection and Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight From Daylight Ove	n opposite direction - all others Roadway Power Lines	Going Straight Ahead Going Straight Ahead Making Right Turn	Going Straight Ahead	ast West West ast West Earl	East West
6972 State Route 6973 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101 101 101		257.13 No E422039 2 257.13 No E998707 2 257.30 No 2766888 2	2019-12-30 08:23 No Apparent Injury 2014-06-18 21:20 No Apparent Injury	0 0 1 0	Passenger Car Passenger Car	- Joseph Cui	At Intersection and Not Related Not at Intersection and Not Related	Overcast	Wet Straight & Level	Daylight Veh	cle Strikes Deer		Going Straight Ahead	ast West East	******
6974 State Route 6975 State Route	Clallam 1 Clallam 1 Clallam 1	101		257.30 No 2766888 2 257.30 No E746578 2 257.30 No E761216 2	2017-12-11 07:49 No Apparent Injury 2018-01-21 13:23 No Apparent Injury	0 0 1 0 0 0 1 0	o Passenge Cat Plokup Panel Truck or Vanette under 10,000 lb Truck (Flatbad Van.net) Plokup Panel Truck or Vanette under 10,000 lb Plokup Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Plokup Panel Truck or Vanette under 10,000 lb Plokup Panel Truck or Vanette under 10,000 lb Plokup Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Overcast	lce Straight & Level Dry Straight & Level	Dark-No Street Lights Veh Daylight Met: Daylight Tree	l Sign Post or Stump (stationary)	Going Straight Ahead Going Straight Ahead Going Straight Ahead		/est East //est East	
8975 Salate Route 8976 Salate Route 8976 Salate Route 8977 Salate Route 8977 Salate Route 8977 Salate Route 8980 Salate Route 8980 Salate Route 8982 Salate Route 8982 Salate Route 8982 Salate Route 8983 Salate Route 8984 Salate Route 8985 Salate Route 8985 Salate Route 8986 Salate Route 8987 Salate Route 8987 Salate Route 8988 Salate Route 8988 Salate Route 8988 Salate Route	Clallam 1 Clallam 1 Clallam 1 Clallam 1 Clallam 1	101 101		257.37 No 2986211 2 257.40 No E809506 2 257.45 No E731120	2015-01-05 07:30 Suspected Minor Injury 2018-06-16 06:12 No Apparent Injury	1 0 1 0	O Prototype and Truck or Variette under 10,000 to Possenger CE / P		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level Dry Straight & Level	Dawn Eart Dawn Veh Dark-No Street Lights Veh Daylight Veh	h Bank or Ledge cle Strikes Deer cle Strikes Peer	Going Straight Ahead Going Straight Ahead		/est East ast West	1
6979 State Route 6980 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101		257.45 No E731120 : 257.46 No E546951 : 257.47 No E923494 :	2016-05-24 10:32 No Apparent Injury 2019-05-23 08:35 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Veh Daylight Veh	cle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead		/est East ast West	#
6981 State Route 6982 State Route	Clallam 1 Clallam 1	101		257.60 No E835202 2 257.61 No E459949 2	2018-09-02 23:47 No Apparent Injury 2015-09-05 12:05 Possible Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Veh Dark-No Street Lights Veh Daylight Woo	cle Strikes Deer d Sign Post	Going Straight Ahead Changing Lanes		/est East	
6983 State Route 6984 State Route	Cialiam 1	101		257.70 No E536574 : 257.73 No E770756 :	2016-03-31 16:08 Suspected Minor Injury 2018-02-18 15:56 No Apparent Injury	4 0 2 0 0 0 2 0	0 Passenger Car 0 Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	loe Straight & Level	Daylight Roa	ring at angle	Changing Lanes Making Left Turn	Going Straight Ahead Going Straight Ahead	ast West West ast South West	East East
6986 State Route 6987 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101		257.74 No E897847 2 257.76 No E727028 2 257.80 No E671868 2	2018-02-18 15:56 No Apparent Injury 2019-02-25 14:07 No Apparent Injury 2017-01-10 08:46 No Apparent Injury 2017-05-15 08:15 No Apparent Injury 2013-08:13 08:20 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car	At Intersection Related but Not at Intersection At Intersection and Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Overcast	Dry Straight & Level Dry Curve & Level Dry Straight & Level	Daylight Sam Daylight Veh	n same direction - both going straight - both moving - sideswipe e direction - both turning right - one stopped - rear end cles Strikes Deer	Making Left Turn Making Right Turn Going Straight Ahead	Going Straight Ahead Making Right Turn	outh East South	East
6988 State Route 6989 State Route 6990 State Route 6991 State Route 6991 State Route	Cialiam 1 Cialiam 1 Cialiam 1 Cialiam 1 Cialiam 1	101					0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight Veh	cle Strikes Deer			/est East /est East	
6990 State Route 6991 State Route		101 101		257.82 No E563386 : 257.90 No E476469 : 257.90 No E49981 :	2015-10-28 10:37 No Apparent Injury 2015-12-22 06:29 Possible Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related	Raining Clear or Partly Cloudy	Wet Straight & Level loe Straight & Grade	Dark-Street Lights On Veh	cle Strikes Deer cle overturned	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Levelle Parks 110	/est East /est East	#=
6993 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101 101							Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Straight & Level	Dark-Street Lights On One Daylight Veh	cle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Legally Parked, Unoccupied	/est East ast West	+
6995 State Route 6996 State Route	Cialiam 1	101		257.92 No E794493 2 257.94 No E969506 2	2018-05-03 13:05 No Apparent Injury 2019-10-09 19:31 No Apparent Injury	0 0 1 0	Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Veh Daylight Veh Dark-No Street Lights Veh	cle overturned cle Strikes Deer	Going Straight Ahead		/est East ast West	
6997 State Route 6998 State Route	Clallam 1	101 101		258.00 No E428511 : 258.00 No E453276 : 258.00 No E457327 :	2015-05-25 17:00 No Apparent Injury 2015-08-12 07:16 No Apparent Injury	0 0 1 0	O Pickup, Panel Truck or Vanete under 10,000 b. Pickup, Panel Truck or Vanete under 10,000 b. Pickup, Panel Truck or Vanete under 10,000 b. O Passenger Car. O Pickup, Panel Truck or Vanete under 10,000 b. O Pickup, Panel Truck or Vanete under 10,000 b. O Pickup, Panel Truck or Vanete under 10,000 b. O Pickup, Panel Truck or Vanete under 10,000 b. O Pickup, Panel Truck or Vanete under 10,000 b. O Pickup, Panel Truck or Vanete under 10,000 b. O Pickup, Panel Truck or Vanete under 10,000 b. O Pickup, Panel Truck or Vanete under 10,000 b. O Pickup, Panel Truck or Vanete under 10,000 b. O Pickup, Panel Truck or Vanete under 10,000 b. O Pickup, Panel Truck or Vanete under 10,000 b. O Pickup, Panel Truck or Vanete under 10,000 b.		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Level	Daylight Terr Daylight Veh	porary Traffic Sign, Barricade or Construction Materi cle Strikes Deer deary Ditch	Going Straight Ahead		/est East /est East	#=
7000 State Route 7001 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101 101		258.00 No E467213 2 258.00 No E873956 2 258.00 No E981551 2	2018-12-18 10:02 Possible Injury 2019-11-12 17:28 No Apparent Injury	0 0 1 0 2 0 2 0	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Raining	Wet Straight & Level Wet Straight & Grade Wet Straight & Grade	Daylight Roa Daylight From	ponery Taffe Sign, Barricade or Construction Materi de Drikes Deer de Drikes Deer deep Otton same direction - both going stasight - both moving - near-end de Britas Eder d	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing	ast West /est East West	East
7002 State Route 7003 State Route	Cialiam Cialiam Cialiam Cialiam Cialiam	101		258.01 No E507537 : 258.03 No E718489 :	2016-01-11 20:43 No Apparent Injury 2017-08-17 12:25 No Apparent Injury	0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level	Dark-No Street Lights Utili Daylight From	y Pole n same direction - all others	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	ast West West	East
7004 State Route 7005 State Route	Cialiam 1	101		258.07 No E921917 : 258.12 No 2978885 :	2019-05-19 22:12 Possible Injury 2018-02-05 13:16 No Apparent Injury	3 0 2 0 0 0 2 0	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Level	Dark-Street Lights On From Daylight From	n same direction - both going straight - both moving - rear-end n same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead Starting From Parked Posi	Going Straight Ahead Going Straight Ahead sition Starting From Parked Position	/est East West lorth South North	East South
7006 State Route 7007 State Route 7008 State Route	Clallam 1 Clallam 1 Clallam 1 Clallam 1 Clallam 1	101 101 101		258.12 No E344427 2	2014-07-20 19:51 Possible Injury	1 0 2 0	0 Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Curve & Level loe Straight & Grad-	Dark-Street Lights On Enter Daylight From Dark-Street Lights On Street	n same direction - both going straight - both moving - rear-end ring at angle n same direction - one left turn - one straight et Light Pole or Base	Starting in Traffic Lane Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead Making Left Turn	orn South East /est East West ast West	West North
7008 Sate Route 7009 Sate Route 7010 State Route 7010 State Route 7011 State Route 7012 State Route 7012 State Route 7013 State Route 7014 State Route 7016 State Route 7016 State Route 7016 State Route 7018 State Route	Clallam	101		258.12 No E504038 2 258.14 No E300877 2	2016-01-01 19:00 Possible Injury 2014-01-15 17:26 No Apparent Injury	1 0 2 0	O Passenger Car Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb O Pickup, Panel Truck or Vanette under 10,000 lb O Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Wet Straight & Grade	Dark-Street Lights On Fron	n same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead	Slowing Stopped for Traffic	/est East West ast Vehicle Stopped	South Vehicle Stopped
7011 State Route 7012 State Route	Clallam 1 Clallam 1 Clallam 1	101		258.14 No E372345 : 258.14 No E617104 :	2014-11-01 19:16 No Apparent Injury 2016-12-06 08:52 No Apparent Injury	0 0 1 0	O Passenger Car. O Pictusp Panel Truck or Vanette under 10,000 lb O Pictusp Panel Truck or Vanette under 10,000 lb O Passenger Car. O Pictusp Panel Truck or Vanette under 10,000 lb O Passenger Car. O Pictusp Panel Truck or Vanette under 10,000 lb O Pictusp Panel Truck or Vanette under 10,000 lb O Pictusp Panel Truck or Vanette under 10,000 lb O Truck Tructor & Semi Trailer	December Co.	Not at Intersection and Not Related	Fog or Smog or Smoke Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Snow/Slush Straight & Grade	Dark-Street Lights On Roa Daylight Stre	n same direction - both going straight - one stopped - rear-end dway Ditch at Light Pole or Base	Going Straight Ahead Going Straight Ahead Slowing Going Straight Ahead		/est East ast West	West
7013 State Route 7014 State Route 7015 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101 101		258.16 No E382585 2 258.22 No 3616146	2014-12-04 06:40 No Apparent Injury 2014-12-04 06:40 No Apparent Injury 2014-11-29 14:40 No Apparent Injury	0 0 2 0 0 0 1 0 0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10 000 lb.	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast Clear or Partly Cloudy		Daylight From Dark-Street Lights On Veh Daylight Brid Dawn Veh		Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing	ast West East //est East	west
7016 State Route 7017 State Route	Clallam 1 Clallam 1	101		258.22 No E876257 2 258.32 No E681904 2	2018-12-23 07:25 No Apparent Injury 2017-06-14 23:00 No Apparent Injury	0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Wet Straight & Grade Dry Straight at Hillor	Dawn Veh Dark-No Street Lights One Daylight Fire	cle Strikes Deer parkedone moving	Going Straight Ahead Going Straight Ahead Going Straight Ahead		ast West	
7018 State Route	Ciallam	101	 	258.50 No E316973	2014-03-11 10:28 No Apparent Injury	0 0 1 0	0 Truck Tractor & Semi-Trailer	4	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level	Daylight Fire	started in vehicle	Going Straight Ahead		/est East	

The content of the	MV DRIVER CONTRIBUTING MV DRIVER CONTRIBUTING	MV_DRIVER CONTRIBUTING MV_DRIVER MV_DRIVER	ER MV DRIVER CONTRIBUTING MV DRIVER CONTRIBUTING	MV DRIVER CONTRIBUTING MV D	RIVER MV_DRIVE	R RICYCLIST CONTRIBUTING BICYCLIST CONTRIBUTING	PICYCLIST CONTRIBUTING BICYCLIST	BICYCLIST BICYCLIST_CONTRIBUTING BICYCLIST_	CONTRIBUTING	BICYCLIST_CONTRIBUTING BICYCL	IST RICYCLIST PEDESTRIAN CONTRIBUTION	PEDESTRIAN CONTRIBUTING	PEDESTRIAN CONTRIBUTING PEDESTRIAN PEDESTRIAN	FIRST IMPACT LOCATION
Series of the se	CIRCUMSTANCE_1_UNIT_1 CIRCUMSTANCE_2_UNIT_1	64	CIRCUMSTANCE_1_UNIT_2 CIRCUMSTANCE_2_UNIT_2	CIRCUMSTANCE_3_UNIT_2 AGE_U	NIT_2 GENDER UNIT_2	CIRCUMSTANCE_1_UNIT_1 CIRCUMSTANCE_2_UNIT_1	CIRCUMSTANCE_3_UNIT_1 AGE UNIT_1	GENDER UNIT_1 CIRCUMSTANCE_1_UNIT_2 CIRCUMSTA		CIRCUMSTANCE_3_UNIT_2 AGE_UN	T_2 GENDER_UNIT_2 CIRCUMSTANCE_1_UNIT_2	CIRCUMSTANCE_2_UNIT_2		
Series of the se	pparently Asleep or Fatigued one	37 Maie 19 Female												Sat reggis condender instruments Milepost Lane 1 Decreasing Miepost
Series of the se	attention lone lone	29 Maie 60 Maie 26 Maie	None		36									Intersecting Road increasing Milepost Lane 2 Decreasing Milepost Lane 4 Decreasing Milepost
Series of the se	None None	42 Male 45 Male												Lane 1 Decreasing Milepost Lane 1 Increasing Milepost
Series of the se	None Driver Not Distracted Unattention Inattention													
Series of the control	Did Not Grant RW to Vehicle Inattention Exceeding Reas. Safe Speed				55 Male									
Series of the control	Exceeding reas, sale Speed None None	24 Marie 24 Female 62 Female												Lane 2 Increasing Miepost
Series of the control	Driver Not Distracted None None	69 Male 37 Female 76 Female												Lane 2 Decreasing Milepost Lane 1 Increasing Milepost
Second S	None Other Contributing Circ Not Listed Inattention Exceedi													Past Right Shoulder Increasing Milepost
Second S	None Driver Not Distracted Under Influence of Alcohol	56 Female 26 Female 47 Female												ane 1 Increasing Milepost ane 2 Increasing Milepost Past Right Shoulder Decreasing Milepost
Second S	Other Contributing Circ Not Listed Under Influence of Alcohol	52 Male 58 Female	Improper Turn/Merge		24 Female									Lane 2 Increasing Milepost
Series of the se	None None													are 2 Decleasing wirepost ane 1 Increasing Milepost ane 1 Increasing Milepost
Series of the se	Driver Not Distracted None Exceeding Reas. Safe Speed	37 Male 58 Male 64 Male												ane 1 Increasing Miepost - ane 1 Increasing Miepost - 39st Right Shoulder Decreasing Miepost
Second	Apparently Asteep or Fatigued Non Motorist on Wrong Side of Road Unknown Distraction													Past Right Shoulder Increasing Milepost
Second	Apparently Asleep or Fatigued Apparently Asleep or Fatigued None None	20 Male 18 Male 42 Male												Lane 1 Decreasing Mileoost
Control Cont	Inattention Other Driver Distractions Inside Vehicle Exceeding Reas. Safe Speed	54 Male												a Median Decreasing Milepost Past Right Shoulder Increasing Milepost
	None Under Influence of Alcohol Under Influence of Drugs	36 Maie 52 Female 26 Maie	None Driver Not Distracted		57 Male 29 Male									Jane 1 increasing Miepost Jane 2 Decreasing Miepost Jane 2 Decreasing Miepost
	None Exceeding Reas. Safe Speed	50 Female 65 Male												Lane 1 Increasing Milepost Past Right Shoulder Increasing Milepost
	Did Not Grant RW to Vehicle Exceeding Reas. Safe Speed	28 Female 26 Female	None		43 Female									Lane 2 Increasing Milepost
	None None	44 Male 50 Female												ane 2 Increasing Milepost ane 1 Increasing Milepost ane 2 Increasing Milepost
Control Cont	None None													
Selection of the select	Under Influence of Alcohol Under Influence of Drugs Exceeding Reas. Safe Speed Operating Handheld Cell Phone None	17 Male												Past Right Shoulder increasing Iwiepost
Selection of the select	None Did Not Grant RW to Vehicle Did Not Grant RW to Vehicle	20 Female 60 Male	None		65 Male									Lane 1 Increasing Milepost Lane 2 Decreasing Milepost
Selection of the select	None None Operating Defective Equipment	60 Male 74 Male 28 Male												Lane 1 Decreasing Miepost Lane 1 Increasing Miepost Past Right Shoulder Increasing Miepost
Selection of the select	Exceeding Reas. Safe Speed Follow Too Closely Exceeding Reas. Safe Speed	18 Female 19 Male	None None		52 Male 26 Male								 	Lane 1 Decreasing Milepost Lane 1 Increasing Milepost
Selection of the select	Institution Other Contributing Circ Not Listed	40 Female 30 Maie 86 Maie	None		68 Male									ins a Decreasing Milepost -ast Right Shoulder Decreasing Milepost
Selection of the select	Did Not Grant RW to Vehicle Inattention				51 Male									
Selection of the select	Inattention Improper Turn/Merge	58 Female 18 Female	Unknown Distraction None		22 Male 57 Male									senseconing Rosau Decreasing Milepost American Milepost
Selection of the select	Under Influence of Alcohol Did Not Grant RW to Vehicle													ast Right Shoulder Decreasing Milepost .ane 2 Decreasing Milepost .ane Decreasing Milepost
Active Service	Did Not Grant RW to Vehicle Did Not Grant RW to Vehicle				68 Male									Lane 2 Increasing Milepost Lane 2 Increasing Milepost
Active Service	Did Not Grant RW to Vehicle Driver Interacting with Passengers, Anim Follow Too Closely Under Influence of Alcohol Exceeding Reas. Safe Speed	26 Female 26 Male 61 Male	None None		62 Female 52 Male									ane 2 Increasing Miepost
Active Service	Improper Turn/Merge Inattention Did Not Inattention Did Not Grant RW to Vehicle	ot Grant RW to Vehicle 65 Female 51 Female	None Driver Not Distracted		23 Female 38 Male									ane 1 Decreasing Miepost ane 1 Increasing Miepost
Active Service	hattention Did Not Grant RW to Vehicle None Inattention	39 Male 27 Male 25 Female	Drier Not Distracted Diel Not Grant RW to Vehicle None		79 Male 82 Male 52 Male									ane 2 Increasing Milepost ane 2 Decreasing Milepost intersecting Road Decreasing Milepost
Second Personal Per	Did Not Grant RW to Vehicle Inattention Did Not Grant RW to Vehicle				26 Female									Lane 1 Decreasing Milepost
Second Personal Per	Und Not statin RVW to Vehicle Under Influence of Alcohol Other Contributing Circ Not Listed	17 Male	None											Jine 1 Decreasing Miepost Jacreasing Other Location Jane 2 Increasing Miepost
Second Personal Per	Apparently As leep or Fatigued Other Contributing Circ Not Listed Under Influence of Drugs Expending Park Safe Spend	20 Male 51 Female 20 Male	None		53 Male									Median Decreasing Milepost ane 1 Increasing Milepost Oran Point Shoulder Increasing Milepost
Common	Apparently Asleep or Fatigued	24 Female	Other Contributing Circ Not Listed Did Not Grant RW to Vehicle		61 Male									Lane 1 Decreasing Milepost
Common	Under Influence of Alcohol None Coerating Defective Equipment	49 Male 71 Male 36 Female												ast Right Shoulder Decreasing Miepost ane 1 Decreasing Miepost ane 1 Decreasing Miepost
An Antonian Care Not Listed Combination Care Not Listed Comb	None	23 Female 62 Female	Disco Met Districted		20 Mala									ast Right Shoulder Increasing Milepost ane 1 Decreasing Milepost
Series of the se	Did Not start RW to Vehicle Other Contributing Circ Not Listed None				26 Male									In Median Decreasing Milepost Lane 2 Increasing Milepost
Second	None None	56 Female 45 Female 20 Male												Lane 1 Increasing Milepost Lane 1 Decreasing Milepost
Column	Under Influence of Drugs Exceeding Reas. Safe Speed None	29 40 Male	None		60 Female									ane 1 Increasing Milepost Vedian Shoulder Increasing Milepost
State Stat	Exceeding Reas. Sate Speed None None	30 Female			68 Female									Lane 1 Decreasing Milepost
Control Cont	Driver Not Distracted Inattention	35 Male	None		44 Male									ast Right Shoulder Increasing Milepost htersecting Road Increasing Milepost
Series of the se	Did Not Grant RW to Vehicle Did Not Grant RW to Vehicle	44 Female 79 Male	None None		39 Female 30 Male									Lane 1 Decreasing Milepost Lane 3 Increasing Milepost
State Stat	Under Influence of Alcohol Inattention Did Not Inattention Did Not Grant RW to Vehicle	ot Grant RW to Vehicle 74 Female 52 Male 47 Male	None Driver Not Distracted None		48 Male 29 Female 23 Female									Lane 1 Increasing Milepost
See 1	Inattention Did Not Grant RW to Vehicle Apparently Fatigued Other Driver Distractions Inside Vehicle	69 Female 20 Female	None		57 Female									ane 1 Increasing Milepost Past Right Shoulder Decreasing Milepost
Series of the se	None Apparently Fatigued	30 Female 69 Female 35 Male	Exceeding Reas. Safe Speed Follow Too Closely											Lane 1 Increasing Milepost
Control Cont	hattention Exceeding Reas. Safe Speed Follow Too Closely Forestring Reas: Safe Speed	87 Maie 32 Maie	Other Contributing Circ Not Listed None		67 Male 35 Male									ane 2 Increasing Milepost ane 2 Decreasing Milepost Control of State Control of Control
Second	Exceeding reas - Safe Speed None Did Not Grant RW to Vehicle	25 Male 67 Male 50 Male	None											Past Right Shoulder Decreasing Milepost Lane 1 Increasing Milepost
Second	Follow Too Closely Inattention None Instruction Instruction Instruction	27 Male 26 Male	None None							-				Lane 1 Increasing Milepost
Martin M	Under Influence of Alcohol Other Contributing Circ Not Listed Inattenti		None		24 Female									Right Shoulder Increasing Milepost Lane 1 Increasing Milepost
Selection of the select	Did Not Grant RW to Vehicle None	78 Male 26 Female 27 Male	None											Lane 2 Decreasing Milepost Lane 1 Decreasing Milepost
Series of the se	Under Influence of Alcohol Other Contributing Circ Not Listed	44 Male 48 Female												ast Right Shoulder Increasing Milepost Past Right Shoulder Decreasing Milepost
Second	Under Influence of Alcohol None	22 Male 47 Male 65 Male												
Sequence of the sequence of th	None None	19 Female 36 Male												Lane 1 Decreasing Milepost
Second Build Second Build	None Exceeding Reas. Safe Speed Improper Passing	41 Female 20 Female 36 Male	Driver Not Distracted		63 Male									Lane 1 Increasing Miepost Past Right Shoulder Increasing Miepost In Median Decreasing Miepost
Series of the se	Exceeding Reas. Safe Speed Improper U-Turn Did Not Grant RW to Vehicle College To Clare by	49 Male 61 Male	None None		69 Female 76 Male									Lane 2 Increasing Milepost
Second	None None	42 Male 45 Female 33 Male	T NAT TO											Lane 1 Decreasing Milepost Lane 1 Increasing Milepost
Seed of the seed o	None None Exceeding Reas. Safe Speed	43 Female 72 Male 24 Engr - 1												Lane 2 Increasing Milepost
	Exceeding Reas. Safe Speed None	65 Female	Notice											Lane 1 Increasing Milepost Lane 1 Increasing Milepost
Solidar Soli	Univer Not Distracted Exceeding Reas. Safe Speed Other Driver Distractions Inside Vehicle None	70 Female 25 Male												ane 1 Decreasing Mlepost ane 1 Decreasing Mlepost ane 1 Decreasing Mlepost
Company of the control of the cont	hattention None	55 Male 51 Female												
Part Right Shoulder Shoulders Part Right S	Apparenty releep or Fatigued Exceeding Reas. Safe Speed None	78 Female 29 Male 17 Male	inatiention		71 Male									Lane 2 Increasing Milepost
According to the content of the co	Other Contributing Circ Not Listed Operating Defective Equipment	18 Male 54 Male	Driver Not Distracted		63 Male									ast Right Shoulder Decreasing Milepost ane 2 Decreasing Milepost
A A A A A A A A A A	Did Not Grant RW to Vehicle	23 Female 17 Maie 19 Maie	Follow Too Closely None		67 Female									Intersecting Road Decreasing Milepost Lane 1 Decreasing Milepost
Second Ress. Cale Speed Second Ress. Cale Speed Ress. Cale Speed Second Ress. Cale Speed Second Ress. Cale Speed Ress. Cale Speed Second Ress. Cale Speed Ress. Cale Speed Ress. Cale Speed Second Ress. Cale Speed Ress.	Under Influence of Alcohol Follow Too Closely Exceeding Reas: Safe Speed	48 Female 72 Male	None											Past Right Shoulder Decreasing Milepost
19 19 19 19 19 19 19 19	Exceeding Reas. Safe Speed	39 Male 23 Male	None		o∠ Male 47 Female									_nus : muteasing Milepost -ast Right Shoulder Increasing Milepost
one	Exceeding Reas. Safe Speed	19 Male 44 Male	None		88 Male									
96 56 Fernick 5 Fernic	Exceeding Reas. Safe Speed	72 Male 53 Female 37 Female												Inglish Shoulder Increasing Miepost ane 2 Decreasing Miepost
	None Operating Defective Equipment								-					Lane 1 Decreasing Milepost

OBJECTID WA, STATE PLANE SOUTH, Y 2010_FORWARD COUNTY YES			DL COL COL_AGE SURFACE MPAIRED MPAIRED CONDITIONS	COL_LIGHT CONDITIONS	COL COL STUDI	COL COL TYPE SEVERITY TEXT	COL_HUMAN COL FACTOR_Decoded MVIW	COL_LIGHT COL_SUI CONDITIONS CONDIT Decoded Decod	TIONS Decoded		nasis Roadway Departure
6841 945728.37 1023041.15 201 6842 945728.32 1023050.39 201 6843 945894.4 1022904.85 201	17 Vehicle-Party Struck Object	Property Damage Only Often/Unknown Und Property Damage Only Inattention 25-3 Property Damage Only Other/Unknown 65-6	4 No Dry 4 No Dry	Dark-Street Lights On At Inters Dark-Street Lights On Not at In Daylight At Driven Daylight Not at In	section and Not Related Straight & Level section and Not Related Straight & Level netcon and Not Related Straight & Level nters ection. Driveway/Roundabout Straight & Level way or Inters ection. Straight & Grade intersection. Driveway/Roundabout Straight & Grade Straight & Level nters ection. Driveway/Roundabout Straight & Level nters excess.	No Vuinerable Road User Involved	None	Daykight		Apparently Acleep, Fatigued or II Apparently Acleep, Fatigued or II No Risky Driving Behaviors Involved Inattention No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	Yes Yes No No No No
6847 946455.82 1022198.43 201	17 1 Vehride Party Animal	Piopenty Damage Only Oher Unknown 25-5 Propenty Damage Only Oher Unknown 35-5 Propenty Damage Only Oher Unknown 45-6 Propenty Damage Only Oher Unknown 45-7 Propenty Damage Only Oher Unknown 65-7 Propenty Damage Only Oher Unknown 65-7 Morror or Possible Injury Pall to Vided 25-5 Morror or Possible Injury Pall to Vided 25-5	No Dry	Daylight Not at In Dark-No Street Lights/Street Lights Off Not at In Dark-Street Lights On At Inters	ntersection Driveway/Roundabout Straight & Level Intersection Driveway/Roundabout Straight & Grade Intersection Driveway/Roundabout Straight & Level Straight & Level Straight & Level Straight & Level	No Winerable Road User Involved No Winerable Road User Involved Older Road User 65+ Older Road User 65+ Older Road User 65+	None None Other/Unknown Other Contribution Circ Not Listed		v Not at interaction/Drieway/Roundabout v At Interaction and Not Related	No Risky Driving Behaviors Involved	No No No No Yes
6851 947006.91 1021801.38 20: 6852 947045.05 1021754.27 20: 6853 948156.83 1021162.43 20: 6854 948156.83 1021162.43 20:	16 1 VehildelParty Struck Object 16 1 VehildelParty Struck Object 16 1 VehildelParty Animal 19 1 VehildelParty Animal	Property Damage Only Insarenzion Und Property Damage Only Speeding Und Property Damage Only Other/Unknown Und Property Damage Only Other/Unknown 55-€	42 S No LosSnowSlust 100 LosSnowSlust 101 S No LosSnowSlust 102 S No LosSnowSlust 102 S No Vet 4 No Vet 4 No Vet	Daylight Not at in	ntersection/Driveway/Roundabout Straight & Grade section and Not Related Straight & Level ntersection: Orneway/Roundabout Straight & Level stersection/Driveway/Roundabout Straight & Level stersection/Driveway/Roundabout Straight & Level straight & Level	No vunerabie Road User Involved No Vulnerable Road User Involved	Fall to vield Inattention Speeding None None	Daylight Usy Daylight Ice/Snow Daylight Daylight Usy Daylight We	y Not at Intersection/Driveway/Roundabout viSlush At Intersection and Not Related viSlush Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout at Not at Intersection/Driveway/Roundabout	Fail to Yield Inattention Speeding No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	Yes Yes Yes No
6856 948559.81 1021043.2[20] 6857 948562.01 1021042.57 20] 6858 948621.07 1021095.35 20] 6859 94875.73 1020987.23 20	17 VehicleParty Animal	Property Damage Only Often/Unknown 65+ Property Damage Only Often/Unknown Und Mnor or Possible Injury Often/Unknown Und	No Dry pr 25 No Dry pr 25 No Dry	Dark-Street Lights On Not at In Daylight Not at In Dark-No Street Lights/Street Lights Off Not at In Dark-Street Lights On Not at In	ntersectionOrivewayRoundabout Straight & Level ntersectionOrivewayRoundabout Straight & Grade ntersectionDrivewayRoundabout Straight & Level ntersectionOrivewayRoundabout Straight & Level ntersectionOrivewayRoundabout Straight & Level section and Not Related Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved	None Other Contributing Circ Not Listed	Dark-Street Lights On We Daylight Dny Dark-No Street Lights/Street Lights Off Dny Dark-Street Lights On Dny	Not at Intersection/Driveway/Roundabout	No Risky Driving Behawiors Involved	No No No Yes
6862 949113.92 1020947.71 20** 6863 949089.63 1020891.09 20** 6864 949120.38 1020883.01 20** 6865 949228.67 1020919.55 20**	15 Vehicle Party Animal 1 Vehicle Party Animal 1 Vehicle Party Animal 1 Vehicle Party Animal 2 Vehicle Party Animal 2 Vehicle Party Animal 3 Vehicle Party Animal 3 Vehicle Party Animal 4 Vehicle Party 4 V	Properly Damage Only Ofter/Unknown 45-6 Properly Damage Only Under Influence of Alcohol/Drugs/Medication 55-6 Mnor or Possible Injury Ofter/Unknown Und	4 No Dry 4 Under Influence of Acohol 45-54 Wet 4 No Dry 4 Under Influence of Acohol 55-64 Other	Daylight At Inters Daylight At Drive Daylight At Drive Daylight At Drive	section and Not Related Straight & Level section and Not Related Straight & Level way or Intersection Straight & Level section and Not Related Straight & Level section and Not Related Straight & Level section and Not Related Level section and Not Related Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved	Under Influence of Alcohol/Drugs/Medication Other Contributing Circ Not Listed Under Influence of Alcohol/Drugs/Medication	Dark-Street Lights On We Davlight Dn	y Al Intersection and Not Related y Al Intersection and Not Related at Al Intersection and Not Related y Al Diversection and Not Related y At Driveway or Intersection kinown Al Intersection and Not Related y Not at Intersection Driveway/Roundabout y Not at Intersection Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved	No Yes No Yes No
6867 949729.45 1020707.86 20 6868 949881.72 1020664.07 20	19 1 Vehicle-Derty		4 No Vet 4 No Dry 4 No Vet 4 No Vet 4 No Ico:SnowSlust	Dark-Street Lights On Not at In Dark-Street Lights On Not at In Daylight Not at In Daylight Not at In	ntersection Driveway/Roundabout Straight & Level Intersection Driveway/Roundabout Straight & Grade Intersection Driveway/Roundabout Straight & Grade Intersection Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved	None Other/Unknown None Speeding Apparently As leep or Fatigued	Dark-Street Lights On We Dark-Street Lights On Do.	y Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout to Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout	No Risky Driving Behawiors Involved Speeding Behawiors Involved Speeding Involved Apparently Asleep, Faligued or III	No No No No Yes
6872 951047.7 1020328.75 20 6873 951760.54 1020129.9 200 6874 952023.66 1020118.81 20 6875 952221.54 1020071.94 20 6876 952313.61 1020008.44 20	15 VerniceParty Struck Object	Unknown Rom wolorist on wrong side of Road 22-3- Property Damage Only Apparently Aeleep, Fatigued or III Und Minor or Possible Injury Apparently Aeleep, Fatigued or III Und Property Damage Only Other Unknown 35-4	4 No Wet y 25 Apparently Asileep, Fatigued or III Under 25 Dry y 25 Apparently Asileep, Fatigued or III Under 25 Dry y 25 Apparently Asileep, Fatigued or III Under 25 Dry y 3 Apparently Asileep, Fatigued or III Dry	Dark-Street Lights On Not at in Dark-Street Lights On Not at in Dawn/Dusk At Inters Dark-Street Lights On Not at in Dark-Street Lights On Not at in Dark-Street Lights On At Inters	ntersection/DrivewayRoundabout Unknown Intersection/DrivewayRoundabout Curve & Grade section and Not Related Curve & Level ttersection/DrivewayRoundabout Curve & Grade section and Not Related Curve & Evel section and Not Related Straight & Level	No Vuinerable Road User Involved	Non Motorist on Wrong Side of Road Apparently Asleep or Fatigued Apparently Asleep or Fatigued None	Dark-Street Lights On	w Not at hitersection/Driveway/Roundabout y Not at hitersection/Driveway/Roundabout y At Intersection and Not Related y Not at Intersection/Driveway/Roundabout at Intersection and Not Related that Intersection and Not Related	No Risky Driving Behaviors Involved Apparently Aeleop, Fatigued or III Apparently Aeleop, Fatigued or III No Risky Driving Behaviors Involved Inattention	Yes Yes Yes No Yes
6877 952320.63 1019985.14 201 6878 952428 1019974.61 201 6879 952694.04 1020017.23 201 6880 952764.96 1020013.89 201	18 1 Vehicle-Party Animal 1 Vehicle-Party Animal 2 Vehicle-Party Animal 2 Vehicle-Partes Silco-waipe 2 Vehicle-Partes Silco-waipe 4 1 Vehicle-Party 3 Vehicle-Party 3 Vehicle-Party 5 Vehicle Party 6 Vehicle Party 7 Vehicle Party 8 Vehicle Party 8 Vehicle Party 9 Vehicle Party 1 Vehicle Party 2 Vehicle Party 3 Vehicle Party 1 Vehicle Party 2 Vehicle Party 3 Vehicle Party 3 Vehicle Party 4 Vehicle Party 5 Vehicle Party 6 Vehicle Party 6 Vehicle Party 6 Vehicle Party 7 Vehicle Party 8 Vehicle Party 1 Vehicle Party	Unknown Pepperty Damage Coly P	4 No Dry 4 Under Influence of Alcohol 45-54 Dry 4 Under Influence of Drugs 25-34 Dry	Daylight Not at In Dark-Street Lights On Not at In	way or Intersection Straight & Level Intersection Driveway/Roundabout Straight & Level Intersection Driveway/Roundabout Straight & Grade Intersection Driveway/Roundabout Straight & Grade Intersection Driveway/Roundabout Straight & Grade Intersection Driveway/Roundabout Straight & Level Intersection Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved	None Under Influence of Alcohol/Drugs/Medication Under Influence of Alcohol/Drugs/Medication None	Daylight Dry Dark-Street Lights On Dry	th Al Driveway or Intersection y Not at Intersection/DrivewayRoundabout y Not at Intersection/DrivewayRoundabout y Not at Intersection/DrivewayRoundabout y Not at Intersection/DrivewayRoundabout	Speeding No Risky Driving Behaviors Involved Under Influence of Auchol/Drugs/Medication Under Influence of Auchol/Drugs/Medication Under Influence of Auchol/Drugs/Medication No Risky Driving Behaviors Involved	Yes No Yes Yes No
6884 953322.71 1019926.91 20: 6885 953323.82 1019926.86 20: 6886 953323.82 1019926.86 20: 6887 953377.1 1019924.26 20:	18 Twhice Party Asimal	Property Damage Only Fail to Yield 25-3 Property Damage Only Speeding 25-3 Property Damage Only Other/Unknown 35-4 Property Damage Only Other/Unknown 45-6	4 NO DPY 4 NO DPY 4 NO DPY 4 NO IcolSnow(Slust 4 NO Wet 4 NO DPY	Daylight Not at in	ntersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved	None Fail to Yield	Daylight Dry	y Not at intersection/Driveway/Roundabout y Not at intersection/Driveway/Roundabout visitush Not at intersection/Driveway/Roundabout to Not at intersection/Driveway/Roundabout y Not at intersection/Driveway/Roundabout	Speeding No Risky Driving Behaviors Involved Fail to Yelid Speeding No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No Yes Yes No No
6889 953851.38 1019901.18 20 6890 953851.76 1019901.16 20 6891 953904.55 101989.59 20 6892 95380.21 101990.21[20	15 Vehicle Party Animal	Minor or Possible Injury Other/Unknown 45-5 Property Damage Only Other/Unknown 55-5 Fatality Under Influence of Alcohol/Drugs/Medication 35-4 Property Damage Only Speeding Und	4 No Dry 5 No Dry 75 No Wet	Daylight Not at in Daylight Not at in Daylight Not at in Dark-No Street Lights/Street Lights Off Not at in Daylight Not at in	htersection/Dineway/Roundabout Straight & Level htersection/Dineway/Roundabout Straight & Grade htersection/Dineway/Roundabout Straight & Level htersection/Dineway/Roundabout Straight & Level htersection/Dineway/Roundabout Straight & Level	Motorcycle No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	None Under Influence of Alcohol/Drugs/Medication Speeding	Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight We Daylight We	y Not at Intersection/Dineway/Roundabout y Not at Intersection/Dineway/Roundabout y Not at Intersection/Dineway/Roundabout y Not at Intersection/Dineway/Roundabout to Not at Intersection/Dineway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication Speeding	No No No Yes Yes
6894 954197.87 1019885.64 201 6895 954336.27 1019939.81 201 6896 954374.52 1019938.91 201	18 Mehicle-Party	Property Damage Only Oher/Unknown Und Property Damage Only Oher/Unknown 55-5 Minor or Possible Injury Oher/Unknown 55-6 Property Damage Only Oher/Unknown 65-7 Manor or Possible Injury Oher/Unknown 65-7 Manor or Possible Injury Oher/Unknown 25-3	17:25 No Wet 17:25 No Wet 14 No Ory N	Dark-No Street Lights/Street Lights Off Not at In Daylight Not at In Dawn/Dusk Not at In Daylight Not at In	ntersection DrivewayRoundabout Straight & Level Intersection DrivewayRoundabout Straight & Level	No Vulnerable Road User Involved Older Road User 65+	None Other Contributing Circ Not Listed None None	Dark-No Street Lights/Street Lights Off We Daylight Dry Dawn/Dusk Dry Dry Dawn/Dusk Dry	y Not at Intersection/Drieway/Roundabout to Not at Intersection/Drieway/Roundabout y Not at Intersection/Drieway/Roundabout y Not at Intersection/Drieway/Roundabout	No Risky Driving Behaviors Involved	No Yes No No Yes
6901 955275.65 1019896.94 201	10 Nehrlad-Party Struck Cépers	Property Damage Only Speeding Und Property Damage Only Speeding Und	4 No Dry	Daylight Not at In Daylight Not at In Daylight Not at In Daylight Not at In Daylight At Drives	Intersection Divewsy/Roundabout Straight & Level Intersection Divewsy/Roundabout Straight & Level Intersection Divewsy/Roundabout Straight & Grade Intersection Divewsy/Roundabout Straight & Grade Intersection Divewsy/Roundabout Straight & Grade Intersection Divewsy/Roundabout Straight & Level Intersection	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Speeding Speeding None Inattention Other Contributing Circ Not Listed	Daylight Dry Daylight Dry Dark-Street Lights On Dry	y At Driveway or Intersection	Speeding Speeding Speeding No Risky Driving Behaviors Involved sasteretics No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No No No Yes
6905 955475.97 1019823.73 20 6906 955453 1019887.15 20 6907 955463.87 1019884.54 20 6908 955465.21 1019882.86 20	12	Property Damage Only Inattention 45-6 Monor or Possible Injury 25-5 25-5 Property Damage Only Inattention 55-4 Minor or Possible Injury Improper Turn/Merge Und Property Damage Only Under Influence of Mcohi/Drugs/Medication 15-5	4 No. West N	Daylight At Drives	way or Intersection Straight & Level construction Straight & Level way or Intersection Curve & Grade	No Vuinerable Road User Involved	Inattention Foliow Too Closely Inattention Improper Turn/Merge	Daylight Dry Dawn/Dusk Dry Daylight Dry Dark-Street Lights On We	y At Driveway or Intersection y At Driveway or Intersection y At Driveway or Intersection of At Driveway or Intersection	ran to retol facility Toro Closely Installention No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication	No No No No No Yes
6910 955498.87 1019883.38 20* 6911 955463.32 1019885.54 20* 6912 955469.01 1019822 20* 6913 955473.94 1019823.08 20*	16 2 Vehicles Parties Royle	Property Damage Only Fail to Yield 45-5 Property Damage Only Instantion Und Minor or Possible Injury Fail to Yield 45-5 Property Damage Only Fail to Yield Und Property Damage Only Fail to Yield 25-3 In Yield 25-3 10-10	4 No Dry 725 No Dry 1725 No Dr	DawnDusk At Drives Daylight At Drives	way or Intersection Straight & Level way or Intersection Straight & Level way or Intersection Straight & Hillicrest way or Intersection Straight & Level	No Vulnerable Road User Involved	Fail to Yield Inattention Fail to Yield Fail to Yield Fail to Yield	Dawn/Dusk Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry	y At Driveway or Intersection of At Driveway or Intersection of At Driveway or Intersection	Fail to Yeld Inattention Fail to Yeld Fail to Yeld Fail to Yeld Fail to Held	No No No No
6916 955467.63 1019825.91;20: 6917 955464.41 1019827.22;20: 6918 955471.46 1019823.58;20: 6919 955475.97 1019823.73;20:	17 2 Vehicles Parties Ress End 17 2 Vehicles Parties Turning 17 2 Vehicles Parties Turning 18 2 Vehicles Parties Angle 18 2 Vehicles Parties Angle 2 Vehicles Parties Sides veripe	Property Damage Only Distracted Driving 25-3	4 Under influence of Alcohol 55-64 Dry No Dry 4 No Vet 4 No Dry	Daylight Intersec Daylight At Drives Daylight At Drives	way or Intersection Straight & Level Curve & Level way or Intersection Straight & Level	Motorcycle Older Road User 65+	Distracted Driving Under Influence of Alcohol/Drugs/Medication Improper Turn/Merge Inattention	Daylight Dry	y Intersection Related but Not at Intersection y At Driveway or Intersection	Distracted Driving Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved Inathenico Inathenico No Risky Driving Behaviors Involved Inathenico No Risky Driving Behaviors Involved	No Yes No No No
9921 955486.88 1019821.58 20 6922 955486.88 1019821.58 20 6923 956365.51 1019840.77 20 6924 956369.73 1019839.55 20 6925 956364.31 1019842.59 20	19	Property Damage Only	4 No Dry No Dry No Dry 4 No Dry	Daylight At Drives	way or Intersection Straight & Level	No Vulnerable Road User Involved Older Road User S6+ No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved Vulnerable Road User Involved	Inationhon Fall to Yield Inationition Fall to Yield Under Influence of Alcohol/Drugs/Medication	Daylight	y At Driveway or Intersection	Inattention Fail to Yield Inattention Fail to Yield Under Influence of Alcohol/Drugs/Medication	No No No No Yes
6927 956908.64 1019794.84.20* 6928 956954.63 1019720.76 20* 6929 956956.2 1019719.3.20* 6930 957211.1 1019732.17 20*	18 Z Ukrides Parties Shrikes or Was Struck by Part or Object of Another Vehicle 1 Ukride Parties Booking 1 Ukride Parties Booking 1 Ukride Parties Booking 1 Ukride Parties Booking 1 Z Ukrides Parties Booking 2 Ukrides Parties Booking 2 Ukrides Parties Booking 3 Ukrides Parties Booking	Property Damage Only Apparently Aeleep, Fatigued or III Und Property Damage Only Other/Unknown 45-5 Property Damage Only Speeding Und Property Damage Only Other/Unknown Und	fr.ds No	Dark-No Street Lights/Street Lights Off Not at In Dark-No Street Lights/Street Lights Off At Driver Dark-No Street Lights/Street Lights Off At Driver Dark-No Street Lights/Street Lights Off Not at In	Intersection/Drinway/Roundabout Curve & Level Intersection/Drinway/Roundabout Straight & Grade way or Intersection way or Intersection Straight & Level Intersection/Drinway/Roundabout Straight & Level Intersection/Drinway/Roundabout Straight & Level Intersection/Drinway/Roundabout Straight & Level	No Wilnerable Road User Involved	Apparently Asleep or Fatigued Other Contributing Circ Not Listed Speeding None	Daylight Dark-No Street Lights Off Dry Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off We Dark-No Street Lights/Street Lights Off Dry	Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Apparently Aleep, Fatigued or III No Risky Driving Behaviors Involved Speeding No Risky Driving Behaviors Involved Speeding No Risky Driving Behaviors Involved Apparently Aleep, Fatigued or III	No Yes No Yes Yes No
6932 958107 1019551.34 201 6933 958761.69 1019417.62 201 6934 958819.2 1019405.76 201	16 Z. Vehrleas Parties Reaar End	Property Damage Only Other/Unknown 65+ Property Damage Only Operating Defective Equipment 35-4	1 NO Dry 1 NO Dry 4 NO Dry 4 NO Dry 4 NO Dry 4 NO Dry 6 Dry 7 25 NO Dry 7 NO Dry 7 NO Dry 8 N	Daylight Not at In Daylight Not at In Dark-No Street Lights/Street Lights Off Not at In	ntersection/Driveway/Roundabout Straight & Level	Older Road User 65+	Operating Handheld Cell Phone None	Daylight Dr.) Daylight Dr.) Davlight Dr.) Daylight Dr.) Dark-No Street Lights/Street Lights Off Dr.) Daylight Dr.)	y Not at Intersection/Drieway/Roundabout y Ab Drieway/or Intersection/Drieway/Roundabout y Ab Drieway/or Intersection/Drieway/Roundabout	Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Distracted Driving No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	Yes No No Yes No
6938 960055.63 1019150.08 20: 6939 960095.24 1019072.61 20: 6940 960615.65 1018965.6 20: 6941 96062.62 1019032.01(20:	14 1 Vehicle/Party Struck Object	Minor or Possible Injury Other/Unknown 55-6 Property Damage Only Other/Unknown Und Property Damage Only Other/Unknown 55-6 Property Damage Only Other/Unknown 45-6	4 No Dry 275 No Dry 4 No Dry 4 No Dry 4 No Dry 4 No Dry 6 No Dry 6 No Dry 6 No Dry 7	Daylight Not at In Daylight No Street Lights /Street Lights Off Not at In Daylight Not at In Daylight Not at In	ntersection/Driveway/Roundabout Straight & Grade ntersection/Driveway/Roundabout Straight & Level ntersection/Driveway/Roundabout Straight & Grade ntersection/Driveway/Roundabout Straight in Sag ntersection/Driveway/Roundabout Straight in Sag	Motorcycle No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Other Contributing Circ Not Listed None None None None None	Daylight Dry Dark-No Street Lights/Street Lights Off Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry	y Not at Intersection/DrivewayRoundabout y Not at Intersection/DrivewayRoundabout y Not at Intersection/DrivewayRoundabout y Not at Intersection/DrivewayRoundabout y Not at Intersection/DrivewayRoundabout	ran to reto. No Risky Driving Behaviors Involved Involved In	Yes No No No No Yes
	15 14-bhride Party Struck Object	Property Damage Cnly Ofher/Unknown 354 Property Damage Only Speeding Und Property Damage Cnly Ofher/Unknown 25-3 Property Damage Only Other/Unknown 25-3	27 28 No Dry 4 Usder Influence of Drugs 26-34 Weet 4 No ModRess@Mail 4 No Weet 4 No Weet 4 No Dry 2 No Dry 2 No Dry	Dark-No Street Lights/Street Lights Off Not at In Daylight Not at In Dark-Street Lights On Not at In Daylight Not at In Daylight Not at In Daylight Not at In Not At Not At Not Not Not Not Not Not Not No	ntersection/Driveway/Roundabout Straight in Sag	No Vuinerable Road User Involved No Vuinerable Road User Involved	None Speeding None None	Daylight Dry	\(\)Slush Not at Intersection\(\)Driveway\(\)Roundabout known Not at Intersection\(\)Driveway\(\)Roundabout tt Not at Intersection\(\)Driveway\(\)Roundabout y Not at Intersection\(\)Driveway\(\)Roundabout		No Yes Yes No Yes
6950 962177.46 1018724.28 201	14 Whitels Party Souch Object 14 Z Whitels Parties All End 14 Z Whitels Parties Tuming 16 Z Whitels Parties Tuming 17 Z Whitels Parties Argie 17 Z Whitels Parties Argie 2 Whitels Parties Tuming	Property Damage Cniy Inattention 35-f Property Damage Cniy Fail to Yield 65+ Property Damage Cniy Fail to Yield 33-4- Property Damage Cniy Fail to Yield 33-4- Minor or Possible Injury Fail to Yield 65+ Minor or Possible Injury More influence of Alcohol/Drugs/Medication 65+	4 No Dry No Dry No Dry	Daylight	way or Intersection Straight & Level way or Intersection Straight & Grade way or Intersection Straight & Grade way or Intersection Straight & Level way or Intersection Straight & Grade way or Intersection Straight & Hilters t way or Intersection Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+ Older Road User 65+	Inattention Fail to Yield Fail to Yield	Dark-Street Lights On Dry Daylight Dry Dark-Street Lights On Dry Davight Dry Daylight Dry Daylight Dry Daylight Dry	y At Driveway or Intersection y At Driveway or Intersection	No Risky Driving Behaviors Involved	No No No No No
6955 962164.75 1018649.18 20* 6956 962166.39 1018646.09 20* 6957 962433.48 1018670.7 20* 6958 962685.53 1018543.78 20*	19 2 Vehicles/Parties Angle 2 Vehicles/Parties Angle 2 Vehicles/Parties Struck Object	Property Damage Only Other/Unknown 25-3	rr 25 No Wet No Dry No Dry 12 5 Apparently Asleep, Fatigued or III Under 25 Dry No loe,SnowiStust	Daylight At Drives Daylight At Drives Daylight Not at In Dark-Street Lights On Not at In	way or Intersection Straight & Level	No vuinerable Koad User Involved		Daylight Dry Daylight Dry Dark-Street Lights On Ice/Snow	y At Driveway or Intersection At Driveway or Intersection y At Driveway or Intersection y Not at Intersection/Driveway/Roundabout v/Slush Not at Intersection/Driveway/Roundabout	Inatention Inatention Inatention Inatention Apparently Aleep, Fatigued or III No Kisky Driving Behaviors involved	No No No Yes No
6960 963729.51 1018404.41 20 6961 964197.25 1018275.22 20 6962 964249.57 1018295.14 20 6963 964234.31 1018294.21 20	15 Nehicle-Party Animal	Property Damage Only Other Unknown 65* Property Damage Only Appearinty Askeep, Fatigued or III 35-4 Property Damage Only Folia William Con Closely 25-3 Property Damage Only Folia William Con Closely 25-3 Property Damage Only Speeding 25-3 Property Damage Only Protective Control of the Contr	No	Daylight Not at In Daylight At Drive Daylight Not at In Daylight Not at In Daylish O Street Lights (Street Lights Off	ntersection/Driveway/Roundabout Straight & Level Intersection/Driveway/Roundabout Straight & Grade way or Intersection Driveway/Roundabout Straight & Level Intersection/Driveway/Roundabout Straight & Level Intersection/Driveway/Roundabout Straight & Level Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Driver Physical Condition Inattention Follow Too Closely Speeding	Daylight Dry Daylight Dry Daylight Dry Dark-No Street Lights/Street Lights Off We	y At Driveway or Intersection y Not at Intersection/Driveway/Roundabout t Not at Intersection/Driveway/Roundabout	No Misky Divining Bishawlors Involved Apparently Alexep, Fatigued or III Inatiention Fiction Too Closely Speeding No Risky Driving Behavlors Involved	Yes No Yes Yes No
6967 964765.88 1018156.03 201 6968 965147.76 1018104.15 201	19 Nebriade Party Animal	Property Damage Only Other/Unknown 35-4	4 No Dry 4 No Dry	Dark-No Street Lights/Street Lights Off At Drives Daylight Not at In Dark-No Street Lights/Street Lights Off Not at In Daylight At Drives	way or Intersection Straight & Level ntersection/Driveway/Roundabout Straight & Level	No Vulnerable Road Hear Involved	Police 160 Closery None Other Contributing Circ Not Listed Under Influence of Alcohol/Drugs/Medication	Daylight Dry	y Not at IntersectionID riveway/Roundabout et At Orivewayor Intersection y Not at Intersection Driveway/Roundabout y Not at IntersectionID riveway/Roundabout y At Drivewayor IntersectionID riveway/Roundabout y At Drivewayor IntersectionID riveway/Roundabout y Not at IntersectionID riveway/Roundabout y At IntersectionID riveway/Roundabout y At Intersection and Not Related	Fail to Yeld Follow Too Closely No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Under Influence of Alcohol/Drags/Medication Under Influence of Alcohol/Drags/Medication	No No No Yes Yes
6972 965962.8 1017930 20: 6973 965762.27 1017766.84 20: 6974 966828.09 1017690.12 20: 6975 966827.43 1017751.33 20:	15 Z Vehicles Parise	Properly Damage Only Fail to Yield Properly Damage Only Properly Damage Only Other Unknown 25-3 Properly Damage Only Unider Influence of Alcohol/Drugs./Medication 45-5 Properly Damage Only Other Unider Influence of Alcohol/Drugs./Medication 45-5 45-6	No	Dark-No Street Lights/Street Lights Off Not at In Daylight Not at In Daylight Not at In	way or Intersection Straight & Level section and Not Related Straight & Level ntersection: Orneway/Roundabout Straight & Level ntersection/Driveway/Roundabout Straight & Level stressection/Driveway/Roundabout Straight & Level straight & Level ntersection/Driveway/Roundabout Straight & Level	Older Road User 65+ No Vulnerable Road User Involved	Fail to Yield None None Under Influence of Alcohol/Drugs/Medication Other Contributing Circ Not Listed	Dark-No Street Lights/Street Lights Off Dry Daylight Ice/Snow Daylight Dry	y Not at Intersection/Driveway/Roundabout y All Intersection and Not Related by All Intersection and Not Related by All Driveway or Intersection by the All Intersection and Not Related by Not at Intersection-Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Fail to Yield No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Under Influence of Alcoholf/Drugs/Medication No Risky Driving Behaviors Involved	No No No Yes Yes
6976 967132.35 1017690.61 20: 6977 967356.54 1017644.39 20: 6978 967614.31 1017591.28 20: 6978 967656.83 1017506.23 20:	15 Vehicle Party Vehicle	Mnor or Possible Injury Under Influence of Nicoho/Orugs/Medication Und Property Damage Only Oher/Unknown 45-5 Property Damage Only Oher/Unknown 65+ Property Damage Only Oher/Unknown Und Property Damage Only Oher/Unknown 35-4	pr 25 Under Influence of Alcohol Under 25 Wet 4 No Dry Dry	Dawn/Dusk Not at in Dawn/Dusk Not at in Dark-No Street Lights/Street Lights Off Not at in Davlight Not at in	ntersection/Driveway/Roundabout Straight & Level ntersection/Driveway/Roundabout Straight & Level ntersection/Driveway/Roundabout Straight & Level ntersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Clider Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Under Influence of Alcohol/Drugs/Medication None None None	Dawn/Dusk We	th Not at Intersection/Dineway/Roundabout y Not at Intersection/Dineway/Roundabout y Not at Intersection/Dineway/Roundabout y Not at Intersection/Dineway/Roundabout y Not at Intersection/Dineway/Roundabout	Under Influence of Alcohol/Drugs/Medication No Risky Driving Behavios Involved No Risky Driving Behaviors Involved	Yes No No No No
6984 969072.5 1017224.39 201	19	Property Damage Chily Other Unknown 30-4- Monor or Possible Injury Speeding Und Moro or Possible Injury Inattention 35-4- Property Damage Chily Property Damage Chily 45-5 Property Damage Chily Improper Turn Merge 55-6 Property Damage Chily Follow Too Closely 35-3	No	n Daylight At Drives	ction Related but Not at Intersection Straight & Level way or Intersection Curve & Level	No Vulnerable Road User Involved	Speeding Inattention Speeding	Daylight Dry Daylight Dry Daylight Dry Daylight Ice/Snow	y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout WSush At Driveway/Roundabout WSush At Driveway/Roundabout Intersection Related but Not at Intersection y At Driveway/Roundabout y Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Speeding Instendion Instendion Speeding Instendion No Risky Driving Behaviors Involved Follow Too Closely	Yes Yes No Yes No
6988 969439.3 1017145.94.20 6989 969523 1017123.16.20 6990 969934.1 1017113.51.20 6991 969928.69 1017103.91.20	19 1 Vehicle/Party Animal	Property Damage Only OtherUnknown 45-5 Property Damage Only OtherUnknown 25-5 Property Damage Only OtherUnknown 35-4 Property Damage Only OtherUnknown 65+ Mnor or Possible Injury Speeding Und	4 NO Dry 4 NO Dry NO Wet 7 NO Wet 8 NO Wet 10 SonowStust	Daylight Not at in	ntersection/Driveway/Roundabout Straight & Level ntersection/Driveway/Roundabout Straight & Level ntersection/Driveway/Roundabout Straight & Level ntersection/Driveway/Roundabout Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Clider Road User 65+ No Vulnerable Road User Involved	None None Speeding	Daylight Dry Daylight We Dark-Street Lights On Ice/Snow	y Not at Intersection/Driveway/Roundabout of Not at Intersection/Driveway/Roundabout of Not at Intersection/Driveway/Roundabout of Not at Intersection/Driveway/Roundabout	Valida Vidu Uniosery No Risky Diving Behaviors Involved Spending Spending Spending	No No No No Yes
6993 969939.92 1017044.75.20 6994 969940.92 1017108.3 20 6995 970043.52 1017023.34 20 6996 970156.51 1017067.55 20 6997 970462.77 1017004.61 20	15 3 Vehicles Parties 15 3 Vehicles Parties 1 Vehicle Party Animal 9 1 Vehicle Party 4 Vehicle Party Overturned 19 1 Vehicle Party 10 1 Vehicle Party 1 Vehicle Party Struck Cobject	Property Damage Only Other/Unknown 65+ Property Damage Only Speeding 25-3 Property Damage Only Speeding 25-3 Property Damage Only Other/Unknown 25-3 Property Damage Only Instension 55-5	No	Daylight Not at In Daylight Not at In Daylight Not at In Daylight Not at In Dark-No Street Lights/Street Lights Off Not at In Daylight Not at In	ntersectionOrivewayNoundabout Straight & Level ntersectionOrivewayRoundabout Straight & Level ntersectionOrivewayRoundabout Straight & Level ntersectionOrivewayRoundabout Straight & Level ntersectionOrivewayRoundabout Straight & Grade	Older Road User 65+ Older Road User 65+ No Winerable Road User Involved	None OtherUnknown Speeding None Inattention	Daylight Dry Daylight Dry Daylight Dry Dark-No Street Lights/Street Lights Off Dry Daylight Dry	NiStush Not at Intersection D frieway/Roundabout v Not at Intersection D frieway/Roundabout	No Risky Driving Behaviors Involved Speeding No Risky Driving Behaviors Involved Inattention	No No No Yes No Yes
6998 970462.77 1017004.61 20 6999 970461.16 1017000.49 1017000.49 20 7000 970458.26 1016937.63 20 7001 970467.59 1017003.46 20 7002 970538.49 1016987.33 20	15 Vehicle Party Animal	Property Damage Cnly Other/Unknown 45-5 Property Damage Cnly Apparently Asleep, Fatigued or III 65-5 Minor or Possible Injury Speeding 25-5 Property Damage Cnly Other/Unknown Und Property Damage Cnly Other/Unknown Und	4 No Dry Apparently Asleep, Fatigued or III 65+ Dry 4 No Wet vr 25 No Wet Wet Wet Wet	Daylight Not at in Daylight Not at in Daylight Not at in Dark-Street Lights On Not at in Dark-No Street Lights/Street Lights Off Not at in	ntersection/Driveway/Roundabout Straight & Level ntersection/Driveway/Roundabout Straight & Level ntersection/Driveway/Roundabout Straight & Grade ntersection/Driveway/Roundabout Straight & Level ntersection/Driveway/Roundabout Straight & Level	No Wilnerable Road User Involved Clider Road User Bis+ No Wilnerable Road User Involved	None Apparently Asleep or Fatigued Speeding None Other Contributing Circ Not Listed	Daylight Dry Daylight Dry Daylight We Dark-No Street Lights On We Dark-No Street Lights/Street Lights Off We	y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout to Not at Intersection/Driveway/Roundabout to Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Apparently Relep, Falligued or III Speeding No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No Yes No No Yes
7005 970977.4 1016892.31 20: 7006 970991.35 1016895.74 20: 7007 970976.11 1016829.34 20: 7008 970976.62 1016895.73 20:	17	Properly Damage Chily Operating Delective Equipment Monor or Possible Injury Under Influence of Alechol/Drugs/Medication Und Properly Damage Chily Properly Damage Chily Ones Unknown Ones Unknown Ones Unknown Und Minor or Possible Injury Under Influence of Alechol/Drugs/Medication Und Minor or Possible Injury Under Influence of Alechol/Drugs/Medication 65- 66- 66- 67- 68-	4 No Dry 275 Under Influence of Alcohol Under 25 Dry 275 No Under 175 No Dry 275 No Dry 4 Under Alcohol Dry 4 Under Influence of Alcohol 45-54 Dry No Influence of Alcohol Alcohol 15-55 No Dry No Influence of Alcohol 15-55 No Dry N	Daylight Not at in	Intersection Oriveway/Roundabout Straight & Level Intersection Oriveway/Roundabout Straight & Grade way or Intersection Cure & Level Section and Not Related Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	Operating Defective Equipment Under Influence of Alcohol/Drugs/Medication Other/Unknown Fail to Yield Under Influence of Alcohol/Drugs/Medication Speeding	Daylight	y Not at Intersection/Dineway/Roundabout y Not at Intersection/Dineway/Roundabout y At Driveway or Intersection y At Driveway or Intersection y At Driveway or Intersection (Slush At Intersection and Not Related	No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved Fail to Yield Under Influence of Alcohol/Drugs/Medication Speeding	No No No No No Yes
7009 97099128 1016895.4 20: 7010 971093.91 1016874.58 20: 7011 971083.9 1016876.64 20: 7012 97109123 1016875.79120:	16 2 Vehicles:Parties Rear-End 14 2 Vehicles:Parties Rear-End 14 1 Vehicle:Party Struck Object 15 1 Vehicle:Party Struck Object 16 1 Vehicle:Party Struck Object 17 18 18 18 18 18 18 18 18	Mnor or Possible Injury Speeding 44.5 Property Damage Chly autension 35.5 Property Damage Chly Apparently Acleep, Fatigued or III Und Property Damage Chly Speeding Und Property Damage Chly Statestion 35.4 Martinion 35.4 35.4	4 No Wet 4 No Dry 6 Jappanently Asleep, Fatigued or III Under 25 1 Under 25 Dry 1 IcosSnow/Slust 1 No Dry	Dark-Street Lights On	way or Intersection Straight & Grade Intersection Oriveway Roundabout Straight & Level Intersection Oriveway Roundabout Straight & Grade	No Vulnerable Road User Involved	Speeding Inattention Apparently As loep or Fatigued Speeding Inattention	Dark-Street Lights On We Dark-Street Lights On Dry Dark-Street Lights On Dry Daylight Ice/Snow	tt At Driveway or Intersection y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout w/Slush Not at Intersection/Driveway/Roundabout	Speeding Apparently An Incept Patigued or III Apparently An Incept Patigued or III Speeding Institution Debasions Inspired	No No Yes Yes No
7013 971166.58 1016859.58 20: 7014 971172.05 1016859.54 20: 7015 971466.12 1016797.67 20: 7016 971463.21 1016798.15 20: 7017 972007.1 1016858.68 20: 7018 972959.83 1016498.12 20:	14 Vehicle Party Asimal	Property Damage Only OherUnknown 65* Property Damage Only Speeding 45-6 Property Damage Only OherUnknown 35-5 Property Damage Only OherUnknown 55-4 Property Damage Only Operating Delective Equipment 35-4	No. Wet 4 No. Ize.Snow(Subt.) 4 No. Wet 4 No. Dry. 4 No. Dry.	Dawn/Dusk Not at In	nters ection/Drivway/Roundabout Straight & Grade nters ection/Drivway/Roundabout Straight in Sag nters ection/Driveway/Roundabout Straight & Grade nters ection/Drivway/Roundabout Straight at Hillcrest thes section/Drivway/Roundabout Straight & Level	Older Road User 65+ No Vulnerable Road User Involved No Winerable Road User Involved No Winerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Speeding None None	Dark-Street Lights On We Daylight Ice/Snow Dawn/Dusk We Dark-No Street Lights/Street Lights Off Dry Daylight Dry	y Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout NSIUsh Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Speeding No Risky Driving Behaviors Involved	No Yes No Yes

OBJECTID JURISDICTION	COUNTY CITY	PRIMARY BLOCK TRAFFICWAY NUMBER	INTERSECTING DIST ROOM FOR FROM FROM REF.POINT POINT.NAME	MILEPOST A SR_ONLY HISTORY SUSPENSE_IND	DATE TIME INJURY_TYPE	NUM NUM NUM NUM NUM NUM INJ FAT VEH PEDS BIK	CES VEH_1_TYPE	VEH_2_TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD_SURFACE ROADWAY CHARACTERISTIC	LIGHTING CONDITION	FRST_COLLISION TYPE_OBJECT_STRUCK	VEH_1 ACTION	VEH.2 ACTION		ASS VEH_2_COMPASS ODIRECTION_FROM DIRECTION_TO
7019 State Route 7020 State Route 7021 State Route 7021 State Route 7022 State Route 7023 State Route 7024 State Route 7025 State Route 7025 State Route 7025 State Route 7025 State Route	Cialiam 1 Cialiam 1	101 101		258.54 No E706492 258.59 No E731119 258.61 No E7464984	2017-08-20 19:10 No Apparent Injury 2017-10-27 15:35 Possible Injury 2014-10-13 05:19 No Apparent Injury	0 0 1 0	Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		At Intersection and Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Operant	Dry Straight & Level	Daylight	Roadway Ditch Vehicle Strikes Deer Vehicle Strikes Deer	Making Left Turn Going Straight Ahead Going Straight Ahead		West North West East South North	
7021 State Route 7022 State Route 7022 State Route	Cialiam 1	101			2018-10-13 US:19 No Apparent Injury 2018-10-29 10:40 No Apparent Injury 2016-11-17 10:50 No Apparent Injury	0 0 1 0	Plokup, Panel Truck or Vanette under 10,000 lb Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb	Dans anger Car	Not at Intersection and Not Related Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Level	Dark-No Street Lights Daylight	venicie strikes Deer Vehicle Strikes Deer From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stoonard for Traffic	East West	Vehicle Backing Vehicle Stopped
7024 State Route 7025 State Route	Clallam 1 Clallam 1	101		258.65 No E517871 258.66 No E954285	2016-02-20 15:50 No Apparent Injury 2019-08-22 13:20 Suspected Mnor Injury 2019-02-08 14:00 Unknown 2018-05-24 07:16 Describle bittery	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Dry Curve & Level Dry Straight & Grade	Daylight Daylight	From same direction - both going straight - one stopped - rear-end Roadway Ditch	Going Straight Ahead Going Straight Ahead	Stopped at Signal or Stop Sign	West East West East	Vehicle Stopped Vehicle Stopped
	Cialiam 1 Cialiam 1	101							Not at Intersection and Not Related Not at Intersection and Not Related	Snowing Overcast	Snow/Slush Straight & Level Dry Straight & Grade	Daylight Daylight	Roadway Ditch Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead		West East East West	
7027 State Route 7028 State Route 7029 State Route	Cialiam 1 Cialiam 1	101 101		258.73 No E553763 258.81 No E846685	2016-06-15 09:15 No Apparent Injury 2018-10-04 19:00 No Apparent Injury 2015-01-02 16:06 No Apparent Injury	0 0 1 0	Passenger Car Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Dry Straight & Grade Dry Curve & Grade	Daylight Dark-Street Lights On	Vehicle Strikes Deer Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead		East West West East	
7030 State Route	Cialiam 1 Cialiam 1	101 101		259.00 No E390063 259.00 No E763875	2015-01-02 16:06 No Apparent Injury 2018-01-29 06:48 No Apparent Injury	0 0 1 0	Passenger Car Passenger Car	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Raining			Guardrail - Leading End	Going Straight Ahead Going Straight Ahead	Going Straight Ahead	East West West East	West East
7031 State Route 7032 State Route 7033 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101 101		259.00 No E966709 259.12 No E967555	2018-01-29 06:48 No Apparent Injury 2019-09-30 18:00 No Apparent Injury 2019-10-04 20:02 No Apparent Injury 2019-08-08 19:00 No Apparent Injury	0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Other	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Level	Daylight Dark-No Street Lights	From same direction - both going straight - both moving - sideswipe From same direction - both going straight - both moving - rear-end Vehicle Strikes Deerr Metal Sign Post	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	West East West East	West East
7034 State Route 7035 State Route	Cialiam 1 Cialiam 1	101					Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Not Related At Intersection and Related	Unknown Overcast				Going Straight Ahead Making Left Turn	Going Straight Ahead	West East North East	East West
7035 State Route 7036 State Route 7037 State Route 7038 State Route 7038 State Route	Cialiam 1 Cialiam 1	101 101		259.18 No E378659 259.18 No E388953	2014-11-27 19:18 No Apparent Injury 2014-11-27 19:18 No Apparent Injury 2014-12-13 19:00 Suspected Mnor Injury 2018-03-30 16:40 No Apparent Injury 2014-06:19 12:00 No Apparent Injury	0 0 2 0	0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related	Overcast Clear or Partly Cloudy	Dry Straight & Level	Dark-Street Lights On Dark-Street Lights On	Entering at angle Entering at angle	Making Right Turn Going Straight Ahead Making Right Turn	Going Straight Ahead Going Wrong Way on Divided Hwy Stopped for Traffic	North West East West	East West North South
7038 State Route 7039 State Route	Cialiam 1 Cialiam 1	101				0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		At Intersection and Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight	Same direction both turning right one stopped rear end Vehicle Strikes Deer	Going Straight Anead		North South West East	North Vehicle Stopped
7040 State Route 7041 State Route	Cialiam 1 Cialiam 1	101		259.27 No E830000 259.30 No 2766959	2018-07-29 09:45 No Apparent Injury 2014-04-30 08:45 No Apparent Injury 2019-01-08 09:18 No Apparent Injury	0 0 2 0 0 0 2 0 0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - both moving - sideswipe From opposite direction - all others Earth Bank or Ledge	Changing Lanes Going Straight Ahead	Changing Lanes Going Straight Ahead	East West West East	East West East West
7042 State Route 7043 State Route	Clallam 1	101		259.41 No E785159	2018-03-28 12:16 No Apparent Injury	0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Overcast	Wet Straight & Grade Dry Curve & Grade	Daylight	Earn Bank or Ledge Metal Sign Post Pendeny Ditth	Going Straight Ahead Going Straight Ahead		East West	
7043 State Route 7044 State Route 7045 State Route 7046 State Route	Cialiam 1	101		259.50 No E920782 259.65 No E920782	2016-06-03 10:24 No Apparent Injury 2019-05-08 03:05 No Apparent Injury 2014-11-29 14:23 No Apparent Injury	0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Dans anger Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Grade	Daylight Dark-No Street Lights Daylight	Roadway Union Vehicle Strikes Deer From same direction - all others	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing	West East West East	West East
7046 State Route 7047 State Route	Cialiam 1	101					Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related At Driveway	Clear or Partly Cloudy	Dry Straight & Grade	Daylight	From same direction - one right turn - one straight	Going Straight Ahead	Making Right Turn	West East	West South
7047 State Route 7048 State Route 7049 State Route 7050 State Route 7051 State Route	Cialiam 1	101		259.86 No E351407 259.90 No E337056 259.90 No E750458	2014-08-13 14:35 No Apparent Injury 2014-06-13 06:51 No Apparent Injury 2017-12-18 16:21 No Apparent Injury 2014-10-11 10:15 No Apparent Injury	0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining Ownerst	Other Straight & Grade Wet Straight & Grade Dry Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - ridar-wine	Going Straight Ahead Going Straight Ahead	Slowing Going Straight Ahead Stopped for Traffic	West East	West East
7051 State Route	Cialiam 1 Cialiam 1	101		259.93 No E364979 259.95 No 2728010	2014-10-11 10:15 No Apparent Injury 2017-09-06 12:30 No Apparent Injury	0 0 2 0		Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb		Raining	Dry Straight & Level Wet Straight & Level Dry Straight & Level		From same direction - both going straight - both moving - sideswipe From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Changing Lanes Going Straight Ahead Slowing	Stopped for Traffic Stopped at Signal or Stop Sign	West East North South	Vehicle Stopped Vehicle Stopped Vehicle Stopped West
7052 State Route 7053 State Route 7054 State Route 7055 State Route 7056 State Route	Cialiam 1	101		259.95 No 3759381 259.95 No E337050	2017-09-06 12:30 No Apparent Injury 2017-09-10 11:15 No Apparent Injury 2014-06-20 18:51 Suspected Minor Injury 2014-09-04 14:40 Possible Injury 2015-07-09 19:05 No Apparent Injury	0 0 2 0	Passenger Car Pickup.Panel Truck or Vanette under 10.000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end Entering at angle	Stopped at Signal or Stop Sign	Going Straight Ahead Going Straight Ahead Going Straight Ahead	South West North East	South West East West
7055 State Route 7056 State Route	Cialiam 1	101					Passenger Car Pickup Panel Truck or Vanette under 10 000 lb.	Passenger Car Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy				Making Left Turn Making Left Turn Changing Lanes	Going Straight Ahead Going Straight Ahead	North East West Fast	East West West
7056 State Route 7057 State Route 7058 State Route	Cialiam 1	101		259.95 No E630713 259.95 No E748254	2017-01-10 08:30 No Apparent Injury 2017-12-15 16:49 No Apparent Injury 2018-02-02 17:00 No Apparent Injury	0 0 1 0	Passenger Car Pickup.Panel Truck or Vanette under 10.000 lb	Pickup.Panel Truck or Vanette under 10,000 lb Pickup.Panel Truck or Vanette under 10,000 lb Pickup.Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Intersection and Related	Raining Clear or Partly Cloudy	Wet Straight & Level Dry Straight at Hillor	Daylight Daylight Dark-No Street Lights	Earth Bank or Ledge From same direction - both going straight - one stopped - rear-end		Stopped at Signal or Stop Sign	West East North South	Vehicle Stopped Vehicle Stopped
7059 State Route 7060 State Route	Cialiam 1 Cialiam 1	101					Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related Not at Intersection and Not Related	Overcast Raining	Diy Straight & Level	DUSK	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end	Going Straight Ahead Starting From Parked Position Going Straight Ahead	Stopped at Signal or Stop Sign Going Straight Ahead	Vehicle Stopped South West East	Vehicle Stopped Vehicle Stopped West East
7061 State Route 7062 State Route	Cialiam 1 Cialiam 1	101 101		260.06 No E926866 260.10 No E350367	2019-05-31 07:23 No Apparent Injury 2014-08-09 14:41 Possible Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade	Daylight	Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead	Going Straight Ahead	East West West East	West East
7063 State Route 7064 State Route	Cialiam 1 Cialiam 1	101							Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level	Dawn	From same direction - both going straight - both moving - rear-end Vehicle Strikes Deer Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East East West	
7064 State Route 7065 State Route 7066 State Route 7067 State Route 7067 State Route	Cialiam 1 Cialiam 1	101		260.10 No E817727 260.11 No E713865	2018-07-13 21:45 No Apparent Injury 2017-08-28 05:05 No Apparent Injury 2017-10-15 01:30 Unknown 2016-08-13 22:52 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Dark-No Street Lights Dark-No Street Lights	Vehicle Strikes Deer Ower Embankment - No Guardrail Present Earth Bank or Ledge	Going Straight Ahead		East West West East	
	Cialiam 1 Cialiam 1	101		260.11 No E732283 260.14 No E574385	2017-10-15 01:30 Unknown 2016-08-13 22:52 No Apparent Injury	0 0 1 0 0 0 1 0	0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy				Going Straight Ahead		West East West	
7069 State Route 7070 State Route 7071 State Route	Cialiam 1 Cialiam 1	101		260.20 No E683525 260.25 No E815316	2017-04-06 22:36 No Apparent Injury 2018-07-06 18:51 No Apparent Injury 2017-09-02 20:45 No Apparent Injury	0 0 1 0 0 0 1 0 0 0 1 0	0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Dark-No Street Lights Daylight Dark-Street Lights On	Roadway Ditch Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East West	
7071 State Route 7072 State Route	Clallam 1 Clallam 1	101		260.27 No E708367 260.30 No E818003	2017-09-02 20:45 No Apparent Injury 2018-07-04 11:30 Suspected Serious Injury 2014-08-15 00:35 Possible Injury	0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Dark-Street Lights On Daylight	Vehicle Strikes Deer Fence From same direction - both going straight - both moving - rear-end	Going Straight Anead		East West East West	
7072 State Route 7073 State Route 7074 State Route 7075 State Route	Ciallam 1 Ciallam 1	101		260.36 No E360370 260.36 No E364879 260.45 No E610032	2014-10-06 06:36 IND Apparent injury	0 0 1 0	Not Stated Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Wet Straight & Grade Dry Straight & Level	Dark-No Street Lights Daylight Dark-No Street Lights	From same direction - both going straight - both moving - rear-end Temporary Traffic Sign, Barricade or Construction Materi	Going Straight Ahead Going Straight Ahead Making Left Turn	Going Straight Ahead	East West West	East West
7076 State Route	Ciallam 1 Ciallam 1	101				0 0 2 0	Passenger Car		At Intersection and Not Related	Overcast Clear or Partly Cloudy	Dry Straight & Grade	Daylight	Vehicle Strikes Deer	Going Straight Ahead	Stopped for Traffic	East North West East	East North
7077 State Route 7078 State Route 7079 State Route 7080 State Route	Cialiam 1 Cialiam 1	101		260.47 No E465550 260.47 No E465554	2015-09-25 08:52 Possible Injury 2015-09-25 08:52 Possible Injury	1 0 3 0 2 0 2 0	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	At Intersection and Related At Intersection and Related	Overcast Overcast	Wet Straight & Level		From same direction - both going straight - one stopped - rear-end Entering at angle	Going Straight Ahead	Stopped for Traffic Stopped at Signal or Stop Sign	West East West East	Vehicle Stopped Vehicle Stopped Vehicle Stopped Vehicle Stopped
7079 State Route 7080 State Route	Cialiam 1	101		201.47 No E650075 260.47 No E727030	2017-03-10 09:30 No Apparent Injury 2017-10-15 11:16 No Apparent Injury	0 0 2 0	u Pickup,Panel Truck or Vanette under 10,000 lb 0 Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related	Other Clear or Partly Cloudy	Omer Straight & Level Dry Straight & Level	Daylight Daylight Daylight	Entering at angle From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing Slowing	west East East West	West North East South
7081 State Route 7082 State Route 7083 State Route 7084 State Route	Cialiam 1 Cialiam 1	101		260.47 No E856264 260.47 No E912754	2018-11-01 18:55 Suspected Minor Injury 2019-04-12 17:24 Suspected Minor Injury 2019-11-21 16:30 Suspected Minor Injury 2017-08-28 16:40 No Apparent Injury 2018-03-13 14:35 No Apparent Injury	2 0 3 0	Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related	Raining Overcast	Wet Straight & Grade Dry Straight & Grade		Entering at angle From same direction - both going straight - one stopped - rear-end	Making Right Turn Going Straight Ahead	Going Straight Ahead Stopped for Traffic	North West West East	East West Vehicle Stopped Vehicle Stopped
7083 State Route 7084 State Route	Cialiam 1	101		201.47 No E987372 260.49 No E706491	2019-11-21 16:30 Suspected Minor Injury 2017-08-28 16:40 No Apparent Injury	2 0 1 0 0 0 2 0	u Pickup,Panel Truck or Vanette under 10,000 lb 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Not Related Not at Intersection and Not Related	Clear Clear or Partly Cloudy			rion same director - boar going sealght - boar moving - sides inpe	Going Straight Ahead Going Straight Ahead	Overtaking and Passing	West East	West East
	Clallam 1 Clallam 1	101 101						Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related	Raining Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level	Daylight Daylight Dark-Street Lights On	Entering at angle Entering at angle	Making Left Turn Making Left Turn Making Left Turn	Going Straight Ahead Making Left Turn	North East North East	East West West North
7086 State Route 7087 State Route 7088 State Route	Cialiam 1	101		260.80 No E739229	2018-12-21 15:50 No Apparent Injury 2019-11-04 17:42 No Apparent Injury 2017-11-21 18:55 Suspected Serious Injury	3 0 3 0	0 Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Overcast	Wet Straight & Level	Dark-Street Lights On	Entering at angle From opposite direction - both moving - head-on	Going Straight Ahead	Going Straight Ahead Going Straight Ahead	West East	East West
7089 State Route 7090 State Route 7091 State Route 7092 State Route	Cialiam 1	101					Pickup,Panel Truck or Vanette under 10,000 lb	Truck Tractor & Semi-Trailer	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Ice Straight & Grade	Daylight	From opposite direction - all others Vehicle overturned From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead Stopped for Traffic	West East	West East Vehicle Stopped Vehicle Stopped
7091 State Route 7092 State Route	Cialiam 1	101		261.15 No E429059	2014-11-29 13:42 Suspected Minor Injury 2017-08-07 15:50 Possible Injury 2015-05-25 08:24 No Apparent Injury	0 0 2 0	0 Passenger Car 0 Truck Tractor & Semi-Trailer	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level		From same direction - both going straight - both moving - rear-end	Going Straight Ahead	Slowing Slowing	West East	Vehicle Stopped Vehicle Stopped West East
7093 State Route 7094 State Route 7095 State Route 7096 State Route 7096 State Route	Cialiam 1 Cialiam 1	101		261.16 No E861960 261.20 No E731113	2017-10-23 15:20 No Apparent Injury 2017-10-23 15:20 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb			Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight	Fire started in vehicle From same direction - both going straight - one stopped - rear-end	Other* Going Straight Ahead	Stopped for Traffic	West East	Vehicle Stopped Vehicle Stopped
7096 State Route 7097 State Route	Cialiam 1	101		261.24 No E779103 261.25 No 2766893	2014-08-19 14:59 No Apparent Injury 2015-06-30 11:50 Possible Injury	0 0 2 0 2 0 2 0		Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Grade Dry Straight & Level	Daylight Daylight	From same direction - both going straight - one stopped - rear-end Entering at angle From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic Stopped for Traffic Making Left Turn	East West	Vehicle Stopped Vehicle Stopped South East
7098 State Route 7099 State Route	Cialiam 1	101		261.25 No E355466 261.25 No E365466	2014-09-07 11:45 Died in Hospital	1 1 2 0	Motorcycle Dickup Panel Truck or Vanette under 10 000 lb.	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight	Entering at angle	Going Straight Ahead	Going Straight Ahead Stopped at Signal or Stop Sign	South North	West East Vehicle Stopped Vehicle Stopped
7100 State Route 7101 State Route	Cialiam 1	101		261.25 No E381915 261.25 No E385112	2014-09-07 11:45 Died in Hospital 2014-12-06 13:23 Suspected Minor Injury 2014-12-04 07:16 No Apparent Injury 2014-12-18 17:14 No Apparent Injury 2014-09-06 13:27 No Apparent Injury	0 0 2 0	Passenger Car Pickup Panel Truck or Vanette under 10 000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Overcast Raining	Wet Straight & Level Wet Straight & Level Wet Straight & Level	Dawn Dark-Street Lights On	From same direction - both going straight - one stopped - rear-end Entering at angle From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Making Right Turn Going Straight Ahead Starting in Traffic Lane	Going Straight Ahead Stooped for Traffic	South East Fast West	East West Vehicle Stopped Vehicle Stopped
7102 State Route	Cialiam 1	101		261.25 No E399241 261.25 No E411140	2015-02-06 13:27 No Apparent Injury 2015-02-19 10:40 No Apparent Injury	0 0 2 0	0 Passenger Car	Passenger Car	At Intersection and Related	Raining Clear or Partly Clouds	Wet Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Starting in Traffic Lane	Stopped for Traffic Stopped for Traffic	Vehicle Stopped West	Vehicle Stopped Vehicle Stopped West Vehicle Stopped
7103 State Route 7104 State Route 7105 State Route	Cialiam 1	101			2015-02-06 13:27 No Apparent Injury 2015-03-19 10:40 No Apparent Injury 2015-08-13 18:31 No Apparent Injury 2015-10:29 17:18 No Apparent Injury		Passenger Car Truck & Trailer Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Not Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end Signal Pole From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped at Signal or Stop Sign	East West East West	Vehicle Stopped Vehicle Stopped
7106 State Route 7107 State Route 7108 State Route 7109 State Route	Cialiam 1 Cialiam 1	101		261.25 No E534053 261.25 No E546204	2016-04-04 15:59 Possible Injury 2016-05-19 12:13 No Apparent Injury 2016-06-07 09:49 No Apparent Injury 2016-07-17 14:14 Possible Injury	1 0 2 0	Passenger Car Passenger Car	Passenger Car Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy	Dry Straight & Level Wet Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - sideswipe	Going Straight Ahead	Stopped at Signal or Stop Sign Going Straight Ahead Stopped for Traffic	West East West East	Vehicle Stopped Vehicle Stopped West East
7108 State Route 7109 State Route	Clallam 1 Clallam 1	101 101		261.25 No E556979 261.25 No E571535	2016-06-07 09:49 No Apparent Injury 2016-07-17 14:14 Possible Injury	0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		At Intersection and Related At Intersection and Related	Raining Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level		From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Changing Lanes Starting in Traffic Lane Starting in Traffic Lane	Stopped for Traffic Stopped at Signal or Stop Sign	West East North South	Vehicle Stopped Vehicle Stopped Vehicle Stopped Vehicle Stopped
7110 State Route 7111 State Route 7112 State Route 7113 State Route 7113 State Route 7114 State Route 7114 State Route	Clallam 1 Clallam 1	101 101				0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car	At Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Level	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end		Stopped for Traffic Stopped at Signal or Stop Sign	East West West East	Vehicle Stopped Vehicle Stopped
7112 State Route 7113 State Route	Clallam 1 Clallam 1	101		261.25 No E740790 261.25 No E745411	2017-05-25 09:23 Possible Injury 2017-11-26 10:41 No Apparent Injury 2017-12-09 17:35 No Apparent Injury 2017-12-15 18:17 No Apparent Injury	0 0 2 0	0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	At Intersection and Related	Raining Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level	Daylight Daylight Dark-Street Lights On	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Starting From Parked Position Going Straight Ahead	Stopped at Signal or Stop Sign Stopped for Traffic	Vehicle Stopped East West East	West Vehicle Stopped Vehicle Stopped Vehicle Stopped
7114 State Route 7115 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101		261.25 No E750716 261.25 No E800472	2017-12-15 18:17 No Apparent Injury 2018-05-10 12:31 Suspected Minor Injury		Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Dark-Street Lights On	From opposite direction - one left turn - one straight From same direction - both going straight - both moving - rear-end	Making Left Turn	Going Straight Ahead Slowing	East South North South	West East North South
7116 State Route 7117 State Route	Cialiam 1 Cialiam 1	101 101		261.25 No E873339 261.25 No E876588	2018-05-10 12:31 Suspected Minor Injury 2018-12-14 13:55 No Apparent Injury 2018-12-25 15:30 Possible Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Overcast Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - both moving - rear-end Entering at angle	Going Straight Ahead Starting in Traffic Lane Going Straight Ahead	Slowing Starting in Traffic Lane Going Straight Ahead	Vehicle Backing East West East	Vehicle Stopped East South North
7118 State Route 7119 State Route 7120 State Route 7121 State Route	Clallam 1 Clallam 1	101			2017-12-02 20:50 Possible Injury	3 0 2 0	Not Stated Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car			Clear or Partly Cloudy Raining	Dry Straight & Level	Daylight Dark-Street Lights On Daylight	Entering at angle From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - rear-end		Going Straight Ahead Going Straight Ahead	North East East West	West East East West
7120 State Route 7121 State Route	Cialiam 1 Cialiam 1	101 101		261.30 No E365884 261.30 No E504035	2017-12-02 20:50 Possible Injury 2014-10-13 16:00 Possible Injury 2016-01-07 16:48 No Apparent Injury	1 0 2 0 0 0 2 0	0 Passenger Car 0 Passenger Car	Pickup, Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Overcast	Dry Straight & Level	Dusk	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped at Signal or Stop Sign	East West East West	East West East Vehicle Stopped
7122 State Route 7123 State Route	Cialiam 1 Cialiam 1	101 101		261.30 No E718485	2017-08-18 11:37 No Apparent Injury	0 0 2 0	0 Passenger Car 0 Passenger Car	Truck (Flatbad, Van, etc) Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight	From same direction - both going straight - both moving - sideswipe From same direction - both going straight - one stopped - rear-end	Changing Lanes Going Straight Ahead	Going Straight Ahead Stopped for Traffic	East West East West	East West Vehicle Stopped Vehicle Stopped
7122 State Route 7123 State Route 7124 State Route 7125 State Route 7126 State Route 7126 State Route 7127 State Route 7127 State Route 7128 State Route 7128 State Route	Cialiam 1 Cialiam 1	101 101		261.30 No E923368 261.30 No E962419	2019-03-26 17:26 No Apparent Injury 2019-05-17 13:00 No Apparent Injury 2019-09-20 15:55 No Apparent Injury	0 0 2 0	0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at intersection and Not Keiated	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - one right turn - one straight Street Light Pole or Base	Going Straight Ahead Going Straight Ahead	Making Right Turn	West East East West	West South
7126 State Route 7127 State Route	Clallam 1 Clallam 1	101		261.31 No E373321 261.31 No E773839	2014-11-07 21:00 No Apparent Injury 2018-02-24 13:56 No Apparent Injury 2015-06-10 15:30 No Apparent Injury	0 0 3 0 0 0 1 0 0 0 2 0	0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection		Dry Straight & Grade		Roadway Ditch From same direction - both going straight - one stopped - rear-end	Making Left Turn Starting in Traffic Lane	Stopped in Roadway	South West Vehicle Stopped West	Vehicle Stopped Vehicle Stopped
7128 State Route 7129 State Route 7130 State Route	Cialiam 1	101		261.35 No E579501	2016-08-29 11:14 No Apparent Injury	0 0 2 0	Passenger Car Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - sideswipe	Going Straight Ahead Going Straight Ahead	Stopped for Traffic Slowing Going Straight Ahead	West East	West East
7130 State Route 7131 State Route	Clallam 1	101		261.36 No E754787	2017-12-29 15:42 Possible Injury	1 0 2 0	O Plassenger Car O Pickup,Panel Truck or Vanette under 10,000 ib Passenger Car O Pickup,Panel Truck or Vanette under 10,000 ib Not State O Passenger Car O Pickup, Panel Truck or Vanette under 10,000 ib O Passenger Car O Pickup, Panel Truck or Vanette under 10,000 ib	Pickup,Panel Truck or Vanette under 10,000 lb	Driveway Related but Not at Driveway	Fog or Smog or Smoke Raining Clear or Partly Cloudy	Wet Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead	Slowing Making Right Turn	West East	West East
7132 State Route 7133 State Route 7134 State Route	Clallam	101		261.42 No 2990155 261.43 No E463264 261.60 No E648903	2015-09-07 11:25 Possible Injury 2016-09-27 11:06 No Apparent Injury	1 0 2 0	0 Not Stated	Passenger Car Passenger Car	At Driveway At Driveway At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Grade Dry Straight & Grade	Daylight Daylight	Entering at angle From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead	Slowing Going Straight Ahead	East West East West	East West
		101		261.50 No E548993 261.51 No 3501113 261.51 No E385842	2016-11-08 08:04 Suspected Minor Injury 2014-12-20 15:28 Suspected Minor Injury	1 0 1 1	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related	Clear or Partly Cloudy	Dry Straight & Grade Wet Straight & Grade Wet Straight & Level	Dawn	From same direction - both going straight - both moving - rear-end Vehicle turning left hits pedestrian From same direction - both going straight - both moving - rear-end	Merging (Entering Traffic) Changing Lanes	Going Straight Ahead	North East Northwest	East West
7137 State Route 7138 State Route	Clallam 1 Clallam 1 Clallam 1	101		261.51 No E437340 261.51 No E621360	2015-06-18 09:29 Dead at Scene 2016-03-02 09:30 Possible Inimv	0 1 1 1	O Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Passenger Car Passenger Car	Passenger Car	At Intersection and Related	Clear or Partly Cloudy Overcast	Wet Straight & Level Dry Straight & Grade Dry Straight & Level	Daylight Daylight	From same direction boiling only stanging stanging real rend. Vehicle going straight hits pedestrian Entering at angle	Changing Lanes Going Straight Ahead Making Left Turn	Going Straight Ahead	East West	East West
7139 State Route 7140 State Route	Clallam 1	101		261.51 No E692846 261.51 No E702470	2017-07-14 09:50 No Apparent Injury 2017-08-14 19:00 Possible Injury	0 0 2 0	Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight	Entering gasatige Entering gasatige Entering at angle Entering at angle	Making Lett Turn	Going Straight Ahead	South West	North South East West
1715 State Route 1715 State Route 1715 State Route 1716 State Route 1716	Cialiam 1 Cialiam 1 Cialiam 1	101		261.51 No E704027 261.51 No E708368	2017-08-10 15:25 No Apparent Injury 2017-08-25 16:15 Possible Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Lar Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb District Care State of Care Care Care Care Care Care Care Care	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight at Hillor Dry Straight & Level	Daylight	Entering at angle Entering at angle	Making Left Turn Going Straight Ahead Making Left Turn	Going Straight Ahead Going Straight Ahead Going Straight Ahead	South North North East	East West East West
7143 State Route 7144 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101								Clear or Partly Cloudy	Dry Straight & Level Wet Straight & Level	Daylight Daylight	From same direction - both going straight - both moving - rear-end Entering at angle	Making Left Turn	Office* Going Straight Ahead Stopped for Traffic	East West North East	East West East West
7145 State Route 7146 State Route	Cialiam	101		261.51 No E985056 261.51 No E995070	2019-11-21 14:00 No Apparent Injury 2019-12-18 16:20 Possible Injury	0 0 2 0 1 0 2 0	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Truck & Trailer	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Overcast	Dry Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - all others From same direction - both going straight - both moving - rear-end	Going Straight Ahead		Vehicle Stopped Vehicle Backing East West	Vehicle Stopped Vehicle Stopped East West
7147 State Route 7148 State Route	Cialiam 1 Cialiam 1	101		261.53 No E713867 261.57 No E339451	2017-09-19 17:19 Possible Injury 2014-07-04 01:24 Possible Injury	2 0 3 0 1 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb Passenger Car Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Level	Daylight Dark-Street Lights On	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - sideswipe	Going Straight Ahead Changing Lanes	Stopped for Traffic Going Straight Ahead	East West West Southwest	East Vehicle Stopped East West
7149 State Route 7150 State Route	Cialiam 1 Cialiam 1	101		261.71 No E504041 261.80 No E484990	2015-12-31 00:56 No Apparent Injury 2015-11-16 16:19 No Apparent Injury	0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb Passenger Car Truck Tractor & Semi-Trailer	Not at Intersection and Not Related	Clear or Partly Cloudy Raining Clear or Partly Cloudy	Dry Straight & Level Wet Straight & Level	Dark-No Street Lights Daylight	Vehicle Strikes Deer From same direction - both going straight - both moving - sideswipe Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	East West West East	West East
7151 State Route 7152 State Route	Clallam 1 Clallam 1	101		261.83 No E551961 261.94 No E425274	2015-05-13 14:10 No Apparent Injury 2015-05-13 14:10 No Apparent Injury	0 0 1 0	Prickup,Panel Truck or Vanette under 10,000 lb Truck Tractor & Semi-Trailer	Truck Tractor & Semi-Trailer	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy					Going Straight Ahead	East West Northeast	West East
7153 State Route 7154 State Route	Clallam 1 Clallam 1	101		261.94 No E768120 262.00 No E924390	2019-05-27 21:10 No Apparent Injury 2019-05-27 21:10 No Apparent Injury	0 0 1 0	0 Pickup.Panel Truck or Vanette under 10.000 lb	December Co.	Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Dry Straight & Level Dry Curve & Level	Dark-No Street Lights Dusk	Vehicle Strikes Deer Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead		West East	Man F.
7156 State Route	Cialiam 1 Cialiam 1	101		262.02 No 2766964 262.02 No 3046965	2018-04-23 12:33 No Apparent Injury	0 0 2 0	Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Curve & Grade	Daylight Daylight	From opposite direction - one left turn - one straight Same direction both turning right both moving sideswipe	Making Left Turn Making Right Turn	Going Straight Ahead Making Right Turn	North East	North East
7157 State Route 7158 State Route 7159 Contr. Doors	Clallam	101		262.02 No 3759331 262.02 No 3759375 262.02 No E435768	2019-02-25 17:30 Possible Injury 2015-06-20 20-06	1 0 2 0	Passenger Car Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade Dry Straight & Level Dry Straight & Level	Dusk Daylight	Entering at angle Entering at angle	Backing Going Straight Ahead Making Left Turn	Making Right Turn Going Straight Ahead Going Straight Ahead Going Straight Ahead	North South	West East
7160 State Route 7161 State Route	Clallam 1	101		262.02 No E435768 262.02 No E520664 262.02 No E523238	2016-02-26 17:04 No Apparent Injury	0 0 2 0	0 Passenger Car 0 Passenger Car	Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb Passenger Car Truck Tractor & Semi-Trailer	At Intersection and Related	Clear or Partly Cloudy Raining Clear or Partly Cloudy	Wet Straight & Level	Daylight	Entering at angle From opposite direction - one left turn - one straight		Going Straight Anead	West South	West East
7161 State Route 7162 State Route 7163 State Route	Cialiam 1 Cialiam 1	101		262.02 No E523338 262.02 No E608860 262.02 No E611573	2016-11-14 10:42 Possible Injury 2016-11-22 15-25 Possible Injury	1 0 2 0	0 Passenger Car 0 Passenger Car	Passenger Car	At Intersection and Related At Intersection and Related	Overcast	Wet Curve & Level	Daylight Daylight	Entering at angle Entering at angle	Making Left Turn Making Left Turn Making Left Turn	Going Straight Ahead Making Left Turn Going Straight Ahead	South West	East South
7164 State Route 7165 Conta Dant	Cialiam 1	101		262.02 No E611573 262.02 No E623961 262.02 No E623961	2016-12-20 16:23 No Apparent Injury	0 0 2 0	Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb Dickup Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight	Entering at angle From opposite direction - one left turn - one straight		Going Straight Ahead	East South	West East
7166 State Route 7167 State Pouts	Cialiam 1 Cialiam 1	101		262.02 No E632917 262.02 No E962417 262.02 No E989735	2019-09-17 17:43 No Apparent Injury 2019-09-17 17:43 No Apparent Injury 2019-12-03 12:10 Supported Service Injury	0 0 2 0	Passenger Car Passenger Car Passenger Car	Truck Tractor & Semi-Trailer Passenger Car Pickup-Panel Truck or Vanette under 10,000 b Passenger Car Pickup-Panel Truck or Vanette under 10,000 b Pickup-Panel Truck or Vanette under 10,000 b Passenger Car Passenger Car	At Intersection and Not Related At Intersection and Polated	Clear or Partly Cloudy Overcast	Dry Straight & Level Other Straight & Level	Dark-No Street Lights Daylight	From opposite direction - one left turn - one straight From same direction - both going straight - both moving - rear-end From opposite direction - one left turn - one straight	Making Left Turn Going Straight Ahead Going Straight Ahead	Going Straight Ahead Slowing Making Left Turn	East West	East West South
7168 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101		262.02 No E989735 262.03 No E908749 262.10 No F709761	2019-04-04 16:30 No Apparent Injury 2017-09-02 15:15 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup Panel Truck or Venette under 10 000 %	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight	From opposite direction - one left turn - one straight From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - sideswine	Going Straight Anead	Stopped for Traffic	West East	Vehicle Stopped Vehicle Stopped West Fast
7170 State Route 7171 State Posts	Ciallam 1 Ciallam 1 Ciallam 1 Ciallam 1	101		262.10 No E709751 262.10 No E904463 262.12 No E606719	2019-03-09 11:37 No Apparent Injury 2016-11-09 17:10 No Apparent Injury	0 0 2 0	Pickup,Panier rruck or vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Fog or Smog or Smoke	Dry Straight & Level	Daylight Dark-Street Links A	From same direction - both going straight - both moving - sideswipe From same direction - both going straight - both moving - sideswipe Vehicle Strikes Deer	Changing Lanes Going Straight Ahead Going Straight Ahead	Going Straight Ahead Slowing	East West	East West
7172 State Route 7173 State Route	Clallam 1	101		262.12 No E606721 262.21 No E588508	2016-11-09 17:10 No Apparent Injury 2016-09-25 06:35 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb		Not at intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Fog or Smog or Smoke Overcast	Dry Straight & Level	Dark-Street Lights On	Vehicle Strikes Deer	Going Straight Ahead		East West West Fee!	
7174 State Route 7175 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101 101 101		262.21 No E588508 262.23 No E717182 262.24 No E974774	2017-06-30 20:34 No Apparent Injury 2019-10-23 19:03 No Apparent Injury	0 0 1 0	O Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast	Dry Straight & Level Dry Straight & Level	Dawn Daylight Dark-No Street Lights	Vehicle Strikes Deer Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East East West	
7176 State Route	Cialiam 1 Cialiam 1	101		262.30 No E392202 262.34 No E380260	2015-01-13 19:09 Dead at Scene 2014-11-29 13:40 No Apparent Injury	0 1 1 1	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb			Overcast Clear or Partly Cloudy	Dry Straight & Level	Dark-No Street Lights Davlight	Vehicle Strikes Deer Vehicle Strikes Deer Vehicle going straight hits pedestrian Bridon Rail - Face	Going Straight Ahead Going Straight Ahead		East West	
Test	Ciallam	101		262.34 No E982826 262.35 No E476747	2019-11-03 18:35 No Apparent Injury 2015-10-27 19:20 Suspected Minor Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Dark-No Street Lights	verlicie gonig stratiget niet peolestrian Bladge Rail - Fore Vehicles Strikes Deer Form same direction - both going straight - both moving - rear-end Vehicle Strikes Deer	Going Straight Ahead		West East East West	
7180 State Route		101		262.35 No E584532 262.35 No E851958	2016-09-12 16:30 No Apparent Injury 2018-10-21 10:51 No Apparent Injury	0 0 2 0	Passenger Car Pickup Panel Truck or Venette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Daylight	venicie strikes Deer From same direction - both going straight - both moving - rear-end Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing	East West	East West
7182 State Route 7183 State Pouts	Clallam	101		262.36 No E380251 262.36 No E702469	2014-11-29 06:30 No Apparent Injury 2017-08-12 18:00 No Apparent Injury	0 0 1 0	Passenger Car Pickup Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Snowing Overrast	Snow/Slush Straight & Level	Dark-No Street Lights	Guardrail - Face	Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East	
7184 State Route	Cialiam 1	101		262.36 No E702468 262.36 No E850614 262.55 No E492179	2018-10-17 19:10 No Apparent Injury 2015-12-03 18:08 Supported \$55-12-15:10-1	0 0 1 0	O Plackspy, Panel Truck or Vanette under 10,000 to Plackspy, Panel Truck or Vanette under 10,000 to Pickup, Panel Truck or Vanette under 10,000 to Pickup, Panel Truck or Vanette under 10,000 to Plackspy, Panel Truck or Vanette under 10,000 to Plackspy, Panel Truck or Vanette under 10,000 to		Not at Intersection and Not Related	Clear or Partly Cloudy Raining	Dry Straight & Level	Dark-No Street Lights	Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead		East West	
7186 State Route 7187 State Route	Clallam	101		262.55 No E492179 262.55 No E954080 262.63 No E929018	2019-08-25 13:20 No Apparent Injury 2019-05-29 06:50 No Apparent Injury	0 0 2 0	Passenger Car Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup Panel Truck or Vanette under 10 000 %	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Dark-Street Lights On Daylight Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - straight	Going Straight Ahead Going Straight Ahead Changing Lanes	Slowing Going Straight Ahead	West East West Fact	West East West Fast
7188 State Route 7189 State Pouts		101		262.55 No E954080 262.63 No E929018 262.65 No E496774 262.69 No E566158	2015-12-15 10:06 No Apparent Injury 2016-07-20 17-25 No Apparent Injury	0 0 2 0	Passenger Car Pickup Panel Track or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Overcast			From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - sideswipe From same direction - both going straight - both moving - sideswipe Grantfall - laedrin Ford	Changing Lanes Going Straight Ahead Going Straight Ahead	Going Straight Ahead Changing Lanes	East West	East West
7188 State Route 7189 State Route 7199 State Route 7190 State Route 7191 State Route 7192 State Route 7192 State Route 7192 State Route 7193 State Route 7194 State Route 7195 State Route 7196 State Route 7197 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101		262.70 No E505019	2016-01-07 14:34 Possible Injury 2019-10-28 00:00 No Apparent Injury	1 0 2 0	Pickup, Panel Truck or Vanette under 10,000 lb Truck Tractor & Semi-Trailer Passenger Car Passenger Car	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Unknown				Changing Lanes Going Straight Ahead	Going Straight Ahead	East West	East West
7192 State Route 7193 State Route	Cialiam 1	101							Not at Intersection and Not Related	Unknown Unknown Clear or Partly Cloudy	Unknown Straight & Level Unknown Straight & Level Dry Straight & Level			Other* Going Straight Ahead	Slowing	East West	East West
7194 State Route 7195 State Route	Cialiam 1 Cialiam 1	101		267.88 No E946360 267.89 No E946358 267.91 No E772153	2019-07-20 18:34 Possible Injury 2018-02-18 09:39 No Apparent Injury	1 0 2 0	Passenger Car Pickup Panel Truck or Vanette under 10 000 lb.	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Snowing	Dry Straight & Level Dry Straight & Level Snow/Slush Curve & Grade		room same direction - doin going straight - door moving - rear-end From same direction - both going straight - both moving - rear-end Over Embankment - No Guardrail Present	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing	West East East West	West West
7196 State Route 7197 State Route	Cialiam 1 Cialiam 1	101		267.94 No E862534 268.00 No 3202363	2018-11-11 19:17 No Apparent Injury 2015-12-06 19:42 No Apparent Injury	0 0 2 0	Passenger Car Pickup Panel Truck or Vanette under 10 000 to	Passenger Car Pickup Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Pelated	Clear or Partly Cloudy Raining	Dry Straight & Level	Daylight	From same direction - both going straight - both moving - sideswipe	Going Straight Ahead Going Straight Ahead	Overtaking and Passing Going Straight Ahead	West East West Fact	West East West Fast
7198 State Route	Clallam 1	101		268.00 No E820501	2018-07-21 18:00 No Apparent Injury	0 0 2 0	0 Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Grade	Daylight	Fallen tree hit by vehicle (on the road) From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead	Going Straight Ahead Stopped for Traffic	West East	Vehicle Stopped Vehicle Stopped

CRCUMSTANCE_1_UNT_1	MV_DRIVER_CONTRIBUTING AGE UNIT_1 WINT_1 UNIT_1	MY_DRIVER_CONTRIBUTING MY_DRIVER_CONTRIBUTING MY_DRI CIRCUMSTANCE_1_UNIT_2 CIRCUMSTANCE_2_UNIT_2 CIRCUM	RIVER_CONTRIBUTING MV_DRIVER	MY_DRIVER GENDER UNT_2 BICYCLIST_CONTRIBUTING CIRCUMSTANCE_1_UNT_1 CIRCUMSTANCE_2_UNIT_1	G BICYCLIST_CONTRIBUTING BICYCLIST BICYCLIST GENDER CICCUMSTANCE_1_UNIT_1 DIFFT AGE GROUNSTANCE_1_UNIT_2 CIRCUMSTANCE_2_UNIT_2	BICYCLIST_CONTRIBUTING BICYCLIST CIRCUMSTANCE_3_UNIT_2 AGE_UNIT_3	BICYCLIST PEDESTRIAN_CONTRIBUTING PEDESTRIAN_CONTRIBUTING GENDER_LINT_2 CIRCUMSTANCE_1_LINT_2 CIRCUMSTANCE_2_LINT_2	PEDESTRIAN CONTRIBUTING PEDESTRIAN PEDESTRIAN PEDESTRIAN RRST_IMPACT_LOCATION CIRCUMSTANCE 3 UNIT 2 AGE UNIT 2 GRODER UNIT 2 City St. Courty Rd & Misc. 2010 forward, St
				UNIT_2	UNIT_1 UNIT_1			
21 None 22 None	18 Maie 19 Female 53 Female 53 Maie							Pass Right Recorder Decreasing Milepost Lans 1 becessing Milepost Lans 1 becessing Milepost Lans 1 becessing Milepost Lans 1 becessing Milepost
23 Follow Too Closely Inattention Instention Instention	68 Female Nor 73 Female Nor 48 Male	one Vone	5	Female Female				Intersection Road Increasing Milepost Intersecting Road Increasing Milepost Intersecting Road Increasing Milepost Past Right Shoulder Increasing Milepost Past Right Shoulder Increasing Milepost Past Right Shoulder Increasing Milepost
26 Exceeding Reas. Safe Speed 27 None	35 Female							Lane 2 Decreasing Milepost
28 None 29 None 20 Apparently Asleep or Fatigued	19 Female 25 Male 79 Male							Lane 1 Decreasing Miepost Lane 2 Increasing Miepost In Median Decreasing Miepost
Apparently Fatigued Follow Too Closely Follow Too Closely	34 Nor 63 Male Nor	None None None	11	Female Male				Lane 2 increasing Miepost Lane 1 increasing Miepost
33	18 Male 71 Male 22 Male No	None		Female				Lane 2 homassing Miepost Increasing Other Location Lane 1 Decreasing Miepost
66 Did Not Grant RW to Vehicle 17 None	18 Male Nor 32 Female Oth	lone Other Contributing Circ Not Listed	61	Male				Lane 1 Decreasing Milepost Lane 2 Decreasing Milepost Une 2 Decreasing Milepost Interacting Road Decreasing Milepost
18 Follow Too Closely None 10 Other Contributing Circ Not Listed	55 Male Nor 44 Female 51 Male Oth	Notes Combusing Circ Not Listed		Male Female				Lane 1 Increasing Milepost
None	39 Female Nor 24 Female	lone		Female Male				Lane 2 Decreasing Milepost Lane 1 Decreasing Milepost He Median Decreasing Milepost He Median Decreasing Milepost He Median Decreasing Milepost He Median Decreasing Milepost
13 Inattention Eating or Orinking	68 Male 334 Male 36 Male 36 Male 36 Female Nor							In Median Decreasing Milepost Past Kight Shoulder Increasing Milepost Lane 1 Increasing Milepost
16 Exceeding Reas. Safe Speed 17 Exceeding Reas. Safe Speed	76 Male Nor	None	2	Male Male				Lane 1 Increasing Milepost Lane 1 Increasing Milepost
16 Euxocering Feast. Sale Speed 7 Euxocering Feast. Sale Speed 8 Follow Too Chosely 10 Euxocering Feast. Sale Speed 11 Euxocering Feast. Sale Speed 12 Euxocering Feast. Sale Speed 13 Euxocering Feast. Sale Speed 13 Euxocering Feast. Sale Speed 14 Euxocering Feast. Sale Speed 15 Euxocering Feast. Sale Speed 16 Euxocering Feast. Sale Speed 17 Euxocering Feast. Sale Speed 18 Euxocering Feast. Sale Speed 19 Europe Too Company 10 Europe Too Company 10 Europe Too Company 10 Europe Too Company 10 Euxocering Feast. Sale Speed 11 Euxocering Feast. Sale Speed 12 Euxocering Feast. Sale Speed 13 Euxocering Feast. Sale Speed 14 Euxocering Feast. Sale Speed 15 Euxocering Feast. Sale Speed 16 Euxocering Feast. Sale Speed 17 Euxocering Feast. Sale Speed 17 Euxocering Feast. Sale Speed 18 Euxocering Feast	67 Male Nor 16 Male Nor 26 Female Nor 32 Male Nor	one kone	6-	Male Male Male				Lane Decreasing Mispost Lane Thromason Mispost
51 Exceeding Reas. Safe Speed 52 Follow Too Closely Inattention	30 Female Nor	None	41	Female Male				
	66 Male Nor 18 Female Nor	Operating Handheld Cell Phone None None	5 2	Female Male Female				Intenecting Road Decreasing Milepost Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost
	32 Matie Nor 20 Matie Nor 20 Female Nor 20 Female Nor 21 Matie Orbital 31 Female Nor	one None	3	Female				Lanza Elemanian Mercorasi mellengan Mercorasi
199 hattention 100 Exceeding Reas. Safe Speed	28 Male Dri 21 Male Nor	one Information	2	Female Male Male				Lane 1 increasing wilepost
51 None	33 Female 47 Male Driv 56 Female	Oriver Not Distracted	61	Male				Lane 1 Decreasing Milepost Lane 1 Increasing Milepost
None	2 Mate work of the control of the co							Lane 1 homasaing Milepost Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost
56 Apparently Asteep or Fatigued Unknown Distraction Cher Contributing Circ Not Listed	26 Male 25 Female							Lane 1 Decreasing Melepost in Melepost in Melepost in Melepost in Park Right Mouder Increasing Melepost in Park Right Mouder Increasing Melepost in Park Right Mouder Increasing Melepost in Melepost
19 Apparently Asleep or Fatigued None	25 Female 46 Male 25 Female 24 Female							Lanc 2 Decreasing Mispost Pass Right Shouldher breasing Mispost Lanc 1 Decreasing Mispost Lanc 1 Decreasing Mispost Lanc 1 Decreasing Mispost
Mone 12 None 13 Other Contribution Circ Net Lister 1 Other Contribution Circ Net Lister 1	25 Male Mor	None		Earnia				Lane 1 Decreasing Milepost Right Shoulder Decreasing Milepost Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost
15 Other Contributing Circ Not Listed 15 Other Contributing Circ Not Listed				Female Male				Lane Decreasing Milepost Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost Past Right Shoulder Decreasing Milepost Lane 2 Encessing Milepost Lane 2 Encessing Milepost
76 None Exceeding Reas. Safe Speed	25 Fernale 25 Mate Not 66 Mate 17 Mate Not 66 Fernale 17 Mate Not 66 Fernale 17 Mate Not 66 Fernale Not 66 Fern	lone None	3:	Female				Lane 2 Increasing Milepost
19 Follow Too Closely 10 Exceeding Reas. Safe Speed	31 Female Nor	None None	21	Female Female				Intersecting Road Increasing Milepost Lane 2 horseasing Milepost Lane 2 Decreasing Milepost
Second Control of the Control of t	45 Male Nor 56 Male Driv	None Driver Not Distracted Other Contributing Circ Not Listed	6:	Pemale Male				Lane 2 Decreasing Milepost Lane 2 Decreasing Milepost Pass Right Rouder Increasing Milepost Pass Right Rouder Increasing Milepost Lane 1 Secrasing Milepost Lane 1 Secrasing Milepost
13 Other Contributing Circ Not Listed Apparently III			5-	Male				Lane 2 Decreasing Milepost
96 Did Not Grant RW to Vehicle Distractions Outside Vehicle 97 Did Not Grant RW to Vehicle Distractions Outside Vehicle 88 Under Influence of Alcohol Over Center Line	73 Maie Nor 57 Female Nor 42 Female Nor 48 Female Nor 67 Maie	one done	7:	Female Male				Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost Lane 2 Decreasing Miepost Lane 2 Decreasing Miepost
19 Over Center Line Out Served on the Center Line Exceeding Reas. Safe Speed	48 Female Nor 47 Male	kone	41	Male Male				Lane 2 Increasing Milepost
11 Exceeding Reas. Safe Speed 15 Follow Too Closely 17 Operating Defecting Equipment	47 Male 45 Male Nor 45 Male Nor 66 Male Nor 23 Male Nor	one vone	20 50	Maie Maie				In Median Increasing Milepost Lane 1 Decreasing Milepost Lane 1 hereasing Milepost Lane 1 hereasing Milepost Lane 2 hereasing Milepost Lane 2 hereasing Milepost
154 Exceeding Reas. Safe Speed 155 Exceeding Reas. Safe Speed	33 Male 57 Female Nor 42 Male Nor 70 Male Nor 68 Male	lone None	61	Male Female				Lane 2 Increasing Missipori Lane 1 Decreasing Missipori Lane 1 Decreasing Missipori
66 Disregard Stop and Go Light 18 Instention 19 Disregard Stop and Go Light	70 Male Nor 68 Male Nor 79 Male Nor	one	71 22	Male Female Earnale				Lane 3 Decreasing Milepost
Disregard stop and Go Light Di	68 Maile Nors 79 Maile Nors 79 Maile Nors 61 Female Nors 62 Female Nors 63 Maile Nors 64 Female Nors 65 Maile Nors 66 Female Nors 66 Female Nors 67 Female Nors 68	one kone	5	Female Female Male Female				Lane 3 Increasing Miepost Lane 2 Increasing Miepost Lane 2 Decreasing Miepost
11 Exceeding Reas. Safe Speed 12 Institution Follow Too Closely 13 Institution Follow Too Closely			3	Male Female Male				Lane 3 Decreasing Milepost
2	33 Male Nor 45 Male S2 Male Nor	None		Female				Lane 3 Increasing Milepost Lane 1 Decreasing Milepost Lane 2 Decreasing Milepost
Inattention Did Not Grant RVI to Venicle	27 Female Nor 34 Female Driv 56 Female Nor 51 Female Nor	one Oriver Not Distracted	3	Female				Lane 3 increasing Miepost Left Turn Lane Increasing Miepost
			2	Male Female Female Female				Lane 3 Increasing Milepost Intersecting Road Decreasing Milepost Lane 2 Decreasing Milepost
11 Exceeding Reas. Safe Speed Follow Too Closely	26 Male Nord 78 Female Nord 772 Male Nord 78 Female Nord 78 Female Nord 88 Female Nord	.one	51	Female Female				Lane 1 Increasing Milepost Lane 2 Increasing Milepost Lane 2 Increasing Milepost Lane 2 Increasing Milepost
	35 Female Nor 88 Female Nor	lone None	7 6	Female Female Female Male				Lane 3 increasing Milepost Interacting Road Deversating Milepost Lane 3 increasing Milepost Lane 3 increasing Milepost
6 hattention Exceeding Reas. Safe Speed 17 Disregard Stop and Go Light Other Driver Distractions Inside Vehicle 18 Disregard Stop and Go Light	88 Female Nor 47 Male Nor 30 Female Nor Nor	one Jone None	2	Male Female				Lane 3 Increasing Milepost Lane 2 Increasing Milepost Lane 2 Increasing Milepost
7. Disregate Step and Go Light Uner univer unstructions into evenine Schenger Step and Go Light Uner univer unstructions into evenine Schenger Step Schenger	Not 35 Male Not 56 Female Not 21 Female Not	None	3:	Male Male				Lane 3 Decreasing Miepost Lane 2 Decreasing Miepost Lane 2 Decreasing Miepost
22 Did Not Grant RW to Vehicle 23 Inattention Follow Too Closely	71 Female Driv	Driver Not Distracted	3:	Male Male Female				Lane 2 Decreasing Milepost Lane 3 Decreasing Milepost
	78 Female Nor 71 Female Nor 20 Male 22 Female	one	6	Female				Right Shoulder Increasing Milepost
	35 Female Driv 35 Male Not	Iriver Not Distracted None						Right Shoulder Increasing Milepost Past Right Shoulder Decreasing Milepost In Median Increasing Milepost
19 Follow Too Closely Do Inattention Operating Handheld Cell Phone 11 Follow Too Closely	23 Male Nor	vone	6	Female Female				In Median Increasing Milepost Lane 2 Decreasing Milepost Lane 3 Decreasing Milepost
Other Contributing Circ Not Listed		None 'one	6 41 7:	Female Female				In Median Increasing Milepost Land 2 Decreasing Milepost Land 2 Decreasing Milepost Land 3 Decreasing Milepost Land 3 Decreasing Milepost Land 3 Decreasing Milepost Land 2 Decreasing Milepost Land 2 Decreasing Milepost
M Did Not Grant PW to Vehicle	34 Male Oth	None None Other Contributing Circ Not Listed None Done None Done Mont Distracted	6 41 7:	Female Female				In Median Increasing Milepost Land 2 Decreasing Milepost Land 2 Decreasing Milepost Land 3 Decreasing Milepost Land 3 Decreasing Milepost Land 3 Decreasing Milepost Land 2 Decreasing Milepost Land 2 Decreasing Milepost
33 Confer community citic Not usseed 33 Confer community citic Not usseed 34 Did Not Grant RW to Vehicle 35 Other Contributing Circ Not Listed 36 Other Contributing Circ Not Listed 46 Instatemblo	64 Female Driv	None Driver Not Distracted	6 4 4 7 7 7 7 7 6 6 8 8 6 6 6 6 6 6 6 6 6 6 6	Female Female Female Female Manue Female Female Female Female Female Female Female Female				N. Median Increasing Milepost
Solid Controlling Unit Not Seed to Seed t	64 Female Driv	None Driver Not Distracted	6 4 4 7 7 7 6 6 6 6 6 6 6 6 6 6 6 6 6 6	Fernals			Other Contribution Circ Nat Listed	In Median Increasing Mepost Laws 2 Decreasing Mepost Laws 1 Increasing Meyors Laws 2 Decreasing Meyors
	Not	None None	6 4 4 4 4 7 7 7 5 5 5 5 5 5 5 5 5 5 5 5 5	Fernale Fernale Fernale Fernale Fernale Fernale Fernale Fernale Fernale Able				In Median Increasing Milepost Lans 2 Decreasing Milepost Lans 1 Increasing Milepost Lans 1 Decreasing Milepost Lans 1 Decreasing Milepost Lans 2 Decreasing Milepost Lans 2 Decreasing Milepost Lans 3 Decreasing Milepost
	64 Fernate 64 Fernate 64 Fernate 65 Fernate 65 Fernate 65 Fernate 65 Motie 66 Motie 66 Motie 66 Fernate 66 Fernate 66 Fernate 67 Fernate 68 Fernate	Notes	6 6 44 44 44 44 44 44 44 44 44 44 44 44	Financia (Financia (Financ				N. Median Increasing Milepost
	64 Fernate Di 40 Fernate Di 20 Marie 20 Marie 30 Marie 30 Marie 30 Marie 30 Marie 30 Marie 30 Fernate No 30 Fernate No 30 Fernate No 30 Marie 30 Fernate No 30 Marie 30 Marie No 30 Marie	Notes And The Character And Th	6 6 7 7 7 7 7 7 7 7 7 7 7 7 7 7 7 7 7 7	Female Female Able Female Able Female Able Female Able Female Able				In Median Increasing Milepost Lane 3 Decreasing Milepost Lane 1 Normaning Milepost Lane 2 Decreasing Milepost Lane 2 Decreasing Milepost Lane 2 Decreasing Milepost Lane 2 Decreasing Milepost Control Contr
March Care R Vir V Wester	64 Female Dis 49 Female Dis 20 Marie Parcel Dis 33 Marie Parcel Dis 40 Female Parcel Dis 33 Marie Parcel Dis 40 Female Parcel	Notes Notes	6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6	Female Female Female Base Female				In Median Increasing Mepost Lane 1 Decreasing Mepost Lane 1 Increasing Mepost Lane 1 Decreasing Mepost Lane 2 Decreasing Mepost Lane 3 Decreasing Mepost
March Care RV N = Nevtice	64 Fernate Dis 40 Fernate Dis 20 Marie Nov. 1 Marie Nov	Notes	6 6 7 7 7 7 6 6 8 8 8 8 8 8 8 8 8 8 8 8	Female Female Female Able Able Female Female Female Female Female Female Able Female Able Able Female Able Female Female Able Female Fe				In Median Increasing Milepost Laws 2 Decreasing Milepost Laws 1 Increasing Milepost Laws 2 Increasing Milepost Laws 2 Increasing Milepost Laws 2 Increasing Milepost Laws 2 Increasing Milepost Laws 3 Decreasing Milepost Laws 4 Decreasing Milepost Laws 5 Decreasing Milepost Laws 5 Decreasing Milepost Laws 6 Decreasing Milepost Laws 7 Decreasing Milepost Laws 8 Decreasing Milepost Laws 9 Decreasing Milepost
March Count Five V Western March Count Fi	64 Fernate Dis 40 Fernate Dis 20 Marie Nov. 1 Marie Nov	Notes	6 6 6 7 7 7 7 6 6 7 8 7 9 7 9 7 9 7 9 7 9 9 9 9 9 9 9 9 9	Fernale Fernale Fernale Able Able Fernale Fernale Fernale Fernale Fernale Fernale Fernale Able Able Fernale Able Fernale Ferna				In Median Increasing Milepost Lane 1 Toncessing Milepost Lane 2 Doncessing Milepost Lane 2 Toncessing Milepost Lane 3 Toncessing Milepost Lane 4 Toncessing Milepost Lane 4 Toncessing Milepost Lane 5 Toncessing Milepost
March Count Five V Western March Count Fi	64 Female 64 Female 74 Female 75 Female 75 Female 76 Female 76 Female 76 Female 77 Female 78 Female 78 Female 79 Female 77 Female 77 Female 77 Female 77 Female 77 Female 77 Female 78 Female 79 Female 70 Fem	Notes Not Detected Control of Con	6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6	Fernals				In Median Increasing Milepost Lare 3 Decreasing Milepost Lare 1 Necessing Milepost Lare 2 Necessing Milepost Lare 2 Necessing Milepost Lare 3 Decreasing Milepost Lare 2 Decreasing Milepost Lare 3 Decreasing Milepost Lare 4 Decreasing Milepost Lare 4 Decreasing Milepost Lare 5 Decreasing Milepos
March Count Five V Western	64 Female 64 Female 74 Female 75 Female 75 Female 76 Female 76 Female 76 Female 77 Female 78 Female 78 Female 79 Female 77 Female 77 Female 77 Female 77 Female 77 Female 77 Female 78 Female 79 Female 70 Fem	Notes Not Detected Control of Con	6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6	Fernals				In Median Increasing Milepost Lans 1 Encessang Milepost Lans 2 Encessang Milepost Lans 3 Encessang Milepost Lans 4 Encessang Milepost Lans 5 Encessang Milepost Lans 5 Encessang Milepost Lans 6 Encessang
March Clauser RV W & Werkelde	64 Female 64 Female 74 Female 75 Female 75 Female 76 Female 76 Female 76 Female 77 Female 78 Female 78 Female 79 Female 77 Female 77 Female 77 Female 77 Female 77 Female 77 Female 78 Female 79 Female 70 Fem	Notes Not Detected Control of Con	6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6	Fernale Fernale Male Fernale Male Fernale Fernale Fernale Fernale Male Fernale Male Male Male Male Male Male Male M			Other Commission Core Not Listed	In Median Increasing Milepost Lane 1 Encessaring Milepost Lane 2 Encessaring Milepost Lane 2 Encessaring Milepost Lane 3 Encessaring Milepost Lane 4 Encessaring Milepost Lane 5 Encessaring Milepost Lane 6 Encessaring Milepost Lane 6 Encessaring Milepost Lane 8 Encessaring Milepost
March Count RV N Webside Basterioren Control Count RV N Webside Basterioren Control Count RV N Webside	64 Female	Down Not Diseased	6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6	Fernale Fernale Male Fernale Male Fernale Male Male Male Male Male Male Male M			Other Commission Core Not Listed	In Median Increasing Milegoot Lane 1 Shoreasing Milegoot Lane 2 Shoreasing Milegoot
March Count RY W S Wester Baseline	64 Female	Down Not Diseased	6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6	Financia			Other Commission Core Not Listed	In Median Increasing Milegost Land 1 Screening Milegost Land 2 Screening Milegost Land 1 Screening Milegost Land 2 Screening Milegost Land 3 Screening
March Count Rivin & Western	64 Female 20	None And The Characted And The Character And The	6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6	Female Female Male Male Female Female Male Female Male Male Male Female Male Male Female Male Female Male Female			Other Commission Core Not Listed	In Median Increasing Milepost Lane 1 Tonosassig Milepost Lane 2 Tonosassig Milepost Lane 3 Tonosassig
March Count Rivin & Western	64 Female 20	None Note of the Note of Note of the Note	6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6	Financia			Other Commission Core Not Listed	In Median Increasing Milegoot Lane 1 Shoreasing Milegoot Lane 2 Shoreasing Milegoot Lane 3 Shoreasing Milegoot Lane 3 Shoreasing Milegoot Lane 4 Shoreasing Milegoot Lane 4 Shoreasing Milegoot Lane 4 Shoreasing Milegoot Lane 5 Shoreasing Milegoot
March Count Rivin & Western	64 Female 20	None Note of the Note of Note of the Note	6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6 6	Female Female Male Male Female Female Male Female Male Male Male Female Male Male Female Male Female Male Female			Other Commission Core Not Listed	In Median Increasing Milegoot Lans 1 Encessang Milegoot Lans 2 Encessang Milegoot Lans 2 Encessang Milegoot Lans 3 Encessang Milegoot Lans 4 Encessang Milegoot Lans 5 Encessang Milegoot Lans 6 Encessang Milegoot Lans 7 Encessang Milegoot Lans 8 Encessang
March Count Rivi N Western	64 Female 20	None Note of the Note of Note of the Note	6 6 6 8 8 6 8 8 6 8 8 8 8 8 8 8 8 8 8 8	Female Female Male Male Female Female Male Female Male Male Male Female Male Male Female Male Female Male Female			Other Commission Care Not Listed	In Median Increasing Milepost Lane 1 Norosang Milepost Lane 2 Norosang
March Color RY V	64 Female 20	None Note of the Note of Note of the Note	6 6 6 8 8 6 8 8 6 8 8 8 8 8 8 8 8 8 8 8	Frenzie Frenzie Male Frenzie Male Frenzie Frenzie Frenzie Frenzie Frenzie Male Frenzie Male Frenzie Male Frenzie F			Other Combissing Circ Nor Listed	Service Services (Services Services Ser
March Care RV N = Newton State Controller Control RV	64 Female 10	None More North Marked Immarked Immarke	6 6 6 8 9 9 9 9 9 9 9 9 9 9 9 9 9 9 9 9	Frenzie Frenzie Male Frenzie Male Frenzie Frenzie Frenzie Frenzie Frenzie Male Frenzie Male Frenzie Male Frenzie F			Other Combissing Circ Nor Listed	In Median Increasing Milepost Lans 1 Noceasing Milepost Lans 2 Noceasing Milepost Lans 2 Noceasing Milepost Lans 3 Noceasing Milepost Lans 4 Noceasing Milepost Lans 5 Noceasing Milepost Lans 6 Noceasing Milepost Lans 7 Noceasing Milepost Lans 8 Noceasing
March Care RV N V Nethods	64 Female 10	None More North Marked Immarked Immarke	6 6 6 8 9 9 9 9 9 9 9 9 9 9 9 9 9 9 9 9	Frenzie Frenzie Male Frenzie Male Frenzie Frenzie Frenzie Frenzie Frenzie Male Frenzie Male Frenzie Male Frenzie F			Other Combissing Circ Nor Listed	In Median Increasing Milepost Lans 1 Norosang Miscost Lans 2 Norosang Miscost Lans 2 Norosang Miscost Gif Female Lans 3 Norosang Miscost Lans 4 Norosang Miscost Lans 4 Norosang Miscost Lans 4 Norosang Miscost Lans 5 Norosang Miscost Lans 1 Norosang Miscost Lans 2 Norosang Miscost Lans 3 Norosang Miscost Lans 3 Norosang Miscost Lans 4 Norosang Miscost Lans 2 Norosang Miscost Lans 3 Norosang Miscost Lans 3 Norosang Miscost Lans 4 Norosang Miscost Lans 2 Norosang Miscost Lans 3 Norosang Miscost Lans 3 Norosang Miscost Lans 4 Norosang Miscost Lans 3 Norosang Miscost Lans 3 Norosang Miscost Lans 4 Norosang Miscost Lans 4 Norosang Miscost Lans 4 Norosang Miscost Lans 4 Norosang Miscost Lans 3 Norosang Miscost Lans 4 Norosang Miscost Lans 4 Norosang Miscost Lans 5 Norosang Miscost Lans 5 Norosang Miscost Lans 6 Norosang Miscost Lans 7 Norosang Miscost Lans 8 Norosang Miscost Lans 9 Norosa
March Care RV N Webster State Controlled Circle Not Listed On the Control RV N Webster On	64 Female 10 10 10 10 10 10 10 1	None More Not Datasted Diseased Junior None Service Se	6 6 6 8 6 8 6 8 6 6 8 6 8 6 6 8 6 8 6 8	Frenzie			Other Combissing Circ Nor Listed	In Median Increasing Milepost Lane 1 Noceasing Milepost Lane 2 Noceasing Milepost Lane 2 Noceasing Milepost Lane 2 Noceasing Milepost Lane 3 Noceasing Milepost Lane 4 Noceasing Milepost Lane 4 Noceasing Milepost Lane 4 Noceasing Milepost Lane 5 Noceasing Milepost Lane 6 Noceasing Milepost Lane 6 Noceasing Milepost Lane 7 Noceasing Milepost Lane 8 Noceasing
March Card RV W Webside	64 Female 10 10 10 10 10 10 10 1	None More Not Datasted Diseased Junior None Service Se	6 6 6 8 6 8 6 6 6 6 6 6 6 6 6 6 6 6 6 6	Frende Fre			Other Combissing Circ Nor Listed	In Median Increasing Milepost Lans 1 Noceasing Milepost Lans 2 Noceasing Milepost Lans 2 Noceasing Milepost Lans 3 Noceasing Milepost Lans 4 Noceasing Milepost Lans 5 Noceasing Milepost Lans 6 Noceasing Milepost Lans 7 Noceasing Milepost Lans 8 Noceasing
14	General Communication Co	Does Note Characted Notes No	6 6 6 8 6 8 6 6 8 6 6 6 6 6 6 6 6 6 6 6	Fernale Fernale Able Fernale Able Fernale Able Fernale Able Abl			Other Contributing Circ Not Listed	In Median Increasing Milepost Lans 1 Noceasing Milepost Lans 2 Noceasing Milepost Lans 2 Noceasing Milepost Lans 3 Noceasing Milepost Lans 4 Noceasing Milepost Lans 5 Noceasing Milepost Lans 6 Noceasing Milepost Lans 7 Noceasing Milepost Lans 8 Noceasing
March Card RV N Webster State Controlled Circle Add Land State Controlled Circle	General Communication Co	Does Note Characted Notes No	6 6 6 8 6 8 6 6 8 6 8 6 8 6 8 6 8 6 8 6	Fernale Fernale Able Fernale Able Fernale Able Fernale Able Abl			Other Contributing Circ Not Listed	In Median Increasing Milepost Lane 1 Shoreasing Milepost Lane 2 Shoreasing Milepost Lane 3 Shoreasing Milepost Lane 4 Shoreasing Milepost Lane 4 Shoreasing Milepost
92 Other Contributing Circ Not Listed	General Color	None Personnel Committee C	6 6 6 8 6 8 6 8 6 8 6 8 6 8 6 8 6 8 6 8	Fernale Fernale Male Fernale Fernale Male Fernale Male Fernale Fernale Male Fernale			Other Contributing Circ Not Listed	In Median Increasing Milepost Lans 1 Norosang Miscost Lans 2 Norosang Miscost Lans 2 Norosang Miscost Lans 3 Norosang Miscost Lans 3 Norosang Miscost Lans 3 Norosang Miscost Lans 3 Norosang Miscost Lans 2 Norosang Miscost Lans 3 Norosang Miscost Lans 2 Norosang Miscost Lans 3 Norosang Miscost Lans 2 Norosang Miscost Lans 3 Norosang Miscost Lans 2 Norosang Miscost Lans 2 Norosang Miscost Lans 3 Norosang Miscost Lans 3 Norosang Miscost Lans 4 Norosang Miscost Lans 5 Norosang Miscost Lans 6 Norosang Miscost Lans 6 Norosang Miscost Lans 8 Norosang Miscost Lans 9 Norosang

OBJECTID WA_STATE_PLANE SOUTH_X SOUTH_X 2010_FORWARD COYEE			OL COL COL_AGE SURFACE ONDITIONS	COL_LIGHT CONDITIONS	COL COL STUD GEOMETRY AREJ	COL COL TYPE SEVERITY VUL_USER TEXT TEXT	COL_HUMAN COL FACTOR_Decoded MVIW			COL_RISKY Intersection BEHAVIOR Area_Name	on Emphasis Roadw ne Area Departs
7021 973506.71 1016351.03 201- 7022 973513.84 1016349.08 2011 7023 973736.99 1016213.17 2011	7 1 Vehicle-Party Stuck Object 7 1 Vehicle-Party Aeimal 4 1 Vehicle-Party Aeimal 8 1 Vehicle-Party Aeimal 2 Vehicle-Party Aeimal 6 2 Vehicles-Partes Rear-End	Property Damage Only Other/Unknown 45- Property Damage Only Other/Unknown 45- Property Damage Only Follow Too Closely 65-	The state of the s	Daylight Not at Daylight At Drive	wway or Intersection Straight & Grade Intersection Drieway/Roundabout Straight & Level Intersection/Drieway/Roundabout Curve & Level Intersection/Drieway/Roundabout Straight & Grade eway or Intersection Straight & Level Straight & Level		Follow Too Closely		At Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout At Driveway or Intersection	No Risky Driving Behaviors Involved Follow Too Closely	Yes No No No No
7024 973725.3 1016305.99 2016 7025 973796.93 1016199.09 2017 7026 973894.31 1016170.99 2017 7027 973982.53 1016208.49 2018	6 2 Vehicles/Parties Rean-End 9 1 Vehicle/Party Struck Object 9 1 Vehicle/Party Struck Object 6 1 Vehicle/Party Animal	Property Damage Only Inattention 65* Minor or Possible Injury Under Influence of Alcohol/Drugs/Medication 45* Unknown Speeding Unit	No Dry 4 Under influence of Acohol 45-54 Dry sown No los/Snow/Slust 4 No Dry	Daylight At Driv Daylight Not at h Daylight Not at Daylight Not at	wwayor Intersection Curve & Level Intersection.DrivewayRoundabout Straight & Grade Intersection.DrivewayRoundabout Straight & Level Intersection.DrivewayRoundabout Straight & Grade	Older Road User 66+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Inattention Under Influence of Alcohol/Drugs/Medication Speeding None	Daylight Dry Daylight Dry Daylight Ice/Snowl	At Driveway or Intersection Not at Intersection/Driveway/Roundabout //Slush Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Inattention Under Influence of Alcohol/Drugs/Medication Speeding No Risky Driving Behaviors Involved	No Yes Yes No No
7031 975522.61 1015742.02 2011	6 Vahriche Parry Animal 1 Vahriche Parry Animal 5 Vahriche Parry Animal 5 Vahriche Parry Struck Object 5 Vahriche Parry 5 Vahriche Parry 9 Vahriche Parry 9 Vahriche Parry 1 Vahriche Parry 1 Vahriche Parry 5 Struck Object 1 Vahriche Parry 5 Vahriche Parry 1 Vahriche Parry 5 Vahri	Property Damage Univ Unercrunknown 25- Property Damage Only Apparently Asleep, Fasigued or III 654 Property Damage Only Apparently Asleep, Fasigued or III 25- Property Damage Only Instension 554 Property Damage Only Other/Unknown Univ	4 No Dry Apparently Asleep, Fatigued or III 65+ Wet 4 Apparently Asleep, Fatigued or III 25-34 Wet	Daylight Not at Dark-No Street Lights/Street Lights Off Not at	Intersection/Driveway/Roundabout Straight & Grade Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Apparenty Asset or Fangued Driver Physical Condition Inattention None	Dark-No Street Lights/Street Lights Off Wet	t Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Apparently Asleep, Fatigued or III Apparently Asleep, Fatigued or III Inattention No Risky Driving Behaviors Involved	No Yes Yes No No
7035 976435.84 1015557.98 201- 7036 976435.84 1015557.98 201- 7037 976435.84 1015557.98 201- 7038 976420.4 1015560.02 201-	9 1 Vehicles-Party Struck Object 4 2 Vehicles-Parties Angle 4 2 Vehicles-Parties Pargie 4 2 Vehicles-Parties Pargie 5 Vehicles-Parties Rear-End	Property Damage Only Paparensy Ascep, Pasgued or III to- Property Damage Only Fall to Yield Und. Property Damage Only Fall to Yield Und. Minor or Possible Injury OtherUnknown 25- Property Damage Only Follow Too Closely 55-5	12	OtherUnknown At Inter Daylight At Driv Dark-Street Lights On At Driv Dark-Street Lights On At Driv	rsection and Not Related Straight & Level eway or Intersection Straight & Grade eway or Intersection Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved	Fail to Yield Fail to Yield None	Daylight Dry Dark-Street Lights On Wet Dark-Street Lights On Dry	known At Intersection and Not Related At Driveway or Intersection	Apparently Asleep, Fatigued or III Fail to Yield Fail to Yield No Risky Driving Behaviors Involved Follow Too Closely	Yes No No No No
7040 976908.19 1015437.98 [201] 7041 977060.32 1015387.59 [201] 7042 977431.82 1015301.35 [201]	1 \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \	Property Damage Only Ohee/Unknown 35- Property Damage Only Ohee/Unknown 45- Property Damage Only Operating Defective Equipment 35- Property Damage Only Apparenty Asleep, Fatigued or III Unic Property Damage Only Intention 65- Intention 65- Intention Office Intention 65- Intention Office Intention Intention 65- Intention Inte	4 No Dry 4 No Dry 4 No Dry er 25 Apparently Asleep, Fatigued or III Under 25 Wet Wet	Daylight Not at Daylight Not at Daylight Not at	Intersection/Drieway/Roundabout Straight & Level Intersection/Drieway/Roundabout Straight & Level Intersection/Drieway/Roundabout Straight & Level Intersection/Drieway/Roundabout Straight & Grade Intersection/Drieway/Roundabout Cure & Grade Intersection/Drieway/Roundabout Cure & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 66+	Other Contributing Circ Not Listed Operating Defective Equipment Driver Physical Condition	Daylight Dry Daylight Dry Daylight Wet Daylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Apparently Asleep, Fatigued or III Inattention	No Yes Yes Yes Yes
7046 979107.5313 1014832.821 2014 7047 979477.15 1014736.68 2011	6 1 Vehicle-Party Struck Object 9 1 Vehicles-Party Aeimal 4 2 Vehicles-Partes Sides-wipe 5 2 Vehicles-Partes I Turning 1 2 Vehicles-Partes Rear-End	Property Damage Only Inattention 25- Property Damage Only Perbulknown 35- Property Damage Only Speeding 35- Mnor or Possible Injury Speeding 65- Property Damage Only Follow Too Closely 65-	4 No loe/Snow/Slust No Dry No Other	Dark-No Street Lights/Street Lights Off Not at h Daylight Not at Daylight At Drivinght Not at Daylight Not at	Intersection Oriveway/Roundabout Straight & Grade Intersection Oriveway/Roundabout Straight & Grade Intersection Oriveway/Roundabout Straight & Grade Straight	Older Road User 65+	Speeding Speeding Follow Too Closely	Daylight Dry Dark-No Street Lights/Street Lights Off Dry Daylight Ice/Snowl Daylight Dry Daylight Open	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout (Slush Not at Intersection/Driveway/Roundabout At Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Inattention No Risky Driving Behaviors Involved Speeding Speeding Follow Too Closely	Yes No Yes No No
7051 980287.83 1014506.98 2014	4 2 Vehicles-Parties Rear-End 7 2 Vehicles-Parties Sideswipe 4 2 Vehicles-Parties Rear-End 7 2 Vehicles-Parties Rear-End 7 2 Vehicles-Parties Rear-End 7 2 Vehicles-Parties Rear-End 7 2 Vehicles-Parties Rear-End	Property Damage Only Speeding Unc Property Damage Only 18 it o Yield 25-5 Property Damage Only Speeding 25-5 Property Damage Only Follow Too Closely 25-7 Property Damage Only Follow Too Closely 25-7 Property Damage Only Unc Unc	4 No Dry er 25 No Dry	Daylight At Drive Daylight At Drive	Intersection/Drieway/Roundabout Straight & Grade Intersection/Drieway/Roundabout Straight & Level Intersection/Drieway/Roundabout Straight & Level Intersection/Drieway/Roundabout Straight & Level Intersection Straight & Level Intersection Straight & Level Intersection Straight & Level	No Vuinerable Road User Involved No Vuinerable Road User Involved No Vuinerable Road User Involved	Speeding Fail b Yield Speeding Follow Too Closely None	Daylight Wet Daylight Dry Daylight Wet Daylight Dry Daylight Dry Daylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout At Driveway or Intersection At Driveway or Intersection	Speeding Fail to Yield Speeding Follow Too Closely No Risky Driving Behaviors Involved	No Yes No No
7056 980379.82 1014480.03 2019	4. 2 Whitelse Plantes Angle 4. 2 Whitelse Plantes Angle 5. 2 Whitelse Plantes 6. 3 Whitelse Plantes 6. 4 Whitelse Plantes 6. 5 Whitelse Plantes 6. 6 Whitelse Plantes 6. 7 White	Property Damage Only Apparently Asleep, Fatigued or III Und Property Damage Only Follow Top Closely Und	17 25 NO Dry 4 No Wet	Daylight At Driv Daylight At Driv Daylight Not at Dark-No Street Lights/Street Lights Off At Driv	xeway or Intersection Straight & Level xeway or Intersection Straight & Level xeway or Intersection Straight & Carde Intersection Driveway/Roundabout Straight & Level xeway or Intersection Straight at Hillcrest	No Vulnerable Road User Involved	hattention	Daylight		Institution Fail to Yield Institution Institution Apparently Asteep, Fatigued or III Follow Too Closely	No No Yes Yes No
7060 980681.9 1014393.84 201: 7061 980933.43 1014398.2 201: 7062 981148.39 1014273.99 201: 7063 981132.45 1014267.2 201:	7 2 Vehicles/Parties Rear-End 9 1 Vehicles/Party Animal 4 2 Vehicles/Parties Rear-End 7 7 1 Vehicle/Party Animal		rr 25 No Vet 4 No Dey 4 No Dry 4 No Dry 5 No Dry	Dark-Street Lights On Not at Daylight Not at Daylight Not at Dark-Street Lights On Not at	wway or Intersection Intersection Diveway/Roundabout Straight & Level Intersection/Driveway/Roundabout Straight & Grade Intersection/Driveway/Roundabout Straight & Grade Straight & Grade Straight & Grade Straight & Grade	No Vulnerable Road User Involved	Speeding None Fallow Too Closely None	Dark-Street Lights On Wet Daylight Dry Daylight Dry Dark-Street Lights On Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Institution Speeding No Risky Driving Behaviors Involved Follow Too Closely No Risky Driving Behaviors Involved	No No No No
7065 981134.11 1014334.14 2011 7066 981188.81 1014251.81 2011 7067 981200.46 1014245.76 2011 7068 981338.82 1014266.23 2011	8 1 Vehicle/Party Jonimal 8 1 Vehicle/Party Jonimal 8 1 Vehicle/Party Jonimal 7 1 Vehicle/Party Over Embankment - No Guardraii Present 7 1 Vehicle/Party Struck Object 5 1 Vehicle/Party Jonimal 9 J	Property Damage Only Other/Unknown 55-1 Property Damage Only Apparently Asleep, Fasigued or III 25-1 Unknown Other/Unknown Unit Property Damage Only Other/Unknown 25-1	4 No Dry 4 Apparently Asleep, Fatigued or III 25-34 Dry sown No Dry J Dry Dry Dry	Dark-No Street Lights/Street Lights Off Not at Dark-No Street Lights/Street Lights Off Not at Dark-No Street Lights/Street Lights Off Not at Dark-Street Lights On Not at	Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved	None Apparently Asleep or Fatigued Other Contributing Circ Not Listed None	Dawn/Dusk Dry Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off Dry Dark-Street Lights/Street Lights Off Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Apparently Asteop, Fatigued or III No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No Yes Yes No
7071 981982.9 1014052.43 201: 7072 982141.85 1014008.84 201: 7073 982461.91 1013916.87 201-	7 1 Vehicle Party Struck Object 8 1 Vehicle Party Avinnal 9 1 Vehicle Party Avinnal 9 1 Vehicle Party Avinnal 8 1 Vehicles Party Struck Object 4 2 Vehicles Party Rear-End 4 2 Vehicles Party Rear-End	Minor or Possible Injury Other/Unknown 25-	42 25 No Dry 44 No UVet	Dark-Street Lights On Not at Daylight Not at Dark-No Street Lights/Street Lights Off Not at	Intersection/DrivewayRoundabout Straight & Level Intersection/DrivewayRoundabout Straight & Level Intersection/DrivewayRoundabout Straight & Level Intersection/DrivewayRoundabout Straight & Grade	No Vulnerable Road User Involved	None None Other Contributing Circ Not Listed	Dark-No Street Lights/Street Lights Off Dry Daylight Dry Dark-Street Lights On Dry Dark-Street Lights On Dry Daylight Dry Dark-No Street Lights/Street Lights Off Wet	t Not at Intersection/Driveway/Roundabout	Apparently Asleep, Fatigued or III No Risky Driving Behaviors involved	No No Yes No
7075 982912.47 1013792.2 2016 7076 983016.43 1013761.81 2019	4 Vehicles Party Survix Object 5 Vehicles Partes Struck Object 6 Vehicles Partes Struck Object 7 Vehicles Partes	Property/Damage Only Fail to Vield 25- Property/Damage Only 0 Percification 25- Property/Damage Only 0 Percification 55- Moor or Possible Injury 1 Perceptrol (Percification) 10 Perceptrol (Perceptrol Perceptrol Percep	4 No Dry rr 25 No Wet No Wet	Daylight At Drive Daylight At Drive	Intersection/Drieway/Roundabout Straight & Level section Related but Not at Intersection Straight & Grade srection and Not Related Straight & Grade seway or Intersection Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Inattention	Daylight Dry Dark-No Street Lights/Street Lights Off Wet Daylight Dry Daylight Wet Daylight Wet Daylight Other/Link	t At Driveway or Intersection	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Inattention Speeding Follow Too Closely	Yes No No No
7080 983008.67 1013765.62 201: 7081 983016.43 1013761.81 2011	7 2 Vehicles Paries Rear-End 8 3 Vehicles Paries Rear-End 9 1 Vehicles Paries Rear-End 9 1 Vehicles Paries Sideswipe 9 3 Vehicles Paries Sideswipe	Property Damage Only Speeding 55-	4 No Dry	Daylight At Drive Dark-Street Lights On At Drive	comp or instruction Straight & Level eway or Interaction Straight & Grade eway or Interaction Straight & Grade eway or Interaction Straight & Grade straight & Grade straight & Grade straight & Grade Interaction and Noti Related Straight & Level Interaction Only wasy Roundabout Straight & Level	No Vuinerable Road User Involved No Vuinerable Road User Involved	Speeding Disregard Traffic Control Device/Sign Inattention Other Contributing Circ Not Listed	Daylight Dry Dark-Street Lights On Wet Daylight Dry	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Intersection and Not Related	Speeding No Risky Driving Behaviors Involved Inattention No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No No No Yes
7085 984327.49 1013410.14 2011 7086 984329.44 1013412.18 2011 7087 984332.49 1013414.54 2011 7088 984683.6 1013315.65 2011	8 2 Vehicles-Paries 8 2 Vehicles-Paries 8 2 Vehicles-Paries 9 2 Vehicles-Paries 9 2 Vehicles-Paries 9 9 1 Vehicles-Paries 9 9 1 Vehicles-Paries 9 1 0 Vehi	Property Damage Only Inattention Unit Property Damage Only Fail to Yield 654 Property Damage Only Fail to Yield 555	No Dry 4 No Dry 4 Under Influence of Alcohol 35-44 Wet	Daylight At Driv Daylight At Driv Dark-Street Lights On At Driv Dark-Street Lights On Not at	eway or Intersection Straight & Level seway or Intersection Straight & Level seway or Intersection Straight & Level seway or Intersection Straight & Level Intersection/Driveway/Roundabout Straight & Level Intersection/Driveway/Roundabout Straight & Level	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	hattention Fail to Yield Fail to Yield June of Alcohol/Drugs/Medication	Daylight Wet Daylight Dry Dark-Street Lights On Dry Dark-Street Lights On Wet	t At Driveway or Intersection At Driveway or Intersection	Inattention Fail to Yield Fail to Yield Fail to Yield Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved	No No No Yes
7090 985589.08 1013013.95 201- 7091 986175.83 1012912.21 201: 7092 986440.82 1012769.42 201: 7093 986500.9 1012758.17 201:	4 1 Vehicle/Party Coerturned 5 2 Vehicles/Parties Rear-End 8 1 Vehicles/Party Fire started in vehicle	Minor or Possible Injury Speeding 45-1 Minor or Possible Injury Speeding 45-5 Property Damage Only Follow Too Closely 65+	4 No loo.SnowStart 4 No loo.SnowStart 4 No Dry No Dry 4 No Dry No Dry 1 No	h Daylight Not at	Intersection/DrivewayRoundabout Straight & Grade Intersection/DrivewayRoundabout Straight & Level Intersection/DrivewayRoundabout Straight & Level Intersection/DrivewayRoundabout Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved	Speeding Speeding Follow Too Closely Operating Defective Equipment	Daylight Ice/Snow/ Daylight Dry Daylight Dry Daylight Dry	\(\subsection \) Not at Intersection/Driveway/Roundabout \(\text{Not at Intersection/Driveway/Roundabout}\) \(\text{Not at Intersection/Driveway/Roundabout}\) \(\text{Not at Intersection/Driveway/Roundabout}\)	Speeding Speeding Follow Too Closely No Risky Driving Behaviors Involved Speeding	Yes No No No No
7096 986958.19 1012631.82 201- 7097 986957.44 1012632.02 2011 7098 986960.09 1012631.3 201- 7099 986957.44 1012632.02 2011	7 2 Webrids Paries Rean End 2 Webrids Paries 4 2 Webrids Paries 4 2 Webrids Paries 4 3 Webrids Paries 5 Rean End 5	Mnor of Possible Injury Speeding 35- Frogerty Damage Only Dis regard Traffic Centrol Device/Sign 25- Moor or Possible Injury Latternion 65- Fatality Disregard Traffic Centrol Device/Sign 65- Mnor or Possible Injury Speeding 55- Frogerty Damage Only properly Tum/Merge 155-	No Dry Dry	Daylight At Driv Daylight At Driv Daylight At Driv Daylight At Driv	ection Related but Not at Intersection Straight & Level seway or Intersection Straight & Grade seway or Intersection Straight & Level seway or Intersection Straight & Level	No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+ Motorcycle and Older Road User 65+	Speeding Disregard Traffic Control Device/Sign Inattention Disregard Traffic Control Device/Sign	Daylight Dry Daylight Dry Daylight Dry	At Driveway or Intersection	Speeding No Risky Driving Behaviors Involved Inattention No Risky Driving Behaviors Involved Speeding	No No No No No
/101 986961.2/ 1012696.69 2014	4 2 Vehicles-Parties Aegle 3 Vehicles-Parties Rear-End 5 2 Vehicles-Parties Rear-End 6 2 Vehicles-Parties 6 1 Vehicles-Parties 7 Vehicles-Parties 8 Rear-End 9 Vehicles-Parties 9 Vehicles-Parties-Parties 9 Vehicles-Parties-Parties-Parties 9 Vehicles-Parties-Par	Property Damage Chiy Improper Turn/Merge 65 Property Damage Chiy Speeding 55 Property Damage Chiy Hasterdon 65* Property Damage Chiy Institution 25* Property Damage Chiy Hasterdon 25* Property Damage Chiy Mediculturion 25*	No Wet 4 No Dry	Dark-Street Lights On At Drivi Daylight At Drivi Daylight At Inter	wway or Intersection Straight & Level section and Not Related Straight & Level wway or Intersection Straight & Level	Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Inattention Other Contributing Circ Not Listed	Daylight Weets Dawn/Dusk Wet Davis-Street Lights On Wet Daylight Wet Daylight Dry Daylight Dry	t At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Intersection and Not Related	No Risky Driving Behaviors Involved Speeding Inattention Inattention No Risky Driving Behaviors Involved	No No No No Yes
7105 986958.53 1012694.41 2019 7106 986964.38 1012640.34 2019	5 3 Vehides/Paries Ran-End 6 2 Vehides/Paries Ran-End 6 2 Vehides/Paries Ran-End 6 2 Vehides/Paries 6 2 Vehides/Paries 6 2 Vehides/Paries Ran-End 6 2 Vehides/Paries Ran-End 6 2 Vehides/Paries Ran-End 6 0 Vehide	Property Damage Only Under Influence of Aicohol/Drugs/Medication 45-Monr or Plossible Injury Under Influence of Aicohol/Drugs/Medication 25-Monr or Plossible Injury Damage Only Institution 25-Monr or Plossible Injury Institution 45-Monr or Plant Only Injury Institution 45-Monr or Plant Only Injury	4 Under Influence of Alcohol 45-54 Dry 4 No Dry 4 No Wet 4 No Wet 4 No Dry 6 Dry 7 Dry 7 Dry 8 Dry 8 Dry 9 D	Dawn/Dusk At Driv Daylight At Driv Daylight At Driv Daylight At Driv Daylight At Driv	wway or Intersection Straight & Level Straight & Level Way or Intersection Straight & Level	No Vulnerable Road User Involved	Under insuence of Aconolibrings/Avedicasion Distracted Driving Inattention Inattention	Dawn/Dusk Dry Daylight Dry Daylight Wet Daylight Dry Daylight Dry Daylight Dry	At Driveway or Intersection	Under Influence of Alcohol/Drugs/Medication Distracted Driving Inattention Inattention Inattention	No No Yes No No
7110 986988.2 1012718.48 [201] 7111 986997.44 1012632.02 [201] 7112 986997.44 1012632.02 [201] 7113 986995.48 1012629.67 [201] 7114 986995.44 1012632.02 [201]	6 2 Vehicles-Parties Rear-End 7 2 Vehicles-Parties Rear-End 7 2 Vehicles-Parties Rear-End 7 3 Vehicles-Parties Rear-End 7 2 Vehicles-Parties Rear-End 7 2 Vehicles-Parties Rear-End	Property Damage Chiy Inattendon 65* Mnor or Possible Injury 25- Speeding 25- Property Damage Chiy Inattention 65* Property Damage Chiy 65* Property Damage Chiy Mproper Turn/Merge 35- 70* 70*	No Dry 4 No Dry No Wet No Dry 4 No Dry 4 No Dry	Daylight At Driv Daylight At Driv Dark-Street Lights On At Driv Dark-Street Lights On At Driv	rsection and Not Related Straight & Grade weway or Intersection Straight & Level	Clider Road User 65+	Speeding Inattention Speeding	Daylight Dry Daylight Wet Dark-Street Lights On Dry	At Intersection and Not Related At Driveway or Intersection	Inattention Speeding Inattention Speeding No Risky Driving Behaviors Involved	No No No No
7116 986960.54 1012629.11 2011 7117 986957.44 1012632.02 2011	8 2 Vehides/Paries Raar-End 8 2 Vehides/Paries Raar-End 8 2 Vehides/Paries Ragie 9 2 Vehides/Paries Angle 9 2 Vehides/Paries Aggle 1 2 Vehides/Paries Ragie 2 Vehides/Paries Ragie 1 2 Vehides/Paries 1 2 Vehides/	Monor or Possible Injury Follow Too Closely 55- Frogerty Damage Only International Control Device/Sign 25- Monor or Possible Injury University 10 September 1 Traffic Control Device/Sign 25- Monor or Possible Injury Speeding Traffic Control Device/Sign 35- Monor or Possible Injury Speeding 35- Monor or Possible Injury 1 Speeding 35- Monor or Possible Injury 1 Speeding 35-	No Dry 4 No Dry 4 No Dry No Dry	Daylight At Driv	weayor Intersection Straight & Level ewayor Intersection Straight & Level ewayor Intersection Straight & Level ewayor Intersection Straight & Level weayor Intersection Straight & Level Intersection.OriewayRoundabout Straight & Level IntersectionOriewayRoundabout Straight & Level	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	Follow Too Closely Inattention Disregard Traffic Control Device/Sign Disregard Traffic Control Device/Sign	Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry		Follow Too Closely Inattention Inattention Inattention In No Risky Driving Behaviors Involved In No Risky Driving Behaviors Involved Speeding International Information Inc.	No No No No No
7121 987215.83 1012619.73 2011 7122 987216.55 1012628.13 2011 7123 987215.44 1012627.8 2011 7124 987222.76 1012560.95 2011	4 2 Vehicles-Parties Rear-End 7 2 Vehicles-Parties Rear-End 9 2 Vehicles-Parties Sterwing 9 2 Vehicles-Parties Rear-End 9 2 Vehicles-Parties Turning	Property Damage Only Follow Too Closely Unc Property Damage Only Fail to Yield 65* Property Damage Only Inattention 65* Property Damage Only Other/Unknown 65*	4 No Dry 725 No Dry No Dry No Dry No Dry No Dry No Dry No Ory	Dawn/Dusk Not at Daylight Not at Daylight Interse Daylight At Driv	Intersection/DrivewayRoundabout Straight & Level Intersection/DrivewayRoundabout Straight & Level ection Related but Not at Intersection Straight & Level eway or Intersection Straight & Level	No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+ Older Road User 65+	hattenthon Follow Too Closely Fail to Yield Inattention Other Contributing Circ Not Listed	Dawin/Dusk Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Intersection Related but Not at Intersection At Driveway or Intersection	Inattention Follow Too Closely Fail to Yield Inattention No Risky Driving Behaviors Involved	No No Yes No No
7127 987277.72 1012610.69 2011 7128 987305.5 1012602.66 2011 7129 987369.56 1012492.86 2011	9 3 Vehicles/Parties 1 Vehicles/Parties 8 2 Vehicles/Parties 8 2 Vehicles/Parties 6 2 Vehicles/Parties 6 2 Vehicles/Parties 7 Rear-End 7 Rear-End 8 Rear-End 8 Rear-End	Property Damage Only Improper Turn/Merge Unit Property Damage Only Inattention 35- Property Damage Only Follow Too Closely 35- Property Damage Only Follow Too Closely Unit	4 No Dry 4 No Dry 725 No Dry	Dark-Street Lights On At Driv Daylight Interse Daylight Not at Daylight Not at	Intersection Driveway/Roundabout Straight & Level exercison Clineway Roundabout Straight & Level exercison Related but Not at Intersection Straight & Grade Intersection Driveway/Roundabout Straight & Level Intersection Driveway/Roundabout Straigh	No Vulnerable Road User Involved No Vulnerable Road User Involved	hattention Follow Too Closely Follow Too Closely	Daylight Dry Dark-Street Lights On Dry Davlight Dry Daylight Dry Daylight Dry Daylight Dry		Apparently Asteep, Fatigued or III No Risky Driving Behaviors Involved Inattention Follow Too Closely Follow Too Closely	Yes Yes No No No
7130 987471.01 1012492.46 2011 7131 987524.43 1012478.76 2011 7132 987851.08 1012460.48 2011 7133 987862.16 1012456.85 2011 7134 988246.63 1012348.32 2014	8 Z Venices/varies Sosswipe 7 2 Vehicles/Paries Rear-End 7 2 Vehicles/Paries Roge 5 2 Vehicles/Paries Rear-End 6 2 Vehicles/Paries Rear-End 6 2 Vehicles/Paries Rear-End	Minor or Possible Injury Follow Too Closely 45- Property Damage Only OherUnknown 25- Minor or Possible Injury OherUnknown 65- Property Damage Only Fail to Yield 55-	4 No Wet 4 No Dry No Dry	Daylight At Drive Daylight At Drive Daylight At Drive Daylight At Drive	intersection.UnivelsityFoundation Straight & Level way Related but Not at Driveway Straight & Level seway or Intersection Straight & Level seway or Intersection Straight & Grade seway or Intersection Straight & Grade seway or Intersection Straight & Grade	No vuinerable Road User Involved	Follow Too Closely Other Contributing Circ Not Listed Other Contributing Circ Not Listed Fail to Yield	Daylight Dry Daylight Wet Daylight Dry Daylight Dry Daylight Dry Daylight Dry	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	inamentori Follow Too Closely No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Fail to Yield	No No No No
7136 988278.17 1012393.76 201- 7137 988294.31 1012334.52 2011 7138 988293.05 1012336.37 2011 7139 988296.25 1012336.57 2011		Minor or Possible Injury	No Dry 4 No Dry	Daylight At Intes Daylight At Driv Daylight At Driv Daylight At Driv Daylight At Driv	wway or Intersection Straight & Grade sraction and Not Related Straight & Level straight & Level Straight & Level Straight & Grade weway or Intersection Straight & Level weway or Intersection Straight & Level weway or Intersection Straight & Level	No Vulnerable Road User Involved Pedestrian Older Road User 65+	Fail to Yield		At Intersection and Not Related At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	No Risky Driving Behaviors Involved Inattention No Risky Driving Behaviors Involved Fail to Yield Fail to Yield	No No No No
7143 988291.05 1012336.29 201	7 2 Vehrluck=Parties	Property Damage Only Fail to Yield 65* Minor or Possible Injury Fail to Yield 35* Minor or Possible Injury Distracted Driving 55*	No	Daylight At Drivi Daylight Interse Daylight At Drivi	weway or Intersection Straight & Level eway or Intersection Straight at Hillcrest eway or Intersection Straight & Level exciton Related but Not at Intersection Straight & Level exercion Formation Straight & Level eway or Intersection Straight & Level eway or Intersection Straight & Level	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Fall to Yield Distracted Driving	Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight Wet Daylight Dry	At Driveway or Intersection Intersection Related but Not at Intersection At Driveway or Intersection At Driveway or Intersection	No Risky Driving Behaviors Involved Fail to Yield Distracted Driving Fail to Yield Distracted Driving Fail to Yield No Risko Driving Rehaviors Involved	No No No No
7147 988378.64 1012312.34 2011 7148 988605.28 1012251.32 2014	9 2 Webinder Parties Stanford 7 Webinder Parties Rean Find 4 2 Webinde Parties 5 Rean Find 4 2 Webinde Parties 5 Webinde Parties 6 1 Webinde Partie 6 1 Webinde Partie 6 1 Webinde Partie 7 Webinde Partie 6 1 Webinde Partie 7 Webinde Partie 8 Side swelpe 6 Webinder 6	Minor or Possible Injury Inattention 65+ Minor or Possible Injury Other/Unknown 25-	No Wet 4 No Dry 4 No Dry	Daylight	every or interaction Interaction Driveray Roundabout Interaction Driverage Interac	No Vulnerable Road User Involved	Inattention Other Contributing Circ Not Listed None	Dark-Street Lights On Dry Dark-No Street Lights/Street Lights Off Dry	At Driveway or Intersection At Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Fail to Yield No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication Isationsion No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication	No No Yes No Yes
7152 990477.31 1011671.71 2011 7153 990489.7 1011739.73 2011 7154 990797.85 1011597.2 2011 7155 990907.76 1011567.22 2011	5 2 Vehicles/Parties Sideswipe 8 1 Vehicle/Party Animal 9 1 Vehicle/Party Animal 4 2 Vehicles/Parties Turning 4 2 Vehicles/Parties Turning	Property Damage Only Other/Unknown 35- Property Damage Only Fail to Yield 45- Property Damage Only Other/Unknown 65+ Property Damage Only Other/Unknown Unit	4 No Dry 4 No Dry	Dark-Street Lights On Not at Daylight Not at Dark-No Street Lights/Street Lights Off Not at	Intersection/Driveway/Roundabout Straight & Grade Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved	None Fail to Yield None None	Dark-No Street Lights On Dry Daylight Dry Dark-No Street Lights/Street Lights Off Dry Dawn/Dusk Dry	Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Fail to Yield No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Fail to Yield	No Yes No No No
7156 99090.76 1011567.21.2011 7157 99092.28 1011568.79.2011 7158 990914.07 1011564.16.2011 7159 990916.44 1011628.06.2011	2 Vehicles/Parties Sideswipe	Property Damage Only	No Dry 4 No Dry 1725 No Dry Dry 1726 No Dry 1726 No Dry 1727 No Dry 1727 No Dry 1728 No Dr	Daylight At Driv Daylight At Driv Dawn/Dusk At Driv Daylight At Driv	seway or Intersection Straight & Level everage or Intersection Curve & Grade everage or Intersection Straight & Level	Older Road User 65+ No Vulnerable Road User Involved	Fail to Yield Improper Backing/Passing/Turning/Parking/Signaling Fail to Yield Under Influence of Alcohol/Drugs/Medication Inattention	Daylight Dry Daylight Dry Dawn/Dusk Dry Daylight Dry Daylight Dry Daylight West	At Driveway or Intersection At Driveway or Intersection	Fail to Yield No Risky Driving Behaviors Involved Fail to Yield Under Influence of Alcohol/Drugs/Medication Inattention	Yes No No No No
7164 990914.89 1011562.43 2016	6 2 Webstes Frantes Turning 6 2 Webstes Frantes 6 2 Webstes Frantes 6 2 Webstes Frantes 6 2 Webstes Frantes 7 Webstes 7	Procerty/Damage Chiy Inattention 65 ft Property/Damage Chiy Fail to Vield 65 ft Moror of Possible Injury Fail to Vield 55 ft Moror of Possible Injury Fail to Vield 65 ft Property/Damage Chiy Fail to Vield Unit Property/Damage Chiy Finger Turn/Merge 65 ft	4 No Wet No Wet No Dry	Daylight At Driv	eway or Intersection Straight & Level eway or Intersection Curve & Level eway or Intersection Straight & Level eway or Intersection Straight & Level eway or Intersection Straight & Level	Older Road User 65+	Fail to Yield	Daylight Dry Daylight Wet Daylight Wet Daylight Dry	At Driveway or Intersection t At Driveway or Intersection t At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Fail to Yield Fail to Yield Fail to Yield Fail to Yield No Risky Driving Behaviors Involved	No No No No No
7168 990966.63 1011552.48 2019	7 2 Weblieds Parlets Turning 2 Weblieds Parlets Rame End 2 Weblieds Parlets 4 Weblieds Parlets 5 See supple 2 Weblieds Parlets 5 See supple 5 Weblieds Parlets 5 Weblieds 5	Poperty Damage Only Property Damage Only Suspended Driving Suspended Serious Injury Suspended Serious Injury ObertUnknown University Property Damage Only Property Damage Only Property Damage Only Suspended Serious	rr 25 No Other 4 No Dry er 25 No Dry	Daylight Not at	wawy or Intersection Staright & Level reaction and Kind Related Straight & Level wawy or Intersection Straight & Level Intersection Divewsy/Roundabout Intersection Divewsy/Roundabout Straight & Level Intersection Divewsy/Roundabout Straight & Level Intersection Divewsy/Roundabout Straight & Level	No Vuinerable Road User Involved No Vuinerable Road User Involved No Vuinerable Road User Involved	hattention Fail to Yield	Daylight Dry Dark-No Street Lights/Street Lights Off Dry Daylight Other/Unit Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry	At Driveway or Intersection At Intersection and Not Related At Intersection and Not Related At Oneway or Intersection Not at Intersection Driveway/Roundabout Not at Intersection Driveway/Roundabout Not at	Distracted Driving No Risky Driving Behaviors Involved Inattention Fail to Yield No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No No Yes Yes
7171 991395.54 1011513.89 2011 7172 991395.54 1011513.89 2011 7173 991844.38 1011368.23 2011 7174 991923.2 1011348.92 2011 7175 991990.92 1011373.89 2019	5 1 Vehicle/Party / Animal 7 1 Vehicle/Party / Animal 9 1 Vehicle/Party / Animal	Property Damage Only	4 No Dry 4 No Dry	Dark-Street Lights On Not at	Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved	None None None	Darkin/Dusk Dry Darkin/Dusk Dry Daylight Dry Dark-No Street Lights/Street Lights Off Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved	No No No No No
7177 992537.59 1011200.88 201. 7178 992539.93 1011200.89 2019 7179 992591.13 1011226.96 2019 7180 992591.34 1011229.03 2019	5 2 Vehicles/Parties Pedestrian 1 Vehicle/Party Struck/Opject 9 1 Vehicle/Party / Acimal 5 1 Vehicle/Party Acimal 2 Vehicles/Parties Rean-End	Property Damage Only Speeding 65* Property Damage Only Other/Unknown Unc Mnor or Possible Injury Other/Unknown 65* Property Damage Only Speeding 65*	87 ZB NO DITY NO Dry NO Dry NO Dry	Dark-No Street Lights/Street Lights Off Not at Daylight Not at	Intersection/University Noundabout Straight & Level Intersection/Driveway/Roundabout Straight & Level Intersection/Driveway/Roundabout Straight & Level	Pedestrian Older Road User 65+ No Winerable Road User Involved Older Road User 65+ Older Road User 65+	speeding None None Speeding	Daylight lce/Snow/ Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off Dry Daylight Dry	Slush Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Speeding No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Speeding	No Yes No No
7182 992656.83 1011171.91 201. 7183 992639.19 1011174.45 201. 7184 992618.31 1011223 2011 7185 993645.61 1010988.26 2011	8 1 Weinide/Party	Property Damage Only OtherUnknown 65+ Property Damage Only Speeding 25- Property Damage Only OtherUnknown 55- Property Damage Only OtherUnknown 35- Minor or Possible Injury Non Motorist on Wrong Side of Road 65-	No Dry 4 No los/snow/Slus† 4 No Dry 4 No Dry No Ory	Daylight Not at h Dark-No Street Lights/Street Lights Off Not at Daylight Not at Daylight Not at Dark-No Street Lights/Street Lights Off Not at Dark-Street Lights On Not at	Intersection/Drieway/Roundabout Straight & Level	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User S5+ Older Road User 55+	None Speeding None None None None	Daylight Dry Dark-No Street Lights/Street Lights Off Lee/Snowl Daylight Dry Dark-No Street Lights/Street Lights Off Dry Dark-Street Lights On Wet	Not at Intersection/Driveway/Roundabout (Slush Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout t Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Speeding No Risky Driving Behaviors Involved	No Yes No No Yes
7187 994052.18 1010824.71 2019 7188 994151.7 1010869.75 2019 7189 994374.93 1010747.44 2019	9 2 Vehicles/Parties Rear-End 9 2 Vehicles/Parties Sideswipe 5 2 Vehicles/Parties Sideswipe 6 1 Vehicle/Partiv Struck Object	Property Damage Only Speeding Unit Property Damage Only Instension 35- Property Damage Only Oftee/Unknown Unit	rr 25 No Bry 4 No Bry rr 25 No Dry rr 25 No Dry	Daylight Not at	Intersection/DrivewayRoundabout Straight & Level Intersection/DrivewayRoundabout Straight & Level Intersection/DrivewayRoundabout Straight & Level Intersection/DrivewayRoundabout Straight & Level	No Vulnerable Road User Involved Older Road User Involved Older Road User Involved	Speeding Inattention None Unknown Distraction	Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry	Not at Intersection/Driveway/Roundabout	Speeding Inattention No Risky Driving Behaviors Involved Distracted Driving Fall to Yield	No Yes Yes Yes Yes
7192 1017502.79 1002339.59 2019 7193 1017715.91 1002187.65 2019	8 Valmicker Paries Solesswipe	Unknown Other/Unknown University Damage Only Under Influence of Ncohol/Drugs/Medication 25- Minor or Possible Injury Speeding 45- Property Damage Only Speeding 25-	lown No Unknown 4 Under Influence of Acohol 25-34 Dry 4 No Dry Ice/Snow/Silus*	Dark-No Street Lights/Street Lights Off Not at Daylight Not at Daylight Not at Daylight Not at	Intersection/Orieway/Roundabout Straight & Level Intersection/Orieway/Roundabout Orieway/Roundabout Or	No Vulnerable Road User Involved	Under Influence of Alcohol/Drugs/Medication	Daylight Dry Daylight Dry Daylight Lee/Snow	Not at Intersection/Driveway/Roundabout known Not at Intersection/Driveway/Roundabout known Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Stush Not at Intersection/Driveway/Roundabout	Distracted Driving No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication Speeding Speeding	Yes Yes No No Yes
7196 1017970.79 1002004.46 2011	5 2 Vehicles/Parties Struck Object or Debris On Road	Property Damage Unity Speeding 25-	4 No Dry	Daylight Not at	Intersection/DrivewayRoundabout Straight & Level Intersection/DrivewayRoundabout Straight & Level Intersection/DrivewayRoundabout Straight & Grade	No Vulnerable Road User Involved	Speeding None Speeding	Daylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Speeding No Risky Driving Behaviors Involved Speeding	Yes No No

OBJECTID JURISDICTION	COUNTY CITY	PRIMARY BLOCK NUMBER	INTERSECTING DIST MIor FROM FT FROM FT REF.POINT FT REF.POINT FT REF.POINT POINT.NAME	MILEPOST A SR_ONLY HISTORY NUMBER SUSPENSE_IND	DATE TIME INJURY_TYPE	NUM NUM NUM NUM NUM NUM PEDS BIKI	S VEH_1_TYPE	VEH_2_TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD_SURFACE ROADWAY CHARACTERISTIC	LIGHTING CONDITION	FIRST_COLLISION TYPE_OBJECT_STRUCK	VEH_1 ACTION	VEH. 2 ACTION	VEH.1.COMPASS DIRECTION.TO DIRE
7199 State Route 7200 Salar Route 7201 Salar Route 7201 Salar Route 7202 Salar Route 7202 Salar Route 7203 Salar Route 7204 Salar Route 7204 Salar Route 7205 Salar Route 7205 Salar Route 7205 Salar Route 7207 Salar Route	Cialiam 1	101		268.00 No E941761 268.02 No E441290	2019-07-19 09:30 No Apparent Injury 2015-07-07 21:14 No Apparent Injury 2018-07-11 08:49 No Apparent Injury	0 0 1 0	0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Level	Dusk	Vehicle Strikes Deer Roadway Ditch	Going Straight Ahead Going Straight Ahead		West East East West
7201 State Route 7202 State Route	Cialiam 1 Cialiam 1	101		268.05 No E823597 268.10 No E651600	2017-03-11 07:57 No Apparent Injury	0 0 2 0	0 Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast	Dry Straight & Grade Dry Straight & Grade	Daylight	All other non-collision From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead	Other*	East West East West
7203 State Route 7204 State Route 7205 State Route	Clallam 1	101		268.10 No E478416 268.21 No E482473	2017-11-19 19:55 Suspected Serious Injury 2015-11-01 20:30 Possible Injury 2015-11-16 19:00 No Apparent Injury	1 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Truck (Flatbad,Van,etc)		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Paining	Wet Straight & Level Wet Straight & Grade Straight & Grade	Dark-No Street Lights Dark-No Street Lights	From opposite direction - both going straight - sideswipe Roadway Ditch Walking Talker Dear	Going Straight Ahead Going Straight Ahead	Going Straight Ahead Going Straight Ahead	West East West East West East West East
7206 State Route 7207 State Route	Cialiam 1	101		268.43 No E530001 268.44 No E897844	2016-03-28 10:39 No Apparent Injury 2019-02-08 12:07 No Apparent Injury	1 0 1 0 0 0 2 0 0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Snowing	Dry Straight & Level Snow/Slush Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead	Going Straight Ahead Going Straight Ahead	East West East West East West East
7207 State Route 7208 State Route 7209 State Route 7210 State Route	Cialiam 1 Cialiam 1	101		268.52 No E548331 268.58 No E311295	2016-05-25 15:53 No Apparent Injury 2014-02-13 13:05 No Apparent Injury 2016-10-26 11:40 No Apparent Injury	0 0 2 0	0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Dry Straight & Grade Other Straight & Level	Daylight Daylight	Entering at angle Guardrail - Leading End Vehicle Strikes Deer	Making Right Turn Other*	Going Straight Ahead	East Northwest West East East West
7210 State Route 7211 State Route 7212 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101					Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb)	Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Grade	Daylight Daylight	Vehicle Strikes Deer Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead		West East East West
7212 State Route 7213 State Route	Clallam 1 Clallam 1	101		268.70 No E496821 268.71 No E669948	2017-05-09 12:12 No Apparent Injury	0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	>	Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy	Wet Curve & Level Dry Dry Curve & Level	Daylight Dark-No Street Lights Daylight	Metal Sign Post Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	East West North South
7215 State Route 7216 State Route	Cialiam 1	101					Passenger Car Passenger Car Passenger Car Pickup Panel Truck or Vanette under 10 000 lit	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Curve & Grade	Daylight Daylight	From opposite direction - both going straight - sideswipe Vehicle Strikes Deer From opposite direction - both going straight - sideswipe	Going Straight Ahead	Going Straight Ahead	West East Wart East
7215 State Route 7216 State Route 7217 State Route 7218 State Route 7218 State Route	Cialiam 1	101		268.94 No E618499 268.95 No E941771	2016-12-08 17:15 No Apparent Injury 2019-06-21 10:02 No Apparent Injury	0 0 1 0	Passenger Car Pickup Panel Truck or Vanette under 10 000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade Dry Straight & Level Dry Curve & Level	Dark-No Street Lights Davlight	Domestic and interference of the control of the con	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Comg Charges Person	West East West East
7219 State Route 7220 State Route 7221 State Route 7222 State Route	Clallam 1	101 101		269.00 No E445079 269.20 No E781615	2015-07-17 16:24 No Apparent Injury 2018-03-21 21:53 No Apparent Injury	0 0 2 0	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Truck (Flatbad,Van,etc)	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end From opposite direction - both going straight - both moving - rear-end From opposite direction - both going straight - sideswipe		Slowing Slowing	East West East West West East West East
7221 State Route 7222 State Route	Cialiam 1	101 101		269.20 No E868366 269.21 No E344683	2018-11-28 08:48 No Apparent Injury 2014-07-23 13:17 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Truck (Flatbad, Van, etc)	Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Raining	Wet Curve & Grade	Dark-No Street Lights Daylight Daylight	Guardrail - Face	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing Going Straight Ahead	East West West East East West
7223 State Route 7224 State Route 7225 State Route 7226 State Route	Cialiam 1	101		269.21 No E348238 269.21 No E447196	2014-08-06 14:08 Suspected Minor Injury 2015-07-26 07:10 No Apparent Injury	0 0 2 0	Motorcycle Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car.	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade Dry Straight & Level	Daylight Daylight	From same direction - one left turn - one straight From same direction - one left turn - one straight	Going Straight Ahead Going Straight Ahead	Stopped in Roadway Stopped for Traffic	East West Vehicle Stopped Vehicle Stopped East West Vehicle Stopped Vehicle Stopped
7225 State Route 7226 State Route 7227 State Route	Cialiam 1 Cialiam 1	101		269.21 No E535381 269.21 No E535381	2016-04-15 15:20 Suspected Serious Injury 2016-04-15 15:20 Suspected Serious Injury 2016-07-26 09-21 No Apparent Injury	3 0 2 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Overcast Clear or Partly Cloudy	Dry Straight & Level Dry Curve & Grade Dry Curve & Level Dry Curve & Grade Curve & Grade	Daylight Daylight	From same direction - both going straight - one stopped - rear-end Entering at angle Entering at angle	Going Straight Ahead Going Straight Ahead Making Left Turn Making Right Turn	Stopped for Traffic Stopped for Traffic Going Straight Ahead Stopped at Signal or Stop Sign	East West Vehicle Stopped Wehicle Stopped South West West East West South Vehicle Stopped Wehicle Stopped
7227 State Route 7228 State Route 7229 State Route	Cialiam 1	101		269.21 No E651949 269.21 No E737108	2017-03-09 11:00 Suspected Minor Injury 2017-11-13 18:19 Possible Injury	2 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Plickup Panel Truck or Vanette under 10,000 lb Passenger Car Plickup Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	At Intersection and Not Related At Intersection and Related	Overcast Raining	Wet Curve & Level	Daylight	From a sumper straing of the strain of the s	Going Straight Ahead	Going Straight Ahead	East West East South
7228 State Route 7229 State Route 7230 State Route 7231 State Route	Cialiam 1 Cialiam 1	101		269.21 No E754788 269.21 No E796945	2017-12-26 16:27 Possible Injury 2018-05-03 17:16 No Apparent Injury	2 0 2 0 0 0 1 0	0 Passenger Car 0 Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	Intersection Related but Not at Intersection At Intersection and Related	Overcast Clear or Partly Cloudy	Wet Curve & Level Wet Curve & Grade Dry Curve & Level	Dusk Daylight	From same direction - both going straight - one stopped - rear-end Guardrail - Face	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Making Left Turn Stopped for Traffic	East West Vehicle Stopped Vehicle Stopped East West
7232 State Route 7233 State Route	Cialiam 1	101 101		269.21 No E846684 269.22 No E866661	2018-10-06 15:55 No Apparent Injury 2018-11-16 08:09 No Apparent Injury	0 0 2 0	0 Passenger Car 0 Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Passenger Car Passenger Car Passenger Car Pickup-Panel Truck or Vanette under 10,000 b	At Intersection and Related Intersection Related but Not at Intersection	Clear or Partly Cloudy Raining		Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead	Stopped for Traffic Stopped for Traffic Stopped for Traffic	East West East West East West Vehicle Stopped Vehicle Stopped East West Vehicle Stopped Vehicle Stopped
7234 State Route 7235 State Route	Cialiam 1	101		269.22 No E926865 269.23 No E302922	2019-05-31 12:57 Possible Injury 2014-01-19 17:38 Possible Injury	1 0 3 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Level	Dark-No Street Lights	From same direction - both going straight - one stopped - rear-end Roadway Ditch	Going Straight Ahead Going Straight Ahead		East West Vehicle Stopped Vehicle Stopped West East
7236 State Route 7237 State Route 7238 State Route	Cialiam 1	101		269.24 No E692845 269.25 No E333411	2017-07-13 11:03 Possible Injury 2014-06-07 12:51 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Wet Curve & Level Dry Straight & Level Dry Curve & Level	Daylight Daylight	From same direction - both going straight - both moving - rear-end Vehicle Strikes Deer Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead	Slowing	East West East West East West
7239 State Route	Clallam 1	101		269.26 No E364883	2014-11-16 16:45 Suspected Minor Injury 2014-10-11 13:20 Suspected Serious Injury	5 0 2 0	Pickup, Panel Truck or Vanette under 10,000 lb District Panel Truck or Vanette under 10,000 lb	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Wet Curve & Level	Daylight	venicie strikes ueer From oppositie direction - both moving - head-on From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	East West West East
7240 State Route 7241 State Route 7242 State Route 7243 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101		269.40 No E722346 269.61 No E830212 269.62 No E806610	2017-10-04 16:02 No Apparent Injury 2018-08-13 13:43 No Apparent Injury 2018-06-08 14:40 No Apparent Injury 2019-09-03 02:05 No Apparent Injury 2019-10-02 22:07 No Apparent Injury	0 0 3 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	Intersection Related but Not at Intersection Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Wet Straight & Level	Daylight Daylight	From same direction - both going stealpht - both mounty - restricted From same direction - both going stealpht - both mounty - reast-end From opposite direction - both going stealpht - both mounty - reast-end From same direction - both going stealpht - both mounty - sideswipe From same direction - both going stealpht - both moving - sideswipe	Going Straight Ahead Going Straight Ahead	Slowing Slowing Going Straight Ahead	West East West East East West East
7243 State Route 7244 State Route	Cialiam 1 Cialiam 1	101		269.62 No E957769 269.62 No E969446	2019-09-03 02:05 No Apparent Injury 2019-10-02 22:07 No Apparent Injury					Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Dark-No Street Lights	Strikes or Was Struck by Object from the Load of Another Vehicle	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead Going Straight Ahead	West East East West West East East West
7244 State Route 7245 State Route 7246 State Route 7247 State Route 7249 State Route	Cialiam 1	101		269.64 No E877690 269.65 No 2766958	2018-12-24 17:25 No Apparent Injury 2014-04-24 09:28 Possible Injury	0 0 2 0 2 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection At Intersection and Related	Overcast	Wet Straight & Level Wet Straight & Level	Dusk Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Slowing Stopped for Traffic	West East West East West East Vehicle Backing Vehicle Stopped
	Clallam 1	101		269.65 No E448151 269.66 No E320815	2015-07-31 16:00 Suspected Minor Injury 2014-03-17 13:14 No Apparent Injury	1 0 2 0 0 0 1 0	0 Other 0 Truck (Flatbad,Van,etc)	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end Roadway Ditch	Going Straight Ahead	Stopped in Roadway	West East Vehicle Stopped Vehicle Stopped East West Vehicle Stopped Vehicle Stopped
7249 State Route 7250 State Route 7251 State Route	Cialiam 1	101		269.80 No E368803	2018-09-06 09:28 Suspected Minor Injury 2014-10-25 08:50 No Apparent Injury 2015-11-28 02:10 Suspected Minor Injury	2 0 3 0	0 Pickup, Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related	Clear or Partly Cloudy Raining	Ury Straight & Level Wet Straight & Level	Daylight	Kubaruary untuk From same direction - both going straight - one stopped - rear-end Vehicle Strikes Deer Oner Embaniment - No Guardrail Present	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic	West East Vehicle Stopped Vehicle Stopped East West Vehicle Stopped
7251 State Route 7252 State Route 7253 State Route	Cialiam 1 Cialiam 1	101		270.00 No E308814	2015-11-28 02:10 Suspected Minor Injury 2014-02-08 22:15 No Apparent Injury 2017-06-12 11:16 No Apparent Injury 2019-08-12 10-17 Done libin Injury	0 0 2 0	0 Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy Snowing	Snow/Slush Straight & Level	Dark-No Street Lights	From opposite direction - both going straight - sideswipe	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	East West West East
7252 State Route 7253 State Route 7254 State Route 7255 State Route 7255 State Route	Clallam 1	101		270.15 No E932263 270.20 No E820464			Passenger Car Pickup Panel Truck or Vanette under 10 000 iii	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Curve & Grade Dry Straight & Grade	Daylight	From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing Slowing Stopped for Traffic	East West East West West East While Stronger
	Cialiam 1 Cialiam 1	101		270.23 No E680593 270.30 No E345254	2017-06-12 11:04 No Apparent Injury 2014-07-26 17:45 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Grade	Daylight Daylight	Vehicle Strikes Deer Roadway Ditch	Going Straight Ahead Going Straight Ahead	Ample and a second	West East West East
7257 State Route 7258 State Route 7259 State Route 7260 State Route	Cialiam 1	101		270.30 No E380573 270.38 No E427530	2014-12-02 18:45 No Apparent Injury 2015-05-24 13:34 No Apparent Injury	0 0 1 0	0 Passenger Car 0 Passenger Car 0 Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Curve & Grade	Dark-No Street Lights Daylight	Voluicle Strikes Deer Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead		East West East West
7260 State Route 7261 State Route	Cialiam 1 Cialiam 1	101		270.44 No E364882 270.48 No E504039	2014-10-11 05:57 No Apparent Injury 2016-01-09 16:15 Suspected Minor Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Overcast	Dry Curve & Grade	Daylight	From opposite direction - all others	Going Straight Ahead	Going Straight Ahead	East West East East West West East
7262 State Route 7263 State Route	Cialiam 1	101		270.56 No E775976 270.58 No E945783	2018-03-04 12:56 Possible Injury 2019-07-17 17:30 Possible Injury	3 0 2 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection Not at Intersection and Not Related	Overcast Overcast	Dry Straight & Grade	Daylight Daylight Dark-Street Lights On	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic Stopped for Traffic	West East Vehicle Stopped Vehicle Stopped West East Vehicle Stopped Vehicle Stopped
7264 State Route 7265 State Route 7266 State Route	Cialiam 1	101		270.76 No E333024	2014-05-25 17:29 Possible Injury 2014-05-25 17:29 Possible Injury	4 0 2 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Driveway	Raining	Wet Curve & Grade	Daylight	Roadway Ditch From opposite direction - one left turn - one straight Entering at angle	Making Left Turn	Going Straight Ahead Going Straight Ahead	East South West East Courth West Fact
7266 State Route 7267 State Route 7268 State Route	Cialiam 1 Cialiam 1	101		270.76 No E513547 270.76 No E972521	2014-05-25 17:29 Possible Injury 2014-07-01 18:26 Suspected Serious Injury 2016-02-06 14:59 Possible Injury 2019-10-19 11:30 Possible Injury	1 0 2 0	Passenger Car Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway within Major Intersection At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Raining	Dry Straight & Grade Wet Curve & Level Wet Curve & Grade	Daylight Daylight	Entering at angle Entering at angle From same direction - both going straight - one stopped - rear-end	Making Left Turn Making Left Turn Slowing	Going Straight Ahead Stopped for Traffic	South West West East West West West Vehicle Stopped
7269 State Route 7270 State Route	Clallam 1	101								Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Dark-No Street Lights	Vehicle Strikes Deer	Slowing Going Straight Ahead Going Straight Ahead		East West West East
7269 State Route 7270 State Route 7271 State Route 7272 State Route	Cialiam 1 Cialiam 1	101 101		270.97 No 2766967 270.98 No E467207	2016-01-26 15:46 Possible Injury 2014-06-13 14:20 Possible Injury 2015-09-23 16:00 No Apparent Injury	1 0 2 0 0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related	Overcast Clear or Partly Cloudy	Dry Curve & Grade Dry Curve & Level Dry Curve & Level	Daylight Daylight	Contenting a transple From same direction - both going straight - both moving - sideswipe	Going Straight Ahead Making Left Turn Going Straight Ahead	Making Left Turn Overtaking and Passing	South West East South West East West East
7273 State Route 7274 State Route 7275 State Route 7276 State Route 7277 State Route	Cialiam 1	101 101		270.98 No E542313 270.98 No E634308	2016-05-09 13:02 No Apparent Injury 2017-01-11 17:30 No Apparent Injury	0 0 2 0	0 Passenger Car 0 Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	At Intersection and Related At Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Curve & Level	Daylight Dark-Street Lights On	Entering at angle From same direction - both going straight - both moving - sideswipe	Making Left Turn	Stopped at Signal or Stop Sign Slowing	East South Vehicle Stopped Vehicle Stopped East West East West
7275 State Route 7276 State Route	Clallam 1 Clallam 1	101		270.98 No E634308 270.98 No E687840 270.98 No E932601	2017-06-26 13:24 Possible Injury 2019-06-12 15:15 No Apparent Injury	3 0 2 0 0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Level Dry Curve & Level	Daylight Daylight	Entering at angle Entering at angle	Going Straight Ahead Making Left Turn Making Left Turn	Going Straight Ahead Making Left Turn	South West West East South West East South
7277 State Route 7278 State Route	Cialiam 1 Cialiam 1	101		270.99 No E469101 270.99 No E594261	2015-10-02 16:05 No Apparent Injury 2016-10-08 11:14 No Apparent Injury	0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car		Raining Clear or Partly Cloudy	Dry Curve & Level Wet Curve & Level Dry Curve & Level	Daylight Daylight	Entering at angle Metal Sign Post	Making Left Turn Going Straight Ahead	Making Left Turn	South West East South West East South
7279 State Route 7280 State Route 7284 State Route	Cialiam 1	101		271.06 No E766306	2018-02-05 04:29 No Apparent Injury 2018-02-05 04:29 No Apparent Injury 2019-10-13 15:14 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb	Truck & Trailer	At Intersection and Related At Driveway	Overcast Clear or Partly Cloudy	Wet Curve & Level Dry Curve & Grade	Daylight Dark-Street Lights On Daylight	From opposite direction - one left turn - one straight Entering at rangle From same direction - both going straight - one stopped - rear-end Weblief nevertured	Making Left Turn Making Right Turn Starting in Traffic Lane	Going Straight Ahead Stopped at Signal or Stop Sign	North East East South South East West East Most Labeled Stopped
7282 State Route 7283 State Route	Cialiam 1	101		271.16 No 2766969 271.21 No E571100	2014-07-21 16:05 Suspected Serious Injury 2016-08-04 11:40 No Apparent Injury	1 0 1 0	O Passenger Car O Flokup Panel Truck or Vanette under 10,000 it O Flokup Panel Truck or Vanette under 10,000 it O Flokup Panel Truck or Vanette under 10,000 it O Flokup Panel Truck or Vanette under 10,000 it O Flokup Panel Truck or Vanette under 10,000 it O Flokup Panel Truck or Vanette under 10,000 it O Flokup Panel Truck or Vanette under 10,000 it O Passenger Car O Passenger Car O Flokup Panel Truck or Vanette under 10,000 it O Flokup Panel Truck or Vanette under 10,000 it	Pickup Panel Truck or Vanette under 10.000 lb	Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy	Dry Straight & Level		Vehicle overturned Same direction both turning right one stopped sideswipe		Stopped at Signal or Stop Sign	West East Vehicle Stopped Vehicle Stopped
7284 State Route 7285 State Route	Cialiam 1 Cialiam 1	101 101		271.21 No E932599 271.21 No E943166	2019-06-05 11:20 Possible Injury 2019-07-23 08:39 Possible Injury	1 0 2 0	0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Curve & Level Dry Straight & Level	Daylight	Entering at angle Entering at angle	Making Right Turn Making Right Turn Making Left Turn	Going Straight Ahead Going Straight Ahead	North West East West North East East West
7286 State Route 7287 State Route 7288 State Route 7289 State Route	Cialiam 1 Cialiam 1	101		271.40 No E836719	2018-09-06 10:29 Possible Injury	1 0 2 0	Pickup, Panel Truck or Vanette under 10,000 lt Pickup, Panel Truck or Vanette under 10,000 lt Passenger Car Pickup, Panel Truck or Vanette under 10,000 lt	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Dry Straight & Level Dry Curve & Level	Daylight	Vehicle overturned From same direction - both going straight - one stopped - rear-end Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead	Stopped for Traffic	West East East Vehicle Stopped
7288 State Route 7289 State Route	Cialiam 1	101		271.41 No E972523 271.49 No E690592	2019-10-18 20:18 No Apparent Injury 2017-07-11 15:11 No Apparent Injury 2017-05-27 20:48 No Apparent Injury	0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy	Dry Straight & Level	Dayright	From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead	Slowing	West East West East
7290 State Route 7291 State Route 7292 State Route	Clallam 1	101		271.50 No E676584 271.50 No E690591	2017-05-27 20:48 No Apparent Injury 2017-07-04 09:25 Suspected Minor Injury 2014-05-29 19-15 No Apparent Injury	1 0 1 0	Motorcycle Passennes Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Driveway within Major Intersection At Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy			From same direction - one right turn - one straight Vehicle overturned Wood Size Deet	Overtaking and Passing Making Right Turn Overtaking and Passing	Making Right Turn	West East West South East North Sant West
7293 State Route 7294 State Route	Cialiam 1	101			2017-07-04 09:25 Suspected Minor Injury 2014-05-29 19:15 No Apparent Injury 2015-12-25 23:28 No Apparent Injury 2015-12-25 23:08 No Apparent Injury		Passenger Car Pickup Panel Truck or Vanette under 10 000 lb	Passenger Car	At Intersection and Related At Intersection and Not Related	Overcast Overcast	loe Curve & Grade	Dark-No Street Lights Dark-No Street Lights	Wood Sign Post From same direction - both going straight - both moving - rear-end Guardrail - Face	Making Right Turn Overtaking and Passing Going Straight Ahead Going Straight Ahead	Slowing	West East West East West Fast
7294 State Route 7295 State Route 7296 State Route 7297 State Route 7297 State Route 7297 Date Route	Cialiam 1 Cialiam 1	101 101		271.70 No E554977 271.75 No E717619	2016-06-10 14:42 Suspected Serious Injury 2017-09-11 21:35 Possible Injury	2 0 4 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy	Wet Curve & Level Dry Straight at Hillor	Daylight Other	Guardraf - Face - Spring shallon - South moving - real-end - Guardraf - Face - Pom opposite direction - both moving - head-on - Form same direction - both pring shallon - both moving - real-end - Form same direction - both pring shallon - Both - South -	Going Straight Ahead Going Straight Ahead Making U-Turn	Going Straight Ahead Slowing	Southwest Northeast Northeast Southwest East West East West
7297 State Route 7298 State Route	Cialiam 1	101		271.84 No E519405 271.90 No E835200	2016-06-10 14:42 Suspected Serious Injury 2017-09-11 21:35 Possible Injury 2016-02-19 14:24 Possible Injury 2018-09-02 20:45 No Apparent Injury 2019-05-27 12:15 No Apparent Injury	0 0 1 0	0 Passenger Car 0 Passenger Car	Passenger Car	Not at Intersection and Not Related	Raining Clear or Partly Cloudy				Making U-Turn Going Straight Ahead	Going Straight Ahead	West West East East West
7299 State Route 7300 State Route	Cialiam 1 Cialiam 1	101 101		271.90 No E925432 271.93 No E885306	2019-05-27 12:15 No Apparent Injury 2019-01-16 16:41 Suspected Minor Injury 2019-06-23 07:22 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Dark-No Street Lights	Vehicle Strikes Deer From same direction - all others Vehicle Strikes Deer	Going Straight Ahead Making U-Tum	Going Straight Ahead	West East East West East
7301 State Route 7302 State Route	Cialiam 1 Cialiam 1	101					Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Overcast	Wet Straight & Grade Vet Straight & Grade	Daylight Dark-No Street Lights	venice Strikes Deer Guardrail - Face Mahide Strikes Deer	Going Straight Ahead Overtaking and Passing		West East
7302 State Route 7303 State Route 7304 State Route 7305 State Route 7306 State Route	Cialiam 1	101		272.19 No E850610 272.33 No E850610	2017-12-05 17:06 No Apparent Injury 2018-10-13 19:10 No Apparent Injury 2018-08-12 12:35 No Apparent Injury 2018-03-23 11:39 No Apparent Injury 2016-03-23 11:39 No Apparent Injury 2016-03-24 10:30 Possible Injury	0 0 1 0	0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Dark-No Street Lights Dark-No Street Lights Dark-No Street Lights	venicie strakes Deer Vehicle Strikes Deer From same direction - both going straight - both moving - sideswipe	Going Straight Ahead Going Straight Ahead Changing Lanes	Going Straight Ahead	West East West East West Wart
7306 State Route 7307 State Route	Cialiam 1	101		272.47 No E412610 272.50 No E347443	2015-03-23 11:39 No Apparent Injury 2014-08-04 09:30 Possible Injury	0 0 2 0	0 Not Stated	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level		From same direction - both going straight - both moving - sideswipe From same direction - both going straight - both moving - sideswipe	Changing Lanes Changing Lanes	Going Straight Ahead Going Straight Ahead	East West East West West East West East
7307 State Route 7308 State Route 7309 State Route	Cialiam 1 Cialiam 1	101						Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Dark-No Street Lights	From same direction - both going straight - both moving - rear-end Vehicle Strikes. Deer From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead	Slowing	East West East West East West
7310 State Route 7311 State Route	Cialiam 1	101		272.71 No E866659 272.73 No E916201	2014-09-10 21:09 No Apparent Injury 2018-11-11 13:35 Possible Injury 2019-04-30 12:20 No Apparent Injury	0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy				Going Straight Ahead Going Straight Ahead	Slowing	East West East West West East West West
7312 State Route 7313 State Route	Ciallam Ciallam Ciallam Ciallam Ciallam	101		272.74 No E640108 272.78 No E645345	2017-02-06 11:09 Possible Injury 2017-02-23 06:04 Possible Injury	1 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Distance Description of Secretary and as 40,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Snowing Clear or Partly Cloudy	Snow/Slush Straight & Level Snow/Slush Straight & Level	Daylight Dark-No Street Lights	Earth Bank or Ledge Earth Bank or Ledge	Changing Lanes Going Straight Ahead	Caina Staniald Aband	West East West East
7315 State Route	Cialiam	101		272.90 No E843327 272.97 No E888135 272.98 No E488705	2019-01-16 17:24 Possible Injury 2015-11-26 07:24 No Apparent Injury	2 0 4 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup.Panel Truck or Vanette under 10,000 b Pickup.Panel Truck or Vanette under 10,000 b Pickup.Panel Truck or Vanette under 10,000 b Passenger Car Passenger Car	Not at Intersection and Not Related Intersection Related but Not at Intersection	Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Dark-No Street Lights Dawn	Earth Bank or Ledge Earth Bank or Ledge Earth Bank or Ledge Firm same direction - both going straight - both moving - sideswipe Firm same direction - both going straight - one stopped - rear-end Firm same direction - both going straight - one stopped - rear-end Firm same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead	Going Straight Ahead Stopped for Traffic Slowing	East West Vehicle Stopped Vehicle Stopped West East West Faxt
7317 State Route 7318 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101		273.11 No E665012	2017-04-22 12:42 No Apparent Injury	0 0 1 0	0 Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Curve & Level	Daylight Dark-No Street Lights	Vehicle Street Over 1 others	Going Straight Ahead Going Straight Ahead Making U-Turn Going Straight Ahead	Going Straight Ahead	East West East West
7319 State Route 7320 State Route	Clallam	101		273.30 No E974962 273.39 No E527291	2019-10-20 02:40 Suspected Serious Injury 2016-03-21 14:40 No Apparent Injury	2 0 2 0 0 0 1 0	0 Passenger Car 0 Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Level	Dark-No Street Lights	From opposite direction - both going straight - sideswipe	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	East West West East East West
7321 State Route 7322 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101		273.47 No E836716 273.52 No E800270	2018-09-02 13:19 Suspected Minor Injury 2018-05-18 13:17 No Apparent Injury	2 0 2 0 0 0 1 0	0 Passenger Car 0 Pickup,Panel Truck or Vanette under 10,000 lb	Plokup.Panel Truck or Vanette under 10,000 to Passenger Car Plokup.Panel Truck or Vanette under 10,000 to Truck Tractor & Semi-Trailer Plokup.Panel Truck or Vanette under 10,000 to Plokup.Panel Truck or Vanette under 10,000 to	Not at Intersection and Not Related Intersection Related but Not at Intersection	Overcast	Dry Straight & Level Dry Straight & Level	Daylight	Vehicle Strikes Deer From same direction - both going straight - both moving - rear-end Roadway Ditter From opposite direction - both going straight - sideswipe	Going Straight Ahead	Slowing	East West East West West East
718 State Route 719 State Route 719 State Route 719 State Route 7211 State Route 7221 State Route 7222 State Route 7222 State Route 7222 State Route 7223 State Route 7225 State Route 7226 State Route 7227 State Route 7228 State Route 7237 State Route 7238 State Route 7239 State Route 7231 State Route 7231 State Route 7232 State Route 7233 State Route 7234 State Route 7235 State Route	Cialiam 1 Cialiam 1	101		273.55 No E559170	2016-06-29 19:29 Possible Injury 2016-06-24 17:21 Possible Injury	1 0 2 0	O Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car	Diruck Tractor & Semi-Trailer Dickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Not at Intersection and Not Related At Intersection and Related At Intersection and Related	Fog or Smog or Smoke Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight	From opposite direction - all others	Overtaking and Passing Overtaking and Passing Going Straight Abead	Going Straight Ahead Going Straight Ahead Stopped in Roadway	West East East West West East East West West East West Vehicle Stopped
7326 State Route 7327 State Route	Cialiam 1 Cialiam 1	101		273.60 No E372346 273.65 No E49980	2014-11-06 21:00 Suspected Minor Injury 2015-12-22 04:49 No Apparent Injury	1 0 2 0	O Pickup Panel Truck or Vanette under 10,000 it Plassenger Car Passenger Car O Pickup Panel Truck or Vanette under 10,000 it O Pickup Panel T	Passenger Car Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level loe Straight & I evel	Dark-No Street Lights Dark-Street Lights Off	From same direction - both going straight - one stopped - sideswipe	Overtaking and Passing Going Straight Ahead Stopped in Roadway Going Straight Ahead	Going Straight Ahead	West Venicle Stopped Vehicle Stopped East West East West East West
7328 State Route 7329 State Route	Clallam	101		273.70 No E380256 273.70 No E773841	2014-11-29 12:30 No Apparent Injury 2018-02-25 07:27 No Apparent Injury	0 0 1 0	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy Sleet or Hail or Freezing Rain	Ice Straight & Level	Davlight	Mailbox			West East West East
7330 State Route 7331 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101		273.70 No E829152 273.75 No E683527	2018-08-02 12:21 No Apparent Injury 2017-06-19 15:15 No Apparent Injury	0 0 1 0	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Overcast	Dry Curve & Level	Daylight	Malibox Tree or Stump (stationary) Roadway Ditch	Going Straight Ahead Going Straight Ahead Going Straight Ahead		East West East West
7332 State Route 7333 State Route	Cialiam 1 Cialiam 1 Cialiam 1	101		273.80 No E885304 273.83 No E331473 274.06 No E353129	2018-12-26 22:13 Suspected Minor Injury 2014-05-30 15:58 No Apparent Injury	1 0 1 0	0 Passenger Car 0 Pickup, Panel Truck or Vanette under 10,000 lb	Dishus Danel To 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	Not at Intersection and Not Related Not at Intersection and Not Related Intersection Related but Not at Intersection	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level Dry Curve & Grade	Dark-No Street Lights Daylight	Roadway Ditch Roadway Ditch		Del . A	West East East East West
7335 State Route 7336 State Route	Clallam	101		274.06 No E891260	2019-02-03 18:24 No Apparent Injury 2014-07-16 09-31 No Apparent Injury	0 0 2 0	O Passenger Car O Rickup Panel Truck or Vanette under 10,000 lb O Passenger Car	Pickup Panel Truck or Vanette under 10,000 b Pickup Panel Truck or Vanette under 10,000 b Passenger Car Pickup Panel Truck or Vanette under 10,000 b Pickup Panel Truck or Vanette under 10,000 b Pickup Panel Truck or Vanette under 10,000 b Pickup Panel Truck or Vanette under 10,000 b	Intersection Related but Not at Intersection Not at Intersection and Not Related At Intersection and Related	Snowing	loe Straight & Level	Dark-No Street Lights	Roadway Ditch From same direction - both going straight - both moving - rear-end One parked-one moving Enteriors at smooth	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Other* Legally Parked, Occupied	West East East East East East East East Ea
7337 State Route 7338 State Route	Clallam 1 Clallam 1 Clallam 1 Clallam 1	101		274.07 No E343237 274.07 No E698879 274.07 No E785165	2017-07-22 10:05 Possible Injury 2018-04-02 14:31 Suspected Moor Injury	1 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Overcast	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Daylight	Entering at angle Entering at angle Fintering at angle From same direction - both going straight - one stopped - rear-end Domestic animal other (cat, dog, etc)	Going Straight Anead	Making Right Turn Going Straight Ahead Stopped for Traffic	East West East North Vehicle Stopped West West East West East Vehicle Stopped Vehicle Stopped
7339 State Route 7340 State Route	Cialiam 1 Cialiam 1	101		274.07 No E785165 274.07 No E793645 274.08 No E345208	2018-04-28 08:50 No Apparent Injury 2014-07-27 14:54 Possible Injury	0 0 1 0	0 Passenger Car 0 Passenger Car	Passenger Car	At Intersection and Not Related At Intersection and Related	Raining Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Wet Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - one stopped - rear-end Domestic animal other (cat dog, etc) From same direction - both going straight - both moving - rear-end	Making Left Turn Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing	East West East West
7341 State Route 7342 State Route	Cialiam 1 Cialiam 1	101		274.08 No E932554 274.08 No E937306	2019-06-15 12:43 No Apparent Injury 2019-07-04 18:01 Possible Injury	0 0 1 0	0 Passenger Car 0 Passenger Car	Passenger Car	At Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - both moving - rear-end	Making Left Turn Going Straight Ahead	Slowing	East South West East West East
7343 State Route 7344 State Route	Clallam 1	101		274.08 No E941770 274.27 No E973692	2019-07-15 13:10 Suspected Minor Injury 2019-10-16 19:35 Suspected Minor Injury	1 0 1 0	0 Passenger Car 0 Passenger Car		At Intersection and Not Related Not at Intersection and Not Related	Overcast	Wet Straight & Level	Daylight Dark-No Street Lights	Fence Vehicle Strikes Deer	Making Left Turn Going Straight Ahead Going Straight Ahead Going Straight Ahead		East West West East
7335 Stein Route 7336 Stein Route 7330 Stein Route 7330 Stein Route 7330 Stein Route 7340 Stein Route 7340 Stein Route 7342 Stein Route 7342 Stein Route 7342 Stein Route 7343 Stein Route 7344 Stein Route 7344 Stein Route 7347 Stein Route 7347 Stein Route 7348 Stein Route	Cialiam 1	101		274.28 No E695079 274.40 No E641706	2019-06-16 12-43 No Apparent Injury 2019-07-04 18-01 Possible Injury 2019-07-04 18-01 Possible Injury 2019-07-16 13-10 Suspected Mnor Injury 2019-10-18 19:35 Suspected Mnor Injury 2017-07-18 11-25 Suspected Serious Injury 2017-07-02-06 23-50 No Apparent Injury 2014-01-19 16-23 No Apparent Injury 2015-07-02 15-28 No Apparent Injury	1 0 1 0	1 Truck (Flatbad, Van, etc) 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Snowing	Dry Straight & Grade Snow/Slush Straight & Grade	Daylight Dark-No Street Lights	Vehicle - Pedalopciist Ouer Embankment - No Guardraii Present Guide Post Guide Post	Going Straight Ahead Going Straight Ahead Going Straight Ahead		East West East West
7347 State Route 7348 State Route	Clallam	101		274.44 No E377817 274.44 No E440136	2014-11-19 06:23 No Apparent Injury 2015-07-02 15:28 No Apparent Injury	0 0 1 0		Total Tender & Co. of Tend	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Cevel Dry Straight & Grade	Daylight	Guide Post Metal Sign Post	Going Straight Anead	potential of the second	East West East West
7348 State Route 7349 State Route 7350 State Route 7351 State Route 7351 State Route	Ciallam (Ciallam Ciallam Ciall	101		274.48 No E872436 274.58 No 2986213 274.58 No 3501118	2018-12-09 20:34 No Apparent Injury 2015-04-21 14:05 Possible Injury 2016-12-22 16:02 No Apparent Injury	1 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	Truck Tractor & Semi-Trailer Passenger Car Passenger Car	Not at Intersection and Not Related At Intersection and Related At Intersection and Related	Raining Overcast	Wet Straight & Grade Wet Straight & Grade Wet Straight & Grade	Daylight Daylight	Metal Sign Post From same direction - all others Entering at angle Entering at angle Entering at angle	Making U-Turn Making Left Turn Making Left Turn	Going Straight Ahead Going Straight Ahead Going Straight Ahead	
7352 State Route 7353 State Pourt	Clallam	101						Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related	Raining Overcast	Wet Straight & Level	Daylight Dayk-Street Lichts A	Entering at angle From opposite direction - one left turn - one straight Vehicle Strikes All Other Non-Domestic Animal	Making Left Turn	Going Straight Ahead Going Straight Ahead	West North East West West Fast
7354 State Route 7355 State Route	Clallam 1 Clallam 1 Clallam 1	101 101 101		274.58 No E337071 274.58 No E415052 274.58 No E565032	2013-04-08 10:07 No Apparent Injury 2015-04-08 10:07 No Apparent Injury 2016-07-11 13:33 Suspected Minor Injury	0 0 2 0	0 Passenger Car 0 Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Not Related At Intersection and Related At Intersection and Related	Overcast Overcast Overcast	Dry Straight & Grade Dry Straight & Grade	Daylight Daylight	Vehicle Strikes All Other Non-Domestic Animal Entering at angle Entering at angle From same direction - both going straight - both moving - rear-end	Going Straight Ahead Making Right Turn Going Straight Ahead	Going Straight Ahead Going Straight Ahead	North West East West South North West Fact
7356 State Route 7357 State Route	Cialiam 1 Cialiam 1	101		274.58 No E650380 274.58 No E725698	2017-03-04 02:25 No Apparent Injury 2017-10-16 15:24 No Apparent Injury	0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10 000 II	Truck Tractor Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Not Related At Intersection and Related	Snowing Clear or Partly Cloudy	Wet Straight & Grade Dry Straight & Grade	Dark-Street Lights On Daylight	Emering a ranger From same direction - both going straight - both moving - rear-end From same direction - all others	Making Right Turn Going Straight Ahead Going Straight Ahead Backing	Going Straight Ahead	West East West East Vehicle Backing Vehicle Stopped Vehicle Stopped
7358 State Route	Clallam	101		274.58 No E725698 274.64 No E649330 274.64 No E835541	2017-02-26 13:30 No Apparent Injury 2018-09-04 07:16 Suspected Minor Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Truck Tractor & Semi-Trailer	Truck Tractor Pictup Parel Truck or Vaneste under 10,000 b Passenger Car Truck (Flathad Mn. ndc) Truck (Flathad Mn. ndc) Truck (Flathad Mn. ndc) Pictup Parel Truck or Vaneste under 10,000 b Pissenger Car Pissenger Car Pissenger Revenue Care Care Care Care Care Care Care Car	Intersection Related but Not at Intersection Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Other Straight & Grade	Daylight	Entering at angle	Backing Making Right Turn Going Straight Ahead	Going Straight Ahead	North West East West West East
7360 State Route 7361 State Route	Cialiam 1 Cialiam 1 Jefferson 0	101P126272 019		0.00 No E393961 0.00 No E298695	2015-01-14 05:20 No Apparent Injury 2014-01-03 21:08 No Apparent Injury	0 0 1 0	0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Wet Straight & Level	Dark-No Street Lights Dark-Street Lights On	Roadway Ditch Vehicle Strikes All Other Non-Domestic Animal Earth Bank or Ledge From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East North South
7362 State Route 7363 State Route	Jefferson (Jefferson (Jefferson (019 019		0.00 No E382000 0.00 No E444080	2014-12-09 10:40 No Apparent Injury 2015-07-14 18:33 No Apparent Injury	0 0 2 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Truck (Flatbad, Van, etc) Pickup, Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Raining Clear or Partly Cloudy Blowing Sand or Dirt or Snow	Wet Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - both moving - rear-end Same direction both turning right one stopped rear end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Making Right Turn Going Straight Ahead	Slowing Stopped at Signal or Stop Sign Stopped at Signal or Stop Sign	North South North South North West Vehicle Stopped Vehicle Stopped
7364 State Route 7365 State Route	Jefferson (019 019		0.00 No E985774	2019-07-31 14:16 No Apparent Injury 2019-11-17 20:20 No Apparent Injury	0 0 2 0	U Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related Intersection Related but Not at Intersection	Blowing Sand or Dirt or Snow Overcast	Ury Straight & Level Wet Straight & Grade	Dark-Street Lights On	From same direction - both going straight - one stopped - rear-end Entering at angle	Stopped in Roadway	Stopped at Signal or Stop Sign Making Left Turn	North South Vehicle Stopped Vehicle Stopped Vehicle Stopped Vehicle Stopped West North
7366 State Route 7367 State Route 7368 State Pourte	Jefferson (Jefferson (Jefferson (Jefferson (019		0.10 No E792052	2018-04-14 15:37 No Apparent Injury	0 0 2 0	0 Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection At Driveway At Driveway	Clear or Partly Cloudy Raining Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - one left turn - one straight From same direction - one left turn - one straight	Going Straight Ahead Overtaking and Passing Overtaking and Passing	Making Left Turn Stopped for Traffic Making Left Turn Stowing	North South Vehicle Stopped Vehicle Stopped South North South West South North South North
7369 State Route 7370 State Route	Jefferson (019 019		0.11 No E698883 0.12 No E920700	2017-08-02 19:09 No Apparent Injury 2019-04-25 19:13 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car)	At Driveway	Clear or Partly Cloudy				Making U-Turn	Slowing	South South West
7368 State Route 7369 State Route 7370 State Route 7371 State Route 7372 State Route 7372 State Route 7373 State Route 7374 State Route 7376 State Route 7376 State Route 7377 State Route 7377 State Route 7377 State Route	Jefferson (019 019		0.15 No E418346 0.31 No E444190	2015-04-19 16:33 No Apparent Injury 2015-07-19 11:31 No Apparent Injury	0 0 2 0	O Passenger Car O Pickup Panel Truck or Vanette under 10,000 it Passenger Car O Pickup Panel Truck or Vanette under 10,000 it Passenger Car O Pickup Panel Truck or Vanette under 10,000 it O Pickup Panel Truck or Vanette under 10,000 it O Pickup Panel Truck or Vanette under 10,000 it O Pickup Panel Truck or Vanette under 10,000 it O Pickup Panel Truck or Vanette under 10,000 it O Pickup Panel Truck or Vanette under 10,000 it O Pickup Panel Truck or Vanette under 10,000 it O Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway Driveway Related but Not at Driveway Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Daylight	Anadway Distai Roadway Distai From opposite direction - both going straight - sideswipe Roadway Distai Vehicle Strikes Deer	Making Left Turn Going Straight Ahead Going Straight Ahead	Going Straight Ahead	South North North South North South
7373 State Route 7374 State Route	Jefferson (019 019		0.40 No E512570 0.70 No E465555	2016-02-01 18:05 No Apparent Injury 2015-09-26 08:20 No Apparent Injury	0 0 1 0	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related	Clear or Partly Cloudy Fog or Smog or Smoke	Dry Straight & Grade Dry Straight & Level	Dark-No Street Lights Daylight	Vehicle Strikes Deer Vehicle Strikes All Other Non-Domestic Animal			South North North South
7375 State Route 7376 State Route	Jefferson (Jefferson (019 019		0.80 No E938586 0.97 No E488701	2019-07-07 21:48 No Apparent Injury 2015-11-18 05:40 No Apparent Injury	0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb)	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Wet Straight & Level Ice Straight & Level	Dark-No Street Lights Dark-No Street Lights	Vehicle Strikes All Other Non-Domestic Animal Vehicle Strikes Deer Rock Bank or Ledge	Going Straight Ahead Going Straight Ahead Going Straight Ahead		South North North South
7378 State Route	Jefferson (019		1.00 No E482176 1.00 No E538573	2016-03-24 19:09 Unknown	0 0 1 0	0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Fog or Smog or Smoke Overcast	Wet Straight & Level	Daylight	Culvert and/or other Appurtenance in Ditch Roadway Ditch	Going Straight Ahead Going Straight Ahead		North South

OBJECTID MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_1_UNIT_1 7199 None	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_2_UNIT_1	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_3_UNIT_1	MV_DRIVER AGE GENUNIT_1 UN	DRIVER MV_DRIVER_CONTRIBUTING NDER CIRCUMSTANCE_1_UNIT_2	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_2_UNIT_2	MY_DRIVER_CONTRIBUTING MY_DRIVER GENER UNT_2 GENER UNT_2 GENER UNT_2 GENER UNT_2 GROUNSTANCE_1_UNT_1	BICYCLIST_CONTRIBUTING CIRCUMSTANCE_2_UNIT_1 CIRCUMSTANCE_3_UNIT_1	BICYCLIST AGE GENDER UNIT_1 GENDER CRCUMSTANCE_1_UNIT_	BICYCLIST_CONTRIBUTING BICYCLIST BICYCLIST GENDER_UNIT_2 GENDER_UNIT_2	PEDESTRIAN, CONTRIBUTING CRCUMSTANCE 1_UNT_2 CRCUMSTANCE 1_UNT_2 CRCUMSTANCE 2_UNT_2 CRCUMSTANCE 3_UNT_2		
7199 None 7200 None 7201 None 7202 Follow Too Closely			24 Femal 36 Male 50 Male	None None		17 Male						Lane 1 hicrosasing Milepost Past Rightl Shoulder increasing Milepost Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost
7201 None 7202 Follow Too Closely 7203 Apparently Asleep or Fatigued 7204 Smoking 7205 Cowr Center Line 7206 Orner Adjusting Judio or Entertainment 7207 Exceeding Reas, Safe Sceed 7207 72	Driver Interacting with Passengers, Anim	Inattention	19 Male 39 Femal 30 Male			35 Male 37 Male						Lane 1 Increasing Mileosot Past Right Shoulder increasing Mileosot Lane 1 Increasing Mileosot Lane 1 Decreasing Mileosot
7206 Driver Adjusting Audio or Entertainment 7207 Exceeding Reas. Safe Speed 7208 Exceeding Reas. Safe Speed	Follow Too Closely		19 Male 29 Male	None None		37 Male 29 Male 54 Female 65 Male						
7208 Exceeding Reas. Safe Speed 7209 Other Contributing Circ Not Listed 7210 None 7211 None	Distractions Outside Vehicle		60 Male 68 Femal	le None							1 1	International Road Decreasing Milepost Past Right Shoulder Decreasing Milepost Lane 1 Increasing Milepost Lane 1 Decreasing Milepost
T211			46 Femal 52 Male 24									Lane 1 Decreasing Mispost Past Right Shortessing Mispost Lane 1 Increasing Mispost Lane 1 Decreasing Mispost
7214 Inattention 7215 None 7216 Apparently III	Over Center Line		58 Femal 54 Male	None le None		75 Female 58 Female						Lane 1 Increasing Milepost Lane 1 Decreasing Milepost
7217 None 7218 None 7219 Exceeding Reas, Safe Speed			54 Male 74 Femal 60 Femal 23 Male	le le None		58 Male						Lane 1 Increasing Miepost Lane 1 Increasing Miepost Lane 1 Decreasing Miepost
	Exceeding Reas. Safe Speed		34 Male 66 Male	None None		68 Female 43 Male						Lane 1 Increasing Miepost Lane 1 Increasing Miepost Right Shoulder Decreasing Miepost
7223 Exceeding Reas. Safe Speed 7224 Driver Interacting with Passengers, Anim	Did Not Grant RW to Vehicle	Follow Too Closely	53 Male 61 Male	None None		69 Male 33 Male						
7224 Driver Interacting with Passengers, Anim 7225 Under Influence of Alcohol 7226 Inattention 7227 Improper Turn/Merge	Did Not Grant RW to Vehicle		61 Male 38 Male 86 Male 56 Femal	None None Ie None		33 Male 39 Female 89 Male 26 Male						Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost Lane 1 Increasing Milepost Intersecting Road Increasing Milepost
7227 Improper Turn/Merge 7228 Inattention 7229 Exceeding Reas. Safe Speed 7230 Inattention 7231 Inattention 7232 Exceeding Reas. Safe Speed 7233 Inattention	Over Center Line Exceeding Reas, Safe Speed		65 Male 44 Male 86 Male 29 Male			78 Male 31 Male 28 Male					1 1	Lane 1 horeasing Milepost Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost Right Shoulder Decreasing Milepost
7231 Inattention 7232 Exceeding Reas. Safe Speed	Exceeding Reas. Safe Speed Exceeding Reas. Safe Speed		84 Femal	le None		59 Male					1 1	Lane 1 Decreasing Milepost
7234 None	Exceeding Reas. Safe Speed		23 Male	Driver Not Distracted None		52 Male 60 Male						Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost Past Right Shoulder Increasing Miepost
7236 Follow Too Closely 7237 None 7238 None			19 Femal 74 Male 61 Femal	le None		77 Male						Lane 1 Decreasing Mispost Lane 1 Decreasing Mispost Lane 1 Decreasing Mispost Lane 1 Decreasing Mispost Lane 1 Increasing Mispost
7239 Under Influence of Drugs 7240 Distractions Outside Vehicle	Inattention Expanding Page Safe Speed		70 Male	None		45 Male 64 Female 66 Female						
7242 Under Influence of Alcohol 7243 None	Exceeding Reas. Safe Speed Eating or Drinking		53 Male 56 Male		Over Center Line	56 Female 33 Male 62 Female						Lane 1 Increasing Miepost Lane 1 Increasing Miepost Lane 1 Increasing Miepost
7244 Operating Defective Equipment 7245 Operating Other Electronic Devices (comp 7246 Inattention	Other Contributing Circ Not Listed Follow Too Closely Exceeding Reas. Safe Speed		28 Femal	le None		63 Male 59 Male 72 Male						Lane 1 Decreasing Milepost Lane 1 Increasing Milepost Lane 1 Increasing Milepost
7235 Lunder Influence of Alcohol 7236 Falor Too Closely 7239 Note 7230 Note 7230 Note 7230 Note 7230 Under Influence of Druge 7240 Under Influence of Druge 7240 Under Influence Alcohol 7240 Under Influence Alcohol 7240 Note 7250 Note 72	Follow Too Closely Follow Too Closely		34 Femal 66 Femal 41 Male 34 Femal			33 Male 66 Female						Lane I Increasing Milepost Lane I Increasing Milepost Lane I Increasing Milepost Lane I Increasing Milepost Past Right Shoulder Increasing Milepost Lane I Increasing Milepost Lane I Increasing Milepost Lane I Increasing Milepost
7250 None 7251 Exceeding Reas. Safe Speed			67 Femal 93 Femal	le None								Lane 1 licreasing Miepost Lane 1 Decreasing Miepost Past Right Shoulder Decreasing Miepost
7252 Over Center Line 7253 Follow Too Closely 7254 hattention	Follow Too Closely		75 Femal	le None		26 Maie 47 Female 74 Maie 64 Maie						Lane 1 Increasing Milepost
7252 Over Center Line 7253 Follow Too Closely 7254 Instension 7255 Follow Too Closely 7255 Follow Too Closely 7256 None 7257 Instension 7258 None			51 Femal 49 Male 29 Male	le None		64 Maie						Claim J. Brownschip Mitspoors Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost Past Right Shoulder Increasing Milepost
7257 isatienston			27 Femal 58 Femal	le le							1 1 1	Pass right strotter Extraoring weepon Lane 1 Decreasing Milepost
7261 None 7262 Exceeding Reas. Safe Speed			28 Femal 68 Femal	le Operating Defective Equipment None		33 Male 23 Male 54 Female						Lane 1 Decreasing Milepost Lane 1 Increasing Milepost
7263 Follow Too Closely 7264 Exceeding Reas. Safe Speed 7265 Inattention	Did Not Grant RW to Vehicle		37 Male 68 Male 62 Male	None		54 Female						Lane 1 Increasing Milepost Past Right Shoulder increasing Milepost Lane 1 Increasing Milepost
7265 hattention 7266 Improper Turn/Merge 7267 Did Not Grant RW to Vehicle 7268 Exceeding Reas, Safe Speed	Inattention		78 Femal	le None		42 Female 44 Female 46 Female 60 Male					1 1	Lett Turn Lane Increasing Milepost Lane 1 Increasing Milepost
7268 Exceeding Reas. Sate Speed 7269 None 7270 Inattention	Follow Too Closely		34 Femal	le None		60 Male					1 1	Lane 1 Increasing Miepost Lane 1 Decreasing Miepost Past Right Shoulder Decreasing Miepost
7209 None 7270 Instension Speed 7270 Instension Speed 7270 Instension 7271 Did Not Grant RW to Vehicle 7272 Other Contributing Circ Not Listed 7273 Improper Turn/Merge 7273 Improper Turn/Merge	Inattention		62 Femal	None Ie Other Contributing Circ Not Listed Ie None		76 Male 30 Male 64 Male						Past Right Shoulder Decreasing Milepost Lane 1 Increasing Milepost Lane 2 Increasing Milepost Instruction (All Increasing Milepost)
7227 Improper Furnifluore 7278 Improper Furnifluore 7278 Improper Furnifluore 7278 Improper Furnifluore 7278 De Nos Gears Rive to Whotele 7279 De Nos Gears Rive to Whotele 7279 Offer Contributing Dic Nos Listed 7279 Offer Contributing Dic Nos Listed 7279 De Nose Furnifluore 7279 De Disconsider State Sign Fashing Red 7280 Improper Furnifluore 7280 Improper Furnifluore 7280 Improper Furnifluore 7280 Improper Furnifluore 7280 Institution	H SMAND THEORY		32 Male 81 Male	None None		56 Moie 56 Moie 38 Female 58 Female						Leaf furn Lano Decreasing Minepost Lano 1 increasing Minepost Lano 1 increasing Milepost Lano 1 increasing Milepost
7276 Did Not Grant RW to Vehicle 7277 Did Not Grant RW to Vehicle 7278 Other Contributing Circ Not Listed			74 Femal	le None le None		63 Male						Lane 1 Increasing Milepost Past Right Shoulder Increasing Milepost
7279 Inattention 7280 Disregard Stop Sign - Flashing Red 7281 Inattention	Did Not Grant RW to Vehicle Did Not Grant RW to Vehicle		74 Male 20 Male 25 Male	Driver Not Distracted None None		35 Female 50 Male 76 Female						Lane 1 Increasing Milepost Lane 1 Increasing Milepost Interaceing Road Increasing Milepost
7282 Exceeding Stated Speed Limit 7283 Improper Turn/Merge			43 Male 61 Femal	le None		51 Female 63 Male						Median Shoulder Increasing Milepost Intersecting Road Increasing Milepost
7284 Did Not Grant RW to Vehicle 7285 Improper Turn/Merge 7286 Inattention	Inattention		61 Femal 87 Male 67 Femal 58 Male	None le None		63 Male 75 Female						Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost Past Right Shoulder Increasing Milepost
7287 Inattention 7288 None 7289 Follow Top Closely	Follow Too Closely		58 Male 31 Male 48 Male 35 Femal	None In None		53 Female					1 1	Lane 1 Decreasing Milepost Lane 1 hiorasing Milepost Lane 1 hiorasing Milepost Lane 1 hiorasing Milepost
7290 Improper Passing 7291 Exceeding Reas. Safe Speed			71 Male 64 Male	Other Contributing Circ Not Listed		49 Female						Lane 1 Increasing Milepost Inters ecting Road Decreasing Milepost Past Right Shoulder Increasing Milepost
7291 Exceeding Reas. Safe Speed 7292 Exceeding Stated Speed Limit 7293 Under Influence of Mohol 7294 Exceeding Reas. Safe Speed	Improper Passing		18 Male 52 Male 45 Male	None		21 Female						Right Shoulder Decreasing Milepost
	Follow Too Closely		79 Male 89 Femal 38 Femal	None le None		61 Male 69 Male 40 Female						Lane 1 Decreasing Mlepost Lane 1 Decreasing Mlepost Lane 1 Increasing Mlepost
7298 None 7299 None	Instantion		32 Male 51 Femal	le		52 Male						Lane 1 Decreasing Milepost Lane 1 Increasing Milepost Lane 1 Increasing Milepost
228 Store	THE SECTION OF		33 Male	Driver Not Distracted		UA PREFE						Lane 1 Increasing Milepost Right Shoulder Increasing Milepost
7303 None 7304 None 7305 Did Not Grant RW to Vehicle			66 Male 35 Femal 35 Femal	le None		72 Maie						Lane 1 Decreasing Miepost Lane 1 Increasing Miepost Lane 2 Decreasing Miepost Lane 2 Decreasing Miepost
7306 Other Contributing Circ Not Listed 7307 Other Contributing Circ Not Listed 7308 Follow Too Closely 7309 None			29 Male 77 Male	None		19 Female 53 Female 72 Male						Lane 1 Decreasing Miepost Lane 1 Increasing Miepost Lane 2 Decreasing Miepost
7310 Exceeding Reas . Safe Speed	Follow Too Closely		70 Male 54 Male	None		42 Female					1 1	Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost
7309			55 Male 72 Male 41 Male 35 Male									Right Shoulder Increasing Miepost Past Right Shoulder Increasing Miepost Past Right Shoulder Increasing Miepost Lane 1 locreasing Miepost Lane 1 locreasing Miepost
7314 Did Not Grant RW to Vehicle 7315 Other Contributing Circ Not Listed 7316 Follow Too Closely			72 Femal 23 Male	None le None None		41 Maie 34 Maie 52 Maie						Lane 1 Decreasing Milenort
7314 Did Not Grant RW to Vehicle 7315 Office Controlling Control Research 7316 Follow Too Closely 7317 Direct Not Distanced 7318 Improper U-Turn 7319 Apparently Assess or Fatigued 7320 None 7321 Inattention 7322 Driver Interacting with Passengers, Anim 7323 Drantatention	Did Not Grant RW to Vehicle Over Center Line		57 Femal 70 Male 22 Mal-	None None		65 Female 20 Female						Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost
7320 None 7321 Inattention	Exceeding Reas. Safe Speed Other Driver Distractions Inside Vehicle	Follow Too Closely	71 Male 33 Femal 60 Male	le None		64 Male						Laine 1 Burdassinj affijoos Laine 1 Deceasinj affijoos Laine 1 Deceasinj affijoos Rijhi Shoulder Deceasinj Mispost Past Rijhi Shoulder Increasinj Mispost Past Rijhi Shoulder Increasinj Mispost
7323 Inattention 7324 Non Motorist on Wrong Side of Road			29 Male	None		44 Male 55 Male 30 Female 82 Male						Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost
7325 Inattention 7326 Other Contributing Circ Not Listed 7327 Exceeding Reas. Safe Speed	Follow Too Closely		87 Femal 50 Male 36 Femal	None		30 Female 82 Male						Lane 1 Increasing Milepost Lane 1 Decreasing Milepost Past Right Shoulder Decreasing Milepost
7328 Exceeding Reas. Safe Speed 7329 Exceeding Reas. Safe Speed 7330 Apparently Asless or Entire and			59 Male 77 Male 51 Femal								1 1	Past Right Shoulder Increasing Milepost
7331 hattention 7332 Exceeding Reas, Safe Speed	Apparently Fatigued		75 Femal	le								Past Right Shoulder Increasing Milepost Past Right Shoulder Decreasing Milepost Past Right Shoulder Decreasing Milepost Past Right Shoulder Decreasing Milepost Past Right Shoulder Increasing Milepost
7334 Exceeding Reas. Safe Speed 7334 Exceeding Reas. Safe Speed 7335 Exceeding Reas. Safe Speed	Follow Too Closely		37 Male 27 Male 21 Femal	None None None None		54 Female 22 Male						Past Right Shoulder Decreasing Miepost Lane 1 Increasing Miepost Past Right Shoulder Decreasing Miepost
7336 Follow Too Closely 7337 Other Contributing Circ Not Listed 7338 Inattention	Exceeding Reas. Safe Speed		72 Male 16 Male 91 Male	Driver Not Distracted None None		20 Maie 77 Maie 77 Maie						Lane 1 Increasing Milepost
7339 None 7340 Exceeding Reas . Safe Speed			20 Femal	ie Univer Not Distracted		// Maile 29 Female						Lane 1 Increasing Miepost Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost
1922 Diser Extraction with Place segger, Aren 1922 Interfection 1924 Non Microsoft Whong Side of Road 1925 Interfection 1926 Interfection With Side of Road 1926 Interfection Read Clinic Management of Page 1926 Interfection Read Clinic Read Side Speed 1926 Secreting Read Side Speed 1927 Secreting Read Side Speed 1928 Secreting Secretin	Exceeding Reas. Safe Speed Exceeding Reas. Safe Speed	Over Center Line	20 Male 60 Femal 77 Femal	le None		29 Female						Past Right Shoulder Increasing Milepost Lane 1 Increasing Milepost Past Right Shoulder Decreasing Milepost
7344 None 7345 Under Influence of Drugs 7346 Operating Defective Equipment		· · · · · · · · · · · · · · · · · · ·	71 Male					None	 57 Male			Lane 1 Increasing Miepost Right Shoulder Decreasing Miepost Past Right Shoulder Decreasing Miepost
7347 Apparently As leep or Fatigued 7348 Apparently As leep or Fatigued	Exceeding Reas. Safe Speed Under Influence of Drugs		52 Male 40 Male 25 Male 21 Male									Carle 1 Notionarily arreptor (April Shoulder Decreasing Milepost Past Right Shoulder Decreasing Milepost [April Decreasing Milepost]
7350 Did Not Grant RW to Vehicle 7351 Did Not Grant RW to Vehicle			25 Femal 84 Male 22 Male	le Over Center Line None None		59 Male 24 Female 80 Male						Lane 1 Decreasing Miepost Lane 1 Increasing Miepost Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost
7352 Did Not Grant RW to Vehicle 7353 None 7354 Did Not Grant RW to Vehicle			85 Male 30 Male	None		20 Female						Lane 1 Increasing Milepost
7355 Inattention 7356 Inattention	Did Not Grant RW to Vehicle Apparently Fatigued		82 Male 73 Male 21 Male			35 Female 63 Male 34 Male						Lane 1 Decreasing Miepost Lane 1 Increasing Miepost Lane 1 Increasing Miepost Inders 1 Increasing Miepost Intersecting Road Decreasing Miepost
7357 Improper Backing 7358 Improper Turn/Merge 7359 Inattention	Unknown Distraction		59 Male 65 Male 50 Male 37 Femal	None None		50 Male 30 Female						Intersecting Road Decreasing Milepost Lane 1 Decreasing Milepost Past Right Shoulder Increasing Milepost Lane 1 Off Ramp Increasing Milepost Side of Mainline
7360 None 7361 hattention	Expeeding Reas, Safe Speed		37 Femal 82 Male	le None		46 14-						Past Right Shoulder Decreasing Milepost
7363 Inattention 7364 Other Contributing Circ Not Listed	Exceeding Reas. Safe Speed Follow Too Closely		27 Femal 29 Femal	ie None le None None		46 Male 54 Female 69 Male						Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost
1947 Agraemity Asiaso or Faligued 1948 Agraemity Asiaso or Faligued 1948 Insprace U-Turn 1949 Agraemity Asiaso or Faligued 1949 Insprace U-Turn 1950 Insprace U-Turn 1950 Insprace 1950	Apparently Fatigued Improper Passing		35 Male 35 Femal 41 Male 39 Male	Unknown Distraction le None None		22 Male 27 Female 73 Male						
7368 Improper Passing 7369 Under Influence of Alcohol 7370 Apparently de Jean or Entirement	Inattention	Apparently Fatigued	85 Male			48 Male						Lane 1 Decreasing Mispost Lane 1 Decreasing Mispost Lane 1 Decreasing Mispost Lane 1 Decreasing Mispost Past Right Shoulder Decreasing Mispost Past Right Shoulder Decreasing Mispost Past Right Shoulder Decreasing Mispost
1786	- AMMERICAN	Apparently Fatigued	66 Male 47 Male 58 Male	None		25 Female						Past Right Shoulder Decreasing Mepost Lane 1 Decreasing Mepost Past Right Shoulder Decreasing Mepost
7373 None 7374 None 7375 None			64 Male 63 Male 21 Male 57 Male									Lane 1 hicrossing Milegost Lane 1 Decreasing Milegost Lane 1 Decreasing Milegost Lane 1 hicrossing Milegost Part Right Shoulder Increasing Milegost
7376 Exceeding Reas. Safe Speed 7377 Unknown Distraction 7378 Exceeding Page Safe Second		1	20 Femal	le								Past Right Shoulder Decreasing Milepost
coording reas, paid Speed		+		+		· · · · · · · · · · · · · · · · · · ·	· · · · · · · · · · · · · · · · · · ·	 	 			Past Right Shoulder Decreasing Mlepost

OBJECTID WA_STATE_PLANE SOUTH_X SOUTH_Y 2010_FORWARD COMPANY SOUTH_Y 2010_FORWARD		COL COL, HUM CO SEVERITY FACTOR AG	CONDI	TIONS		UDY COL TYPE SEVERITY REA VUL_USER TEXT TEXT	. COL_HUMAN COL FACTOR_Decoded MVIW	COL_LIGHT COL_SL CONDITIONS CONDIT Decoded Deco	RFACE COL_LOCATION Decoded	COL_RISKY Intersection Area_Name Area_Name	phasis Roadway Area Departure
7201 1018398.23 1001620.56 2018 7202 1018577.72 1001427.84 2017 7203 1018577.72 1001427.84 2017	9 1 Vehicle-Party Alminal 5 1 Vehicle-Party Struck Object 8 1 Vehicle-Party All Other Non-Collision 7 2 Vehicle-Party All Other Non-Collision 7 3 Vehicle-Strates Sides swipe	Property Damage Only Often/Unknown 65+ Property Damage Only Often/Unknown Under Property Damage Only Often/Unknown 35-44 Property Damage Only Follow Too Closely 45-54 Suspected Sensor Inlury Apparently Releep, Fatigued or III Under	No Dr. 25 No Dr. No Dr. No Dr. No Dr. 25 Apparently Asleep, Fatigued or III Under 25 Under 25 Under 25	ry Dawn/Dusk ry Daylight ry Daylight let Daylight	Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Level	(Older Road User 65+ No Vulnerable Road User Involved	None None Follow Too Closely Apparently Asleep or Fatigued	Daylight Dr Dawn/Dusk Dr Daylight Dr Daylight Or Daylight Wr	et Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Follow Too Closely Apparently Asleep, Fatigued or III	No Yes No No Yes
7207 1019784.81 1000122.97 2019	5 1 Vehicles/Party Struck Object 5 2 Vehicles/Parties Acimal 6 2 Vehicles/Parties Rear-End 9 2 Vehicles/Parties Rear-End 9 2 Vehicles/Parties Rear-End 2 Vehicles/Parties Acimal	Mnor or Possible Injury Distracted Driving 35-44 Property Damage Only Over Center Line 25-34 Property Damage Only Distracted Driving Under Property Damage Only Speeding 25-34 Property Damage Only Speeding 25-34 Property Damage Only Speeding 25-34	No los/Snov	w/Slush Daylight	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved	Speeding	Dark-No Street Lights/Street Lights Off With Dark-No Street Lights/Street Lights Off Standing Daylight Dr. Daylight Daylight De/Snoo	y Not at Intersection/Driveway/Roundabout	Distracted Driving No Risky Driving Behaviors Involved Distracted Driving Speeding Speeding	Yes Yes No No
7210 1020399.94 999466.65 2016 7211 1020506.95 999352.49.2016	6 2 Vehicles/Paries Angle	Property Damage Only Speeding 25-34 Property Damage Only Debrufknown 55-64 Property Damage Only Other/Unknown 65+ Minor or Possible Injury Other/Unknown 45-54 Property Damage Only Under Influence of Alcohol/Drugs/Medication 45-54	No Oth No W No D: Under influence of Acchol 45-54 W	ry Daylight er Daylight et Daylight ry Daylight et Dark-No Street Lights/Street Lights Off	AR Drivway or Intersection Straight & Grade Not at IntersectionOrivewayRoundsbout Straight & Level Not at IntersectionOrivewayRoundsbout Straight & Level Not at IntersectionDrivewayRoundsbout Straight & Grade Not at IntersectionDrivewayRoundsbout Curve & Level Curve & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Clider Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Other Contributing Circ Not Listed None		Not at Intersection/DrivewayRoundabout Not at Intersection/DrivewayRoundabout	Speeduring No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication	Yes No No Yes
7215 1021105.19 998628.71 2016		strico un Pussateri Biblar Vinterchilacorem re Property Damage Chyl Inder Stellacore of Alcohol Drugs Medication 45-04 Property Damage Chyl Inder Stellacore of Alcohol Drugs Medication 45-04 Monto or Possible Istiput hasheston 55-04 Property Damage Chyl Monto or Possible Istiput y Assertation 55-04 Monto or Possible Istiput y Apparently Aleiep, Fatigued or III 45-04	No	ry Daylight ry Daylight ry Daylight ry Daylight	Not at Intersection/Drieway/Roundabout Curve & Level Not at Intersection/Drieway/Roundabout Unknown Not at Intersection/Drieway/Roundabout Curve & Level Not at Intersection/Drieway/Roundabout Curve & Grade Not at Intersection/Drieway/Roundabout Statight & Level Not at Intersection/Drieway/Roundabout Statight & Level	No Vulnerable Road User Involved Older Road User Set Older Road User Set	None	Dark-No Street Lights/Street Lights Off Will Daylight Dr.	y Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Institution Ins	No Yes No Yes
7219 1021254.13 997683.39 2015	5 2 Vehicles/Parties Rear-End	Manor or Possible livius / Popernify Aeleep, Fadgued or III 45-54 Property Damage Only Denetlihinzone 65-9 Property Damage Only Ober-Unknown 55-9 Property Damage Only Denetlihinzone 15-64 Property Damage Only Section (Under Property Damage Only Section (Under Property Damage Only Denetlihinzone) 25-34 Property Damage Only Denetlihinzone 15-64 Property Denetlihinzone 15-64 Property Denetlihinzone 15-64 Pro	25 No Dr No W	ry Daylight	Not at intersection.OrivewayRoundabout Straight & Level Not at Intersection.OrivewayRoundabout Curve & Level Not at Intersection.OrivewayRoundabout Straight & Level Not at Intersection.OrivewayRoundabout Straight & Level Not at Intersection.OrivewayRoundabout Curve & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	None Speeding Follow Too Closely	Daylight Dr Daylight Dr Daylight Dr Dark-No Street Lights/Street Lights Off W	y Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors involved No Risky Driving Behaviors Involved Speeding Follow Too Closely No Risky Driving Behaviors Involved	No No No Yes
7223 1021215.94 996569.01 2014 7224 1021221.67 996567.81 2015 7225 1021218.08 996569.82 2017	4 1 Vehicle/Party Struck Object 4 2 Vehicles/Parties Turning 5 2 Vehicles/Parties Turning 5 1 Vehicles/Parties Rear-End	Property Damage Only Inattention 65* Minor or Possible Injury Speeding 45-54 Property Damage Only Distracted Driving 55-64 Minor or Possible Injury Under Influence of Alcohol/Drugs/Medication 35-44	No Wi No Dr	let Daylight ry Daylight ry Daylight	Al Driveway or Inters ection Curve & Grade At Driveway or Inters ection Curve & Grade At Driveway or Inters ection Straight & Level At Driveway or Intersection Curve & Grade	Older Road User 65+ Motorcycle No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+	Inattention Speeding Distracted Driving Under Influence of Alcohol/Drugs/Medication	Daylight Wi Daylight Dr Daylight Dr Daylight Dr	at At Driveway or Intersection y At Driveway or Intersection y At Driveway or Intersection y At Driveway or Intersection	Inattention Speeding Distracted Driving Under Influence of Alcohol/Drugs/Medication	Yes No No No
7226 1021212.74 996571.78 2016 7227 1021215.94 996569.01 2016 7228 1021216.96 996590.09 2011	6 2 Vehicles/Parties Angle 6 2 Vehicles/Parties Angle 7 2 Vehicles/Parties Head-On	Suspected Serious Injury Inattention 654+ Property Damage Only Improper Turn/Merge 55.64-6 Minor or Possible Injury Inattention 65.7 Minor or Possible Injury Speeding 35-44 Minor or Possible Injury Instention 65+	No Dr No Dr No W No W No W No W	et Daylight	All Drineway or Intersection Cure & Level All Drineway or Intersection Cure & Grade All Intersection and Not Related Cure & Level All Drineway or Intersection Cure & Level Intersection Related but Not at Intersection Cure & Cu	Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved	Improper Turn/Merge	Daylight	y At Driveway or Intersection y At Driveway or Intersection et At Intersection and Not Related at At Driveway or Intersection et Intersection Related but Not at Intersection	Inattention No Risky Driving Behaviors Involved Inattention Speeding	No No Yes No
7231 1021215.74 996571.65 2018 7232 1021215.74 996571.65 2018	7 2 Webides Paries Turning 7 2 Webides Paries Rame find 8 1 1 1 1 1 1 1 1 1	Property Damage Only Inattention 25-34 Property Damage Only Speeding 65+ Property Damage Only Inattention 35-44	No	let Dark-No Street Lights/Street Lights Off et Dawn/Dusk y Daylight let Daylight et Daylight y Daylight y Daylight	AD Driveway or Intersection Curve & Level AD Driveway or Intersection Curve & Level AD Driveway or Intersection Curve & Level Intersection Related but Not at Intersection Curve & Grade Intersection Related but Not at Intersection Curve & Universection Curve & Univ	Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+	Speeding Inattention Speeding Inattention None	Daylight Wi Daylight Wi	y AD Priveway or Intersection at Driveway or Intersection the AD Priveway or Intersection intersection Related but Not at Intersection y Intersection Related but Not at Intersection y Not at Intersection/Driveway/Roundabout	Speeding Speeding Speeding No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication	Yes No No
7236 1021230.3 998427.11.2013	7 2 Vehicles/Parties Rear-End		25 Under Influence of Alcohol Under 25 Dr 25 No W No Dr No Dr No Dr	et Daylight	Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Curve & Level Curve & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved	None None	Daylight Wi	y Not at Intersection/DrivewayRoundabout tot Not at Intersection/DrivewayRoundabout y Not at Intersection/DrivewayRoundabout y Not at Intersection/DrivewayRoundabout tot Not at Intersection/DrivewayRoundabout	Under Influence of Alcoholi/Drugs/Medication Follow Too Closely No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Under Influence of Alcoholi/Drugs/Medication	No No No
7240 1021533.86 995624.84 2017	2 Vehicles/Parties Rear-End	Property Damage Only Distracted Driving 65+	Discontinuence of Drogs	ry Daylight	Intersection Related but Not at Intersection Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Distractions Outside Vehicle Inattention	Daylight Dr	word at intersection/Driveway/Roundabout y Intersection Related but Not at Intersection y Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout	Orizer insulentia of industrial original original original original original insulentian in Under Insulence of Alcohol/Drugs/Medication Involved Insulence or Alcohol/Drugs/Medication Insulence or A	No No Yes Yes
7244 1022326.51 994775.43 2019 7245 1022411.84 994707.33 2016 7246 102243.22 994686.41 2014 7247 1022453.05 994687 2019	9 2 Vehicles/Parties Strikes or Was Struck by Part or Object of Another Vehicle 8 2 Vehicles/Parties Rear-End 4 2 Vehicles/Parties Rear-End 5 2 Vehicles/Parties Rear-End		No Dr No WW No WW No Dr		Not at Intersection/Driveway/Roundabout Straight & Level Intersection Related but Not at Intersection Straight & Level At Driveway or Intersection Straight & Level At Driveway or Intersection Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	Operating Defective Equipment Operating Other Electronic Devices (comp Inattention	Dark-No Street Lights/Street Lights Off Dr	y Not at intersection/Driveway/Roundabout intersection Related but Not at Intersection at At Driveway or Intersection y At Driveway or Intersection	No Risky Driving Behaviors Involved Distracted Driving Inattention Inattention	No No No No
7249 1022483.57 994625.85 2018 7250 1023021.73 994110.66 2014 7251 1023402.43 993738.45 2018	1 Vehicle Party Struck Object	Minor or Possible Injury Inattention 25:34 Property Damage Only Other/Unknown 65+ Minor or Possible Injury Speeding 66+	No Dr No Dr No W No Dr	ry Daylight let Daylight ry Dark-No Street Lights/Street Lights Off	At Driveway or Intersection Straight & Level At Driveway or Intersection Not at Intersection/DrivewayRoundabout Straight & Level Not at Intersection/DrivewayRoundabout Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+	Inattention Inattention None Speeding	Daylight Dr	y At Driveway or intersection At Driveway or Intersection Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout	Inattention Inattention No Risky Driving Behaviors Involved Speeding	Yes No No Yes
7253 1023777.03 993372.4 2017	4 2 Vehicles/Parties Sideswipe 7 2 Vehicles/Parties Rear-End 9 3 Vehicles/Parties Rear-End 8 2 Vehicles/Parties Rear-End 7 1 Vehicles/Party Arimal	Property Damage Only Over Center Line 65+ Property Damage Only Follow Too Closely 65+ Minor or Possible Injury Inattention 25-34 Property Damage Only Follow Too Closely 45-54 Property Damage Only Oher/Unknown 45-54	No Ion/Sinot No Dr. No Dr. No Dr. No Dr. No Dr. No Dr.	w/Slush Dark-No Street Lights /Street Lights Off ry Daylight ry Daylight ry Daylight ry Daylight	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Curve & Grade Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Grade	Older Road User 65+ Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Follow Too Closely Inattention		wSlush Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Follow Too Closely Inattention Follow Too Closely No Risky Driving Behaviors Involved	No No No No
7257 1024907.75 992239.34 2014 7258 1024907.42 992239.69 2014 7259 1025208.29 991915.36 2015 7260 1025410.39 991691.21 [2014	1 Vehicle/Party Struck Object 1 Vehicle/Party Animal	Property Damage Only Inattention 25:34 Property Damage Only OtherUnknown 25:34 Property Damage Only OtherUnknown 55:64 Property Damage Only OtherUnknown 25:34	No Dr No Dr No Dr No Dr	ry Daylight ry Dark-No Street Lights/Street Lights Off ry Daylight ry Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Curve & Grade Not at Intersection/Driveway/Roundabout Straight & Grade	No Vuinerable Road User Involved No Vuinerable Road User Involved No Vuinerable Road User Involved No Vuinerable Road User Involved	None	Dark-No Street Lights/Street Lights Off Dr Daylight Dr Dark-No Street Lights/Street Lights Off Dr	y Not at intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout	Inattention No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	Yes No No No
7262 1025756.62 991170.25 2018 7263 1025809.45 991082.72 2019 7264 1026170.64 990540.07 2016	6 2 Vehides/Parties Sideswipe 8 2 Vehides/Parties Rear-End 9 4 Vehides/Parties Rear-End 6 1 Vehide/Party Struck Object	Minor or Possible Injury Other/Unknown 25-34 Minor or Possible Injury Speeding 65+ Minor or Possible Injury Follow Too Closely 35-44 Property Damage Only Speeding 65+	No Dr No Dr No Dr No W	ry Daylight ry Daylight ry Daylight et Dark-Street Lights On	Not at intersection/Driveway/Roundabout Curve & Grade Not at Intersection Related but Not at Intersection Not at Intersection/Driveway/Roundabout Curve & Grade Not at Intersection/Driveway/Roundabout Curve & Grade	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+	Speeding Follow Too Closely Speeding	Daylight Dr Daylight Dr Daylight Dr Dark-Street Lights On Wr	y Not at Intersection/Driveway/Roundabout y Intersection Related but Not at Intersection y Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Speeding Follow Too Closely Speeding	Yes No No Yes
7266 1026383.27 990303.1 2014 7267 1026383.27 990303.1 2014 7268 1026362.4 990322.56 2015	4 2 Vehicles-Parties Tuming 4 2 Vehicles-Parties Angle 6 2 Vehicles-Parties Angle 9 3 Vehicles-Parties Rear-End 8 1 Vehicles-Party Arrival Vehicles-Party Arrival 1 Vehicle-Party Vehicl	Minor or Possible Injury Inatlention 55-64	No Wh No Dr. No Wh No Wh No Wh No Dr.	et Daylight ry Daylight et Daylight let Daylight ry Dark-No Street Lights/Street Lights Off	Al Drinwayor Intersection Cure & Grade Al Drinwayor Intersection Straight & Grade Al Drinwayor Intersection Straight & Grade Al Drinwayor Intersection Cure & Level Al Drinwayor Intersection Cure & Grade Not at Intersection Straight & Level	No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+ No Vulnerable Road User involved No Vulnerable Road User involved No Vulnerable Road User involved	Improper Turn/Merge Fail to Yield Speeding	Daylight Dr Daylight Wi Daylight Wi	et Al Driveway or Intersection y Al Driveway or Intersection et Al Driveway or Intersection et Al Driveway or Intersection y Not at Intersection/Driveway/Roundabout	Inattention No Risky Driving Behaviors Involved Fail to Yield Speeding No Risky Driving Behaviors Involved	No No No
7270 1026950.8 998888.36 2016 7271 1027368.22 989784.32 2014 7272 1027418.47 989785.99 2016 7273 1027412.99 989785.15 2016	6 1 Vehicle/Party Struck Object 4 2 Vehicles/Parties Angle 5 2 Vehicles/Parties Sideswipe 6 2 Vehicles/Parties Angle	Mnor or Possible Injury Inattention Under Mnor or Possible Injury Fail to Yield 65+ Property Damage Only Oher Unknown 55-64 Property Damage Only Improper Turn/Merge 65+	25 No Dr.	ry Daylight ry Daylight ry Daylight ry Daylight	Not at Intersection/Driveway/Roundabout Curve & Grade At Driveway or Intersection Curve & Level At Driveway or Intersection Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+	hattention Fail to Yield Other Contributing Circ Not Listed Improper Turn/Merge	Daylight Dr Daylight Dr Daylight Dr Daylight Dr	y Not at Intersection/Driveway/Roundabout y At Driveway or Intersection y At Driveway or Intersection y At Driveway or Intersection	Inattention Fail to Yield No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	Yes No Yes No
7274 1027414.34 999786 2017 7275 1027413.94 989783.16 2017 7276 1027417.84 989782.81 2019 7277 1027461.96 989783.45 2019	9 2 Vehicles/Parties Angle 5 2 Vehicles/Parties Angle	Property Damage Only Inattention 25:34 Minor or Possible Injury Fail to Yield 65+ Property Damage Only Fail to Yield 65+ Property Damage Only Fail to Yield 65+	No Dr No Dr No Dr		At Intersection and Not Related Curve & Level At Driveway or Intersection Curve & Level At Driveway or Intersection Curve & Level At Driveway or Intersection Curve & Level	No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+ Older Road User 65+	Fail to Yield Fail to Yield Fail to Yield	Daylight Dr Daylight Dr Daylight Dr	y At Intersection and Not Related y At Driveway or Intersection y At Driveway or Intersection y At Driveway or Intersection At Driveway or Intersection	hattention Fail to Yield Fail to Yield Fail to Yield	Yes No No
7279 1027451.7 989783.29]2013 7280 102781.5 989836.85 2018 7281 1027837.64 9898.48.36 2011	6 1 Vehicla-Party Suruk Object 7 2 Vehiclas-Partes Turning 8 2 Vehicles-Partes Argie 9 2 Vehiclas-Partes 4 Vehicles-Partes	Properly Damage Only OfficerUnknown 55-64 Properly Damage Only Instension 65+ Properly Damage Only Disregard Traffic Control Device/Sign Under Properly Damage Only Instension 25-34 Suspected Serious Injury Speeding 35-44	No W No Dr. Dr. 25 No VW No Dr. No Dr.	ry Daylight let Dark-Street Lights On ry Daylight	A Drinwayor Intersection Cure & Level A Drinwayor Intersection Cure & Level A Drinwayor Intersection Cure & Level A Drinwayor Intersection Cure & Cure & Level A Drinwayor Intersection Cure & Crade Not at Intersection TrinwayARoundabout Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User involved No Vulnerable Road User involved Motorocke	Inattention Disregard Traffic Control Device/Sign Inattention	Daylight Dr Dark-Street Lights On Wi Daylight Dr	et At Driveway or Intersection y At Driveway or Intersection et At Driveway or Intersection y At Driveway or Intersection y Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Inattention No Risky Driving Behaviors Involved Inattention Consoling	No No No No
7283 1028510.87 990209.74 2016 7284 1028516.57 990215.75 2015 7285 1028514.47 990208.07 2015 7286 102898.03 990487.32 2014	6 2 Vehicles/Parties Sideswipe 9 2 Vehicles/Parties Angle 9 2 Vehicles/Parties Angle 4 1 Vehicle/Party Overturned	Property Damage Only	No	ry Daylight ry Daylight ry Daylight ry Daylight ry Daylight	At Driveway or Intersection At Driveway or Intersection Straight & Level At Driveway or Intersection Curve & Level At Driveway or Intersection Straight & Level Not at Intersection/DrivewayRoundabout Straight & Level	No Wilnerable Road User Involved Older Road User 65+ Older Road User 65+ No Wilnerable Road User Involved	mproper Turn/Merge Fail to Yield Improper Turn/Merge Improper Turn/Merge Inattention	Daylight Dr. Daylight Dr. Daylight Dr. Daylight Dr. Daylight Dr. Daylight Dr.	y At Driveway or Intersection y At Driveway or Intersection	No Risky Driving Behaviors Involved Fail to Yield No Risky Driving Behaviors Involved Inattention	Yes No No Yes
7287 1029335.26 990745.73 2018 7288 1029397.45 990779.68 2019 7289 1029652.78 991078.09 2011 7290 1029692.72 991149.26 2017	8 2 Vehicles/Parties Rear-End 9 1 Vehicles/Parties Rear-End 7 3 Vehicles/Parties Rear-End 7 2 Vehicles/Parties Turning	Minor or Possible Injury Inattention 25:34 Property Damage Only Other/Unknown 45:54 Property Damage Only Follow Too Closely 35:44 Property Damage Only Improper Passing 65+	No Dr No Will No Dr No Dr	ry Daylight let Dark-Street Lights On ry Daylight ry Daylight	Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Curve & Grade Not at Intersection/Driveway/Roundabout Straight & Level At Driveway or Intersection Curve & Level		hattention None Follow Too Closely Improper Backing/Passing/Turning/Parking/Signaling	Daylight Dr	y Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout y At Driveway or Intersection	Inattention No Risky Driving Behaviors Involved Follow Too Closely No Risky Driving Behaviors Involved	No No No
7292 1029693.45 991149.29 2017 7292 1029693.45 991149.29 2017 7293 1029761.43 991296.65 2018 7294 1029751.05 991304.96 2016 7295 1030025.06 992153.31 2017	7 1 Vehicle Party Overtumed 4 1 Vehicle Party Struck Object 5 2 Vehicles Parties Rear-End 5 1 Vehicle Party Struck Object 6 1 Vehicle Party Struck Object 6 4 Vehicles Parties Head-On	Minor or Possible Injury Speeding 55-64 Property Damage Only Speeding Under Influence of Alcohol/Drugs Medication Property Damage Only Under Influence of Alcohol/Drugs Medication Property Damage Only Speeding 45-54 Suspeeded Serious Injury Dev Center Line 65+	25 No Dr Under Influence of Alcohol 45-54 Ice/Snov	w/Slush Dark-No Street Lights/Street Lights Off	At Drivewayor Intersection Curve & Grade At Intersection and Not Related Curve & Grade At Drivewayor Intersection Curve & Grade At Intersection and Not Related Curve & Grade At Intersection and Not Related Curve & Grade Not at Intersection DrivewayRoundabout Curve & Level	Motorcycle No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+	Speeding Speeding Under Influence of Alcohol/Drugs/Medication Speeding Lane Departure	Daylight Dr Dark-No Street Lights/Street Lights Off Ice/Snov Dark-No Street Lights/Street Lights Off Ice/Snov	iknown At Driveway or Intersection y At Intersection and Not Related wSlush At Driveway or Intersection wSlush At Intersection and Not Related at Not at Intersection Priveway/Roundabout	Speeding Under Influence of Alcohol/Drugs/Medication Speeding Under Influence of Alcohol/Drugs/Medication Speeding No Risky Driving Behaviors Involved	Yes No Yes Yes
7297 1030423.98 992785.8 2016 7298 1030588.19 993034.51 2018	6 4 WhitesEParies Head-On 7 3 WhitesEParies Rear-End 6 6 2 WhitesEParies Turning 8 1 WhiteSPary Struck Object or Debris On Road 9 9 1 WhiteSPary Avinal 9 2 WhitesEParies Turning 9 2 WhitesEParies Turning 9	Suspected Serious Injury Over Center Line 65+ Minor or Possible Injury Hasterion 55- Minor or Possible Injury Fail to Yield 35-44 Property Damage Only OtherUnknown 25-34 Property Damage Only OtherUnknown 45-54	No De No Wi	et Daylight ry Daylight	Not at Intersection On-ewayRoundabout Cure & Level And at Intersection On-ewayRoundabout Straight at Hillicrest. At Driewayor Intersection Straight at Level Not at Intersection On-ewayRoundabout Straight & Level Not at Intersection On-ewayRoundabout Straight & Level Not at Intersection On-ewayRoundabout Straight & Level Not at Intersection On-ewayRoundabout		Inattention Fail to Yield None	Daylight Dr	at Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout At Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Inattention Fail to Yield No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No No No
7301 1030909.82 993516.88 2019 7302 1030972.04 993610.28 2014	9 1 Vehicle/Party Animal	Property Damage Only OherUnknown 45.54 Minor or Possible Inlury mpoer TurnMerge 45.64 or Property Damage Only OherUnknown 25.34 or Property Damage Only Speeding 45.64 or Property Damage Chiy OherUnknown 65.e	No Dr No Dr No W No Dr Lt Dr	ry Daylight let Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved	Speeding	Daylight Dr Dark-No Street Lights/Street Lights Off Wi	y Not at Intersection/Driveway/Roundabout at Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Speeding No Risky Driving Behaviors Involved Speeding No Risky Driving Behaviors Involved	No No Yes No
7305 1032074.73 995520.28 2018 7306 1032127.78 995617.8 2015 7307 1032203.31 995756.66 2014	8 2 Vehicles/Parties Sideswipe 5 2 Vehicles/Parties Sideswipe	Property Damage Only		ry Daylight	Not at htersection Ofneway/Roundabout Straight & Level	No Vulnerable Road User Involved Older Road User 65+	Fail to Yield Other Contributing Circ Not Listed Other Contributing Circ Not Listed Follow Too Closely	Dark-No Street Lights/Street Lights Off Dr Dark-No Street Lights/Street Lights Off Dr Daylight Dr Daylight Dr Daylight Dr Daylight Dr Daylight Dr Daylight Dr Daylight Dr	y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Fail to Yield No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Follow Too Closely Follow Too Closely	Yes Yes Yes No
7309 1032457.77 996224.5 2014 7310 1032718.6 996704.36 2018	1 Vehicle-Party Asimal	Property Damage Only Oher/Unknown 65+ Minor or Possible Injury Speeding 45-54 Property Damage Only Oher/Unknown 55-64		ry Daylight	Not at Intersection/Driveway/Roundabout Straight & Level Intersection Related but Not at Intersection Straight & Level Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Level	Older Road User 65+	None Speeding None	Daylight Dr Daylight Dr Daylight Ice/Snov	v/Slush Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Speeding No Risky Driving Behaviors Involved Speeding	No No No Yes
7314 1033206.91 997604.82 2018 7315 1033382.83 997929.21 2019	7 Wehidas-Party Sudexwipe 9 4 Wehidas-Party Sudexwipe 9 4 Wehidas-Partes Sudexwipe 9 4 Wehidas-Partes Ras-End 5 2 Wehidas-Partes Ras-End 7 1 Wehidas-Party Suminal 8 2 Wehidas-Partes Tuminal 7 1 Wehidas-Partes 1 1 1 1 1 1 1 1 1	Monor of Possible Injury Speeding 65+	No Icolson No Dr No Dr S No Dr Dr Dr	ry Daylight ry Dark-No Street Lights/Street Lights Off ry Dawn/Dusk	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level Intersection Related but Not at Intersection Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User 1000-1000-1000-1000-1000-1000-1000-100	Fail to Yield Other Contributing Circ Not Listed Follow Too Closely	Daylight Dr. Dark-No Street Lights/Street Lights Off Dr. Dawn/Dusk Dr.	wStush Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout y Intersection Related but Not at Intersection Not at Intersection Polymeration	Speeding Fail to Yield No Risky Driving Behaviors Involved Follow Too Closely No Risky Driving Behaviors Involved	Yes Yes No No
7319 1034399.42 999302.72 2019	9 2 Vehicles/Parties Sideswipe 6 1 Vehicle/Party Animal 8 2 Vehicles/Parties Rear-End	Property Damage Unity Improper i unrivierge 65+ Suspected Serious Injury Apparently Asleep, Fatigued or III Under Property Damage Only Other/Unknown 65+ Minor or Possible Injury Inattention 25-34	No Dr. 25 Apparently Asleep, Fatigued or III Under 25 Dr. No Dr. No Dr.	ry Daylight ry Dark-No Street Lights/Street Lights Off ry Dark-No Street Lights/Street Lights Off ry Daylight ry Daylight ry Daylight	Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level	Older Road User 65+ No Vulnerable Road User Involved	Improper Backing/Passing/Turning/Parking/Signaling Apparently Asleep or Fatigued None Institution	Daylight Dr. Dark-No Street Lights/Street Lights Off Dr. Dark-No Street Lights/Street Lights Off Dr. Daylight Dr. Daylight Dr.	y Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Apparently Asleep, Fatigued or III No Risky Driving Behaviors Involved Inattention	No Yes No No
7324 1035607.08 999791.72.2011 7324 1035608.31 999833.74 2016 7325 1035605.89 999832.7 2016	7 5 venicles/Parties Sideswipe 6 2 Vehicles/Parties Sideswipe 9 2 Vehicles/Parties Rear-End	Property Damage Only Distracted Driving 55-64	No Dr No Dr No Dr No Dr No Dr No Dr	ry Daylight	Intersection Related but Not at Intersection Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level At Driveway or Intersection Straight & Level At Driveway or Intersection Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Non Motorist on Wrong Side of Road		y Intersection Related but Not at Intersection y Not at Intersection/Driveway/Roundabout y At Driveway or Intersection y At Driveway or Intersection y Not at Intersection/Driveway/Roundabout	Distracted Driving Instention No Risky Driving Behaviors Involved Instention Un Driving Rehaviors Involved Un Driving Rehaviors Involved	Yes Yes Yes No
7327 1036092.69 1000041.23 2018 7328 1036334.81 1000144.94 2014	4 2 Vehrloes/Parties Sideswipe 5 1 Vehrloe/Parties Struck Object 4 1 Vehrloe/Party Struck Object 8 1 Vehrloe/Party Struck Object 8 1 Vehrloe/Party Struck Object 8 1 Vehrloe/Party Struck Object 9 1 Vehrloe/Party Struck Obje	Property Damage Only Speeding 35-44 Property Damage Only Speeding 55-64 Property Damage Only Speeding 65* Property Damage Only Speeding 65* Property Damage Only Apparently Aeleep, Fatigued or III 45-54	No lee/Sno No lee/Sno No lee/Sno Apparently Asleep, Fatigued or III 45-54 W W	w/Slush Dark-No Street Lights/Street Lights Off w/Slush Daylight w/Slush Dark-No Street Lights/Street Lights Off et Daylight	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Curve & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved	Speeding Speeding Speeding Apparently As leep or Fatigued	Dark-No Street Lights/Street Lights Off Ice/Snor Daylight Ice/Snor Dark-No Street Lights/Street Lights Off Ice/Snor Daylight W	wSlush Not at Intersection/Driveway/Roundabout wSlush Not at Intersection/Driveway/Roundabout wSlush Not at Intersection/Driveway/Roundabout at Not at Intersection/Driveway/Roundabout	No Risky Driving Behawors Involved Speeding Speeding Speeding Speeding Speeding Speeding Speeding Speeding Speeding	Yes Yes Yes Yes
7331 1036578.85 1000243.12 2017 7332 1036828.63 1000320.04 2018	8 1 Vehicle/Party Struck Object 4 1 Vehicle/Party Struck Object	Property Damage Only Inattention 65* Mnor or Possible Injury Speeding Under Property Damage Only Speeding 35-44 Mnor or Possible Injury Speeding 25-34	No Dr 25 No Wi	ry Daylight let Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Straight & Level	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Speeding Speeding	Daylight Dr	y Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout	Inattention Speeding Speeding Speeding Speeding	Yes Yes Yes No
7335 1038207.93 1000315.7 2019 7336 1038277.2 1000315.94 2014		Property Damage Only Follow Too Closely 65+ Minor or Possible Injury Other/Unknown Under	25 No Dr	w/Slush Dark-No Street Lights/Street Lights Off ry Daylight ry Daylight ry Daylight let Daylight	Not at Intersection-Driveway/Roundabout Straight & Level At Driveway or Intersection Straight & Level R Intersection and Not Related Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User involved Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Follow Too Closely Other Contributing Circ Not Listed	Daylight Dr	wSlush Not at Intersection/Driveway/Roundabout y At Driveway or Intersection y At Driveway or Intersection y At Driveway or Intersection at Intersection and Not Related	Speeding Follow Too Closely No Risky Driving Behaviors Involved Inathention No Risky Driving Behaviors Involved Secretification	No No No No
7340 1038303.11 1000315.2014 7341 1038301.62 1000315.07 2015 7342 1038278.29 1000313.7 2015 7343 1038278.29 1000313.7 2015	4 Z venicias/r-aries Rear-End 9 1 Vehicle/Party Struck Object 9 2 Vehicle/Party Struck Object 9 1 Vehicle/Party Struck Object	Minor or Possible Injury Speeding Under	25 No Dt 25 No Dt	ry Daylight ry Daylight ry Daylight ry Daylight	At Driveway or Intersection Straight & Level At Driveway or Intersection Straight & Level At Intersection and Not Related Straight & Level At Intersection and Not Related Curve & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	Speeding Improper Turn/Merge Speeding Other Contributing Circ Not Listed	Daylight Dr Daylight Dr Daylight Dr Daylight Dr	y At Driveway or Intersection At Driveway or Intersection y At Intersection and Not Related y At Intersection and Not Related	No Risky Driving Behaviors Involved Speeding No Risky Driving Behaviors Involved	No Yes No Yes
7344 1039319.81 1000272.32 2015 7345 1039355.37 1000279.32 2017 7346 103998.09 1000258.08 2017 7347 1040198.76 1000250.97 2014	9 1 Vehicle/Party Animal 7 2 Vehicles/Parties Pedalcyclist 7 1 Vehicle/Party Over Embankment - No Guardraii Present 4 1 Vehicle/Party Struck Object	Minor or Possible Injury Other/Unknown 65+ Suspected Serious Injury Under Influence of Alcohol/Drugs/Medication 45-54 Property Damage Only Operating Defective Equipment 35-44 Property Damage Only Under Influence of Alcohol/Drugs/Medication 25-34	Under Influence of Drugs	let Dark-No Street Lights/Street Lights Off ry Daylight will such Dark-No Street Lights/Street Lights Off ry Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Level	Older Road User 65+ Pedaloydist No Vulnerable Road User Involved No Vulnerable Road User Involved	None Under Influence of Alcohol/Drugs/Medication Operating Defective Equipment Apparently Asleep or Fatigued	Dark-No Street Lights/Street Lights Off Will Daylight Dr. Dark-No Street Lights/Street Lights Off Ice/Snov	et Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout wSlush Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication	No No Yes Yes
7349 1040198.76 1000230.97.2018 7349 1040408.96 1000243.87 2018 7350 1040935.89 1000226.09 2018 7351 1040943.46 1000222.85 2018	5 1 Vehicle/Party Struck Object 8 2 Vehicles/Parties Turning 5 2 Vehicles/Parties Angle 6 2 Vehicles/Parties Angle	Property Damage Only Apparenty Aeleep, Faigued or III Under Property Damage Only Property Damage Only Improper Turn/Merge 25-34. Minor or Possible Injury Fail to Yeld 65+ Property Damage Only Fail to Yeld Under Under Minor or Possible Injury Fail to Yeld	25 Apparenty Astreep, Fatigues of III	ry Daylight let Dark-No Street Lights/Street Lights Off let Daylight let Daylight		No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+	Apparently As leep or Fatigued Improper Backing/Passing/Turning/Parking/Signaling Fail to Yield Fail to Yield Fail to Yield	Dark-No Street Lights/Street Lights Off With Daylight W. Daylight W.	y Not at intersection/Driveway/Roundabout Not at intersection/Driveway/Roundabout Not at intersection/Driveway/Roundabout At Driveway or intersection At Driveway or intersection At Driveway or intersection	Apparently Asleep, Fatigued or III No Risky Driving Behaviors Involved Fail to Yield Fail to Yield Fail to Yield	No No No No
7353 1040935.89 1000226.09 2014 7354 1040944.36 1000221.66 2015 7355 1040935.89 1000226.09 2016 7356 1040935.89 1000226.09 2016	4 1 VehideParty Animal 5 2 Vehides/Parties Angle 6 2 Vehides/Parties Angle 7 2 Vehides/Parties Rear-End	Property Damage Only Ofher Unknown 25:34 Property Damage Only Fail to Yield 65+ Mnor or Possible Injury Inattention 65+ Property Damage Only Inattention Under	No Dr No Dr 25 No W	ry Daylight ry Daylight let Dark-Street Lights On	Al Intersection and Not Related Straight & Level At Driveway or Intersection Straight & Grade At Driveway or Intersection Straight & Grade At Driveway or Intersection Straight & Grade At Intersection and Not Related Straight & Grade	No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+ No Vulnerable Road User involved	None Fail to Yield Inattention Inattention	Dark-Street Lights On Dr Daylight Dr Daylight Dr Dark-Street Lights On Wi	y At Intersection and Not Related y At Driveway or Intersection y At Driveway or Intersection at Intersection and Not Related	No Risky Driving Behaviors Involved Fail to Yield Inattention Inattention	No No No No
7358 1041252.7 1000215.33 2017 7359 1041250.62 1000216.5 2018 7360 994513.63 1010713.58 2018	7 2 Vehicles/Parties Backing 7 2 Vehicles/Parties Angle 8 1 Vehicle/Party Struck Object 5 1 Vehicle/Party Animal 4 Vehicle/Party Animal 4 Vehicle/Party Struck Chiect	Property Damage Only Improper Backing 55-64 Property Damage Only Improper Turn/Merge 65+ Mnor or Possible Injury Inattention 45-54 Property Damage Only Other/Unknown 35-44	NO Oth No Dr No Dr No Dr	ry Daylight oer Daylight ry Daylight ry Dark-No Street Lights/Street Lights Off	At Driveway or Intersection Straight & Grade Intersection Related but Not at Intersection Straight & Grade	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User involved No Vulnerable Road User involved Older Road User 65+ Older Road User 65+	Improper Turn/Merge Inattention None	Daylight Dr Daylight Other/Ur Daylight Dr Dark-No Street Lights/Street Lights Off Dr	y At Driveway or Intersection intersection Related but Not at Intersection y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Inattention No Risky Driving Behaviors Involved Inattention	No No Yes No
7362 1092684.83 940146.34 2014 7363 1092687.6 940147.51 2016 7364 1092684.83 940146.34 2019	4 1 Vehicle-Party Struck Object 4 2 Vehicles-Parties Rear-End 5 2 Vehicles-Parties Rear-End 9 2 Vehicles-Parties Rear-End 9 2 Vehicles-Parties Agente	Property Damage Only Other/Unknown 65+ Property Damage Only Other/Unknown 35-44	No W No Dr No Dr	ry Daylight ry Daylight	At Driveway or Intersection Straight & Level At Driveway or Intersection Straight & Level At Driveway or Intersection Straight & Level	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User 65+ No Vulnerable Road User involved	Inattention Inattention Other Contributing Circ Not Listed	Daylight Wi Daylight Dr Daylight Dr Daylight Dr Wi Dark-Street Lights On Wi	at At Driveway or Intersection at At Driveway or Intersection by At Driveway or Intersection by At Driveway or Intersection by At Driveway or Intersection at Intersection Related but Not at Intersection	Inattention Inattention Inattention Inattention Inattention No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No No No
7369 1092870.74 940672.53 2017	7 1 Vehicle/Party Struck Object	Minor or Possible Injury Apparently Asteep, Fatigued or III 35-44 Property Damage Only Inattention 35-44 Property Damage Only Improper Passing 35-44 Property Damage Only Index Indicators 65-64 Property Damage Only Indicators 10-64 Property Damage Only	Under Influence of Acohol 65+ Dr	et Daylight ry Daylight ry Daylight	Intersection Related but Not at Intersection Straight & Level At Driveway or Intersection Straight & Level At Driveway or Intersection Straight & Level Not at Intersection(Driveway/Roundabout) Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	Apparently Asleep or Fatigued Inattention Improper Backing/Passing/Turning/Parking/Signaling Under Influence of Alcohol/Drugs/Medication	Daylight Dr. Daylight VW Daylight Dr. Daylight Dr. Daylight Dr.	y Intersection Related but Not at Intersection et At Driveway or Intersection y At Driveway or Intersection y Not at Intersection/Driveway/Roundabout	Apparently Asleep, Fatigued or III Inattention No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication	No No No Yes
7370 1092885.81 940733.57 2015 7371 1092942.05 940898.28 2015 7372 109323188 941679.72 2015	9 1 Vehicle/Party Struck Object 5 2 Vehicles/Parties Sideswipe 5 1 Vehicle/Party Struck Object 6 1 Vehicle/Party Animal	Property Damage Only Apparently Asleep, Fatigued or III 65+ Property Damage Only Follow Too Closely 45-54 Property Damage Only Under Influence of Alcohol/Drugs/Medication 55-64 Property Damage Only Other/Unknown 55-64	Apparently Asleep, Fatigued or III 65+ Dr No Dr Under Influence of Acohol 55-64 Dr No Dr	ry Daylight ry Daylight ry Daylight ry Daylight ry Dark-No Street Lights/Street Lights Off	At Driveway or Intersection Straight & Level Driveway Related but Not at Driveway Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Grade	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Apparently As leep or Fatigued Follow Too Closely Under Influence of Alcohol/Drugs/Medication None	Daylight	y At Driveway or Intersection Driveway Related but Not at Driveway y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout	Apparently Asleep, Fatigued or III Follow Too Closely Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved	Yes Yes Yes No
7373 1053378.47 942149.82 2017 7374 1053902.16 943649.83 2017 7375 1094076.96 944150.03 2017 7376 1094373.61 94500.04 2017 7376 1094373.61 94500.04 2017 7377 1094425.95 945150.62 2017	5 1 Vehicle-Party Animal 9 1 Vehicle-Party Animal 5 1 Vehicle-Party Struck Object 5 1 Vehicle-Party Struck Object 1 Vehicle-Party Struck Object 3 Vehicle-Party Struck Object	Property Damage Only Other Unknown 65-64 Property Damage Only Other Unknown Under Property Damage Only Speeding 55-64 Property Damage Only Distracted Driving Under Unknown Speeding Unknown	25 No William Link 10	ry Daylight et Dark-No Street Lights/Street Lights Off w/Slush Dark-No Street Lights/Street Lights Off et Dark-No Street Lights/Street Lights Off	Not at Intersection DnewayRoundabout Straight & Level	No Vulnerable Road User Involved	None Speeding Unknown Distraction Speeding	Daykight Dr. Dark-No Street Lights/Street Lights Off Will Dark-No Street Lights/Street Lights Off Ice/Snor Dark-No Street Lights/Street Lights Off Will Daykight Will Daykight Will Daykight	st Not at Intersection/Driveway/Roundabout wSlush Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Speeding Distracted Driving	No Yes Yes

OBJECTID JURISDICTION	COUNTY CITY	PRIMARY BLOCK TRAFFICWAY NUMBER	INTERSECTING FROM ML.or FROM TRAFFICWAY FROM FR.EF.POINT	REFERENCE MILEPOST A HISTORY B SUSPENSE IND NUMBER	DATE TIME INTUDY TYPE	NUM NUM NUM NUM PEDS B	WER VEH_1_TTPE	VEH_2_TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD_SURFACE ROADWAY CHARACTERISTIC	LIGHTING CONDITION	RRST_COLLISION TYPE_OBJECT_STRUCK	VEH_1 ACTION	VEH_2 ACTION		H.1.COMPASS VEH.2.COMPASS VEH.2.COMPASS DIRECTION_TO DIRECTION_FROM DIRECTION_TO
7379 Cate Poole 7381 State Route 7381 State Route 7381 State Route 7383 State Route 7383 State Route 7385 State Route 7385 State Route 7386 State Route 7387 State Route 7387 State Route 7388 State Route 7389 State Route 7389 State Route 7389 State Route	Jefferson Jefferson	019 019		1.00 No E822762 2 1.00 No E892048 2	018-07-28 17:43 No Apparent Injury 019-02-04 06:00 No Apparent Injury 019-07-19 20:14 Possible Injury 017-12-24 19:42 No Apparent Injury 019-09-16 17:05 Suspected Minor Injury 019-10-15 06:49 No Apparent Injury	0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Snowing	Dry Straight & Grade Ice Straight & Grade	Daylight Dark-No Street Lights	Vehicle Strikes Deer Roadway Ditch	Going Straight Ahead Going Straight Ahead Going Straight Ahead		North South North South	i n
7381 State Route 7382 State Route	Jefferson (019 019		1.00 No E945781 2 1.05 No E753079 2	019-07-19 20:14 Possible Injury 017-12-24 19:42 No Apparent Injury	0 0 1 0	Motorcycle Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Snowing	Dry Straight & Level Snow/Slush Curve & Grade	Daylight Dark-No Street Lights	Vehicle overturned Over Embankment - No Guardrail Present	Going Straight Anead		South North South North	1
7383 State Route 7384 State Route 7385 State Route	Jefferson	019 019 019		1.09 No E839228 2 1.18 No E971205 2 1.20 No E888648 2	018-09-16 17:05 Suspected Minor Injury 019-10-15 06:49 No Apparent Injury 018-11-08 06:40 No Apparent Injury	0 0 1 0	0 Passenger Car 0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve at Hillcrest Dry Straight & Grade Dry Straight & Grade		Tree or Stump (stationary) Vehicle Strikes Deer Vehicle Strikes All Other Non-Domestic Animal	Going Straight Ahead Going Straight Ahead Going Straight Ahead		South North South North South North	
7386 State Route 7387 State Route	Jefferson	019 019			019-10-15 06:49 No Apparent Injury 018-11-08 06:40 No Apparent Injury 015-12-29 21:15 No Apparent Injury 017-06-28 08:39 No Apparent Injury				Not at Intersection and Not Related Not at Intersection and Not Related	Fog or Smog or Smoke Clear or Partly Cloudy	Wet Straight & Grade Dry Straight & Level	Dark-No Street Lights Davlight	Roadway Ditch Roadway Ditch	Going Straight Ahead Going Straight Ahead		North South	h
7388 State Route 7389 State Route	Jefferson (019 019		1.41 No E553878 2 1.47 No E608859 2	016-06-05 15:10 Suspected Minor Injury 016-11-10 04:35 No Apparent Injury	2 0 1 0	O Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Fog or Smog or Smoke Clear or Partly Cloudy	Dry Straight & Grade Wet Curve & Grade	Daylight Dark-No Street Lights Daylight	Rock Bank or Ledge Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead		North South South North	
7390 State Route 7391 State Route	Jefferson (019 019		1.50 No E880046 2 1.53 No E926308 2	019-01-01 15:34 No Apparent Injury 019-06-01 17:06 Possible Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Motorcycle		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Grade	Daylight Daylight	Roadway Ditch Roadway Ditch			North South North South	à l
7391 State Route 7392 State Route 7392 State Route 7393 State Route 7394 State Route 7394 State Route 7396 State Route 7396 State Route 7397 State Route 7398 State Route 7398 State Route 7399 State Route	Jefferson	019 019		1.53 No E938491 2 1.58 No E846991 2 1.50 No E846991 2	13-80-101 (13-38) No Apparent nayor 19-80-60 (17-36) Possable hipury 19-80-70 (99-35) Possable hipury 19-80-70 (19-80) (19-80) (19-80) (19-80) (19-80) 19-80-70 (19-80) (19-80) (19-80) (19-80) (19-80) 19-80-70 (19-80) (19-80) (19-80) (19-80) (19-80) 19-80-70 (19-80) (19-80) (19-80) (19-80) (19-80) (19-80) 19-80-70 (19-80) (19-80) (19-80) (19-80) (19-80) (19-80) (19-80) 19-80-70 (19-80) (1	0 0 1 0	0 Truck (Flatbad, Van, etc) 0 Passenger Car 0 Truck (Flatbad Van etc)	Not Stated	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade Dry Curve & Grade Dry Curve & Level	Daylight Dark-Street Lights On	Noadray Distri. Noadray Distri. From same direction - both going straight - both moving - sideswipe Whileic overturned Over Embankment - No Guardrail Present	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	North South South North	North South
7395 State Route 7396 State Route	Jefferson (019 019		1.62 No E414170 2 1.63 No E340326 2	015-04-02 15:38 No Apparent Injury	0 0 3 0	0 Truck Tractor & Semi-Trailer 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy				Going Straight Ahead	Going Straight Ahead Making Left Turn	North South	North South
7397 State Route 7398 State Route	Jefferson Jefferson Jefferson	019 019		1.63 No E588125 2 1.63 No E603580 2	016-09-16 16:00 Suspected Serious Injury 016-11-01 17:43 Possible Injury	2 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Overcast Clear or Partly Cloudy	Dry Straight in Sag Wet Straight & Level	Daylight Dark-Street Lights On	From same direction - one left turn - one straight Entering at langle From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Making Left Turn Going Straight Ahead	Going Straight Ahead Stopped for Traffic	East South North South	South North North Vehicle Stopped Vehicle Stopped
7399 State Route 7400 State Route 7401 State Route 7402 State Route	Jefferson (019 019		1.63 No E934704 2 1.70 No E990424 2	019-04-16 08:29 Possible Injury 019-12-02 17:25 No Apparent Injury	1 0 2 0 0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear	Dry Straight & Level Wet Straight & Level			Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic	North South North South	North East
7401 State Route 7402 State Route	Jefferson	019 019		1.89 No E685007 2	017-06-24 16:53 No Apparent Injury	0 0 1 0	0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy	Wet Straight & Level Dry Curve & Level Dry Curve & Level	Daylight Daylight	Roadway Ditch Vehicle Strikes Deer Vehicle Strikes Deer	Going Straight Ahead		North South South North	
7403 State Route 7404 State Route 7405 State Route 7405 State Route 7406 State Route	Jefferson	019 019		2.59 No E440133 2 2.60 No E583882 2	014-11-22 21:00 No Apparent Injury 015-06-28 13:45 Possible Injury 016-09-06 15:30 No Apparent Injury 019-12-29 13:44 No Apparent Injury	2 0 2 0	0 Passenger Car 0 Truck (Pathad Van etc)	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Intersection Related but Not at Intersection At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade	Dark-No Street Lights Daylight Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic Stopped for Traffic	South North	Vehicle Stopped Vehicle Stopped
7406 State Route 7407 State Route	Jefferson Jefferson	019 019		2.60 No E998706 2 2.61 No E323208 2	019-12-29 13:44 No Apparent Injury 014-04-21 10:50 Suspected Minor Injury	0 0 2 0	Passenger Car Truck Tractor	Pickup,Panel Truck or Vanette under 10,000 lb Truck Tractor	Intersection Related but Not at Intersection At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Grade	Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end From same direction - one left turn - one straight	Going Straight Ahead Going Straight Ahead	Slowing Making Left Turn	South North South North	South North a South West
7408 State Route 7409 State Route	Jefferson (019 019		2.61 No E528342 2 2.70 No E609394 2	016-03-16 17:46 Suspected Serious Injury 016-11-09 20:05 Possible Injury	1 0 3 0	O Truck Tractor Pickup Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Passenger Car Passenger Car Passenger Car Passenger Car	Passenger Car	At Intersection and Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level Dry Curve & Grade	Daylight Dark-No Street Lights	From same direction - one left turn - one straight Vehicle overturned Roadway Ditch Tree or Stump (stationary)	Going Straight Ahead	Stopped in Roadway	South North North South	h Vehicle Stopped Vehicle Stopped
7410 State Route 7411 State Route	Jefferson (019 019		2.71 No E467211 2 2.71 No E551960 2	015-09-26 00:35 Possible Injury 016-06-08 17:50 Possible Injury	1 0 1 0	Passenger Car Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Wet Curve & Level Dry Curve & Grade	Dark-No Street Lights Daylight	Roadway Ditch Tree or Stump (stationary)	Going Straight Ahead Going Straight Ahead Going Straight Ahead		North South North South	i .
7412 State Route 7413 State Route 7414 State Route	Jefferson Jefferson Jefferson Jefferson	019 019		2.90 No E604480 2 2.91 No E831968 2 3.00 No E232682 2	018-08-25 00:29 Suspected Minor Injury	1 0 1 0	U Passenger Car O Pickup, Panel Truck or Vanette under 10,000 lb O Pickup, Panel Truck or Vanette under 10,000 lb O Pickup, Panel Truck or Vanette under 10,000 lb O Pickup, Panel Truck or Vanette under 10,000 lb O Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level	Dark-No Street Lights Dark-No Street Lights Dark-No Street Lights	venicie strikes Deer Roadway Ditch Mikiela Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead		North South	1
7415 State Route 7416 State Route	Jefferson Jefferson	019 019		3.00 No E658682 2 3.01 No E808710 2	017-03-25 16:04 No Apparent Injury 018-06-11 19:13 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight	Vehicle overturned Roadway Ditch	Going Straight Ahead Going Straight Ahead		Southeast West North South	h
7400 Stells Route 7410 Stells Route 7411 Stells Route 7411 Stells Route 7411 Stells Route 7414 Stells Route 7414 Stells Route 7414 Stells Route 7415 Stells Route 7416 Stells Route 7417 Stells Route 7418 Stells Route 7418 Stells Route	Jefferson (019 019		3.10 No E949454 2 3.11 No E331933 2	018-06-11 19:13 No Apparent Injury 019-08-11 12:48 Suspected Minor Injury 014-06-01 08:40 No Apparent Injury 017-01-28 07:33 No Apparent Injury 018-06-09 07:37 No Apparent Injury	1 0 1 0	0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade Dry Straight & Level Dry Straight & Level	Daylight Daylight	Tree or Stump (stationary) Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead		North South North South	0
7419 State Route 7420 State Route	Jefferson (019 019							Not at Intersection and Not Related Not at Intersection and Not Related	Raining	Wet Straight & Level Wet Straight & Level	Dawn Daylight	Roadway Ditch Roadway Ditch			South North South North	
7421 State Route 7422 State Route	Jefferson I	019 019		3.32 No E841675 2 3.32 No E841676 2	018-09-24 15:08 No Apparent Injury 018-09-24 15:27 No Apparent Injury	0 0 1 0	O Passenger Car O Pickup,Panel Truck or Vanette under 10,000 lb O Pickup,Panel Truck or Vanette under 10,000 lb O Passenger Car O Passenger Car O Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Daylight Dark-No Street Lights	Other Objects Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead		South North North South	á
7425 State Route 7426 State Route	Jefferson (019		3.60 No E634964 2	017-01-20 17:40 Possible injuly 017-01-20 13:15 Dead at Scene	4 1 3 0	0 Passenger Car 0 Truck (Dathed Van etc)	Truck (Flatbad, Van, etc)	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Dry Curve & Level	Davlight	Noauway Unu i From opposite direction - both moving - head-on	Going Straight Ahead Going Straight Ahead	Going Straight Ahead	South North	North South
742.5 Scata Route 742.5 State Route 742.5 State Route 742.5 State Route 742.6 State Route 742.7 State Route 742.7 State Route 742.8 State Route 742.9 State Route 743.9 State Route 743.1 State Route 743.1 State Route 743.1 State Route	Jefferson Jefferson	019		3.62 No E416476 2 3.70 No E611891 2	015-04-13 15:11 No Apparent Injury 016-11-20 17:50 No Apparent Injury	0 0 1 0	0 Truck (Flatbad, Van, etc) 0 Truck (Flatbad, Van, etc) 0 Truck Tractor & Semi-Trailer 0 Pickup, Panel Truck or Vanette under 10,000 lb 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy	Wet Curve & Level Dry Straight & Level	Daylight Daylight Dark-No Street Lights	Over Embankment - No Guardrall Present Roadway Ditch Roadway Ditch	Going Straight Ahead Going Straight Ahead Going Straight Ahead		South North South North	
7428 State Route 7429 State Route	Jefferson Jefferson	019 019		3.72 No E524238 2 3.83 No E801941 2	016-03-12 16:10 No Apparent Injury 018-05-22 16:36 Possible Injury	0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Truck (Flatbad, Van, etc)	Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end	Slowing	Stopped for Traffic Slowing	South North	Vehicle Stopped Vehicle Stopped South North
7430 State Route 7431 State Route	Jefferson Jefferson	019 019		3.90 No E822102 2 4.00 No E370919 2	018-07-26 21:15 No Apparent Injury 014-10-31 09:20 No Apparent Injury	0 0 1 0	Passenger Car Pickup Panel Truck or Vanette under 10.000 lb		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining	Wet Straight & Level	Daylight	From same direction - both going straight - both moving - rear-end Vehicle Strikes Derr Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead		North South North South	
7432 State Route 7433 State Route 7434 State Route	Jefferson (019 019		4.00 No E533098 2 4.15 No E442893 2	016-04-06 13:46 No Apparent Injury 015-07-12 12:56 Suspected Minor Injury 018-01-31 05:24 No Apparent Injury 019-08-14 15:25 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Curve & Level	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From opposite direction - both going straight - sideswipe	Going Straight Ahead	Stopped for Traffic Going Straight Ahead	South North South North	h Vehicle Stopped Vehicle Stopped h North South
7435 State Route 7436 State Route	Jefferson	019 019		4.25 No E764756 2 4.31 No E951379 2	318-U1-31 U5:24 No Apparent Injury	0 0 1 0 0 0 2 0 0 0 0 0 0 0 0 0 0 0 0 0	Passenger Car	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related Intersection Related but Not at Intersection	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade Dry Straight & Level	Dark-No Street Lights Daylight	Over Embankment - No Guardrail Present From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead	Slowing Stopped in Roadway	North South South	th North South h Vehicle Stopped Vehicle Stopped
7437 State Route 7438 State Route	Jefferson Jefferson	019 019			014-05-07 17:12 No Apparent Injury 016-07-23 14:55 No Apparent Injury	0 0 2 0	Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Passenger Car Passenger Car	And a common of common at the state of	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end From opposite direction - both going straight - sideswipe	Going Straight Ahead Going Straight Ahead Slowing	Stopped for Traffic	South North South North	h Vehicle Stopped Vehicle Stopped h North South
7432 Stath Roote 7433 Stath Roote 7434 Stath Roote 7434 Stath Roote 7435 Stath Roote 7436 Stath Roote 7436 Stath Roote 7437 Stath Roote 7438 Stath Roote 7439 Stath Roote 7439 Stath Roote 7439 Stath Roote 7449 Stath Roote 7440 Stath Roote	Jefferson Jefferson	019 019		4.63 No E731121 2 4.63 No E744490 2	017-11-02 18:57 Possible Injury 017-12-06 11:50 Possible Injury	1 0 2 0	Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Motorcycle	Pickup,Panel Truck or Vanette under 10,000 lb Truck (Flatbad,Van,etc)	At Intersection and Related At Intersection and Not Related	Raining Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level	Dark-No Street Lights Daylight	From same direction - both going straight - one stopped - rear-end From opposite direction - both going straight - sideswipe From same direction - both going straight - one stopped - rear-end From same direction - both going straight - toes thought - both moving - rear-end	Slowing Going Straight Ahead Going Straight Ahead	Going Straight Ahead Stopped for Traffic Slowing	South North South North	South Vehicle Stopped South North
7440 State Route 7442 State Route 7442 State Route 7443 State Route 7444 State Route 7444 State Route 7444 State Route 7445 State Route 7445 State Route 7446 State Route 7447 State Route 7447 State Route 7448 State Route	Jefferson Jefferson	019 019		4.63 No E931771 2 4.65 No E608861 2	019-06-18 17:02 Possible Injury 016-11-15 14:18 No Apparent Injury	1 0 2 0 0 0 1 0	U Motorcycle O Pickup,Panel Truck or Vanette under 10,000 lb O Pickup,Panel Truck or Vanette under 10,000 lb O Passenger Car O Passenger Car O Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Related Not at Intersection and Not Related	Overcast Raining				Going Straight Ahead Going Straight Ahead	Slowing	South North South North	South North
7443 State Route 7444 State Route	Jefferson Jefferson Jefferson	019 019		4.77 No E350369 2 4.83 No E365886 2	014-10-16 20:40 No Apparent Injury 014-10-12 20:55 No Apparent Injury	0 0 2 0	U Passenger Car 0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast	Ury Straight & Level Wet Straight & Grade	Dark-No Street Lights Dark-No Street Lights	Roadway Ditch From opposite direction - both going straight - sideswipe Roadway Ditch Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead	Going Straight Ahead	South North	North South
7445 State Route 7446 State Route 7447 State Poute	Jefferson Jefferson Jefferson	019 019		5.49 No E689403 2 5.51 No E394131 2 5.52 No E892622 2	015-01-22 00:30 Possible Injury	0 0 1 0 1 0 1 0 0 0 1 0	O Passenger Car O Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Raining Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level	Daylight Dark-No Street Lights Daylight	Vehicle Strikes Deer Uilvert and/or ofter /Appurtenance in Ditch Earth Bank or Lodge Uillin/ Pole District Strikes Deer	Going Straight Ahead Going Straight Ahead		North South South North	
7448 State Route 7449 State Route	Jefferson (019 019			015-12-15 10:05 No Apparent Injury	0 0 1 0	0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Fog or Smog or Smoke	Dry Straight & Level Wet Curve & Grade	Dark-No Street Lights Davlight	Utility Pole Suardrail - Face	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead		South North South North	
7449 State Route 7450 State Route 7451 State Route 7452 State Route	Jefferson Jefferson	019 019		6.07 No 3501109 2 6.22 No E899368 2	716-06-23 23:30 INO Apparent injury	0 0 1 0	o rassenger car		Not at Intersection and Not Related Driveway Related but Not at Driveway	Overcast Overcast	Dry Straight & Level Wet Straight & Level	Dark-No Street Lights Dark-No Street Lights	Guardrail - Face Dear Embanshment - No Guardrail Present Roadway Ditch Culied and/or other Appurtenance in Ditch Utility Pole	Going Straight Ahead Making Right Turn Going Straight Ahead		North South South East	
7452 State Route 7453 State Route	Jefferson	019 019				0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Raining Overcast	Wet Straight & Level Dry Curve & Level	Daylight Dark-No Street Lights	Culvert and/or other Appurtenance in Ditch Utility Pole			South North South North	
7453 State Route 7454 State Route 7455 State Route 7456 State Route 7457 State Route	Jefferson I	019 019		6.42 No E655820 2 6.43 No E351405 2	016-05-27 (06:11 Possible Injury 017-03-17 (2005 No Apparent Injury 014-08-12 (13:23 No Apparent Injury 015-08-04 (15:43 Suspected Minor Injury 016-12-09 (04:19 No Apparent Injury 018-07-15 (12:49 Suspected Serious Injury	0 0 1 0	0 Passenger Car 0 Truck (Flatbad, Van, etc)	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Driveway	Raining Clear or Partly Cloudy Clear or Partly Cloudy	wet Straight & Level	Dark-No Street Lights	Roadway Ditch From same direction - both going straight - one stopped - sideswipe Vehicle overturned	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped in Roadway	South North North South	h North Vehicle Stopped
7456 State Route 7457 State Route	Jefferson	019 019		6.43 No E449404 2 6.56 No E618024 2 6.73 No E618024 2	015-08-04 15:43 Suspected Minor Injury 016-12-09 04:19 No Apparent Injury	0 0 1 0	0 Motorcycle 0 Pickup, Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related	Snowing				Going Straight Anead		North South North South	<u> </u>
7457 Subte Roude 7457 Subte Roude 7459 Subte Roude 7459 Subte Roude 7459 Subte Roude 7461 Subte Roude 7461 Subte Roude 7461 Subte Roude 7463 Subte Roude 7463 Subte Roude 7463 Subte Roude 7464 Subte Roude 7465 Subte Roude	Jefferson Jefferson	019 019		6.85 No E722365 2 7.00 No E78865 3	017-10-04 22:23 No Apparent Injury	0 0 1 0	U Pickup,Panel Truck or Vanette under 10,000 ib O Passenger Car O Pickup,Panel Truck or Vanette under 10,000 ib O Passenger Car	Truck (Flathard Van etc.)	Not at Intersection and Not Related Not at Intersection and Not Related Driveway Related but Not at Driveway	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Level Dry Straight & Level	Daylight Dark-No Street Lights Daylight	carer paint or Ledge Vehicle Strikes Deer From same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	South North	h North Vehicle Stopped
7461 State Route 7462 State Route	Jefferson Jefferson	019 019		7.50 No E557253 2 7.58 No E574384 2	016-06-23 18:43 No Apparent Injury 016-08-01 05:30 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Raining Fog or Smog or Smoke	Dry Curve & Grade	Daylight I	Roadway Ditch Roadway Ditch			North South North South	h
7463 State Route 7464 State Route	Jefferson Jefferson Jefferson	019 019		8.08 No E662523 2 8.11 No E536264 2	017-04-11 09:35 No Apparent Injury 016-04-16 07:27 No Apparent Injury	0 0 1 0	O Passenger Car Pickup-Panel Truck or Vanette under 10,000 lb Pickup-Panel Truck or Vanette under 10,000 lb Truck (Flatbad Vanetc) Truck (Flatbad Vanetc)		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Grade Wet Curve & Grade	Daylight Daylight	Utility Pole Tree or Stump (stationary)	Going Straight Ahead Going Straight Ahead Going Straight Ahead		South North South North	3
7465 State Route 7466 State Route	Jefferson (019 019		8.15 No E493489 2 8.17 No E380258 2	015-12-08 08:00 No Apparent Injury 014-11-29 08:20 Suspected Minor Injury	1 0 1 0	Truck (Flatbad, Van, etc) Pickup, Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy		Daylight Daylight	Tree or Stump (stationary)	Going Straight Anead		South North North South	a
7466 State Route 7467 State Route 7468 State Route 7469 State Route	Jefferson	019 019		8.50 No 2729646 2 8.50 No E75843 2 8.56 No E56623 2	014-07-04 10:27 Possible Injury 018-01-14 06:52 Suspected Minor Injury	2 0 2 0	O Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Passenger Car Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Fog or Smog or Smoke	Wet Curve & Grade Dry Curve & Level	Daylight Dark-No Street Lights Daylight	Earth Bank or Ledge From same direction - one left turn - one straight From opposite direction - one stopped - head-on Vehicle Strikes Deer	Overtaking and Passing Stopped in Roadway Going Straight Ahead	Making Left Turn Going Straight Ahead	North South Northeast Vehicl North South	h North East cle Stopped South North
7409 Soate Noutie 7470 Sate Route 7471 State Route 7471 State Route 7473 State Route 7473 State Route 7473 State Route 7474 State Route 7475 State Route 7476 State Route 7477 State Route 7477 State Route 7477 State Route	Jefferson (019 019		8.59 No E387820 2 8.65 No E92629 2	014-12-26 06:45 No Apparent Injury	0 0 1 0	Passenger Car Passenger Car		Not at Intersection and Not Related	Overcast Clear or Partly Cloudy		Dark-No Street Lights	Vertical Strikes Deer Utblirk Pole Utblirk Pole	Going Straight Ahead		South North	6
7472 State Route 7473 State Route	Jefferson J	019 019		8.90 No E702471 2 9.05 No E975255 2	0719-05-24 07:56 No Apparent Injury 0717-08-10 19:30 No Apparent Injury 0719-10-25 13:55 No Apparent Injury 0714-02-10 10:52 No Apparent Injury	0 0 2 0	0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Driveway	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Daylight	From opposite direction - both going straight - sideswipe Entering at angle	Going Straight Ahead Going Straight Ahead Making Left Turn	Going Straight Ahead Going Straight Ahead	North South East South	South North North South
7474 State Route 7475 State Route	Jefferson (019 019		9.09 No E307375 2 9.09 No E420586 2	014-02-10 10:52 No Apparent Injury 015-04-26 11:57 No Apparent Injury 017-10-20 17:20 No Apparent Injury 018-07-08 10:09 No Apparent Injury	0 0 3 0	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related	Clear or Partly Cloudy Overcast	Wet Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic	South North North South	h Vehicle Stopped Vehicle Stopped h Vehicle Stopped Vehicle Stopped
7476 State Route 7477 State Route	Jefferson I Jefferson I	019 019		9.09 No E726819 2 9.09 No E815696 2	017-10-20 17:20 No Apparent Injury 018-07-08 10:09 No Apparent Injury	0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Overcast Clear or Partly Cloudy	Wet Dry		Entering at angle Entering at angle	Going Straight Ahead	Going Straight Ahead Starting in Traffic Lane	South North North South	West East a East West
7478 State Route 7479 State Route 7480 State Route 7481 State Route	Jefferson (019 019			018-08-04 17:58 Possible Injury 018-12-31 10:08 No Apparent Injury	0 0 2 0	O Passenger Car O Pickup,Panel Truck or Vanette under 10,000 lb O Pickup,Panel Truck or Vanette under 10,000 lb O Passenger Car O Notes Dead Truck and Notes to 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end Entering at angle	Going Straight Ahead Making Left Turn	Stopped at Signal or Stop Sign Going Straight Ahead Going Straight Ahead	North South South North	Vehicle Stopped Vehicle Stopped North
7481 State Route 7482 State Route	Jefferson	019		9.20 No 2766894 2 9.22 No F64542 2	018-12-31 10:08 No Apparent Injury 019-12-24 05:55 No Apparent Injury 015-01-05 09:34 No Apparent Injury 017-02-03 15:30 Possible Injury	0 0 2 0	Passenger Car Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb	At Driveway At Driveway	Raining Clear or Partly Cloudy	wet Straight & Level	Dayright	Entering at angle From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end	Making Left Turn Starting in Traffic Lane Going Straight Ahead Going Straight Ahead	Making Left Turn	North South	North South
7482 State Route 7483 State Route 7483 State Route 7484 State Route 7485 State Route 7486 State Route 7486 State Route	Jefferson Jefferson Jefferson	019 019		9.31 No E327807 2 9.89 No E859516 2	014-05-12 16:17 No Apparent Injury 018-11-01 19:00 No Apparent Injury	0 0 2 0	O Passenger Car O Pickup, Panel Truck or Vanette under 10,000 lb O Pickup, Panel Truck or Vanette under 10,000 lb O Pickup, Panel Truck or Vanette under 10,000 lb O Passenger Car O Birkup, Panel Truck or Vanette under 10,000 lb O Passenger Car O Birkup, Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Driveway Intersection Related but Not at Intersection	Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level Wet Straight & Level	Daylight Dark-Street Lights On	Form same direction - both going studyst - book intending - town-end - form - both going studyst - book intending - town-end - form - both going studyst - both moving - rear-end - form same direction - both going studyst - both moving - rear-end - form same direction - both going studyst - one stopped - rear-end - form same direction - both going studyst - one stopped - rear-end	Going Straight Ahead Making Right Turn Going Straight Ahead	Slowing Stopped for Traffic Going Straight Ahead	South North East North	h South North h Vehicle Stopped Vehicle Stopped h South North
7485 State Route 7486 State Route	Jefferson (019 019		9.90 No E590214 2 9.90 No E775978 2	016-09-26 12:42 No Apparent Injury 018-02-27 12:06 No Apparent Injury	0 0 3 0	Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related Driveway Related but Not at Driveway	Clear or Partly Cloudy Overcast	Dry Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead	Stopped for Traffic	North South North South	h Vehicle Stopped Vehicle Stopped h Vehicle Stopped Vehicle Stopped
7488 State Pouts	Jefferson (019 019		10.01 No E637262 2 10.04 No E922787 2	017-01-26 11:25 No Apparent Injury 019-05-14 08:37 No Apparent Injury	0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Intersection Related but Not at Intersection	Clear or Partly Cloudy Raining	Dry Straight & Level Wet Straight & Level	Daylight Daylight	From same direction - all others From same direction - both going straight - one stopped - rear-end	Making U-Turn Going Straight Ahead	Going Straight Ahead Stopped for Traffic	South South South North	h North South h Vehicle Stopped Vehicle Stopped
7489 State Route 7490 State Route 7490 State Route 7490 State Route 7490 State Route 7491 State Route 7492 State Route 7492 State Route 7493 State Route 7494 State Route 7494 State Route 7495 State Route 7496 State Route 7497 State Route 7498 State Route 7498 State Route 7499 State Route	Jefferson	019 019		10.05 No E343238 2 10.05 No E43228 2	014-07-17 12:36 No Apparent Injury 015-06-04 15:56 Suspected Minor Injury	2 0 2 0	O Pickup, Parel Truck or Varente under 10,000 B. O Pissenger Christop Francis Truck or Varente under 10,000 B. O Pissenger Christop Francis Truck or Varente under 10,000 B. O Pickup, Parel Truck or Varente under 10,000 B. O Pickup Parel Truck or Varente under 10,000 B. O Pickup Parel Truck or Varente under 10,000 B. O Pickup Parel Truck or Varente under 10,000 B. O Pickup Parel Truck or Varente under 10,000 B. O Pickup Parel Truck or Varente under 10,000 B. O Pickup Parel Truck or Varente under 10,000 B. O Pickup Parel Truck or Varente under 10,000 B. O Pickup Parel Truck or Varente under 10,000 B. O Pickup Parel Truck or Varente under 10,000 B. O Pickup Parel Truc	Passenger Car Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Daylight	From same direction - both going straight - one stopped - rear-end Entering at angle Entering at angle	Going Straight Ahead Making Left Turn	Making Left Turn Going Straight Ahead	South North West North	h South West h North South ! North South
7491 State Route 7492 State Route 7493 State Route	Jefferson Jefferson Jefferson	019 019		10.05 No E569079 2 10.05 No E560079 2 10.05 No E560033 2	016-02-10 14:47 No Apparent Injury 016-06-28 15:21 Suspected Minor Injury 016-07-09 14:23 No Apparent Injury	2 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb	Motorcycle Pickup Panel Truck or Vanette under 10 000 lb	At Intersection and Related At Intersection and Related At Intersection and Not Related	Overcast Clear or Partly Cloudy Overcast	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight	Entering at angle From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead Stopped in Roadway	South North	h Vehicle Stopped Vehicle Stopped h North South
7494 State Route 7495 State Route	Jefferson	019		10.05 No E859966 2 10.05 No E984695 2	018-11-09 14:25 No Apparent Injury 019-11-20 17:25 Possible Injury	0 0 2 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Intersection Related but Not at Intersection	Overcast Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level	Daylight Dark-Street Lights On	From same direction - both going straight - both moving - rear-end From same direction - both going straight - one stopped - rear-end From same direction - both going straight - both moving - rear-end	Slowing Going Straight Ahead	Slowing Stopped in Roadway Slowing	South North South North	h Vehicle Stopped Vehicle Stopped h South North
7496 State Route 7497 State Route	Jefferson Jefferson Jefferson	019 019		10.27 No. E843220 2	118-00-27 16-45 No Apparent Injury	0 0 1 0	0 Dickup Panel Truck or Vanette under 10 000 lb		At Interrection and Not Palated	Overcast Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Daylight	Entering at angle Domestic animal (horse, cow, sheep, etc) Pedalcyclist All Other Involvements "ONE UNIT - PEDALCYCLIST ONLY or PEDALCYCLIST STR	Making Left Turn Going Straight Ahead	Going Straight Ahead	East South North South	th South North
7498 State Route 7499 State Route	Jefferson (019 019		10.41 No E833508 2 10.50 No E299709 2	018-08-19 11:31 Possible Injury 014-01-02 13:25 Suspected Minor Injury	1 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Division of the contract of th	Not at Intersection and Not Related Not at Intersection and Not Related	Fog or Smog or Smoke Overcast	Dry Straight & Level Dry Straight & Level	Daylight	Vehicle going straight hits pedestrian	Going Straight Ahead Going Straight Ahead		South North South North	
7500 State Route 7501 State Route 7502 State Pour	Jefferson	019 019		10.52 No E923370 2 10.60 No E435933 2 10.67 No E435933 2	219-05-17 13:26 Suspected Minor Injury 215-06-20 15:05 No Apparent Injury 214-01-24 16:11 Dozeilia Injury	1 0 2 0 0 0 2 0	U Prickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup Panel Truck or Vanette under 40,000 "	Passenger Car Pickup Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Driveway At Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Daylight	Entering at angle From same direction - both going straight - both moving - rear-end	Making Left Turn Going Straight Ahead	Going Straight Ahead Making Right Turn Slowing	west North North South	North South North West
7503 State Route 7504 State Route	Jefferson Jefferson Jefferson	019 019		10.67 No E302950 2 10.68 No E548995 2 10.68 No E746100 2	016-05-12 15:51 Suspected Minor Injury	1 0 2 0	O Pickup, Panel Truck or Vanette under 10,000 lb O Pickup, Panel Truck or Vanette under 10,000 lb O Pickup, Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy	Dry Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end Entering at angle	Going Straight Ahead Going Straight Ahead Making Left Turn	Stopped for Traffic Going Straight Ahead	North South	h South North th Vehicle Stopped Vehicle Stopped th South North
7505 State Route 7506 State Route	Jefferson Jefferson Jefferson	019		10.68 No E746100 2 10.68 No E881150 2 10.79 No E593532 2	019-01-08 08:49 Possible Injury 016-09-29 13:40 Possible Injury	1 0 2 0	0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Driveway	Raining Raining Clear or Partly Cloudy	Wet Straight & Level Wet Straight & Level Dry Straight & Level	Daylight Daylight	Entering at angle Entering at angle From same direction - one left turn - one straight	Making Left Turn Making Left Turn Going Straight Ahead	Going Straight Ahead Going Straight Ahead Stopped for Traffic	East South South North	South North
7507 State Route 7508 State Route	Jefferson (019 019		10.79 No E768972 2 10.80 No 3691499 2	018-02-09 11:12 Possible Injury 014-10-10 20:23 Suspected Minor Injury	1 0 2 0 1 0 2 1	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car	At Driveway Not at Intersection and Not Related	Overcast Overcast	Dry Straight & Level Wet Straight & Level	Daylight Uther	Entering at angle Vehicle going straight hits pedestrian	Going Straight Ahead	Going Straight Ahead	West North North South	North South
7509 State Route 7510 State Route	Jefferson Jefferson Jefferson	019 019		10.80 No E918514 2 10.82 No 3120479 2	019-05-08 06:46 No Apparent Injury 014-05-21 19:00 Possible Injury	0 0 1 0 2 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Daylight	Utility Pole From same direction - one right turn - one straight	Going Straight Ahead Going Straight Ahead	Making Right Turn	South North South North	South East
7499 State Route 7490 State Route	Jefferson I	019 019		10.82 No E440130 2 10.87 No E755047 2	215-U/-01 12:05 Suspected Serious Injury 018-01-03 08:52 No Apparent Injury	1 0 1 0 0 0 2 0	Pickup Parell Truck or Varente under 10,000 b Diskup Parell Truck or Varente under 10,000 b	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	loe Straight & Level	Daylight Daylight	DBITY Pole From same direction - both going straight - both moving - sideswipe	Going Straight Ahead Changing Lanes	Going Straight Ahead	reorth South South North	South North
7514 State Route 7515 State Route	Jefferson I Jefferson I	019 019		10.88 No E522404 2 10.92 No E29837 1 10.93 No E932266 2	014-01-06 10:30 No Apparent Injury 014-01-06 10:30 No Apparent Injury 019-06-14 10:42 No Apparent Injury	0 0 2 0	O Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10 000 lb	At Driveway At Intersection and Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Wet Straight & Level Dry Straight & Level	Davlight	From opposite direction - one left turn - one straight From opposite direction - one left turn - one straight Entering at angle	Making Left Turn Making Left Turn Making Left Turn	Going Straight Ahead Going Straight Ahead Going Straight Ahead	North East East South	South North h North South
7516 State Route 7517 State Route	Jefferson Jefferson	019 019		10.95 No E788100 2 11.17 No E385110 2	018-04-03 03:00 Unknown 014-12-08 21:10 Unknown	0 0 1 0	0 Passenger Car 0 Not Stated		Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Raining	Wet Straight & Level Wet Straight in San	Dark-Street Lights On Dark-No Street Lights	Tree or Stump (stationary) Fire Hydrant	Going Straight Ahead		North South	,
7518 State Route 7519 State Route	Jefferson Jefferson Jefferson Jefferson	019 019		11.20 No E856922 2 1 128 No E593533 2	018-11-02 10:39 Suspected Minor Injury 016-09-13 12:48 Possible Injury	2 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	At Driveway Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level Dry Curve & Level	Daylight Daylight	Tree or Saump (stationary) Fire Hydratt Intelring at angle Vehicle - Pedalcyclist	Making Left Turn Going Straight Ahead	Going Straight Ahead	East South South North	South North
7520 State Route 7521 State Route	Jefferson	019 019		11.35 No E736793 2 11.40 No E583684 2	017-11-15 12:06 No Apparent Injury 016-09-06 07:10 No Apparent Injury	0 0 2 0 0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast	Wet Curve & Level	Daylight Dawn	From same direction - both going straight - both moving - sideswipe	Changing Lanes Going Straight Ahead	Going Straight Ahead	North South South North	h North South
7522 State Route 7523 State Route	Jefferson I	019 019		11.50 No E598217 2	116-10-15 14:40 No Apparent Injury	0 0 3 0	0 Dickup Panel Truck or Vanette under 10 000 lb	Dickup Panel Truck or Vanette under 10 000 lb	Not at Intersection and Not Related	Raining Raining	Wet Curve & Level Wet Curve & Level	Daylight Daylight	Vernous Deer Tree or Stump (stationary) From opposite direction - both going straight - sideswipe	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	South North North South	a South North
7915 State Route 7916 State Route 7917 State Route 7917 State Route 7917 State Route 7917 State Route 7918 State Route 7920 State Route 7920 State Route 7921 State Route 7922 State Route 7924 State Route 7924 State Route 7924 State Route 7925 State Route 7927 State Route 7927 State Route 7927 State Route	Jefferson	019 019		11.50 No E683522 2 11.51 No E90465 2 11.64 No E90465 2	019-03-15 15:35 No Apparent Injury	0 0 2 0	O Passenger Car O Pickup Panel Truck or Vanette under 10,000 ib Passenger Car Pickup Panel Truck or Vanette under 10,000 ib Passenger Car Pickup Panel Truck or Vanette under 10,000 ib Passenger Car Pickup Panel Truck or Vanette under 10,000 ib	Passenger Car	At Driveway At Intersection and Polyted At Intersection and Polyted	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Curve & Level Dry Straight & Level	Daylight	From same direction - both going straight - one stopped - rear-end From opposite direction - one left turn - one straight	Going Straight Ahead Making Left Turn Making Left Turn	Stopped in Roadway Going Straight Ahead Going Straight Ahead	North East	h South Vehicle Stopped South North North North
7527 State Route 7528 State Route	Jefferson Jefferson Jefferson Jefferson	019 019		11.61 No E338261 2	014-06-23 16:10 No Apparent Injury	0 0 2 0	0 Pickup.Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Related	Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight I	Entering at angle From same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end	Making Right Turn Going Straight Ahead	Stopped at Signal or Stop Sign Stopped at Signal or Stop Sign	East North	h Vehicle Stopped Vehicle Stopped t Vehicle Stopped Vehicle Stopped
7529 State Route 7530 State Route	Jefferson Jefferson Jefferson Jefferson	019 019		11.62 No E340742 2 11.62 No E500583 2	014-07-10 08:50 No Apparent Injury 015-12-30 13:36 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway within Major Intersection At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Daylight	rioni same direction - both going straight - one stopped - rear-end From same direction - both going straight - one stopped - rear-end Roadway Ditch	Stopped in Roadway Going Straight Ahead Making Left Turn	Going Straight Ahead Stopped at Signal or Stop Sign	Vehicle Stopped Vehicl East West	cle Stopped East West t Vehicle Stopped Vehicle Stopped
7531 State Route 7532 State Route	Jefferson (019 019		11.62 No E527664 2 11.62 No E69829 2	016-03-13 03:41 No Apparent Injury 017-08-05 14:05 No Apparent Injury	0 0 1 0	O Pickup,Panel Truck or Vanette under 10,000 lb O Pickup,Panel Truck or Vanette under 10,000 lb O Passenger Car O Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy	Dry Straight & Level	Daylight	From same direction - one right turn - one straight	Making Right Turn	Stopped at Signal or Stop Sign	West East East North	
7533 State Route 7534 State Route	Jefferson Jefferson	019 019		11.62 No E811934 2 11.64 No E302964 2	018-06-24 05:50 No Apparent Injury 014-01-24 09:03 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Related At Driveway	Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Daylight	Building Entering at angle	Going Straight Ahead Making Left Turn Making Left Turn	Going Straight Ahead Going Straight Ahead	South North East South	3 South North
7536 State Route 7537 State Poute	Jefferson	019 019		11.65 No E928867 2 11.69 No E809508 1 11.73 No E809508 1	018-06-15 09:05 No Apparent Injury	0 0 1 0	O Prisup Panel Truck or Variente under 10,000 B. O Prisup Panel Truck or Variente under 10,000 B. O Prisup Panel Truck or Variente under 10,000 B. O Prisup Panel Truck or Variente under 10,000 B. O Prisup Panel Truck or Variente under 10,000 B. O Prisup Panel Truck or Variente under 10,000 B. O Prisup Panel Truck or Variente under 10,000 B. O Prisup Panel Truck or Variente under 10,000 B. O Passenger Car	r-assienger uar	At Driveway Not at Intersection and Not Polymer	Fog or Smog or Smoke Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Daylight Daylight Daylight Dark-Street Lights On	Entering at angle Retaining Wall (concrete, rock, brick, etc.) Vehicle - Pedalcyclist	Making Left Turn Backing Going Straight Ahead	Going Straight Ahead	East South Vehicle Backing Vehicl North South	th South North cle Backing
7538 State Route 7539 State Route	Jefferson Jefferson Jefferson	019 019		11.73 NO E456693 2 11.84 No E508383 2 11.89 No E238440 2	016-01-10 00:26 Possible Injury	1 0 1 0	Passenger Car Pickup Panel Truck or Vanette under 10,000 lb Passenger Car Pickup Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Wet Curve & Level	Dark-Street Lights On Dark-Street Lights On	Vehicle - Pedalcyclist Over Embankment - No Guardrail Present	Going Straight Ahead		North South South	
7540 State Route 7541 State Route	Jefferson (019 019		11.89 No E360859 2 11.89 No E48077 2	014-09-27 12:47 No Apparent Injury 015-11-28 13:39 Suspected Minor Injury	0 0 1 0	0 Passenger Car 1 Passenger Car		At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Curve & Level Dry Straight & Level		Vehicle - Pedalcyclist Wood Sign Post Vehicle - Pedalcyclist	Making Left Turn Going Straight Ahead Going Straight Ahead		Northwest South South North	east a
7542 State Route 7543 State Route	Jefferson Jefferson Jefferson	019 019		11.89 No E648123 2 11.89 No E851960 2	017-02-26 16:15 No Apparent Injury 018-10-19 13:00 No Apparent Injury	0 0 1 0	0 Passenger Car 0 Truck & Trailer		At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Wet Curve & Level	Daylight Daylight	Vehicle - Pedalcyclist Deur Embankment - No Guardraii Present Vehicle overturned	Making Left Turn Making Left Turn Other*		South West South West	
7544 State Route 7545 State Route	Jefferson	020 020		0.00 No E428523 2 0.00 No E482752 2	015-05-16 13:57 No Apparent Injury 015-11-15 10:40 No Apparent Injury	0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Not Stated Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related Intersection Related but Not at Intersection	Clear or Partly Cloudy	Wet Straight & Grade	Daylight	Vehicle overturned Same direction both turning left both moving sides wipe From same direction both going straight - one stopped - rear-end	Going Straight Ahead	Making Left Turn Stopped for Traffic	West East East West	t West East t Vehicle Stopped Vehicle Stopped
7546 State Route 7547 State Route	Jefferson	020 020		0.00 No E843325 2 0.05 No E396165 2	018-09-25 16:43 No Apparent Injury 015-01-19 00:59 Suspected Minor Injury	0 0 2 0 1 0 2 0	O Pickup,Panel Truck or Vanette under 10,000 lb O Passenger Car O Passenger Car O Truck Tractor & Semi-Trailer O Passenger Car O Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight at Hillor	Daylight	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Other* Making Left Turn	Stopped at Signal or Stop Sign	North South	th Vehicle Stopped Vehicle Stopped
7548 State Route 7549 State Route 7550 State Route 7550 State Route 7551 State Route 7552 State Route 7552 State Route 7554 State Route 7554 State Route 7555 State Route 7556 State Route 7556 State Route 7557 State Route	Jefferson (020 020		0.09 No E594789 2 0.09 No E97500 0	019-08-12 19:20 No Apparent Injury	0 0 1 0	0 Truck Tractor & Semi-Trailer 0 Passenger Car	Pickup Panel Truck or Vanette under 10,000 lb	At Intersection and Related	Clear or Partly Cloudy	Dry Curve & Grade	Daylight	Vehicle overturned	Making Left Turn	Going Straight Ahead	West South	west East West East
7551 State Route 7552 State Route	Jefferson Jefferson	020 020		0.11 No E674549 2 0.11 No E674549 2	017-05-20 16:45 No Apparent Injury 018-10-02 15:12 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related Intersection Related but Not at Intersection Intersection Related but Not at Intersection	Clear or Partly Cloudy Clear or Partly Cloudy Overcast	Dry Straight & Grade Dry Curye & Level	Daylight Daylight	Entering at angle From same direction - both going straight - both moving - rear-end From same direction - both going straight - both moving - rear-end	Making Right Turn Going Straight Ahead Going Straight Ahead	Going Straight Ahead Going Straight Ahead Slowing	East West North South	East West
7553 State Route 7554 State Route	Jefferson (020 020		0.14 No E327806 2 0.14 No E334429 2	014-05-12 16:37 No Apparent Injury 014-06-07 19:43 Suspected Minor Injury	0 0 1 0	0 Passenger Car 0 Motorcycle	*	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy						West East East West	t
7555 State Route 7556 State Route	Jefferson Jefferson Jefferson	020 020		0.18 No E763871 2 0.21 No E704029 2	018-01-23 11:30 No Apparent Injury 017-08-22 07:30 No Apparent Injury	0 0 2 0 0 0 1 0	U Truck (Tabolf & April Truck or Vanette under 10,000 ib Passenger Car O Prokup Parel Truck or Vanette under 10,000 ib Prokup Parel Truck or Vanette under 10,000 ib O Passenger Car O Motorcyte O Motorcyte O Motorcyte O Prokup Parel Truck or Vanette under 10,000 ib O Motorcyte O Prokup Parel Truck or Vanette under 10,000 ib O Prokup Parel Truck or Vanette under 10,000 ib O Passenger Car	Passenger Car	At Intersection and Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy	Dry Curve & Grade Wet Curve & Grade Dry Curve & Grade	Daylight Dawn	Guardrall - Face From same direction - both going straight - one stopped - rear-end Earth Bank or Ledge Metal Sign Post	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic	East West South North	t West Vehicle Stopped
7557 State Route 7558 State Route	Jefferson (020 020		0.22 No E465553 2 0.25 No E808642 2	015-09-26 10:10 No Apparent Injury 018-06-12 07:36 No Apparent Injury	0 0 1 0	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Passenger Car	<u> </u>	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade Dry Straight & Level	Daylight Daylight	Metal Sign Post Vehicle Strikes Deer	Going Straight Ahead Going Straight Ahead		West East South North	

MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_1_UNIT_1	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE 2_UNIT_1	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_3_UNIT_1 MV_DRIVER AGE GEND UNIT_1 UNIT_1	VER MV_DRIVER_CONTRIBUTING ER CIRCUMSTANCE_1_UNIT_2	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_2_UNIT_2	MV_DRIVER_CONTRIBUTING	MV_DRIVER N	MV_DRIVER GENDER BICYCL	IST_CONTRIBUTING BICYCLIST_CONTRIBUTING CIRCUMSTANCE_2_UNIT_1	BICYCLIST_CONTRIBUTING AGE	BICYCLIST GENDER BICYCLIST_CONTRIBUTING CIRCUMSTANCE 1 UNIT 2	BICYCLIST_CONTRIBUTING CIRCUMSTANCE_2_UNIT_2	BICYCLIST_CONTRIBUTING BII CIRCUMSTANCE_3_UNIT_2 AG	ICYCLIST	BICYCLIST PEDESTRIAN_CONTRIBUTING	PEDESTRIAN_CONTRIBUTING	PEDESTRIAN_CONTRIBUTING PEDESTRIAN PEDESTRIAN GROUNT_2 GENDER_UNIT_2	RRST_IMPACT_LOCATION City_St_County_Rd_& Misc_2010_forward_St
Decomposition of the composition	CIRCUMSTANCE_Z_UNIT_T	58 Female		CIRCUMSTANCE_Z_UNIT_Z	CIRCUMSTANCE_3_UNIT_2	AGE_UNII_2	UNIT_2 CIRCUI	STANCE_1_UNIT_1 CIRCUMSTANCE_2_UNIT_	UNIT_1	UNIT_1 CROUMSTANCE_1_UNIT_2	CIRCUMSTANCE_Z_UNIT_Z	CIRCUMSTANCE_3_UNIT_2 AG	E_UNII_2 GE	ENDER_UNIT_2 CIRCUMSTANCE_1_UNIT_2	CIRCUMSTANCE_Z_UNIT_Z		City_St_Louinty_Nd_a_misc_zoru_proware_st ine 1 Decreasing Milepost st Right Shoulder increasing Milepost
1 Under Influence of Alcohol 2 Exceeding Reas. Safe Speed	Exceeding Reas. Safe Speed	35 Naie 36 Maie 37 Maie														R	ist Right shoulder increasing awepost ght Shoulder Increasing Milepost ist Right Shoulder Increasing Milepost
3 Operating Handheld Cell Phone 4 None		64 Female														P	sst Right Shoulder Increasing Milepost ine 1 Increasing Milepost
6 Operating Defective Equipment 7 Inattention	Distractions Outside Vehicle	39 Female 46 Maie 30 Female 49 Female														P	ne 1 Increasing Miepost st Right Shoulder Decreasing Miepost st Right Shoulder Decreasing Miepost
Apparently As leep or Fatigued None		34 Female 60 Female 16 Female														I P	ist Right Shoulder Decreasing Milepost ine 1 Increasing Milepost ist Right Shoulder Decreasing Milepost
Other Contributing Circ Not Listed Other Contributing Circ Not Listed None	Apparently Fatigued	68 Male 40 Male	Other Contributing Circ Not Listed													P P	st Right Shoulder Decreasing Milepost sst Right Shoulder Increasing Milepost sne 2 Decreasing Milepost sne 2 Decreasing Milepost
3 Exceeding Reas. Safe Speed 4 Apparently Fatigued		71 Male														P	ast Riight Shoulder Increasing Milepost ne 2 Decreasing Milepost ast Riight Shoulder Decreasing Milepost ast Riight Shoulder Decreasing Milepost
Follow Too Closely Driver Adjusting Audio or Entertainment Improper Turn/Merce	nt Inattention	53 Male 20 Male 67 Female	Follow Too Closely None None			72 Mc 63 Mc 37 Mc	Vale Vale										ne 1 Decreasing Miepost me 1 Decreasing Miepost ne 1 Decreasing Miepost ne 1 Increasing Miepost ne 1 Decreasing Miepost
O Fellow Top Clearly	Exceeding reas, one opera	18 Male 75 Female	None None Driver Not Distracted Driver Not Distracted			34 Ms 78 Ms											ine 1 Decreasing Milepost
9 Pairow Tou Unitsell's 9 Reparently Asleep or Fatigued 1 None 2 None 3 Other Contributing Circ Not Listed 4 Distractions Outside Vehicle Inattention 6 Inattention		87 Male 51 Male 59 Female 58 Female														P	st Right Shoulder Decreasing Miepost ne 1 Decreasing Miepost ne 1 Increasing Miepost
3 Other Contributing Circ Not Listed 4 Distractions Outside Vehicle	Follow Too Closely	56 Female 65 Female	None			76 M:											ine 1 Increasing Milepost
6 Inattention 6 Inattention	Follow Too Closely	65 Female 36 Male 27 Female	None None			26 M 44 M	Vale Vale										me 1 Increasing Miepost me 1 Increasing Miepost me 1 Increasing Miepost
8 Exceeding Reas. Safe Speed 9 Inattention	Follow Too Closely	54 Male 63 Female 85 Male				43 M 60 Fe	Female									l L	ne 1 Increasing Milepost ne 1 Increasing Milepost ast Right Shoulder Increasing Milepost
7 Distractions Outside Vehicle 8 Exceeding Reas. Safe Speed 9 Inattention 0 Inattention 1 Other Contributing Circ Not Listed	Exceeding Reas. Safe Speed Distr.	85 Male stractions Outside Vehicle 20 Male 74 Female														P	st Right Shoulder Increasing Milepost st Right Shoulder Increasing Milepost st Right Shoulder Decreasing Milepost
2 None 3 Under Influence of Alcohol 4 Driver Not Distracted 5 Operation Defective Equipment	Apparently Fatigued	58 Male 45 Male 37 Male 55 Male														P	ine 1 Decreasing Milepost st Right Shoulder Decreasing Milepost ine 1 Decreasing Milepost st Right Shoulder Increasing Milepost
5 Operating Defective Equipment 6 Under Influence of Alcohol 7 Under Influence of Alcohol		55 Male 43 Male														P	ast Right Shoulder Increasing Milepost ast Right Shoulder Decreasing Milepost
7 Under Influence of Alcohol 8 None 9 Apparently As least or Estimated	Apparently Fatigued	43 Male 21 Male 59 Male 38 Female														P	ast Right Shoulder Decreasing Milepost ast Right Shoulder Decreasing Milepost ner Decreasing Milepost ast Right Shoulder Increasing Milepost
7 Under Influence of Alcohol 8 None 9 Apparently Asleep or Fatigued 0 Apparently Asleep or Fatigued 1 None 2 None 3 Other Contributing Circ Not Listed	Inattention																
2 None 3 Other Contributing Circ Not Listed	Distractions Outside Vehicle	24 Female 25 Fem				2011										P	ne 1 Increasing Milepost ne 1 Decreasing Milepost st Right Shoulder Increasing Milepost ne 1 Decreasing Milepost
4 Under Influence of Drugs 5 Apparently Asleep or Fatigued 6 None 7 Under Influence of Alcohol	Over Center Line	70 Male 46 Male	None			22 M										P	Intel [®] 1 Ue [®] Cessing Mepost Right Shoulder Increasing Mepost St Right Shoulder Increasing Mepost Only Shoulder Increasing Mepost St Right Shoulder Increasing Mepost
7 Under Influence of Alcohol 8 Exceeding Reas. Safe Speed					l		Female										ine 1 Increasing Milepost
7 Under Influence of Alcohol 8		45 Female 61 Female 34 Female	None			47 Ma	vale									L L	ine 1 Increasing Mlepost me 1 Decreasing Mlepost me 1 Decreasing Mlepost
2 Inattention 3 Under Influence of Alcohol	Follow Too Closely					76 Ma 53 Ma	Vale Vale										ine 1 increasing Milepost ine 1 Decreasing Milepost
2 Inattention 3 Under Influence of Alcohol 4 Other Contributing Circ Not Listed 5 Operating Defective Equipment 6 Inattention	Inattention Distractions Outside Vehicle	41 Male 37 Male 22 Male 68 Male 44 Male	None None			74 Fe										P	ast Right Shoulder Decreasing Miepost ne 1 Decreasing Miepost ne 1 Increasing Miepost
7 Follow Too Clorely		24 Male er Center Line 57 Male	None None				emale emale									L L	ine 1 Increasing Milepost ine 1 Decreasing Milepost
9 Exceeding Reas. Safe Speed 0 Follow Too Closely	Exceeding Reas. Safe Speed Over Follow Too Closely	65 Male 65 Male	None None			41 Ma 57 Ma	Visie Visie								-		ine 1 Increasing Milepost ine 1 Increasing Milepost
8 Instention discourse 8 Instention Reas. Safe Speed 9 Exceeding Reas. Safe Speed 0 Follow Too Closely 1 Exceeding Reas. Safe Speed 2 Apparently Acleep or Fatigued 3 Other Contributing Circ Not Listed 4 Exceeding Reas. Safe Speed 5 None	Over Center Line	42 Female 31 Male 83 Male 22 Male	None			39 Fe	Female Vale									P	ine 1 Increasing Milepost ist Right Shoulder Increasing Milepost
4 Exceeding Reas. Safe Speed 5 None		22 Male 62 Male														L	ine 1 Decreasing Milepost st Right Shoulder Increasing Milepost ne 1 Decreasing Milepost
4 Exceeding Reas. Safe Speed None 6 Other Contributing Circ Not Listed 7 Inattention 8 Under Influence of Drugs	Exceeding Reas. Safe Speed Apparently Fatigued Inattention	57 Female 19 Male 23 Male														P P	st Right Shoulder Decreasing Miepost st Right Shoulder Increasing Miepost st Right Shoulder Increasing Miepost
Annarently Asleen or Fatigued		19 Female 16 Male															
Apparently Asleep or Fatigued Improper Turn/Merge Inattention Apparently Asleep or Fatigued		C) Male 57 Female 19 Male 23 Male 19 Male 19 Female 10 Female 67 Female 22 Male 24 Male 25 Male 25 Male														P P	ast Right Shoulder Decreasing Milepost ast Right Shoulder Increasing Milepost ast Right Shoulder Increasing Milepost
4 Other Contributing Circ Not Listed 5 Inattention	Follow Too Closely	29 Male 42 Male 64 Male 23 Male	None			72 M	Vale									I P	ast Right Shoulder Increasing Milepost ast Right Shoulder Increasing Milepost ne 1 Decreasing Milepost
4 Other Contributing Circ Not Listed 5 Inattention 6 Exceeding Reas. Safe Speed 7 None 8 Inattention 9 None 0 Unknown Distraction 1 Exceeding Reas. Safe Speed 2 Exceeding Reas. Safe Speed		23 Male 48 Male														L P	ine 1 Decreasing Milepost ne 1 Decreasing Milepost st Right Shoulder Increasing Milepost st Right Shoulder Increasing Milepost st Right Shoulder Increasing Milepost
9 None Unknown Distraction		48 Male 61 Male 334 Male 66 Female 43 Male	None			39 M:	Vale									P U	st Right Shoulder Increasing Milepost ne 1 Decreasing Milepost ne 1 Decreasing Milepost ne 1 Decreasing Milepost St Right Shoulder Decreasing Milepost St Right Shoulder Decreasing Milepost
1 Exceeding Reas. Safe Speed 2 Exceeding Reas. Safe Speed	Improper Passing	43 Male 58 Male														P	ist Right Shoulder Increasing Milepost
3 Apparently As leep or Fatigued 4 Reading or Writing 5 Expending Pears Safe Speed		58 Maie 67 Maie 63 Female 27 Maie 22 Female 22 Female 22 Maie														P R	st Right Shoulder Increasing Milepost ght Shoulder Increasing Milepost st Right Shoulder Increasing Milepost
6 Exceeding Reas. Safe Speed 7 Improper Passing		20 Female															
		22 Male	None			20 Ma										P	sst Right Shoulder Increasing Milepost ine 1 Increasing Milepost
8 Inattention 9 None	Non Motorist on Wrong Side of Road Did N	Not Grant RW to Vehicle 66 Female 76 Female	None			20 M: 71 M:										P L	sat Right Shoulder Increasing Mlepost men 1 Increasing Mlepost men 1 Increasing Mlepost men 1 Increasing Mlepost men 1 Decreasing Mlepost men 1 Decreasing Mlepost
1 Exceeding Reas. Salf Speed 2 Exceeding Reas. Salf Speed 3 Apparently Acleego or Fatigued 4 Reading or Winting 5 Exceeding Reas. Salf Speed 6 Exceeding Reas. Salf Speed 7 Improper Passing 9 None 9 None 1 Under Influence of Drugs 1 Under Influence of Drugs 1 None 1 Under Influence of Drugs 1 None 1 Non		Not Grant RW to Vehicle 66 Female 76 Female 36 Female	None	Operating Handheld Cell Phone		20 Ma 71 Ma	Male Male									P	st Right Shoulder Increasing Milepoot nee 1 horeasing Milepoot nee 1 Decreasing Milepoot sat Right Shoulder Increasing Milepoot sat Right Shoulder Increasing Milepoot need to the Mil
3 Did Not Grant RW to Vehicle		Not Grant RW to Vehicle	None Inatention None None	Operating Handheld Cell Phone		71 Ms Ms 72 Fe 34 Ms	Maie Waie Wale Female Wale									P	self Royld Shoulder horsasine Milegost self Royld Shoulder horsasine Milegost self Tomassine Milegost self Tomassine Milegost self Tomassine Milegost self Royld Shoulder horsasine Milegost self Demossing Milegost self Demossing Milegost self Demossing Milegost self Tomassing Milegost
3 Did Not Grant RW to Vehicle	Apparently Fatigued Over	Not Grant RW to Vehicle	None Inatention None None	Operating Handheld Cell Phone		71 Ms Ms 72 Fe 34 Ms	Maie Waie Wale Female Wale										sel Bohr Stooder Increasing Missest sel Bohr Stooder Increasing Missest sel Doctorasing Missest sel Do
Name of the control of the cont	Apparently Fatigued Over Follow Too Closely	Nes Grant RW to Vehicle of Families Famili	None Inatiention None None None None None None None No	Operating Handheld Cell Phone		71 Ms Ms 72 Fe 34 Ms 17 Fe 73 Fe 31 Ms 51 Ms 41 Ms	Male Male Male Wale Wale Female Male Female Male Male Male Male Male Male									P P P P P P P P P P	set Epul Studies Transaction Mispost in a Tourisan Studies Transaction Mispost in a Tourisan Mispost in a Tourisan Mispost in a Tourisan Mispost in a Tourisan Mispost in Epul Studies Transaction Mispost and Epul Studies Transaction Mispost and Epul Studies Transaction Mispost and Epul Studies Transaction Mispost in a Tourisan Mi
3 Did Not Grant RW to Vehicle 4 Inattention 5 Operating Defective Equipment 6 Did Not Grant RW to Vehicle 7 None 8 Unknown Distraction 9 Improper Turn/Merge 0 None 1 Ereanting Page: Safe Shapet	Apparently Feligued Over Feliow Too Closely Feliow Too Closely Feliow Too Closely Come Combining Circ Nort Listed Other Combining Circ Nort Listed Feliow Too Closely	Nes Grant RW to Vehicle of Families Famili	None Inatiention None None None None None None None No	Operating Handheld Cell Phone		71 Mc Mc 72 Fc 34 Mc 17 Fc 73 Fc 13 1 Mc 61 Mc 44 Mc 44 Mc 45 Mc 46 Mc 65 Mc	Male Male Fernale Male Fernale Male Male Male Male Male Male Male M										sel Bohd Stooder Increasing Mispost - In School Stooder Increasing Mispost - In School Stooder Increasing Mispost - In Commany Mispost - In Decreasing Mispost - In De
3 Did Not Grant RW to Vehicle 4 Inattention 5 Operating Defective Equipment 6 Did Not Grant RW to Vehicle 7 None 8 Unknown Distraction 9 Improper Turn/Merge 0 None 1 Ereanting Page: Safe Shapet	Apparently Faliqued Oxer Fallow Too Closely	Net Gant RW to Vehicle GE Female	None mattension None None None None None None None No	Operating Handheld Cell Phone		71 Ms MM 72 Fe 34 Ms 177 Fe 73 Fe 73 Fe 73 Fe 41 Ms 41 Ms 48 Ms 54 Ms 54 Ms	Male Male Male Wale Wale Female Male Female Male Male Male Male Male Male									P P P P P P P P P P	set Boht Stooder Increasing Magnet in Florit Stooder Magnet
3 Did Not Grant RW to Vehicle 4 Inattention 5 Operating Defective Equipment 6 Did Not Grant RW to Vehicle 7 None 8 Unknown Distraction 9 Improper Turn/Merge 0 None 1 Ereanting Page: Safe Shapet	Apparently Feligued Over Feliow Too Closely Feliow Too Closely Feliow Too Closely Feliow Too Closely Other Contributing Circ Not Listed Office Contributing Circ Not Listed Feliow Too Closely	Net Grant RW to Vehicle 66 Female 76	Notice	Operating Handheld Cell Phone		71 M M. 72 Fe 34 M 17 Fe 73 Fe 31 M 51 M 41 M 48 M 56 Fe 25 M 17 M 26 Fe 26 Fe 26 Fe 26 Fe 26 Fe 26 Fe 27 M	Male Male Male Male Male Male Male Male										sel Bohd Stooder Increasing Magnet set Tourisating Magnet set Touris
3 On Not Coart RV to Notice hastendon Copraring Defective Equipment Did Not Claraft Not Vo Notice University State Volume University State Horizont Distriction State Claraft Horizont Licendring Research Licendring Research Follow To Closely Improper Turn Marge Hastendon Hastendon Follow To Closely Improper Turn Marge Hastendon Hastend	Apparently Faligued Over Follow Too Closely Fallow Too Closely Other Constituting Circ Not Listed Follow Too Closely Other Constituting Circ Not Listed Follow Too Closely Fallow Too Closely Institution Exceeding Reas. Safe Speed Institution Institution	Not Grant RW to Vehicle GE Female	Notice Institution in Notice	Operating Handhold Cell Phone		71 M M. 72 Fe 34 M 17 Fe 73 Fe 31 M 51 M 41 M 48 M 56 Fe 25 M 17 M 26 Fe 26 Fe 26 Fe 26 Fe 26 Fe 26 Fe 27 M	Male Male Male Value Fernale Male Fernale Male Fernale Male Fernale Male Fernale Male Male Male										sel Bohd Stooder Increasing Magnet set Tourness of Magnet set Tourne
3 On Not Coart RV to Notice hastendon Copraring Defective Equipment Did Not Claraft Not Vo Notice University State Volume University State Horizont Distriction State Claraft Horizont Licendring Research Licendring Research Follow To Closely Improper Turn Marge Hastendon Hastendon Follow To Closely Improper Turn Marge Hastendon Hastend	Apparently Faligued Over Follow Too Closely Fallow Too Closely Other Constituting Circ Not Listed Follow Too Closely Other Constituting Circ Not Listed Follow Too Closely Fallow Too Closely Institution Exceeding Reas. Safe Speed Institution Institution	Net Gant RW to Vehicle GE Female	None Institution None	Operating Handhold Cell Phone		71 M M M 72 Fe 34 M 17 Fe 73 Fe 31 M 61 M 64 M 65 Fe 25 M 35 Fe 30 M 38 Fe 71 Fe 71 M	ubbis Adai										sel Bout Souder Increasing Magnet are I Doctorsamy Magnet
3 On Not Coart RV to Notice hastendon Copraring Defective Equipment Did Not Claraft Not Vo Notice University State Volume University State Horizont Distriction State Claraft Horizont Licendring Research Licendring Research Follow To Closely Improper Turn Marge Hastendon Hastendon Follow To Closely Improper Turn Marge Hastendon Hastend	Apparently Faligued Over Follow Too Closely Fallow Too Closely Other Constituting Circ Not Listed Follow Too Closely Other Constituting Circ Not Listed Follow Too Closely Fallow Too Closely Institution Exceeding Reas. Safe Speed Institution Institution	Net Grant RVV to Vehicle	Notes Pastersion Proces Proces Proces On Fore Court RW to Vehicle Notes On Fore Court RW to Vehicle Notes	Operating Handheld Cell Phone		71 Mb 1 M 1 M 1 M 1 M 1 M 1 M 1 M 1 M 1 M	shaha haka haka haka haka haka haka haka										sel Bohd Shoulder Increasing Magnet set Tournessing Magnet set Tournessing Magnet set Tournessing Magnet set Tournessing Magnet set Right Shoulder Increasing Magnet set Tournessing Magnet set I Increasing Magnet set Tournessing Magnet
3 On Not Coart RV to Notice hastendon Copraring Defective Equipment Did Not Claraft Not Vo Notice University State Volume University State Horizont Distriction State Claraft Horizont Licendring Research Licendring Research Follow To Closely Improper Turn Marge Hastendon Hastendon Follow To Closely Improper Turn Marge Hastendon Hastend	Apparently Faligued Over Follow Too Closely Fallow Too Closely Other Constituting Circ Not Listed Follow Too Closely Other Constituting Circ Not Listed Follow Too Closely Fallow Too Closely Institution Exceeding Reas. Safe Speed Institution Institution	Net Grant RVV to Vehicle	Notes Pastersion Proces Proces Proces On Fore Court RW to Vehicle Notes On Fore Court RW to Vehicle Notes	Operating Handheld Cell Phone		71 MM MM MM MM 12 Fe MM 11 Fe 13 MM 14 MM MM 14 MM	stable in the st										set Boht Stooder Increasing Magnet in a Tournamy Magnet in a Tournamy Magnet in the Tournamy Magnet in a Tournamy Magnet in a Tournamy Magnet in a Tournamy Magnet in a Tournamy Magnet in the Tournamy Magnet in Tour
3 On Not Coart RV to Notice hastendon Copraring Defective Equipment Did Not Claraft Not Vo Notice University State Volume University State Horizont Distriction State Claraft Horizont Licendring Research Licendring Research Follow To Closely Improper Turn Marge Hastendon Hastendon Follow To Closely Improper Turn Marge Hastendon Hastend	Apparently Faligued Over Follow Too Closely Fallow Too Closely Other Constituting Circ Not Listed Follow Too Closely Other Constituting Circ Not Listed Follow Too Closely Fallow Too Closely Institution Exceeding Reas. Safe Speed Institution Institution	Not Grant RVV to Vehicle 16 Female 17 Female 18 Female 19 Female 19 Female 19 Marie 19 Marie 10 Marie 11 Marie 12 Marie 13 Marie 14 Marie 15 Marie 16 Marie 17 Marie 18 Marie 19 Marie 19 Marie 10 Marie 10 Marie 10 Marie 11 Marie 12 Marie 13 Marie 14 Marie 15 Marie 16 Marie 17 Marie 18 Marie 19 Marie 19 Marie 10 Marie 10 Marie 11 Marie 12 Marie 13 Marie 14 Marie 15 Marie 16 Marie 17 Marie 18 Marie 19 Marie 19 Marie 10 Marie 10 Marie 11 Marie 12 Marie 13 Marie 14 Marie 15 Marie 16 Marie 17 Marie 18 Marie 19 Marie 19 Marie 10 Marie 10 Marie 11 Marie 12 Marie 13 Marie 14 Marie 15 Marie 16 Marie 17 Marie 18 Marie 19 Marie 10 Marie 10 Marie 11 Marie 12 Marie 13 Marie 14 Marie 15 Marie 16 Marie 17 Marie 18 Marie 19 Marie 19 Marie 10 Marie 10 Marie 10 Marie 11 Marie 12 Marie 13 Marie 14 Marie 15 Marie 16 Marie 17 Marie 18 Marie 19 Marie 10 Marie	Notice residention Notice Notice No	Operating Handheld Cell Phone		71 MM M M 1 M M 1 M M 1 M M 1 M M 1 M M M M	stable shake				Did Not Grant RW to Vehicle		Mask	de None		P P P P P P P P P P	set Both Stooder Increasing Magnet set Double Stooder Increasing Magnet set Both Stooder Increasing Magnet set Both Stooder Increasing Magnet set Both Stooder Increasing Magnet set Increasing Magne
3 On Not Coart RV to Notice hastendon Copraring Defective Equipment Did Not Claraft Not Vo Notice University State Volume University State Horizont Distriction State Claraft Horizont Licendring Research Licendring Research Follow To Closely Improper Turn Marge Hastendon Hastendon Follow To Closely Improper Turn Marge Hastendon Hastend	Apparently Faligued Over Follow Too Closely Fallow Too Closely Other Constituting Circ Not Listed Follow Too Closely Other Constituting Circ Not Listed Follow Too Closely Fallow Too Closely Institution Exceeding Reas. Safe Speed Institution Institution	Not Grant RVV to Vehicle 16 Female 17 Female 18 Female 19 Female 19 Female 19 Vehicle 19 Vehicle 10 Vehicle 10 Vehicle 10 Vehicle 10 Vehicle 11 Vehicle 12 Vehicle 13 Vehicle 14 Vehicle 15 Vehicle 16 Vehicle 17 Vehicle 18 Vehicle 19 Vehicle 19 Vehicle 10 Vehicle 10 Vehicle 11 Vehicle 12 Vehicle 13 Vehicle 14 Vehicle 15 Vehicle 16 Vehicle 17 Vehicle 18 Vehicle 19 Vehicle 19 Vehicle 10 Vehicle 10 Vehicle 11 Vehicle 12 Vehicle 13 Vehicle 14 Vehicle 15 Vehicle 16 Vehicle 17 Vehicle 18 Vehicle 19 Vehicle 19 Vehicle 10 Vehicle 10 Vehicle 11 Vehicle 12 Vehicle 13 Vehicle 14 Vehicle 15 Vehicle 16 Vehicle 17 Vehicle 17 Vehicle 18 Vehicle 19 Vehicle 19 Vehicle 10 Vehicle 10 Vehicle 11 Vehicle 12 Vehicle 13 Vehicle 14 Vehicle 15 Vehicle 16 Vehicle 17 Vehicle 17 Vehicle 18 Vehicle 19 Vehicle 19 Vehicle 19 Vehicle 10 Vehicle 10 Vehicle 10 Vehicle 11 Vehicle 12 Vehicle 13 Vehicle 14 Vehicle 15 Vehicle 16 Vehicle 17 Vehicle 17 Vehicle 18 Vehicle 19 Vehicle 19 Vehicle 10 Vehi	Notice reatminion Notice No	Operating Mandheld Cell Phone		71 MM M M 1 M M 1 M M 1 M M 1 M M 1 M M M M	stable shake				Did Not Grant RW to Vehicle		Maste	ate None		P P P P P P P P P P	set Both Stooder Increasing Magnet set Tourneaung Magnet set Tourn
3 On Not Coart RV to Notice hastendon Copraring Defective Equipment Did Not Claraft Not Vo Notice University State Volume University State Horizont Distriction State Claraft Horizont Licendring Research Licendring Research Follow To Closely Improper Turn Marge Hastendon Hastendon Follow To Closely Improper Turn Marge Hastendon Hastend	Apparently Faligued Over Follow Too Closely Fallow Too Closely Other Constituting Circ Not Listed Follow Too Closely Other Constituting Circ Not Listed Follow Too Closely Fallow Too Closely Institution Exceeding Reas. Safe Speed Institution Institution	Not Grant RVV to Vehicle 16 Female 17 Female 18 Female 19 Female 19 Female 19 Vehicle 19 Vehicle 10 Vehicle 10 Vehicle 10 Vehicle 10 Vehicle 11 Vehicle 12 Vehicle 13 Vehicle 14 Vehicle 15 Vehicle 16 Vehicle 17 Vehicle 18 Vehicle 19 Vehicle 19 Vehicle 10 Vehicle 10 Vehicle 11 Vehicle 12 Vehicle 13 Vehicle 14 Vehicle 15 Vehicle 16 Vehicle 17 Vehicle 18 Vehicle 19 Vehicle 19 Vehicle 10 Vehicle 10 Vehicle 11 Vehicle 12 Vehicle 13 Vehicle 14 Vehicle 15 Vehicle 16 Vehicle 17 Vehicle 18 Vehicle 19 Vehicle 19 Vehicle 10 Vehicle 10 Vehicle 11 Vehicle 12 Vehicle 13 Vehicle 14 Vehicle 15 Vehicle 16 Vehicle 17 Vehicle 17 Vehicle 18 Vehicle 19 Vehicle 19 Vehicle 10 Vehicle 10 Vehicle 11 Vehicle 12 Vehicle 13 Vehicle 14 Vehicle 15 Vehicle 16 Vehicle 17 Vehicle 17 Vehicle 18 Vehicle 19 Vehicle 19 Vehicle 19 Vehicle 10 Vehicle 10 Vehicle 10 Vehicle 11 Vehicle 12 Vehicle 13 Vehicle 14 Vehicle 15 Vehicle 16 Vehicle 17 Vehicle 17 Vehicle 18 Vehicle 19 Vehicle 19 Vehicle 10 Vehi	Notice reatminion Notice No	Counting Handheld Cell Phone		71 M M AM M	stable shake				Did Not Grant RW to Vehicle	10	Masic	Mone Mone		P P P P P P P P P P	set Both Stooder Increasing Magnet set Tourneasing Magnet set Tourneasing Magnet set Tourneasing Magnet set Tourneasing Magnet set Royal Stooder Increasing Magnet set Incre
3 On Not Coart RV to Notice hastendon Copraring Defective Equipment Did Not Claraft Not Vo Notice University State Volume University State Horizont Distriction State Claraft Horizont Licendring Research Licendring Research Follow To Closely Improper Turn Marge Hastendon Hastendon Follow To Closely Improper Turn Marge Hastendon Hastend	Apparently Faligued Over Follow Too Closely Fallow Too Closely Other Constituting Circ Not Listed Follow Too Closely Other Constituting Circ Not Listed Follow Too Closely Fallow Too Closely Institution Exceeding Reas. Safe Speed Institution Institution	Net Grant RVV to Vehicle (a) Female (b) Female (c) Female (d) Female (d) Female (d) Female (d) Female (d) Female (e) Female (e) Female (f) Mate (f) Ma	Norse statements Norse	Cperating Handheld Cell Phone		71 M Ab A	Andrew Communication of the Co				Out Not Grant RW to Vehicle	10	A Apai	ole None			sel Bohr Stooder Increasing Magnet ser I Docreasing Ma
3 On Not Coart RV to Notice hastendon Copraring Defective Equipment Did Not Claraft Not Vo Notice University State Volume University State Horizont Distriction State Claraft Horizont Licendring Research Licendring Research Follow To Closely Improper Turn Marge Hastendon Hastendon Follow To Closely Improper Turn Marge Hastendon Hastend	Apparently Faligued Over Follow Too Closely Fallow Too Closely Other Constituting Circ Not Listed Follow Too Closely Other Constituting Circ Not Listed Follow Too Closely Fallow Too Closely Institution Exceeding Reas. Safe Speed Institution Institution	Net Grant RVV to Vehicle 66 Female 76 Female 77 77 77 77 77 77 77	Norse Institution Norse Norse	Coexisting Handheld Cell Phone		71 M Ab A	stable shake				Out Not Grant RW to Vehicle	10	Akai	de None Other Contributing Circ Not Listed		6 hhs 6 6 hs 6 5 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	sel Body Stooder Increasing Magnet ser I Decreasing Ma
3 On Not Coart RV to Notice hastendon Copraring Defective Equipment Did Not Claraft Not Vo Notice University State Volume University State Horizont Distriction State Claraft Horizont Licendring Research Licendring Research Follow To Closely Improper Turn Marge Hastendon Hastendon Follow To Closely Improper Turn Marge Hastendon Hastend	Apparently Faligued Over Follow Too Closely Fallow Too Closely Other Constituting Circ Not Listed Follow Too Closely Other Constituting Circ Not Listed Follow Too Closely Fallow Too Closely Institution Exceeding Reas. Safe Speed Institution Institution	Net Grant RVV to Vehicle 76 Female 77 Female 78 Female 79 Female 70 Female 71 Female 73 Male 74 Male 75 Male 75 Male 76 Male 77 Male 77 Female 77 Female 78 Female 79 Male 70 Male 70 Male 71 Male 71 Male 72 Male 73 Male 74 Male 75 Male 76 Male 77 Male 77 Male 78 Male 79 Male 70 Male 71 Male 71 Male 72 Male 73 Male 74 Male 75 Male 76 Male 77 Male 78 Male 79 Male 70 Male 71 Male 71 Male 72 Male 73 Male 74 Male 75 Male 76 Male 77 Male 78 Male 79 Male 71 Male 71 Male 72 Male 73 Male 74 Male 75 Male 76 Male 77 Male 78 Male 79 Male	Norse stateminion Norse	Coexisting Handhold Cell Phone		71 M. M. A. M.	Adole				Old Not Grant RW to Vehicle	10	Man	de None Other Contributing Circ Not Listed		5 Abbe 5 Abb 5 Abbe 5 Abb 5	sel Body Stooder Increasing Magnet ser I Docreasing Ma
Do Not Court Riv to Vehicle Tatterides A state rice A	Apparently Fatigued Oner Fation Too Closely Exceeding Reas. Safe Speed Distances Could be While O Exceeding Reas. Safe Speed Distances Could be While O Exceeding Reas. Safe Speed Distances Could be While O Fation Too Closely Exceeding Reas. Safe Speed Onto Fation Too Closely Exceeding Reas. Safe Speed Onto Fation Too Closely Could Not Grant RW to Vehicle Order Closely Fation Too Closely Fation Too Closely Did Not Grant RW to Vehicle Fation Too Closely Fation Too Closely Nation Too Closely Onto Closely Ont	Net Grant RVV to Vehicle 76 Female 77 Female 78 Female 79 Female 70 Female 71 Female 73 Male 74 Male 75 Male 75 Male 76 Male 77 Male 77 Female 77 Female 78 Female 79 Male 70 Male 70 Male 71 Male 71 Male 72 Male 73 Male 74 Male 75 Male 76 Male 77 Male 77 Male 78 Male 79 Male 70 Male 71 Male 71 Male 72 Male 73 Male 74 Male 75 Male 76 Male 77 Male 78 Male 79 Male 70 Male 71 Male 71 Male 72 Male 73 Male 74 Male 75 Male 76 Male 77 Male 78 Male 79 Male 71 Male 71 Male 72 Male 73 Male 74 Male 75 Male 76 Male 77 Male 78 Male 79 Male	Norse stateminion Norse	Operating Handhold Cell Phone		71 MA 1 MA	Adulta Ad			Problemsion .	Did Not Grant RW to Wehide	10	Model	ate None Other Contributing Circ Nort Listed			set Both Stooder Increasing Magnet set Toursaing Ma
Do Not Court Riv to Vehicle Tatterides A state rice A	Apparently Fatigued Oner Fation Too Closely Exceeding Reas. Safe Speed Distances Safe Speed Distances Safe Speed Exceeding Reas. Safe Speed Distances Safe Speed Distances Safe Speed Distances Safe Speed Distances Safe Speed Onto Fation Too Closely Exceeding Reas. Safe Speed Distances Safe Speed Onto Fation Too Closely Fation Too Closely Distances Safe Speed Distances Safe Speed Onto Fation Too Closely Distances Safe Speed Onto Distances Safe Safe Speed Onto Distances Safe Speed Onto Distances Safe Safe Safe Speed Onto Distances Safe Safe Safe Safe Safe Safe Safe Safe	Net Grant RVV to Vehicle 76 Female 77 Female 78 Female 79 Female 79 Female 79 Female 79 Male 79 Male 79 Male 70 Male 70 Male 71 Male 72 Female 73 Male 74 Male 75 Male 76 Female 77 Male 77 Female 78 Female 79 Male 70 Male 70 Male 71 Male 72 Female 73 Male 74 Female 75 Male 76 Female 77 Male 78 Female 79 Male 70 Male 71 Male 71 Male 72 Female 73 Male 74 Male 75 Male 76 Male 77 Male 77 Male 78 Male 79 Male 70 Male 71 Male 71 Male 72 Female 73 Male 74 Male 75 Male 76 Male 77 Male 78 Male 79 Male 70 Male 70 Male 71 Male 72 Male 73 Male 74 Male 75 Male 76 Male 77 Male 78 Male 79 Male 70 Male 70 Male 71 Male 72 Female 73 Male 74 Male 75 Male 76 Male 77 Male 78 Male 79 Male 70 Male 70 Male 71 Male 72 Female 73 Male 74 Male 75 Male 76 Male 77 Male 78 Male 79 Male 70 Male 71 Male 72 Female 73 Male 74 Male 75 Male 76 Male 77 Male	Norse stateminion Norse	Operating Handhold Cell Phone		71 MA 1 MA	stable in the st			Problemsion .	Out Not Grant RW to Vehicle		Male	ate None Ceter Contributing Circ Not Listed			set Both Stooder Increasing Magnet set Donnard Stooder Increasing Magnet set Donnard Stooder Increasing Magnet set Donnard Magnet set Tomat Donnard Magnet set S
Do Not Court Riv to Vehicle Tatterides A state rice A	Apparently Fatigued Oner Fation Too Closely Exceeding Reas. Safe Speed Distances Safe Speed Distances Safe Speed Exceeding Reas. Safe Speed Distances Safe Speed Distances Safe Speed Distances Safe Speed Distances Safe Speed Onto Fation Too Closely Exceeding Reas. Safe Speed Distances Safe Speed Onto Fation Too Closely Fation Too Closely Distances Safe Speed Distances Safe Speed Onto Fation Too Closely Distances Safe Speed Onto Distances Safe Safe Speed Onto Distances Safe Speed Onto Distances Safe Safe Safe Speed Onto Distances Safe Safe Safe Safe Safe Safe Safe Safe	Net Grant RVV to Vehicle 76 Female 77 Female 78 Female 79 Female 79 Female 79 Female 79 Male 79 Male 79 Male 70 Male 70 Male 71 Male 72 Female 73 Male 74 Male 75 Male 76 Female 77 Male 77 Female 78 Female 79 Male 70 Male 70 Male 71 Male 72 Female 73 Male 74 Female 75 Male 76 Female 77 Male 78 Female 79 Male 70 Male 71 Male 71 Male 72 Female 73 Male 74 Male 75 Male 76 Male 77 Male 77 Male 78 Male 79 Male 70 Male 71 Male 71 Male 72 Female 73 Male 74 Male 75 Male 76 Male 77 Male 78 Male 79 Male 70 Male 70 Male 71 Male 72 Male 73 Male 74 Male 75 Male 76 Male 77 Male 78 Male 79 Male 70 Male 70 Male 71 Male 72 Female 73 Male 74 Male 75 Male 76 Male 77 Male 78 Male 79 Male 70 Male 70 Male 71 Male 72 Female 73 Male 74 Male 75 Male 76 Male 77 Male 78 Male 79 Male 70 Male 71 Male 72 Female 73 Male 74 Male 75 Male 76 Male 77 Male	Norse stateminion Norse	Operating Handheld Cell Phone		71 MA 1 MA	stable in the st			Problemsion .	Out Not Grant RW to Webside	10	Male			5 Males S	set Body Stooder Increasing Magnet set Dourseaung Magnet set S
Do Not Court Riv to Vehicle Tatterides A state rice A	Apparently Fatigued Oner Fation Too Closely Exceeding Reas. Safe Speed Distances Safe Speed Distances Safe Speed Exceeding Reas. Safe Speed Distances Safe Speed Distances Safe Speed Distances Safe Speed Distances Safe Speed Onto Fation Too Closely Exceeding Reas. Safe Speed Distances Safe Speed Onto Fation Too Closely Fation Too Closely Distances Safe Speed Distances Safe Speed Onto Fation Too Closely Distances Safe Speed Onto Distances Safe Safe Speed Onto Distances Safe Speed Onto Distances Safe Safe Safe Speed Onto Distances Safe Safe Safe Safe Safe Safe Safe Safe	Net Grant RVV to Vehicle 76 Female 77 Female 78 Female 79 Female 79 Female 79 Female 79 Male 79 Male 79 Male 70 Male 70 Male 71 Male 72 Female 73 Male 74 Male 75 Male 76 Female 77 Male 77 Female 78 Female 79 Male 70 Male 70 Male 71 Male 72 Female 73 Male 74 Female 75 Male 76 Female 77 Male 78 Female 79 Male 70 Male 71 Male 71 Male 72 Female 73 Male 74 Male 75 Male 76 Male 77 Male 77 Male 78 Male 79 Male 70 Male 71 Male 71 Male 72 Female 73 Male 74 Male 75 Male 76 Male 77 Male 78 Male 79 Male 70 Male 70 Male 71 Male 72 Male 73 Male 74 Male 75 Male 76 Male 77 Male 78 Male 79 Male 70 Male 70 Male 71 Male 72 Female 73 Male 74 Male 75 Male 76 Male 77 Male 78 Male 79 Male 70 Male 70 Male 71 Male 72 Female 73 Male 74 Male 75 Male 76 Male 77 Male 78 Male 79 Male 70 Male 71 Male 72 Female 73 Male 74 Male 75 Male 76 Male 77 Male	Norse stateminion Norse	Operating Handheld Cell Phone		71 Ma 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	stable in the st			Interdion Interdion None	Did Not Grant RW to Wehide	10	Male				sel Boht Stooder Increasing Magnet sel Tournamy Magnet sel Tournam
Do Not Court Riv to Vehicle Tatterides A state rice A	Apparently Fatigued Oner Fation Too Closely Exceeding Reas. Safe Speed Distances Safe Speed Distances Safe Speed Exceeding Reas. Safe Speed Distances Safe Speed Distances Safe Speed Distances Safe Speed Distances Safe Speed Onto Fation Too Closely Exceeding Reas. Safe Speed Distances Safe Speed Onto Fation Too Closely Fation Too Closely Distances Safe Speed Distances Safe Speed Onto Fation Too Closely Distances Safe Speed Onto Distances Safe Safe Speed Onto Distances Safe Speed Onto Distances Safe Safe Safe Speed Onto Distances Safe Safe Safe Safe Safe Safe Safe Safe	NN Grant RVV to Vehicle 176 Female 176 Female 177 Share 177 Share 178 Share 179 Share 170 Share 170 Share 170 Share 170 Share 171 Share 171 Share 172 Share 173 Share 174 Share 175 Share 176 Share 177 Share 178 Share 179 Share 170 Share 170 Share 170 Share 170 Share 171 Share 171 Share 172 Share 173 Share 174 Share 175 Share 176 Share 177 Share 178 Share 179 Share 170 Share 170	Notes Pasterioron Proces Proces Proces Out Not Coars RW to Vehicle Notes Out Not Coars RW to Vehicle Notes Notes	Operating Handheld Cell Phone		71 Ma 1	Adulte Adulte			Interdion Interdion None	Old Not Grant RW to Welvole	10	Male			56 bhis	set Both Stooder Increasing Magnet set Donnard Stooder Increasing Magnet set Donnard Stooder Increasing Magnet set Donnard Magnet set Townard Magn
3 On the Occurrent of the Whelse Laboration of the Country of the Whelse Laboration of the Country of the Count	Apparently Feligued One Felion Toc Closely Felion Toc Closely Fallow Toc Closely Fa	NN Grant RVV to Vehicle 176 Female 176 Female 177 Share 177 Share 178 Share 179 Share 170 Share 170 Share 170 Share 170 Share 171 Share 171 Share 172 Share 173 Share 174 Share 175 Share 176 Share 177 Share 178 Share 179 Share 170 Share 170 Share 170 Share 170 Share 171 Share 171 Share 172 Share 173 Share 174 Share 175 Share 176 Share 177 Share 178 Share 179 Share 170 Share 170	Notes Pasterioron Proces Proces Proces Out Not Coars RW to Vehicle Notes Out Not Coars RW to Vehicle Notes Notes	Counting Handheld Cell Phone		71 Ma 1	Adulte Adulte			Interdion Interdion None	Old Nort Grant RWV to Wehode	10	Male				set Both Stooder Increasing Magnet set Toursang Magnet set Toursan
3 On the Occurrent of the Whelse Laboration of the Country of the Whelse Laboration of the Country of the Count	Apparently Feligued One Felion Toc Closely Felion Toc Closely Fallow Toc Closely Fa	NR Grant RVV to Vehicle 76 Female 87 Female 98 Female 99 Made 90 Made 90 Made 91 Made 91 Made 93 Made 94 Made 95 Female 96 Made 97 Made 98 Made 99 Made 99 Made 90 Made 91 Made 91 Made 92 Made 93 Made 94 Made 95 Female 96 Made 97 Made 97 Made 98 Made 99 Made 99 Made 90 Made 90 Made 91 Made 91 Made 92 Made 93 Made 94 Made 95 Made 96 Made 97 Made 98 Made 99 Made 99 Made 90 Made 91 Made 91 Made 92 Made 93 Made 94 Made 95 Made 96 Made 97 Made 98 Made 99 Made 99 Made 99 Made 90 Made 90 Made 90 Made 91 Made 92 Made 93 Made 94 Made 95 Made 96 Made 97 Made 98 Made 99 Made 99 Made 99 Made 90 Made 90 Made 90 Made 90 Made 90 Made 91 Made 92 Made 93 Made 94 Made 95 Made 96 Made 97 Made 98 Made 99 Made 99 Made 99 Made 90 Made	Norse restaurcion Norse Nor	Counting Handheld Cell Phone Counting Handheld Cell Phone		71 Ma 72 Par 73 Par 74 Par 75 Par 76 Par 77 Par 78 Par 78 Par 78 Par 78 Par 78 Par 78 Par	Abbie Abbie Fernale Fernale Abbie Abbie			Interdion Interdion None	Did Not Grant RW to Vehicle	10	Male				set Body Stooder Increasing Magnet are I Dozessay Magnet are I Doz
3 On the Occurrent of the Whelse Laboration of the Country of the Whelse Laboration of the Country of the Count	Apparently Feligued One Felion Toc Closely Felion Toc Closely Fallow Toc Closely Fa	NN Grant RVV to Vehicle 17 Female 18 Female 19 Female 19 March 19 March 19 March 19 March 19 March 10 March 11 March 12 March 13 March 14 March 15 March 16 March 17 March 18 March 19 March 19 March 10 March 10 March 11 March 12 March 13 March 14 March 15 March 16 March 17 March 18 March 19 March 19 March 10 March 10 March 10 March 11 March 12 March 13 March 14 March 15 March 16 March 17 March 18 March 19 March 19 March 10 March 10 March 10 March 11 March 12 March 13 March 14 March 15 March 16 March 17 March 18 March 19 March 19 March 10 March 10 March 10 March 10 March 11 March 12 March 13 March 14 March 15 March 16 March 17 March 18 March 19 March 19 March 10 March 10 March 10 March 11 March 12 March 13 March 14 March 15 March 16 March 17 March 18 March 19 March 19 March 10 March 10	Norse reatention Norse Norse	Counting Handheld Cell Phone		71 M M M M M M M M M M M M M M	shahe			Interdion Interdion None	Did Not Grant RW to Vehicle	10	Male				set Both Stooder Increasing Magnet set Tournessing Magnet set Magnet Stooder Increasing Magnet set Magnet Stooder Increasing Magnet set Magnet Stooder Increasing Magnet set Tournessing Magnet set Tournessin
3 On the Occurrent of the Whelse Laboration of the Country of the Whelse Laboration of the Country of the Count	Apparently Feligued One Felion Toc Closely Felion Toc Closely Fallow Toc Closely Fa	NNG Grant RVV to Vehicle 17 Female 17 Care 18 Care 19 Care 19 Care 10 Care 11 Care 12 Care 13 Made 14 Care 15 Care 16 Care 17 Care 18 Care 19 Made 10 Care 10 Care 11 Care 12 Care 13 Care 14 Care 15 Care 16 Care 17 Care 18 Care 19 Care 10 Care 11 Care 11 Care 12 Care 13 Care 14 Care 15 Care 16 Care 17 Care 18 Care 19 Care 10 Care 11 Care 12 Care 13 Care 14 Care 15 Care 16 Care 17 Care 18 Care 19 Care 19 Care 10 Care 10 Care 11 Care 12 Care 13 Care 14 Care 15 Care 16 Care 17 Care 18 Care 19 Care 10 Care 11 Care 12 Care 13 Care 14 Care 15 Care 16 Care 17 Care 18 Care 19 Care 19 Care 10 Care 11 Care 11 Care 12 Care 13 Care 14 Care 15 Care 16 Care 17 Care 18 Care 19 Care 10 Care 10 Care 11 Care 12 Care 13 Care 14 Care 15 Care 16 Care 17 Care 18 Care 19 Care 10 Care 11 Care 12 Care 13 Care 14 Care 15 Care 16 Care 17 Care 18 Care 19 Care 10 Care 11 Care 12 Care 13 Care 14 Care 15 Care 16 Care 17 Care 18 Care 19 Care 10 Care 11 Care 12 Care 13 Care 14 Care 15 Care 16 Care 17 Care 18 Care 19 Care 10 Care 11 Care 12 Care 13 Care 14 Care 15 Care 16 Care 17 Care 18 Care 19 Care 10 Care 10 Care 10 Care 11 Care 12 Care 13 Care 14 Care 15 Care 16 Care 17 Care 18 Care 19 Care 10 Care 10 Care 11 Care 12 Care 13 Care 14 Care 15 Care 16 Care 17 Care 18 Care 19 Care 10 Care	Norse statemins Norse	Operating Mandheld Cell Phone Operating Mandheld Cell Phone		71 M.	Abbie			Interdion Interdion None	Did Not Grant RW to Wehole	10	Male			6 hhis C C C C C C C C C C C C C C C C C C C	set Body Stooder Increasing Magnet set Doctorsaming Magnet set Toctorsaming Ma
3 On the Occurrent of the Whelse Laboration of the Country of the Whelse Laboration of the Country of the Count	Apparently Feligued One Felion Toc Closely Felion Toc Closely Fallow Toc Closely Fa	NR Grant RVV to Vehicle 76 Female 87 Center Line 87 Center Line 88 Female 98 Female 99 Made 90 Made 90 Made 91 Made 91 Made 93 Made 94 Made 95 Female 96 Made 97 Made 98 Made 99 Made 90 Made 90 Made 90 Made 91 Made 91 Made 92 Made 93 Made 94 Made 95 Female 96 Made 97 Made 97 Made 98 Made 99 Made 99 Made 90 Made 91 Made 91 Made 92 Made 93 Made 94 Made 95 Made 96 Made 97 Made 97 Made 98 Made 99 Made 99 Made 90 Made 90 Made 90 Made 91 Made 92 Made 93 Made 94 Made 95 Made 96 Made 97 Made 98 Made 99 Made 99 Made 90 Made 90 Made 90 Made 91 Made 93 Made 94 Made 95 Made 96 Made 97 Made 98 Made 99 Made 99 Made 99 Made 90 Made 90 Made 90 Made 90 Made 90 Made 91 Made 92 Made 93 Made 94 Made 95 Made 96 Made 97 Made 97 Made 98 Made 99 Made 99 Made 90 Made 91 Made 91 Made 92 Made 93 Made 94 Made 95 Made 96 Made 97 Made 97 Made 98 Made 99 Made 90 M	Norma restamentors Norma No	Operating Handheld Gell Phone		71 Ma 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	Adultation of the control of the con			Interdion Interdion None	Did Not Gent RW to Vehicle	10	Male			5 Abbot 2 Abbot 2 Abbot 3 Abbot 4 Abbot 5 Abbo	set Body Stooder Increasing Magnet set Decreasing Magnet set Decreasing Magnet set Decreasing Magnet set Technologies (Magnet Magnet) set Technologies (Magnet) set Technologi
3 On the Occurrent of the Whelse Laboration of the Country of the Whelse Laboration of the Country of the Count	Apparently Feligued One Felion Toc Closely Felion Toc Closely Fallow Toc Closely Fa	NR Grant RVV to Vehicle 76 Female 87 Center Line 87 Center Line 88 Female 98 Female 99 Made 90 Made 90 Made 91 Made 91 Made 93 Made 94 Made 95 Female 96 Made 97 Made 98 Made 99 Made 90 Made 90 Made 90 Made 91 Made 91 Made 92 Made 93 Made 94 Made 95 Female 96 Made 97 Made 97 Made 98 Made 99 Made 99 Made 90 Made 91 Made 91 Made 92 Made 93 Made 94 Made 95 Made 96 Made 97 Made 97 Made 98 Made 99 Made 99 Made 90 Made 90 Made 90 Made 91 Made 92 Made 93 Made 94 Made 95 Made 96 Made 97 Made 98 Made 99 Made 99 Made 90 Made 90 Made 90 Made 91 Made 93 Made 94 Made 95 Made 96 Made 97 Made 98 Made 99 Made 99 Made 99 Made 90 Made 90 Made 90 Made 90 Made 90 Made 91 Made 92 Made 93 Made 94 Made 95 Made 96 Made 97 Made 97 Made 98 Made 99 Made 99 Made 90 Made 91 Made 91 Made 92 Made 93 Made 94 Made 95 Made 96 Made 97 Made 97 Made 98 Made 99 Made 90 M	Norma restamentors Norma No	Operating Handhold Cell Phone		71 M.	Adulte Ad			Praterision Praterision None None	Old Not Grant RW to Vehicle	16	Male				set Both Stooder Increasing Magnet set Toursaing Ma
3 On the Occurrent of the Whelse Laboration of the Country of the Whelse Laboration of the Country of the Count	Apparently Feligued One Felion Toc Closely Felion Toc Closely Fallow Toc Closely Fa	NN Grant RVV to Vehicle 17 Female 18 Female 19 Female 19 Female 19 Female 19 Female 19 Female 10 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Nutro 18 Female 19 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Nutro 18 Female 19 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Nutro 18 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 19 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 10 Fema	Norte reatention Norte Norte	Operating Handhold Cell Phone		71 Me 1	Adulte Ad			Praterision Praterision None None	Ood Not Grant RW to Vehicle	56	Male	male		56 bhis	set Both Stooder Increasing Magnet set Doctorsamp Magnet set Toctorsamp Magnet set Tocto
3 On the Occurrent of the Whelse Laboration of the Country of the Whelse Laboration of the Country of the Count	Apparently Feligued One Felion Toc Closely Felion Toc Closely Fallow Toc Closely Fa	NN Grant RVV to Vehicle 17 Female 18 Female 19 Female 19 Female 19 Female 19 Female 19 Female 10 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Nutro 18 Female 19 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Nutro 18 Female 19 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Nutro 18 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 19 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 10 Fema	Norte reatention Norte Norte	Operating Handhold Cell Phone		71 Me 1	Adulte Ad			None None None		10	Abole Ferr	male			set Both Stooder Increasing Magnet set Toursang Magnet set Toursan
3 On the Occurrent of the Whelse Laboration of the Country of the Whelse Laboration of the Country of the Count	Apparently Feligued One Felion Toc Closely Felion Toc Closely Fallow Toc Closely Fa	NN Grant RVV to Vehicle 17 Female 18 Female 19 Female 19 Female 19 Female 19 Female 19 Female 10 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Nutro 18 Female 19 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Nutro 18 Female 19 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Nutro 18 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 19 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 10 Fema	Norte reatention Norte Norte	Operating Handheld Cell Phone		71 May 171 May	which which was a state of the			None None None		55	Abole Ferr	male			sel Bohd Shoulder Increasing Magnet set Toursang Magnet set Toursa
3 On the Old Court First to Netheria **Laterianian** **Laterianian** **Laterianian** **Laterianian** **Laterianian** **Laterianian** **Laterianian** **Laterianianianianianianianianianianianianiani	Apparently Fatigued Oner Fation Too Closely Exceeding Reas. Safe Speed Distances Counted Whole Exceeding Reas. Safe Speed Fation Too Closely Exceeding Reas. Safe Speed Obstances Counted Whole Fation Too Closely Exceeding Reas. Safe Speed Obstances Fation Too Closely Exceeding Reas. Safe Speed Obstances Fation Too Closely Exceeding Reas. Safe Speed Obstances Obstances Obstances Obstances Obstances Obstances Obstances Do Not Grant RW to Vehicle Underwork Valences Underwork Valences Underwork Valences Obstances Obstances Valences Obstances Valences Obstances Valences Obstances Valences Obstances Valences Obstances Valences Valences Obstances Valences Valenc	NN Grant RVV to Vehicle 17 Female 18 Female 19 Female 19 Female 19 Female 19 Female 19 Female 10 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Nutro 18 Female 19 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Nutro 18 Female 19 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Nutro 18 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 19 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 10 Fema	Norte reatention Norte Norte	Operating Handheld Cell Phone		71 May 1 May	Abbie			None None None		10 10	Abole Ferr	male		56 bhis 2 1 Mse 2 1 Mse 3 2 1 Mse 4 2 1 Mse 5 3 4 5 5 5 5 5 5 5 5 5 5 5 5 5 5 5 5 5	set Both Stooder Increasing Magnet set Doctorsamp Magnet set Toctorsamp Magnet set Tocto
3 On the Occurrence of the Country of the Netherland Landstoner. And the Country of the Country	Apparently Fatigued Oner Fation Too Closely Exceeding Reas. Safe Speed Distances Counted Whole Exceeding Reas. Safe Speed Fation Too Closely Exceeding Reas. Safe Speed Obstances Counted Whole Fation Too Closely Exceeding Reas. Safe Speed Obstances Fation Too Closely Exceeding Reas. Safe Speed Obstances Fation Too Closely Exceeding Reas. Safe Speed Obstances Obstances Obstances Obstances Obstances Obstances Obstances Do Not Grant RW to Vehicle Underwork Valences Underwork Valences Underwork Valences Obstances Obstances Valences Obstances Valences Obstances Valences Obstances Valences Obstances Valences Obstances Valences Valences Obstances Valences Valenc	NN Grant RVV to Vehicle 17 Female 18 Female 19 Female 19 Female 19 Female 19 Female 19 Female 10 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Nutro 18 Female 19 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Nutro 18 Female 19 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Nutro 18 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 19 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 10 Fema	Norte reatention Norte Norte	Operating Handheld Cell Phone		71 MA 72 PA 73 PA 74 PA 75 PA 76 PA 76 PA 76 PA 76 PA 76 PA 77 PA 76 PA 76 PA 76 PA 76 PA 77 PA	Adulte Ad			None None None		10	Abole Ferr	male		50 Males 21 Males 22 Males 3	set Body Stooder Increasing Magnet set Decreasing Magnet set Decre
3 On the Occurrence of the Country of the Netherland Landstoner. And the Country of the Country	Apparently Fatigued Oner Fation Too Closely Exceeding Reas. Safe Speed Distances Counted Whole Exceeding Reas. Safe Speed Fation Too Closely Exceeding Reas. Safe Speed Obstances Counted Whole Fation Too Closely Exceeding Reas. Safe Speed Obstances Fation Too Closely Exceeding Reas. Safe Speed Obstances Fation Too Closely Exceeding Reas. Safe Speed Obstances Obstances Obstances Obstances Obstances Obstances Obstances Do Not Grant RW to Vehicle Underwork Valences Underwork Valences Underwork Valences Obstances Obstances Valences Obstances Valences Obstances Valences Obstances Valences Obstances Valences Obstances Valences Valences Obstances Valences Valenc	NN Grant RVV to Vehicle 17 Female 18 Female 19 Female 19 Female 19 Female 19 Female 19 Female 10 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Nutro 18 Female 19 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Nutro 18 Female 19 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Nutro 18 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 19 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 10 Fema	Norte reatention Norte Norte	Cyerating Handheld Cell Phone		71 M M 72 M M 72 M M 72 M M 72 M M M 72 M M M M	Abbie			None None None None		10	Abole Ferr	male			set Both Stooder Increasing Magnet set Toursang Magnet set Toursan
3 On the Occurrence of the Country of the Netherland Landstoner. And the Country of the Country	Apparently Fatigued Oner Fation Too Closely Exceeding Reas. Safe Speed Distances Counted Whole Exceeding Reas. Safe Speed Fation Too Closely Exceeding Reas. Safe Speed Obstances Counted Whole Fation Too Closely Exceeding Reas. Safe Speed Obstances Fation Too Closely Exceeding Reas. Safe Speed Obstances Fation Too Closely Exceeding Reas. Safe Speed Obstances Obstances Obstances Obstances Obstances Obstances Obstances Do Not Grant RW to Vehicle Underwork Valences Underwork Valences Underwork Valences Obstances Obstances Valences Obstances Valences Obstances Valences Obstances Valences Obstances Valences Obstances Valences Valences Obstances Valences Valenc	NN Grant RVV to Vehicle 17 Female 18 Female 19 Female 19 Female 19 Female 19 Female 19 Female 10 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Nutro 18 Female 19 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Nutro 18 Female 19 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Nutro 18 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 19 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 10 Fema	Norte reatention Norte Norte	Counting Mandheld Cell Phone		71 M. M. 17 M.	Abbie Marie			None None None None		56	Feet Feet Feet Model	male			set Both Stooder Increasing Magnet set Toursaing Ma
3 On the Occurrent of the Whelse Laboration of the Country of the Whelse Laboration of the Country of the Count	Apparently Fatigued Oner Fation Too Closely Exceeding Reas. Safe Speed Distances Counted Whole Exceeding Reas. Safe Speed Fation Too Closely Exceeding Reas. Safe Speed Obstances Counted Whole Fation Too Closely Exceeding Reas. Safe Speed Obstances Fation Too Closely Exceeding Reas. Safe Speed Obstances Fation Too Closely Exceeding Reas. Safe Speed Obstances Obstances Obstances Obstances Obstances Obstances Obstances Do Not Grant RW to Vehicle Underwork Valences Underwork Valences Underwork Valences Obstances Obstances Valences Obstances Valences Obstances Valences Obstances Valences Obstances Valences Obstances Valences Valences Obstances Valences Valenc	NN Grant RVV to Vehicle 17 Female 18 Female 19 Female 19 Female 19 Female 19 Female 19 Female 10 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Nutro 18 Female 19 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Nutro 18 Female 19 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Nutro 18 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 19 Female 10 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 19 Female 10 Female 10 Female 11 Female 12 Female 13 Female 14 Female 15 Female 16 Female 17 Female 18 Female 19 Female 10 Fema	Notes reatminion Notes Note	Operating Handhold Gell Phone		71 M. M. 17 M.	Adole de la companya dela company			None None None None		65	Feet Feet Feet Model	male		50 Motes 21 Motes 22 Motes 23 Motes 24 Motes 25 Motes 26 Motes 26 Motes 27 Motes 28 Motes 29 Motes 20 Motes 30 Motes 40 Mot	set Both Stooder Increasing Magnet set Toursang Magnet set Toursan

OBJECTID WA_STATE_PLANE SOUTH_Y 2010_FORWARD CT				COL COL_LIGHT SURFACE CONDITIONS CONDITIONS	COL COL ST LOCATION GEOMETRY AF	DY COL TYPE SEVERITY TEXT TEXT	COL_HUMAN COL FACTOR_Decoded MVIW	COL_LIGHT COL_SUI CONDITIONS CONDIT Decoded Decode		COL_RISKY Intersection BEHAVIOR Area_Name Emphasis
7381 1094425.95 945150.62120	318 1 Vehicle-Party Animal 199 1 Vehicle-Party Struck Object 319 1 Vehicle-Party Overturmed 317 1 Vehicle-Party Over Embankment - No Guardrail Present	Property Damage Only Speeding 35-4		Dry Daylight Ice/Snow/Slush Dark-No Street Lights/Street Lights Off Dry Daylight Ice/Snow/Slush Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved Motorcycle No Vulnerable Road User Involved		Daylight Dry Dark-No Street Lights/Street Lights Off Ice/Snow Daylight Dry Dark-No Street Lights/Street Lights Off Ice/Snow	Not at Intersection/Driveway/Roundabout Visiush Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Visiush Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Speeding Under Influence of Alcohol/Drugs/Medication Speeding
7383 1094550.96 945610.48 20	318 1 Vehicle/Party Struck Object 199 1 Vehicle/Party Animal 18 1 Vehicle/Party Animal 316 1 Vehicle/Party Struck Object	Minor or Possible Injury Distracted Driving 55-4 Property Damage Only Other/Unknown 35- Property Damage Only Other/Unknown 45-5 Property Damage Only Operating Defective Equipment 25-3	1 No 4 No	Dry Daylight Dry Dawn/Dusk Dry Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Curve at Hillcrest Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Grade	No Vuinerable Road User Involved No Vuinerable Road User Involved No Vuinerable Road User Involved	Operating Handheld Cell Phone None None	Daylight Dry Dawn/Dusk Dry Dark-No Street Lights/Street Lights Off Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Distracted Driving No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved
7387 1094814.63 946657.05 20°	017 1 Vehicle/Party Struck Object		4 Apparently Asleep, Fatigued or III 25-34	Dry Daylight	Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved		Dark-No Street Lights/Street Lights Off We	t Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Inattention Apparently Asleep, Fatigued or III
7391 1095067.3 947880.27 20	16 1 Wehlde/Party	Menor er Possible Injusy Apparenty Aslesep, Fatigued or III 25-5- Property Damage Only Defeutlufknown 35-4- Property Damage Only Obset/Unknown Unc. Menor er Possible Injusy Obset/Unknown 35-4- Menor er Possible Injusy Obset/Unknown 35-4-	No	Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Unknown Not at Intersection/Driveway/Roundabout Straight & Grade	No Vulnerable Road User Involved	Other Contributing Circ Not Listed	Daylight Dry		No Risky Driving Behaviors Involved
7392 1095067.1 947856.5 207 7393 1095039.15 948122.34 207 7394 1095025.8 948197.63 207 7395 1094960.94 948366.33 207	319 2 Vehicles/Parties Sideswipe 18 1 Vehicle/Party Overturned 18 1 Vehicle/Party Overturned 18 1 Vehicle/Party Over Embankment - No Guardrail Present 3 Vehicles/Parties Rear-End	Unknown Speeding Unk Suspected Serious Injury Apparently Asleep, Fatigued or III 65+ Property Damage Only Follow Too Closely 45-6	own No		Not at Intersection. Driveway Roundabout Curve & Grade Not at Intersection. Oriveway Roundabout Curve & Grade Not at Intersection. Oriveway Roundabout Curve & Level At Driveway or Intersection Straight & Level	No Vulnerable Road User Involved Older Road User 65+	Speeding Driver Physical Condition Follow Too Closely	Daylight Dry Dark-Street Lights On Dry Dark-Street Lights On Dry Daylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Speeding Apparently Asleep, Fatigued or III Follow Too Closely
7396 1094953.96 948394.64 20 7397 1094945.44 948376.06 20 7398 1094953.96 948394.64 20	114 2 Vehides/Parties Turning 16 2 Vehides/Parties Angle 116 2 Vehides/Parties Rear-End	Property Damage Only Distracted Driving Unc Suspected Serious Injury Improper Turn/Merge 65+ Minor or Possible Injury Inattention Unc	rr 25 No	Dry Daylight Dry Daylight Wet Dark-Street Lights On	At Driveway or Intersection Straight & Level At Driveway or Intersection Straight in Sag At Driveway or Intersection Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved	Driver Adjusting Audio or Entertainment Improper Turn/Merge Inattention		At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Distracted Driving No Risky Driving Behaviors Involved Inattention
7400 1094816.87 948737.98.20 7401 1094699.37 949032.28.20	2 Vehides/Parties Rear-End	Minor or Possible Injury Follow Too Closely 65* Property Damage Only Apparently Acleep, Fatigued or III 65* Property Damage Only Other Unknown 45* Property Damage Only Other Unknown 55*	No	Dry Daylight Wet Dark-No Street Lights /Street Lights Off Wet Daylight Dry Daylight	At Drivwway or Intersection Straight & Level Not at Intersection/Drivway/Roundabout Straight & Level Not at Intersection/Drivway/Roundabout Straight & Level Not at Intersection/Drivway/Roundabout Curve & Level	Older Road User 65+ Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Apparently Asleep or Fatigued	Daylight Dry Dark-No Street Lights/Street Lights Off We Daylight We	At Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Follow Too Closely Apparently Asleep, Fatigued or III NO Risky Driving Behaviors Involved NO Risky Driving Behaviors Involved
7403 1093536 953173.31 201 7404 1093518.9 953221.03 201 7405 1093491.45 953297.12 201	14 1 Vehicle Party	Property Damage Only Other/Unknown 55-6 Minor or Possible Injury Distracted Driving 65+	4 No	Dry Dark-No Street Lights/Street Lights Off Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Straight & Level Intersection Related but Not at Intersection Straight & Grade AD Driveway or Intersection Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved		Dark-No Street Lights/Street Lights Off Dry Daylight Dry Daylight Dry		No Risky Driving Behaviors Involved Distracted Driving Inattention
7406 1093504.38 953278.7 201 7407 1093489.99 953324.93 201 7408 1093498.02 953300.31 201	119 2 Vehicles/Parties Rear-End 104 2 Vehicles/Parties Turning 116 3 Vehicles/Parties Turning	Suspected Serious Injury Speeding 55-6	4 No	Dry Daylight Dry Daylight Dry Daylight	Intersection Related but Not at Intersection Straight & Level At Driveway or Intersection Straight & Grade At Driveway or Intersection Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Distractions Outside Vehicle Speeding	Daylight Dry Daylight Dry Daylight Dry Daylight Dry	Intersection Related but Not at Intersection At Driveway or Intersection At Driveway or Intersection	Inattention Distracted Driving Speeding
	1016 1 Vehicle/Party	Mnor or Possible Injury Inattention 65+ Mnor or Possible Injury Inattention Unc Mnor or Possible Injury Other/Unknown 65+ Possible Injury Other/Unknown 65+	No N	Dry Dark-No Street Lights/Street Lights Off Wet Dark-No Street Lights/Street Lights Off Dry Daylight Met Onto No Street Lights (Street Lights Off	Not at Intersection/Driveway/Roundabout Curve & Grade	Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+	Other Contributing Circ Not Eisted	Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off We Daylight Dry Light Street Lights Off We	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Inattention Inattention No Risky Driving Behaviors Involved
7413 1093378.06 954897.06 20 7414 1093283.91 955354.64 20 7415 1093283.31 955356.66 20	116 1 Vehicle/Party Animal 108 1 Vehicle/Party Struck Object 114 1 Vehicle/Party Animal 117 1 Vehicle/Party Overturned	Property Damage Only Other/Unknown 554 Mnor or Possible Injury Under Influence of Alcohol/Drugs/Medication 45-4 Property Damage Only Other/Unknown 35- Property Damage Only Operating Defective Equipment 55-4	Under Influence of Alcohol and Apparently Asleep, Fatigued or III 45-54 No 4 No 4 No	Wet Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off Dry Daylight	Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved	Under Influence of Alcohol/Drugs/Medication Other/Unknown	Dark-No Street Lights/Street Lights Off We Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off Dry Daylight Dry	tt Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved
7416 1093265.31 955408.19 20	318 1 Vehicle/Party Struck Object 199 1 Vehicle/Party Struck Object 14 1 Vehicle/Party Almal 17 1 Vehicle/Party Struck Object	Property Damage Only Under Influence of Micohol/Drugs/Medication 35-5- Minor or Possible Injury Under Influence of Micohol/Drugs/Medication Unic Property Damage Only Oftee/Unknown 55-6	Under Influence of Alcohol 35-44 Under Influence of Alcohol and Apparently Asleep, Fatigued or III Under 25 No No	Dry Daylight	Not at Intersection/Driveway/Roundabout Curve & Grade Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Under Influence of Alcohol/Drugs/Medication Under Influence of Alcohol/Drugs/Medication None	Daylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Under Influence of Alcohol/Drugs/Medication Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved
7420 1092778.69 956954.44 20°	117 1 Vehicle/Party Struck Object 1018 1 Vehicle/Party Struck Object 118 1 Vehicle/Party Struck Object 118 1 Vehicle/Party Animal	Property Damage Only Apparently Asleep, Fatigued or III 55-6	4 No 4 Apparently Asleep, Fatigued or III 35-44 4 Apparently Asleep, Fatigued or III 55-64 r 25 No	Dry Daylight Wet Dawn/Dusk Wet Daylight Dry Daylight	Not at Intersection/Oriveway/Roundabout Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved		Daylight We	t Not at Intersection/Driveway/Roundabout t Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout	Apparently Asleep, Fatigued or III Apparently Asleep, Fatigued or III No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved
7422 1092772.46 959972.95.20 7423 1092788.87 956945.05.20 7424 1092702.47 958403.26.20 7425 1092894.22 958378.98.20	319 1 Vehicle/Party Struck Object	Property Damage Only Other Unknown Unc Property Damage Only Other Unknown 45-5 Minor or Possible Injury Other Unknown 55-5 Fability Under Influence of Alcohol/Drugs/Medication 65-6 Droperty Damage Only Under Influence Only Und	4 No 4 No 5 No 65+ Under Influence of Drugs 65+ 4	Dry Daylight Dry Dark-No Street Lights/Street Lights Off Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Curve & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+	Under Influence of Alcohol/Drugs/Medication	Dark-No Street Lights/Street Lights Off Dry Daylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication
	118 1 Vehilde/Party Over Embankment - No Guardrail Present 116 1 Vehilde/Party Struck Object 116 1 Vehilde/Party Struck Object 116 2 Vehildes/Party Struck Object	Property Damage Only Speeding Unc	Apparently Asleep, Fatigued or III 65+ 1 No No No No No No No	Dry Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved	None Under Influence of Alcohol/Drugs/Medication	Daylight Dry Daylight We Dark-No Street Lights/Street Lights Off Dry Daylight We	t Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Apparently Asleep, Fatigued or III No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication Speeding
7429 1093140.92 959532.62 20 7430 1093268.68 959878.31 20 7431 1093374.96 960365.29 20	118 2 Vehiclos/Parties Rear-End 18 1 Vehiclos/Party Aerimal 114 1 Vehiclos/Party Aerimal	Minor or Possible Injury Follow Too Closely 45-5 Property Damage Only Other/Unknown 55-4 Property Damage Only Other/Unknown 25-3	4 No 4 No 4 1 No 4 No 4	Dry Daylight Dry Dark-No Street Lights/Street Lights Off Wet Daylight	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Follow Too Closely None None	Daylight Dry Dark-No Street Lights/Street Lights Off Dry Daylight We	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Follow Too Closely No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved
7432 1093378.48 960389.97 20 7433 1093367.76 961176.93 20	016 2 Vehicles/Parties Rear-End 015 2 Vehicles/Parties Sideswipe	Property Damage Only Inattention 45-4 Minor or Possible Injury Under Influence of Alcohol/Drugs/Medication 35- Property Damage Only Operating Defective Equipment Unic	4 No 4 Under Influence of Alcohol 35-44 No 9 7 55 No	Dry Daylight Dry Daylight Dry Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Curve & Level	No Vulnerable Road User Involved	Inattention	Daylight Dry	Not at Intersection/Driveway/Roundabout	inattention Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved
7437 1092984.02 963692.61 20	18 1 Wehlde/Party Over Embankment - No Guardrall Present	Minor or Possible Injury Inattention 55+ Property Damage Only Follow Too Closely Unc	rr 25 No	Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight	Intersection Related but Not at Intersection Straight & Level At Driveway or Intersection Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Inattention Follow Too Closely	Layinght Dry Daylight Dry Daylight Dry Daylight Dry Co-	Intersection Related but Not at Intersection At Driveway or Intersection	No Risky Driving Behaviors Involved Inattention Follow Too Closely Inattention
7437 1092984 02 963892.61 20** 7438 1092985.6 963693.2e 20** 7439 1092985.61 963892.68 20** 7440 1092980.22 963868.39 20** 7441 1092981.06 963892.3e 20** 963892.3e 20** 963892.3e 20**	116 2 Vehicles/Parties Sideswipe 117 2 Vehicles/Parties Rean-End 119 2 Vehicles/Parties Rean-End 119 2 Vehicles/Parties Rean-End	Property Damage Only Inattention 554 Minor or Possible Injury Speeding 65+ Minor or Possible Injury Follow Too Closely 65- Minor or Possible Injury Speeding 35-4	I No	Dry Daylight Wet Dark-No Street Lights/Street Lights Off Dry Daylight Dry Daylight	# Driveway or Intersection Straight & Level # Driveway or Intersection Straight & Level # Intersection and Not Related Straight & Level # Driveway or Intersection Curve & Level	No Vulnerable Road User Involved Older Road User 65+ Motorcycle and Older Road User 65+ No Vulnerable Road User Involved	Institution Speeding Follow Too Closely Speeding	Daylight Dry Dark-No Street Lights/Street Lights Off We Daylight Dry Daylight Dry	A Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Institution Speeding Follow Too Closely Speeding
7442 1092966.97 963775.59 20 7443 1092709.4 964342.83 20 7444 1092521.44 964596.74 20	316 1 Vehicle/Party Struck Object 314 2 Vehicles/Parties Sideswipe 314 1 Vehicles/Party Struck Object	Property Damage Only Apparently Asleep, Fasigued or III 25: Property Damage Only Other/Unknown 65+ Property Damage Only Speeding Unc	Apparently Asleep, Fatigued or III No No r 25 No	Wet Dark-No Street Lights/Street Lights Off Wet Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Grade	No Vuinerable Road User Involved Older Road User 65+ No Vuinerable Road User Involved	Speeding Apparently Asleep or Fatigued Other Contributing Circ Not Listed Speeding	Daylight We Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off We	t Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout t Not at Intersection/Driveway/Roundabout	Apparently Asleep, Fatigued or III No Rissty Driving Behaviors Involved Speeding
7445 1091767.2 967831.95.20 7446 1091768.33 967979.21 20 7447 1091754.9 968067.96.20	117 1 Vehicle/Party Animal 115 1 Vehicle/Party Struck Object 118 1 Vehicle/Party Struck Object	Property Damage Only Other/Unknown 55-6 Minor or Possible Injury Other/Unknown 55-6	4 No 4 No 725 No 9725 Under Influence of Drugs Under 25	Dry Daylight Wet Dark-No Street Lights/Street Lights Off Dry Daylight Dry Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level	No Vuinerable Road User Involved No Vuinerable Road User Involved No Vuinerable Road User Involved	None Other Contributing Circ Not Listed Inattention Under Influence of Alcohol/Drugs/Medication	Daylight Dry Dark-No Street Lights/Street Lights Off We	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Inattention
7449 1091487.43 970621.01 201	116 1 Vehicle/Party Struck Object 1015 1 Vehicle/Party Struck Object 116 1 Vehicle/Party Struck Object 117 1 Vehicle/Party Struck Object	Property Damage Only Speeding Unc	17 25 Under Influence of Drugs Under 25 17 25 No 17 25 18 25 Apparently Asleep. Fatigued or III Under 25	Wet Daylight	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+		Daylight Dry Dark-No Street Lights/Street Lights Off Dry Daylight We Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off We	/ Not at Intersection/Driveway/Roundabout t Not at Intersection/Driveway/Roundabout / Not at Intersection/Driveway/Roundabout t Driveway Related but Not at Driveway	Under Influence of Alcohol/Drugs/Medication Speeding Apparently Asleep, Fatigued or III No Risky Driving Behaviors Involved
7453 1091551.5 972678.09 20°	316 1 Vehicle/Party Struck Object 316 1 Vehicle/Party Struck Object	Property Damage Only Inattention 65+ Minor or Possible Injury Apparently Asleep, Fatigued or III 25-	No No No No Apparently Asleep, Fatigued or III 25-34	Dry Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Curve & Level	Older Road User 65+ No Vulnerable Road User Involved	Apparently Asleep or Fatigued	Daylight We Dark-No Street Lights/Street Lights Off Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Inattention Apparently Asleep, Fatigued or III
7456 1091536.82 972764.17 201	317 1 Vehicle Party Struck Object 316 2 Vehicles Parties Sides wipe 315 1 Vehicle Party Overturned 316 1 Vehicle Party Struck Object		* 140 4 No 1725 No 174	Wet Dark-No Street Lights/Street Lights Off Dry Daylight Dry Daylight Loe/Snow/Slush Dark-No Street Lights/Street Lights Off	At Driveway or Intersection Straight & Level At Driveway or Intersection Curve & Level		Speeding Speeding	Dark-No Street Lights/Street Lights Off We Daylight Dry Daylight Dry Dark-No Street Lights/Street Lights Off Ice/Snow	t Not at Intersection/Driveway/Roundabout At Driveway or Intersection At Driveway or Intersection (Slush Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Inattention Speeding No Risky Driving Behaviors Involved
7458 1091204.45 974321.02 20: 7459 1091119.24 974949.23 20: 7460 1090857.4 975691.92 20: 7461 1089938.13 978159.98 20:	10	Suspected Serious Injury Inattention 554 Property Damage Only Other/Unknown 255 Suspected Serious Injury Distracted Driving 65+	4 No	Dry Daylight Dry Dark-No Street Lights/Street Lights Off Dry Daylight Wet Daylight		No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	Inattention None	Daylight Dry Dark-No Street Lights/Street Lights Off Dry Daylight Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Driveway Related but Not at Driveway	Inattention No Risky Driving Behaviors Involved Distracted Driving
7462 1089787.79 978553.69 20°	116 1 Vehicle Party Struck Object 116 1 Vehicle Party Struck Object 17 1 Vehicle Party Struck Object 16 1 Vehicle Party Struck Object	Property Damage Only Speeding 35 Property Damage Only Speeding 55-6	4 No 4 No 5 No 6 No 7 No 9	Dry Daylight	Not at Intersection.Oriveway/Roundabout Straight & Level Not at Intersection.Oriveway/Roundabout Curve & Grade Not at Intersection.Oriveway/Roundabout Straight & Level Not at Intersection.Oriveway/Roundabout Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved	Speeding Speeding	Daylight We Daylight Dry Daylight Dry Daylight Dry	t Not at Intersection/Driveway/Roundabout	Speeding Speeding Apparently Asleep, Fatigued or III
7465 1088490.92 981248.92 201 7466 1088446.89 981344.36 201	015 1 Vehicle/Party Struck Object 014 1 Vehicle/Party Struck Object		4 No 4 No	Wet Daylight loe/Snow/Slush Daylight	Not at Intersection/Driveway/Roundabout Curve & Grade Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved	Reading or Writing Speeding Speeding	Daylight Ur Daylight Uce/Snow	t Not at Intersection/Driveway/Roundabout /Slush Not at Intersection/Driveway/Roundabout	Distracted Driving Speeding Speeding
7466 1088446.89 981344.36.20 7467 1087549.71 982831.81.20 7468 1087535.75 982850.11.20 7469 1087352.73 983093.9.20	14 2 Vehicles/Paries Turning	Minor or Possible Injury Improper Passing Unc	r 25 No No No	Dry Daylight Wet Dark-No Street Lights/Street Lights Off Dry Daylight Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Curve & Level	No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+		Daylight Dry Dark-No Street Lights/Street Lights Off We Daylight Dry D	At Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Inattention No Risky Driving Behaviors Involved
7470 1087256.95 983203.81.20 7471 1087020.96 983431.88.20 7472 1085914.27 984140.09.20 7473 1085256.79 984531.49.20	114 1 Vehicle/Party Struck Object	Property Damage Only Apparently Asleep, Fatigued or III 35- Property Damage Only Under Influence of Alcohol/Drugs:Medication 5-4- Property Damage Only Other/Unknown 65+ Property Damage Only Fail to Yield 65+	4 Apparently Asleep, Fatigued or III 35-44 Under Influence of Drugs and Apparently Asleep, Fatigued or III 55-64 No No	Dry Dark-No Street Lights/Street Lights Off Wet Daylight Dry Daylight Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Straight & Level At Driveway or Intersection Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ Older Road User 66+	Apparently Asleep or Fatigued Under Influence of Alcohol/Drugs/Medication None Fail to Yield		Not at Intersection/Driveway/Roundabout t Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout y At Driveway or Intersection	Apparently Asleep, Fatigued or III Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved Fail to Yield
7474 1085064.5 984645.2 201 7475 1085060.56 984649.46 201 7476 1085052.92 984651.99 201	314 3 Vehicles/Parties Rear-End	Property Damage Only Inattention 35-4 Property Damage Only Operating Defective Equipment 65+ Property Damage Only Fail to Yield 45-5	4 No No	Wet Daylight Dry Daylight	At Driveway or Intersection Straight & Level	No Vulnerable Road User Involved Older Road User 65+	Inattention		t At Driveway or Intersection	Inattention
7478 1085055.43 984647.54 20	118 2 Vehicles/Parties Angle	Property Damage Only Fail to Yield 45-5		Wet Daylight	At Driveway or Intersection Unknown	No Vulnerable Road User Involved		Daylight Dry Daylight We	t At Driveway or Intersection	No Risky Driving Behaviors Involved Fail to Yield
7480 1085055.75 984647.3 20°	018 2 Vehicles/Parties Rear-End 018 2 Vehicles/Parties Angle	Property Damage Only Other/Unknown 65+ Minor or Possible Injury Distracted Driving 45-6	1 NO NO 1	Wet Daylight Dry Daylight Dry Daylight	At Driveway or Intersection Unknown At Driveway or Intersection Unknown At Driveway or Intersection Straight & Level At Driveway or Intersection Straight & Level	Older Road User 65+	Fail to Yield None Unknown Distraction	Daylight Dry Daylight Dry	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Fail to Yield No Risky Driving Behaviors Involved Distracted Driving
7481 1084512.99 984876.42 20** 7482 1084406.42 984882.45 20**	2 Vehicles/Parties Angle	Property Damage Cnly OheoUnknown 65- Mnor or Possible Injury Distracted Driving 45-f Property Damage Cnly Improper TurnMerge 45-f Property Damage Cnly Property Damage Cnly 55-d Property Damage Cnly Speeding 45-f Moor or Possible Injury Speeding 45-f	4 No 4 No 4 No 7 No 9	Wet Daylight Dry Daylight Dry Daylight Dry Daylight Wet Darly-Street Lights On Wet Daylight Wet Daylight	Al Driveway of Intersection Unknown Al Driveway of Intersection Straight & Level Al Driveway of Intersection Straight & Level Al Driveway of Intersection Unknown Al Driveway of Intersection Unknown Al Driveway of Intersection Straight & Level Al Driveway of Intersection Straight & Level Al Driveway of Intersection Straight & Level	Older Road User 65+ No Vulnerable Road User Involved	Fail to Yield None Unknown Distraction Inproper Turn/Merge None Speeding Institution Insti	Univigent	A Driveway or Intersection	Fail by Visid No Risky Driving Behaviors Involved Distracted Driving No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Speeding Installance Install
7481 1084512.99 984876.42[20] 7482 1084406.42 984882.45[20] 7483 1083887.29 984906.48[20] 7484 1082917.9 987379.11[20]	21 Verlicies Paries Angle	Properly Danage Chily Montchindrown 56 - Montchindr	4 No 4 No 4 No 7 No 9	Wet	All Diveway or Intersection Unknown All Diveway or Intersection Straight & Level Unknown All Diveway or Intersection Straight & Level Straight &	Clider Road User 65+ No Villerastie Road User Involved	Fail to Yell del Unknown Distraction Unknown Distraction Whose Turnshape Whose Turnshape Speeding Patternion Follow Too Closely Improper Turnshape Improper Turnshape Turnshape Too Closely	Daylight We Daylight Dn Daylight Dn Daylight Dn Daylight Dn Dark-Street Lights On We Daylight We Daylight We Daylight We	A Dineway or Intersection	Part b Held No. Robert Street
7481 1094512.99 964976.42 20 954976.42 20 7482 1094409.62 954982.74 20 7482 1094409.62 954982.74 20 7483 1093897.29 964906.42 20 74845 1092977.9 967379.1 967379.1 74845 1092977.9 967379.1 967424.37 20 20 74845 109297.1 987424.37 20 20 74845 109297.3 967424.37 20 20 74845 109297.3 967424.37 20 20 74845 109297.3 967424.3 96907.2 78 20 7485 109297.2 96907.2 78 20 7485 109297.2 96907.2 9	2	Property Damage Chry	4 No 4 No 4 No 7 No 9	West	All Diseases of International Control of Inter	Older Road User 65+ No Vulnerable Road User Involved	Fail to Vield Women Distraction Improper Turnshorps Improper Turnshorps None Speeding Improper Speeding Improper Follow Too Coxedy Follow Too Coxedy Improper Improp	284/pdf Very	A Dineway or intersection Dineway Related but Not at Dineway Dineway Related but Not at Dineway	Fail by Visid No Risky Driving Behaviors Involved Distracted Driving No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Speeding Installance Install
7481 1084512 99 964976.42 964962 74 7482 108462 62 964962 74 96496	18	Properly Demango Chiy Protect Indexions Section	4 No 4 No 4 No 7 No 9	West	All Diseases of Historication Obtained and All Control of Historication Obtained All Control of Historicatio	Other Road User 56+ No Withmestilke Road User Insuland	Fail to Widd Juntoneum Distraction Improper TrumMenge Improper TrumMenge Improper TrumMenge Institution Improper TrumMenge Institution Improper TrumMenge Improper T	200/pipelin	A Deways or Interaction	Pall to Yard Cong Rehistoris Instituted Distriction of the Congress of the Con
7481 1084512-90 884874-22 20 7481 1084512-90 884874-22 20 7483 1082867-20 8868054-30 7484 1082877-9 987277-11 20 7485 1082887-20 987272-77 108272-11 20 7485 1082897-30 987272-77 108272-31 7487 108289-37 9888227-12 20 7488 108289-38 988231-32 20 7488 108289-38 988231-32 20 7488 108289-38 988231-32 20 7489 108289-38 988231-32 20 7489 108289-38 988231-32 20 7489 988231-32 20 7489 988231-32 20 7489 988231-32 20 7489 988231-32 20 7489 988231-32 20 7489 988231-32 20 7489 988231-32 20 7489 988231-32 20 7489 988231-32 20 7489 988231-32 20 7489 988231-32 20 7489 988231-32 20 7489 988231-32 20 7489 988231-32 20 7489 988231-32 20 7489 988231-32 20 7489 988231-32 20 7489 988231-32 20 7489 988231-32 20	18	Properly Densing City Frogerly Densign City	No	West	A Disease or Historica Color C	Other Road User 56+1 New Waternahle Road User Installed	Fail to Vield Worseam Distraction Improper Turnshring Improper Tu	Object O	A Dewayor Interaction	Feb to Year Comp Between Involved
7411 106451291 988672.22) 988672.22) 7472 7482 106451291 988672.22) 988672.22) 7472 7484 10627129 987771112 987743712 7486 7485 10627129 987771117 987443720 748743720	18	Properly Densigo Chiy	1 No	West	A Disease or Helman ecloria A Disease or Hel	Other Road User 56+1 New Waternathe Road User Installed	Fail to Vield Western Distanction Improper Turnsheige None Speeding Speeding Speeding Fail Fail Fail Fail Fail Fail Fail Fail	Object O	A Deways or Interfacion A Deways or Interfacion A Deways or Interaction A Deway or Interaction I A Deways or Interaction A Deways or Interaction A Deways or Interaction I	Fall to Yard Comp Behaviors Involved Distracted Christian No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Speciality No Risky Driving Behaviors Involved Speciality No Risky Driving Behaviors Involved Statistics of Control No Risky Driving Behaviors Involved Statistics of Control No Risky Driving Behaviors Involved Statistics of Control No Risky Driving Behaviors Involved Speciality Under treatment of Machinal Christy Behaviors Speciality Under treatment of Machinal Christy Behaviors Speciality Ostatistics Fallow Too Control Statistics Fallow Too Contey Ostatistics Fallow Too Contey Ostatistics Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved
7411 1004512-91 98475-22 9 98475-	13	Property Demand Coly Proceedings Procedure Pro	No	West	A Disease or Historication Observation Obs	Other Road User 65+ No Whenshile Road User Insched A William Committee Com	Fail to Vietal Jacobson Distanction Improsed Turnfeldings None Specification Specifi	Objective Obje	A Disease of Interaction A Character of Interaction A Character of Interaction A Disease of I	Pall to Year Company Services Invalved Contracted Christian Company Services Contracted Christian Company Comp
7481 108451.99 98847.42.29 7485 108405.42.39 98847.42.29 7486 108291.79 887778.11.29 7486 108291.79 887778.11.29 7486 108291.79 887778.11.29 7486 108291.79 887748.37 7487 108291.79 887748.37 7488 108291.79 887748.37 7489 108278.30 887788.32 7489 108278.30 887788.32 7489 108278.30 88788.32 7489 108278.30 88788.32 7489 108278.30 88788.32 7489 108278.30 88788.32 7489 108278.30 88788.30 7489 108278.30 88788.30 7489 108278.30 88788.30 7489 108278.30 88788.30 7489 108278.30 88788.30 7489 108278.30 88788.30 7489 108278.30 88788.30 7489 108278.30 88788.30 7489 108278.30 88788.30 7489 108278.30 88788.30 7499 108278.30 88788.30 7499 108278.30 88788.30 7499 108278.30 88788.30 7499 108278.30 88788.30 7499 108278.30 88788.30 7499 108278.30 88788.30 7499 108278.30 887888.30 7499 108278.30 887888.30	11	Property Denning Chip Proceedings Property Denning Chip	No	West	At Disease of Hiller section Oldstroom (All Control of Hill Co	Other Road User 65+ No Whenshilk Road User Insched A William Committee Com	Fail to Vield Whose on Distancion Improper Turniburge None Speeding Speedi	Description	A Dewayor Interaction A Dewayor Extension A Deway	Fall to Yard Comp Behaviors Involved Distracted Christian No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Speciality No Risky Driving Behaviors Involved Speciality No Risky Driving Behaviors Involved Statistics of Control No Risky Driving Behaviors Involved Statistics of Control No Risky Driving Behaviors Involved Statistics of Control No Risky Driving Behaviors Involved Speciality Under treatment of Machinal Christy Behaviors Speciality Under treatment of Machinal Christy Behaviors Speciality Ostatistics Fallow Too Control Statistics Fallow Too Contey Ostatistics Fallow Too Contey Ostatistics Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved
7481 108451299 988475.4239 7485 108405.423 988475.4239 7486 10829129 888724.3739 7486 10829129 887291.1139 7486 10829129 887291.1139 7486 10829129 887291.1139 7486 10829129 887291.1139 7486 10829129 887291.1139 7489 10829129 887291.1139 7489 10829129 887291.1139 7489 10829129 887291.1139 7489 10829129 887291.1139 7489 10829129 887291.1139 7489 10829129 887291.1139 7489 10829129 887291.1139 7489 10829129 88729 88823 88729 7489 10829129 88729	10	Properly Dismage City	1 No	West	A Disease of Historica Colonia	Other Road User 654 No Withmestilk Road User Insuland Other Road User 654 No Withmestilk Road User Insuland Other Road User 164 No Withmestilk Road User Insuland Other Road User 164 No Withmestilk Road User Insuland Other Road User 654 No Withmestilk Road User Insuland Other Road User 654 No Withmestilk Road User Insuland	Fail to Vield Junctions Distraction Improper Trunkleage Improper	Description	A Draway or Interaction A Chaway or Interacti	Pall to Year Company C
7481 1084512-29 984274-22 20 7482 1084512-29 984274-22 20 7482 1084512-20 984274-22 20 7482 1084512-20 984274-22 20 7483 1082817-2 984274-21 20 7485 1082817-2 984274-21 20 7485 1082817-2 984274-21 20 7485 108281-21 984274-21 20 7485 108281-21 984274-21 984274-21 20 7485 108281-21 984274-21 984274-21 20 7485 108281-21 984274-21 984274-21 20 7486 108281-21 984274-21 984274-21 20 7487 108281-21 984274-21 984274-21 20 7487 108281-21 984274-21 984274-21 20 7487 108281-21 984274-21 984274-21 20 7487 108281-21 984274-21 984274-21 20 7487 108281-21 984274-21 984274-21 20 7487 108281-21 984274-21 984274-21 20 7487 108281-21 984274-21 984274-21 20 7487 108281-21 984274-21 984274-21 20 7488 108281-21 984274-21 984274-21 20 7489 108281-21 984274-21 20 7489 108281-21 984274-21 20 7489 108281-21 984274-21 20 7489 108281-21 984274-21 20 7489 108281-21 984274-21 20 7489 108281-21 984274-21 20 7489 108281-21 984274-21 20 7489 108281-21 984274-21 20 7489 108281-21 984274-21 20 7489 108281-21 20 7489 108281-21 20 7489 108281-21 20 7489 108281-21 20 7489 108281-	18	Properly Densage Cuty	No	West	A Disease or Historication Christian A Chr	Other Road User 654- No Waterstells Road User incided Other Road User 164- No Waterstells Road User incided Other Road User 164- No Waterstells Road User incided Other Road User 164- No Waterstells Road User incided Other Road User 164- No Waterstells Road User incided	Fail to Vield Western Distanction Improper Turnsheige None Speeding Speedi	Description	A Draway or Interaction A Chaway or Interacti	Pail to Yard Company Services revised Districted Division Company Services
7481 (08451-296) 884874-2 20 104510-296 (1848-206) 884874-2 20 17483 (1848-206) 884874-2 20 17484 (1848-206) 884874-2 20 17484 (1848-206) 884874-2 20 17485 (1848-206) 884874-2 20 17487 (1848-206) 884874-2 20 17487 (1848-206) 884874-2 20 17487 (1848-206) 884874-2 20 17487 (1848-206) 884874-2 20 17488 (1848-206) 884874-2 20 17489 (1848	18	Properly Damage Coly Properly	No	West	A Disease or Historication Outdoors (A Disease) A Level A Disease or Historication Outdoors (A Disease) A Level A Disease or Historication Outdoors (A Disease) A Disease or Historication Outdoors (A Disease) A Disease or Historication Outdoors (A Disease) A Level A Disease or Historication Outdoors (A Level A Disease) A Level A Disease or Historication Outdoors (A Level A Disease) A Level A Disease or Historication Outdoors (A Level A Disease) A Level A Disease or Historication Outdoors (A Level A Disease) A Level A Disease or Historication Outdoors (A Level A Disease) A Level A Disease or Historication Outdoors (A Level A Disease) A Level A Disease or Historication Outdoors (A Level A Disease) A Level A Disease or Historication Outdoors (A Level A	Other Road User 65+ No Whenshile Road User Insched A Warner of the Committee of the Commi	Fail to Weld Junctions Distancies Improper Turnfleinge Improper Turnflei	Description	A Dewayor of Interaction A Dewayor of Interaction A Dewayor intera	Pall to Yand Company Services Invalved Distracted Chronic Company Services
7481 1064612 99 984874 22 20 7481 1064612 99 984874 22 20 7481 1064612 99 984874 22 20 7481 1064612 99 987375 11 20 7481 1064612 99 987375 11 20 7481 1064612 99 987375 12 20 7481 1064612 99 987375 12 20 7482 1064612 99 98734 12 20 7482 1064612 99 98734 12 20 7483 1064612 99 98734 12 20 7483 1064612 99 988188 12 20 7483 1064612 99 988188 12 20 7483 106478 99 98818 12 20 7484 106478 99 98818 12 20 7485 106478 99 98818 12 20 7485 106478 99 98818 12 20 7485 106478 99 98818 12 20 7485 106478 99 98818 12 20 7485 106478 99 98818 12 20 7485 106478 99 98818 12 20 7485 106478 99 98818 12 20 7485 106478 99 98818 12 20 7485 106478 99 98818 12 20 7485 106478 99 98818 12 20 7485 106478 99 98818 12 20 7485 106478 99 98818 12 20 7485 106478 99 98818 12 20 7485 106478 99 99 99 99 99 99 99 99 99 99 99 99 99	18	Properly Damage Coly Properly	No	West	A Disease or Historication Obtained A Level A Disease or Historication Obtained A Level A Disease or Historication Obtained A Level A Observator or Historication Obtained A Disease or Historication Obtained A D	Other Road User 564- No Waternathe Road User Installed No Waternathe Road User Insta	Fail to Vield Whose on Distanction Improper Turnsheige None Speeding Speed	Description	A Dewayor interaction	Pair to Year Control of Control o
7481 086412 29 984874.2 20 7481 086412 29 985874.2 20 7481 0868717 9878778 17 7481 086887 19 9878778 17 7481 086887 19 987741.7 30 7481 108687 19 987741.7 30 7481 108687 19 987741.7 30 7482 108678 19 987741.7 30 7482 108678 19 987741.7 30 7483 108678 19 987741.7 30 7483 108678 19 987741.7 30 7484 108678 19 987741.7 30 7485 108678 19 987741.7 30 7485 108678 19 987741.7 30 7485 108678 19 987741.7 30 7485 108678 19 987741.7 30 7487 108678 19 987741.7 30 7487 108678 19 987741.7 30 7487 108678 19 987741.7 30 7487 108678 19 987741.7 30 7487 108678 19 987741.7 30 7487 108678 19 987741.7 30 7487 108678 19 987741.7 30 7487 108678 19 987741.7 30 7487 108678 19 987741.7 30 7487 108678 19 987741.7 30 7487 108678 19 987741.7 30 7488 108678 19 987741.7 30 7488 108678 19 987741.7 30 7489 108678 19 987741.7 30 7489 108678 19 987741.7 30 7489 108678 19 987741.7 30 7489 108678 19 987741.7 30 7489 108678 19 98741.7 30 7489 108678 19	13	Properly Denising Chily Properly Denising Children Properly Denising Chily Properly Denising Chily Pro	No	West	A Chewary or Helmeschon Obstoom A Chewary or Helmeschon Obstoom A Chewary or Helmeschon A Helmeschon A Chewary or Helmeschon A	Other Road User 654 No Whenshile Road User Insched See Whenshile Road User Insched No Whenshile Road User Insched	Fail to Vield Justices Distances on Inspect Fundament States on St	Oblight Obli	A Dewayor interaction A Dewayor trensaction	Pair to Year of States and Pair to Year of States and Pair to Year of States and Pair to Year of Year
7481 100645 28 00000 28 00000 28 00000 28 00000 28 00000 28 00000 28 00000 28 00000 28 00000 28 00000 28 00000 28 00000 28 000000 28 000000 28 000000 28 000000 28 000000 28 000000 28 00000000	10	Properly Dismage Chy Properly	No	West	At Dimeway or Helms ection At Dimeway or Helms ection At Dimeway or Helms ection A Dimeway or Helms ection Simplifi & Level A Dimeway or Helms ection Simplifi & Level A Dimeway or Helms ection Simplifi & Level A Dimeway or Helms ection A Dimeway or Helms ection Simplifi & Level A Dimeway or Helms ection Simplifi & Level	Other Road User 165- No Watershild Road User Insuland See Watershild Road User Insuland No Watershild Roa	Fail to Vield Junctions Distraction Improper TrumMerge Improper TrumM	State	A Dewayor interaction	Part to Year Common Section (1997) Districted Division (1997) Divisio
7481 1086512-99 8985712-09 7485 1086512-99 8985712-09 7485 1086512-99 8985711-09 7485 1086512-99 8985711-09 7485 1086512-99 8985711-09 7485 1086511-99 7485 10	10	Properly Densigo Chily	1 No	West	A Disease or Historication Charleson	Other Road User 654- No Waterselbe Road User incided of the Waterselbe Road User for Incided of the Waterselbe Road User incided of the W	Fall to Vield Whose on Distanction Improper Turnsheige None Speeding Speed	Statement	A Dewayor interaction	Park to Year Company C
7481 100657 29 08 98872 42 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2	19	Properly Danage Coly Properly	No	West	A Dimension of Historication Control of Historican Control of Hist	Other Road User 654 No Watersaftie Road User Insuland See Watersaftie Road User Insuland No Watersaftie Road User Insu	Fail to Vield Junctions Distanction Improper Turnfleinge Improper	Solight Soli	A Dewayor interaction	Park to Year Construction Control of Construction Cont
7481 1064512 29 984374 22 22 74482 1064512 29 984374 22 22 74482 1064512 29 984374 22 22 74482 1062517 2 987373 11 22 74483 1062517 2 987373 11 22 74485 1062517 2 987374 11 22 74485 1062517 2 987374 11 22 74485 1062517 2 98734 11 22 74485 1062517 2 98734 11 22 74485 1062517 2 98734 11 22 74485 1062517 2 98734 11 22 74485 1062518 2 98531 11 2 74485 1062518 2 98531 11 2 74485 10	13	Properly Densage Chily Properly Densage Children Properly Densage Children Properly Densage	No	West	A Chewary or Hebraschon Observation Observ	Other Road User 165-1 No Waternathe Road User Incahend Oker Road User Incahend No Waternathe Road User Incahend No Waternath	Fail to Vield Distances Distances on Special State of Special Special State of Special Sp	State	A Dewayor of testinacion A Dewayor of testi	Fait to Year Country C
7481 1064612 29 984774.2 20 7481 1066612 29 984774.2 20 7481 10628717 9 984774.2 20 7481 10628717 9 984774.3 20 7481 10628717 9 984774.3 20 7481 10628717 9 984774.3 20 7481 1062871 9 984774.3 20 7482 1062871 9 986774.3 20 7483 1062871 9 986774.3 20 7483 1062871 9 986774.3 20 7484 1062871 9 986774.3 20 7485 1062871 9 986774.3 20 7485 1062871 9 986774.3 20 7485 1062871 9 986774.3 20 7487 1062871 9	13	Properly Disnage Coly	No	West	A Dimension of Historication of Chickonam (A Dimension) of Histori	Other Road User 654 No Watersaftie Road User Insuland See Watersaftie Road User Insuland No Watersaftie Road User Insu	Fail to Vield Junctions Distanction Improper Turnflenge Improper T	Statistical	A Dewayor interaction A Dewayor treasaction	Pair to Year of State of Pair to Year of
7481 106612 29 86474.2 20 7484 106612 29 86474.2 20 7484 1062217 2 9875771 12 7485 106287 2 9875771 2 7485 106287 2 987572 1 7487 198757 2 9875771 2 7487 198757 2 9875771 2 7488 106287 2 987577 2 7488 106287 2 98857 2 98857 2 7488 106287 2 98857 2 98857 2 7488 10628 2 98757 2 98857 2 7488 10628 2 98757 2 98857 2 7489 10628 2 98757 2 98857 2 7489 10628 2 98757 2 98857 2 7489 10628 2 98757 2 7489 10628 2 7489 106	13	Nepotry Damage Coly Property D	1	West	A Disease of Historication Charleson	Other Road User 65- No Watersaftie Road User incubed No Watersaftie Road	Fail to Vield Junctions Distraction Improper TrumMerge Improper TrumMe	Statement	A Dewayor interaction A Dewayor treataction	Part to Year Construction of Construction of Construction
7481 1004051290 884874.2 20 1004051290 884874.2 20 1004051290 884874.2 20 1004051290 884874.2 20 1004051290 894874.2 20 1004051290 8947424.3 20 1004051290 8947424.3 20 1004051290 8947424.3 20 1004051290 8947424.3 20 1004051290 8947424.3 20 1004051290 89487424.3 20 100405	19	Properly Damage Coly Properly	No	West Osubah Oru O	At Oneway or Helmsection Shapith & Level At Oneway or Helmsection At Oneway or Helmsection Shapith & Level At Oneway or Helmsection At Oneway or Helmsection Shapith & Level At Oneway or Helmsection Shapith & Level At Oneway or Helmsection At Oneway or Helmsection Shapith & Level At	Other Road User 654 No Whetenshile Road User Insuland No Whetenshile Road User Insul	Fail to Vield Junteren Distancies Improper Turnfleinge Improper Turnflei	Objective Obje	A Demayor of Interaction A Demayor of Interaction A Demayor intera	Pall to Yeard Section of Section
7481 1006817 20 98007 21 98007 21 21 74 10 1006817 20 98007 21 21 74 10 1006817 20 98007 21 21 74 10 1006817 20 98007 21 21 74 10 1006817 20 98007 21 21 74 10 1006817 20 98007 21 21 74 10 1006817 20 98007 21 21 74 10 1006817 20 98007 21 21 74 10 1006817 20 98007 21 21 74 10 1006817 20 98007 21 21 74 10 1006817 20 98007 21 21 74 10 1006817 20 98007 21 21 74 10 1006817 20 98007 21 21 74 10 1006817 20 98007 21 21 74 10 1006817 20 98007 21 21 74 10 1006817 20 98007 21 21 74 10 1006817 20 98007 21 21 74 10 1006817 20 98007 21 74 10 10068	19	Properly Damage Coly Properly	1 No	West	A Dimension of International Control of the Control of	Other Road User 165- No Watersafelb Road User incushed No Watersafelb Road User incu	Fail to Weld Justices Distances on Interesting States of the States of State	State	A Disease of Interaction A Disease of Inter	Fail to Yeard Treatment of Control of Contr
7-86	10	Properly Densigo Chiy	No	West	A Dimension of Inflamentation A Dimension Related but Not at Inflamentation Straight A Level There is a second inflamentation A Dimension of Inflamenta	Other Road User 165- No Waternathe Road User incubed on the Waternathe Road User incubed on Waternathe Road U	Fail to Vield Fail t	State	A Demayor of testinacion. A Demayor of testinac	Fail to Year South Principle Princip
7-841 106-61-29 9 98-97-6-2-20 7-841 106-61-29 9 98-97-6-2-20 7-841 106-61-29 9 98-97-6-2-20 7-841 106-97-97-97-97-97-97-97-97-97-97-97-97-97-	10	Properly Danago Chy Proper	No	West	A Chemistry of Histonication A Chem	Other Road User 65- Ne Waternathe Road User incidend No Waternathe Road User incidend Older Road User 16- Preferent waternathe Waternathe Road Older Road User 16- Preferent Waternathe Road User incidend Older Road User 16- No Waternathe Road User incidend No Waternathe	Fail to Weld Distances of Participated Control	State	A Disease of treatedon. A Disease of treatedo	Part D. Yang Destinated Division Destinated Destin
7481 106612 29 84574.2 20 7481 106612 29 84574.2 20 7481 10662 29 84574.2 20 7481 10622 21 20 7481 10622 21 20 7481 10622 21 20 7481 10622 21 20 7481 10622 21 20 7481 10622 21 20 7482 21 20 7482 21 20 7483 21 20 7484 21 20 7485 21	10	Properly Densigo Chiy	1	West	A Dimension of Helmandschol A Dimension of H	Other Road User 65- No Waternable Road User incubed No Waternabl	Fail to Vield Junctions Distancision Improper Turniflenge Junctions Distancision Improper Turniflenge Junctions Junc	State	A Demayor of testinacion. A Demayor of testinac	Fail to Yeard Search Control of Search Control o

	COUNTY CITY	PRIMARY BLOCK TRAFFICWAY NUMBER	INTERSECTING DIST ML.C COMP.DR REFERENCE MLEPOST BUSINESSES, IND. TRAFFCWAY REFORM FIREFORM POINT, MAME MLEPOST BUSINESSES, IND. MAMBE	DATE TIME MOST_SEVERE INJURY_TYPE	E NUM NUM NUM NUM NU INJ FAT VEH PEDS BIK	IM VEH_1_TYPE	VEH_2_TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD_SURFACE ROADWAY CHARACTERISTIC	LIGHTING CONDITION	FIRST_COLLISION TYPE_OBJECT_STRUCK	VEH_1 ACTION	VEH_2 ACTION	VEH_1_COMPASS VEH_1_COMPASS VEH_2_COMPASS DIRECTION_FROM DIRECTION_TO DIRECTION_FROM DIRECTION_TO
755	Jefferson 02 Jefferson 02	20	0.32 No E70975 0.33 No E441290	2017-09-07 18:58 No Apparent Injury 2015-07-07 15:40 No Apparent Injury 2017-01-26 04:55 Suspected Serious In	0 0 1 0	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade Dry Curve & Grade	Daylight Roadwa Daylight Guardra	y Ditch il - Face	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	South North Southwest Northeast
7562 State Route 7563 State Route	Jefferson 02 Jefferson 02	20 20 20	0.37 No E443452	2015-07-15 09:48 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb		Not at intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade Dry Straight & Grade	Daylight Roadwa	nk or Ledge y Ditch	Going Straight Ahead Going Straight Ahead	Going Straight Anead	West East East West West East West East
7564 State Route 7565 State Route 7566 State Route	Jefferson 02 Jefferson 02 Jefferson 02	20 20 20	0.40 No E40508	2015-02-27 14:50 No Apparent Injury	0 0 1 0	0 Truck (Flatbad,Van,etc)		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Raining Other	Wet Curve & Grade Wet Curve & Grade Wet Curve & Grade	Daylight Roadwa Daylight Roadwa Daylight Roadwa	y Disch y Disch v Disch	Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East West East West East
7567 State Route 7568 State Route	Jefferson 02 Jefferson 02	20	0.47 No E65340 0.47 No E65340 0.48 No 272964	2017-03-15 14:14 No Apparent Injury 2014-06-29 13:28 No Apparent Injury	0 0 1 0	Pickup.Panel Truck or Vanette under 10,000 lb Passenger Car Other Passenger Car Passenger Car Pickup.Panel Truck or Vanette under 10,000 lb Pickup.Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Wet Curve & Grade	Daylight Roadwa				West East West East
7570 State Route 7571 State Route	Jefferson 02 Jefferson 02 Jefferson 02	20 20 20	0.50 No E76531 0.50 No E871533 0.50 No E87333	2018-01-05 10:39 Dead at Scene 2018-12-10 07:16 No Apparent Injury 2018-12-17 07:06 Suspected Minor Injury	0 1 2 0 0 0 1 0 ry 1 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Truck Tractor & Semi-Trailer	Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Overcast Overcast		Dark-No Street Lights Venicle	overturned	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	West East East West West East West West
7572 State Route 7573 State Route 7574 State Route	Jefferson 02 Jefferson 02	20 20 20	0.80 No E90405 0.62 No E9133 0.72 No E709354	2019-03-19 08:50 No Apparent Injury 2019-09-14 11:13 No Apparent Injury 2017-09-03 03:55 Possible Injury	0 0 1 0	O Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade Wet Curve & Grade Dry Straight & Grade	Daylight Roadwa Daylight Earth Ba Dark-No Street Lights Tree or S	y Ditch ink or Ledge	Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East West East
7575 State Route 7576 State Route 7577 State Route 7578 State Route	Jefferson 02 Jefferson 02	20 20	0.80 No E67113 0.82 No E49682	2017-05-06 16:00 Possible Injury 2015-12-14 20:45 No Apparent Injury	2 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Grade Wet Straight & Grade	Daylight Roadwa Dark-No Street Lights Roadwa	y Ditch	Going Straight Ahead		West East West East
7577 State Route 7578 State Route 7579 State Route	Jefferson 02 Jefferson 02 Jefferson 02	20 20 20	0.82 No E69476: 0.92 No 3691491 0.95 No E94898	2017-07-22 12:05 Possible Injury 2014-09-18 05:25 No Apparent Injury 2019-07-30 22:18 No Apparent Injury	2 0 2 0 0 0 1 0 0 0 3 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Truck Tractor & Semi-Trailer	Intersection Related but Not at Intersection Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Raining Clear or Partly Cloudy				Going Straight Ahead Going Straight Ahead Going Straight Ahead Stopped in Roadway	Slowing Going Straight Ahead	West East West East Northeast Southwest Vehicle Stopped Vehicle Stopped South North
7580 State Route 7581 State Route	Jefferson 02 Jefferson 02	20	0.97 No E61304 0.98 No E43217	2016-11-19 18:30 No Apparent Injury 2015-12-08 20:35 No Apparent Injury	0 0 1 0	0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining Raining Clear or Partly Cloudy	Wet Curve & Grade Wet Curve & Grade Snow/Slush Straight & Grade	Dark-No Street Lights Guardra Dark-No Street Lights Roadwa	il - Face y Disch	Going Straight Ahead Going Straight Ahead Going Straight Ahead		East West South North
7976 Salet ROute 7579 Salet Route 7580 Salet Route 7580 Salet Route 7581 Salet Route 7582 Salet Route 7582 Salet Route 7584 Salet Route 7584 Salet Route 7584 Salet Route 7585 Salet Route 7586 Salet Route	Jefferson 02 Jefferson 02	20 20	1.00 No E5/912 1.00 No E5/912 1.00 No E55498	2015-01-17 01:25 Died in Hospital 2016-06-11 09:32 Suspected Minor Injur	2 1 1 0 ry 1 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related	Overcast Overcast Clear or Partly Cloudy	Wet Curve & Grade	Dark-No Street Lights Over Em Daylight Roadwa		Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East East West
7585 State Route 7586 State Route 7587 State Route	Jefferson 02 Jefferson 02 Jefferson 02	20 20 20	1.00 No E634301 1.00 No E726421 1.02 No E754041	2017-01-10 23:30 No Apparent Injury 2017-10-22 05:22 Possible Injury 2017-12-31 19:54 No Apparent Injury	0 0 1 0 1 0 1 0	Passenger Car Passenger Car Pickup Panel Truck or Vanette under 10.000 lb		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Other Clear or Partly Cloudy	Wet Curve & Grade Wet Curve & Grade Dry Curve & Grade	Daylight Roadwa Dark-No Street Lights Roadwa Dark-No Street Lights Roadwa Dark-No Street Lights Roadwa	y Ditch	Going Straight Ahead		South North East West East West
7587 State Route 7588 State Route 7589 State Route 7590 State Route 7591 State Route 7591 State Route	Jefferson 02 Jefferson 02 Jefferson 02 Jefferson 02	20	1.02 NO E70932 1.02 NO E70932 1.07 NO E76532 1.07 NO E625355	2018-02-02 18:11 No Apparent Injury 2016-12-12 08:10 No Apparent Injury	0 0 1 0	O Pleasonger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Wet Straight & Grade Wet Straight & Grade	Dark-No Street Lights Roadwa Dark-No Street Lights Vehicle: Daylight Roadwa	Strikes Deer y Ditch	Going Straight Ahead Going Straight Ahead Other* Going Straight Ahead		East West West East
7591 State Route 7592 State Route	Jefferson 02 Jefferson 02	20 20	1.10 No E50031 1.24 No E50753 1.34 No E50753	2018-11-22 20:08 Suspected Serious In 2016-01-04 01:00 Suspected Minor Injur	ijury 2 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Overcast		Dark-No Street Lights Earth Ba		Going Straight Ahead Going Straight Ahead	Going Straight Ahead	West East West East West West
7592 State Route 7593 State Route 7594 State Route 7595 State Route	Jefferson 02 Jefferson 02 Jefferson 02	20 20 20	1.38 No E99798. 1.40 No E571101 1.40 No E71849.	2016-01-04 01:00 Suspected Minor Injur 2019-12-21 03:00 No Apparent Injury 2016-08-03 12:56 Possible Injury 2017-09-09 23:25 No Apparent Injury	0 0 1 0 1 0 1 0 0 0 1 0	0 Passenger Car 0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy Raining	Wet Curve & Grade Dry Curve & Grade Wet Curve & Grade	Dark-No Street Lights Roadwa Daylight Guardra Dark-No Street Lights Roadwa	y Disch il - Face y Disch	Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East East West East West
7596 State Route 7597 State Route 7598 State Route 7599 State Route 7599 State Route 7501 State Route 7601 State Route 7602 State Route 7603 State Route 7603 State Route	Jefferson 02 Jefferson 02	20	1.50 No 3728356 1.50 No E79895:	2015-03-26 18:36 No Apparent Injury 2018-05-04 19:30 No Apparent Injury	0 0 1 0	0 Passenger Car 0 Passenger Car	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight Roadwa	y Ditch	Going Straight Ahead Overtaking and Passing	Going Straight Ahead	South North West East West East
7599 State Route 7599 State Route 7600 State Route	Jefferson 02 Jefferson 02	20 20 20	1.50 No E97659 1.62 No E90216 1.94 No E38511	2018-05-26 17:50 Possible Injury 2014-12-14 19:58 No Apparent Injury	1 0 2 0	0 Passenger Car 0 Passenger Car 0 Passenger Car	Other	Not at Intersection and Not Related Not at Intersection and Not Related At Intersection and Not Related	Overcast Clear or Partly Cloudy	Dry Curve & Grade Dry Curve & Grade Dry Curve & Grade	Dark-No Street Lights Roadwa Daylight From op Dark-Street Lights Off Vehicle:	me direction - both going straight - both moving - sideswipe y Ditch posite direction - both going straight - sideswipe Brikes Elik	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	East West West East East West East West
7601 State Route 7602 State Route 7603 State Route	Jefferson 02 Jefferson 02 Jefferson 02	20 20 20	1.84 No E65967 1.84 No E82908 1.84 No E92908	2015-03-26 18:36 No Apparent Injury 2018-05-04 19-30 No Apparent Injury 2019-10-30 06:30 No Apparent Injury 2019-10-30 06:30 No Apparent Injury 2016-05-26 17-50 Possible Injury 2016-05-23 06:14 Suspected Mnor Injury 2018-08-13 06:14 Suspected Mnor Injury 2018-08-13 06:14 Suspected Mnor Injury 2018-08-13 25:14 Suspected Mnor Injury 2018-08-13 25:14 Suspected Mnor Injury 2019-11-09 18:59 No Apparent Injury	1 0 2 0 ry 1 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car Motorcycle Passenger Car	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Fog or Smog or Smoke	Dry Curve & Grade Dry Curve & Grade Wet Curve & Grade	Daylight From sa Dark-No Street Lights Entering Dark-Street Lights On Entering	me direction - one lest turn - one straight at angle	Going Straight Ahead Making Left Turn Going Straight Ahead	Making Left Turn Going Straight Ahead Going Straight Ahead	North South North East South West West East South North Fast West
7604 State Route 7605 State Route 7606 State Route 7607 State Route	Jefferson 02 Jefferson 02	20 20	1.09 No E78855 2.00 No E49937	2018-04-13 13:23 Possible Injury 2015-12-25 23:26 No Apparent Injury	1 0 1 0	0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy				Going Wrong Way on Divided Hwy Going Straight Ahead Going Straight Ahead		East West East West
7606 State Route 7607 State Route 7608 State Route	Jefferson 02 Jefferson 02 Jefferson 02	20 20 20				Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy		Dark-No Street Lights From sa Dark-No Street Lights Roadwa Dark-No Street Lights Vehicle	y Ditch	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing	
7609 State Route 7610 State Route 7611 State Route	Jefferson 02 Jefferson 02 Jefferson 02	20 20 20	2.10 No E938000 2.16 No E850607	2019-07-06 20:42 No Apparent Injury 2018-10-01 16:55 Dead at Scene	1 1 2 0	0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast	Dry Curve & Level Wet Curve & Grade Dry	Dusk Guardra Davlight From on		Going Straight Ahead Going Straight Ahead Starting in Traffic Lane	Going Straight Ahead Stopped for Traffic	West East East West East Obside Stepand Utstald Co.
7612 State Route 7613 State Route	Jefferson 02 Jefferson 02 Jefferson 02	20 20	220 No E77292 230 No E37392 230 No E61304	2014-11-17 05:25 Suspected Minor Injury 2016-11-17 16:20 Suspected Serious In	0 0 3 0 ry 1 0 1 0 ijury 1 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Motorcycle	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	lce Curve & Grade Dry Straight & Level	Dark-No Street Lights Rock Ba Dusk Earth Ba	nk or Ledge nk or Ledge	Going Straight Ahead Going Straight Ahead		East West Vehicle Stopped Vehicle Stopped East West Southwest Northeast
1996 Sale Node Sale Node	Jefferson 02 Jefferson 02 Jefferson 02	20 20 20	2.30 No E755041 2.30 No E762564 2.34 No E40304	2018-01-03 15:00 Possible Injury 2018-01-25 08:20 No Apparent Injury 2015-02-19 20:20 No Apparent Injury	1 0 2 0 0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car Pristup Penell Truck or Venette under 15,000 to	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Raining Raining	Wet Straight & Grade Wet Curve & Grade Wet Straight & Grade	Daylight From sa Daylight Guardra Dark-No Street Lights Guardra	in or Ledge in or Ledge in or Ledge ii - Face ii - Face	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic	East West Vehicle Stopped Vehicle Stopped West East Vest East
7617 State Route 7618 State Route	Jefferson 02 Jefferson 02 Jefferson 02 Jefferson 02	20	2.05 No E52834 2.25 No E52834 2.70 No E61465	2016-03-15 15:49 Suspected Serious In 2016-11-28 09:00 Possible Injury	jury 5 0 1 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight in Sag		il - Through, Over or Under			West East East West
7619 State Route 7620 State Route 7621 State Route	Jefferson 02 Jefferson 02 Jefferson 02	20 20 20	2.72 No E83823 2.74 No E39242 2.75 No E62069	2015-01-11 03:25 Suspected Minor Injur 2016-05-10 00:00 Unknown	y 1 0 1 0 y 1 0 1 0	0 Passenger Car 0 Not Stated		Not at intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Unknown Clear or Partly Cloudy Unknown	Dry Curve & Level	Dark-No Street Lights Guardra		Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead		E351
7622 State Route 7623 State Route 7624 State Poute	Jefferson 02 Jefferson 02	20 20 20	2.78 No E82695 2.78 No E82695 2.80 No E82695 2.80 No E36503	2018-07-27 13:50 No Apparent Injury 2014-12-21 10:10 No Apparent Injury 2019-09-09 22:10 Poss No Injury	0 0 1 0	Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast Raining	Dry Curve & Grade Wet Curve & Grade Wet Curve & Grade	Daylight Earth Ba Daylight Earth Ba	nk or Ledge nk or Ledge nk or Ledge	Going Straight Ahead Going Straight Ahead Going Straight Ahead		South North East West West West
7624 State Route 7625 State Route 7626 State Route 7627 State Route 7628 State Route	Jefferson 02 Jefferson 02 Jefferson 02	20 20	2.89 No E95833 3.00 No E54125 3.04 No E87539	2016-05-06 10:43 No Apparent Injury 2018-12-21 21:05 No Apparent Injury	0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Curve & Grade	Daylight Vehicle	nk or Ledge Strikes Deer (stationary)	Going Straight Ahead		East West East
7627 State Route 7628 State Route 7629 State Route	Jefferson 02 Jefferson 02 Jefferson 02	20 20 20	3.13 NO E580091	2016-08-22 16:07 No Apparent Injury	0 0 1 0	U Pickup,Panei Truck or vanette under 10,000 lb		Not at intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Curve & Grade Dry Curve & Grade	Daylight Earth Ba Daylight Vehicle:	nk or Ledge Strikes Deer Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East East West South North
7629 State Route 7630 State Route 7630 State Route 7631 State Route 7631 State Route 7632 State Route 7632 State Route 7634 State Route 7634 State Route 7635 State Route 7635 State Route	Jefferson 02 Jefferson 02	20 20	3.18 No E87474 3.25 No E38433	2018-12-17 17:08 Possible Injury 2014-12-19 01:19 No Apparent Injury	1 0 1 0	0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Raining Overcast	Wet Curve & Grade Dry Straight & Grade	Dark-No Street Lights Roadwa Dark-No Street Lights Fallen tr	y Ditch ee hit by vehicle (on the road)	Going Straight Ahead Going Straight Ahead		West East West East
7632 State Route 7633 State Route 7634 State Route	Jefferson 02 Jefferson 02 Jefferson 02	20 20 20	328 No E38543 333 No E74747 337 No E88529	2014-12-19 01:31 No Apparent Injury 2017-11-06 09:15 No Apparent Injury 2018-12-21 21:00 No Apparent Injury	0 0 1 0 0 0 1 0 0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Truck (Flatbad,Van,etc) Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Raining		Dark-No Street Lights Fallen to Daylight Fire star Dark-No Street Lights Fallen ro		Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East East West West East
7635 State Route 7636 State Route 7637 State Route	Jefferson 02 Jefferson 02	20 20 20	3.49 No E76387	2018-01-27 10:00 No Apparent Injury 2018-09-16 07:37 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast	Wet Curve & Level Wet Curve & Grade	Daylight From sa Dawn Roadwa	ck hit by vehicle (on the road) me direction - both going straight - both moving - rear-end y Ditch	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing	East West East West West East West
7637 State Route 7638 State Route 7639 State Route 7639 State Route 7640 State Route 7641 State Route	Jefferson 02 Jefferson 02 Jefferson 02	20 20 20	3.59 NO E272/UZ 3.52 No E39233 3.66 No E957484	2017-10-13 09:53 No Apparent Injury 2015-01-14 08:20 No Apparent Injury 2019-09-01 19:05 Suspected Minor Injury	0 0 1 0 0 0 1 0 ry 1 0 1 0	0 Truck (Flatbad Van.etc) 0 Pickup, Panel Truck or Vanette under 10,000 lb 0 Matorsycle 0 Pickup, Panel Truck or Vanette under 10,000 lb 0 Truck (Flatbad Van.etc) 0 Passengen Car.		Not at Intersection and Not Related	Fog or Smog or Smoke Clear or Partly Cloudy	Dry Curve & Grade	Daylight Kodowa Daylight Vehicle	Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East West East East West
7640 State Route 7641 State Route 7642 State Route	Jefferson 02 Jefferson 02 Jefferson 02	20 20 20	3.68 No E872436 3.69 No E798731	2018-12-11 19:45 Possible Injury 2018-05-16 13:51 No Apparent Injury 2018-09-07 06:15 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Truck (Flatbad,Van,etc)		At Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Overcast	Wet Curve & Grade Dry Curve & Grade Wet Curve & Grade		gn Post Strikes Deer nk or Ledge	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East East West East West
7642 State Route 7643 State Route 7644 State Route 7645 State Route	Jefferson 02 Jefferson 02	20 20	3.78 No E72855 3.80 No E98283	2016-09-07 06:15 No Apparent Injury 2017-10-25 15:55 Suspected Minor Injur 2019-11-11 17:20 Suspected Minor Injur	ry 1 0 1 0 ry 1 0 1 0	Motorcycle Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Overcast	Dry Curve & Grade Dry Curve & Grade	Daylight Roadwa Dark-No Street Lights Wood Si	y Ditch gn Post	Going Straight Ahead Going Straight Ahead Going Straight Ahead		East West East West
7645 State Route 7646 State Route 7647 State Route	Jefferson 02 Jefferson 02 Jefferson 02	20 20 20	3.96 No E70126 3.98 No E94619 4.19 No E92835	2017-08-12 13:55 No Apparent Injury 2019-07-31 13:04 Suspected Minor Injury 2019-06-07 18:02 No Apparent Injury	y 1 0 1 0 0 0 1 0	O Passenger Lat' O Motorcycle O Pickup Panel Truck or Vanette under 10,000 ib O Pickup Panel Truck or Vanette under 10,000 ib O Passenger Car O Pickup Panel Truck or Vanette under 10,000 ib O Passenger Car O Pickup Panel Truck or Vanette under 10,000 ib O Passenger Car O Pickup Panel Truck or Vanette under 10,000 ib O Pickup Panel Truck or Vanette under 10,000 ib O Pickup Panel Truck or Vanette under 10,000 ib	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Level Dry Straight & Level	Daylight Roadwa	y Dilch	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing	West East West East South North West East
7646 State Route 7647 State Route 7647 State Route 7649 State Route 7649 State Route 7659 State Route 7651 State Route 7651 State Route 7653 State Route	Jefferson 02 Jefferson 02	20	4.30 No E815150	2018-07-03 16:58 No Apparent Injury 2017-09-14 11:44 No Apparent Injury	0 0 1 0	0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Roadwa Daylight Earth Ba	y Ditch nk or Ledge	Going Straight Ahead Going Straight Ahead		West East West East
7650 State Route 7651 State Route 7652 State Route	Jefferson 02 Jefferson 02	20 20 20	4.42 NO E49347 4.43 No E819171 4.60 No E65582	2016-11-17 16:21 No Apparent Injury 2018-07-19 22:20 No Apparent Injury 2017-03-21 13:05 Possible Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level Dry Straight & Level	Dark-No Street Lights Roadwa Daylight From sa	y Ditch	Going Straight Ahead Going Straight Ahead Slowing	Slowing	East west West East East West East West
7653 State Route 7654 State Route 7655 State Route	Jefferson 02 Jefferson 02	20 20 20	4.68 No E40917(4.68 No E40917(4.70 No E47770)	2015-03-16 13:44 No Apparent Injury 2015-03-16 13:44 No Apparent Injury 2016-08-19 12:50 No Apparent Injury	0 0 2 0	Passenger Car Passenger Car Passenger Car Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb		Clear or Partly Cloudy				Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing	East West East West West East Sast
7654 State Route 7655 State Route 7656 State Route 7657 State Route	Jefferson 02 Jefferson 02	20 20	4.80 No E31981 4.80 No E39557	2014-04-03 14:24 Possible Injury 2015-01-26 12:20 No Apparent Injury	1 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Grade		Voltch posite direction - all others	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	West East East West West East
7658 State Route 7659 State Route 7660 State Route	Jefferson 02 Jefferson 02 Jefferson 02	20 20 20	4.83 No E47731 4.90 No E403360 5.21 No E424099	2015-10-28 07:16 No Apparent Injury 2015-02-24 20:22 Possible Injury 2015-05-11 00:58 Suspected Minor Injury	0 0 1 0 1 0 2 0 ry 1 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Truck Tractor & Semi-Trailer	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining Overcast Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level Dry Straight & Grade	Dark-No Street Lights Vehicle: Dark-No Street Lights From op Dark-No Street Lights Vehicle:	Strikes Deer	Going Straight Ahead	Slowing	East West East South North East
7661 State Route 7662 State Route	Jefferson 02 Jefferson 02	20	5.22 No 52-00-00 55-00 No E30858	2018-04-16 11:25 Dead at Scene 2014-02-16 18:30 No Apparent Injury	0 1 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Raining Raining				Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East West East
7657 State Route 7658 State Route 7659 Sante Route 7659 Sante Route 7659 Sante Route 7659 Sante Route 7650 Sante Route 7650 Sante Route 7651 Sante Route 7653 Sante Route 7654 Sante Route 7656 Sante Route	Jefferson 02 Jefferson 02 Jefferson 02 Jefferson 02	20 20 20	5.50 No E30938 5.50 No E73672 5.60 No E44361	2017-11-14 06:41 Suspected Minor Injury 2015-07-10 07:12 Suspected Minor Injury	ry 1 0 1 0 ry 1 0 1 0	O Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pictup Panel Truck or Vanette under 10,000 to Truck Tractor & Semi-Trailer Pictup Panel Truck or Vanette under 10,000 to Pictup Panel Truck or Vanette under 10,000 to	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining Raining Raining	Wet Straight & Level Wet Straight & Grade Oil Curve & Level	Dark-No Street Lights Fallen tr Daylight Fallen tr Daylight Vehicle Daylight Vehicle	ee introy vericie (on the road) ee hit by vehicle (on the road) overhurned	Going Straight Ahead Going Straight Ahead Going Straight Ahead		East West West East North South
7666 State Route 7667 State Route 7668 State Route	Jefferson 02 Jefferson 02 Jefferson 02	20 20 20	5.65 No E831965 5.76 No E45754 5.90 No E45754	2018-08-24 19:38 No Apparent Injury 2015-08-31 14:42 No Apparent Injury 2014-06-12 12:02 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Raining Clear or Partly Cloudy	wet Curve a Level			Going Straight Ahead Going Straight Ahead Going Straight Ahead		North South West East West Fact
7669 State Route 7670 State Route	Jefferson 02 Jefferson 02	20 20	6.00 No E76174 6.04 No E68924	2018-01-23 12:05 Suspected Minor Injury 2017-06-22 22:30 No Apparent Injury	ry 1 0 1 0 0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related Intersection Related but Not at Intersection	Raining Clear or Partly Cloudy	Dry Straight & Level Wet Straight & Level Dry Straight & Level	Dark-No Street Lights Earth Ba	Strikes Deer y Ditch nk or Ledge	Making Left Turn		West East East South
7609 State Route 7670 State Route 7671 State Route 7672 State Route 7673 State Route 7674 State Route 7674 State Route	Jefferson 02 Jefferson 02 Jefferson 02 Jefferson 02	20 20 20	6.05 No E635656 6.05 No E96056 6.08 No E40267	2017-06-15 16:10 Suspected Minor Injury 2019-09-13 16:20 Possible Injury 2015-02-16 09:50 No Apparent Injury	1 0 2 0 1 0 2 0 0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Driveway At Driveway	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight From sa Daylight From sa Daylight From sa	me direction - one left turn - one straight me direction - one left turn - one straight me direction - both going straight - both moving - rear-end aii - Face	Going Straight Ahead Overtaking and Passing Slowing	Making Left Turn Slowing Slowing	North South North Vehicle Stopped East West East West East West East West
7674 State Route 7675 State Route	lefferson 02	20 20 20	6.16 No E645344 6.18 No E40511	2017-02-21 12:51 No Apparent Injury 2015-02-18 03:30 No Apparent Injury	0 0 1 0	0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Driveway Related but Not at Driveway	Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Dark-No Street Lights From sa	me direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead	Stopped for Traffic	West East Vehicle Stopped Vehicle Stopped Cont. Mark
7677 State Route 7678 State Route	Jefferson 02 Jefferson 02 Jefferson 02	20 20	6.25 No E44129	2015-07-04 21:41 No Apparent Injury	0 0 3 0	0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Entering Dark-Street Lights On Entering	at angle at angle	Going Straight Ahead Merging (Entering Traffic) Going Straight Ahead	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Vehicle Stopped South West East South North East West
7679 State Route 7680 State Route 7681 State Route	Jefferson 02 Jefferson 02 Jefferson 02	20 20 20		2015-12-15 18:45 Suspected Minor Injury 2016-06-27 16:40 Suspected Minor Injury 2017-08-17 15:05 Possible Injury 2017-11-22 17:20 No Apparent Injury 2018-08-20 2018 Suspected Minor Injury		Passenger Car Pickup Panel Truck or Vanette under 10 000 lb.	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Truck Tractor & Semi-Trailer	At Intersection and Related At Intersection and Related	Raining Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level Dry Straight & Level	Dark-Street Lights On From sa Daylight Entering Daylight Entering Dark-Street Lights On Entering	me direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Making Left Turn	Stopped for Traffic Going Straight Ahead Going Straight Ahead	South North Vehicle Stopped Vehicle Stopped East West West East North East West West
7682 State Route 7683 State Route	Jefferson 02 Jefferson 02	20 20 20	6.25 No E73923 6.25 No E73923 6.25 No E82915 6.26 No E82915	2017-11-22 17:20 No Apparent Injury 2018-08-02 12:55 Suspected Minor Injur	0 0 2 0 ry 1 0 2 0				Raining Overcast			atangle atangle	Stopped in Roadway Starting in Traffic Lane	Going Straight Ahead Going Straight Ahead	Vehicle Stopped Vehicle Stopped South North West East West East West East West Savet Moreh
7874 State Rocke	Jefferson 02 Jefferson 02 Jefferson 02	20 20 20	625 No E86099 625 No E87099 625 No E89211	2018-12-22 16:25 No Apparent Injury 2019-03-04 16:39 Suspected Minor Injury	0 0 2 0 ry 1 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Materials	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Overcast Clear or Partly Cloudy	Dry Wet Straight & Level Dry Straight & Level	Daylight Entering Dusk Entering Daylight Entering	atangle atangle atangle	Going Straight Ahead Starting in Traffic Lane Starting in Traffic Lane	Going Straight Ahead Going Straight Ahead Going Straight Ahead	North
7687 State Route 7688 State Route 7689 State Route	Jefferson 02 Jefferson 02 Jefferson 10	20 20 01	6.25 No E95120 6.27 No E97525 274.67 No E97525	2019-08-13 15:48 No Apparent Injury 2019-10-25 11:07 No Apparent Injury 2019-05-09 15:00 No Apparent Injury	0 0 2 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Motorcycle Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Truck Tractor & Semi-Trailer	At Intersection and Related Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level Wet Straight & Level	Daylight Entering Daylight From sa	at angle me direction - both going straight - both moving - rear-end	Going Straight Ahead	Going Straight Ahead Slowing	West East East West East West East West East West South
7690 State Route 7691 State Route	Jefferson 10 Jefferson 10	01	274-07 NO EM190-07 274-07 NO E4190-07 274-74 NO E530722	2017-02-06 00:55 No Apparent Injury 2016-03-28 01:05 No Apparent Injury	0 0 1 0 0 0 2 0	0 Passenger Car 0 Passenger Car	Truck Tractor & Semi-Trailer	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Snowing Clear or Partly Cloudy	Snow/Slush Straight & Grade Dry Straight & Grade	Dark-No Street Lights From sa	me direction - one right turn - one straight ink or Ledge me direction - both going straight - both moving - rear-end Striker Dever	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Making Right Turn Slowing	East West East West
7991 Sale Route 7992 Sale Route 7993 Sale Route 7993 Sale Route 7993 Sale Route 7999 Sale Route	Jefferson 10 Jefferson 10 Jefferson 10	01 01 01	274.94 No E5710921 To 274.94 No E771392 To 274.94 No E925431 To 274.94 No E925431 To 274.94 No E925431 To 274.94	2016-08-01 08:10 No Apparent Injury 2017-05-28 12:10 No Apparent Injury 2019-05-25 12:43 No Apparent Injury	0 0 1 0 0 0 1 0 0 0 3 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Raining	Dry Straight at Hillor	Daylight Fire star	ted in vehicle	Going Straight Ahead	Going Straight Ahead	East
7695 State Route 7696 State Route	Jefferson 10 Jefferson 10	01	275.00 No 391949 275.00 No 989494 275.00 No 989494 275.00 No E996371	2016-08-01 (08.10) No Apparent Injury 2017-05-28 12:10 No Apparent Injury 2019-05-25 12:43 No Apparent Injury 2014-10-05 19:32 No Apparent Injury 2019-09-18 11:06 No Apparent Injury 2014-11-29 07-8 No Apparent Injury	0 0 1 0	0 Passenger Car 0 Truck Tractor & Semi-Trailer	, , , , , , , , , , , , , , , , , , , ,	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast		Daylight From sa Dark-No Street Lights Vehicle: Daylight Roadwa		Overtaking and Passing Going Straight Ahead Going Straight Ahead		West East South North
7697 State Route 7698 State Route 7699 State Route	Jefferson 10 Jefferson 10 Jefferson 10	01 01	275.17 No E69888 275.25 No E693360	2017-07-30 21:55 No Apparent Injury 2017-07-17 00:25 Possible Injury	0 0 1 0	0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Dawn Earth Ba Dark-No Street Lights Vehicle : Dark-No Street Lights Guardra	in or Leage Strikes Deer I - Face	Going Straight Ahead Going Straight Ahead Going Straight Ahead		Mest East
7700 State Route 7701 State Route 7702 State Route	Jefferson 10	01		2017-10-29 19:45 No Apparent Injury	0 0 1 0	O Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Dickup, Panel Truck or Vanette under 10,000 lb Dickup, Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy Overcast Spowing	Dry Straight at Hillor Wet Straight & Grade Snow/Sturb	Daylight From op Dark-No Street Lights Vehicle :	posite direction - both going straight - sideswipe	Going Straight Ahead	Going Straight Ahead	West East East West West West West West
7703 State Route 7704 State Route	Jefferson	01	275.40 No E771.25 275.48 No E4170.25 275.56 No E84170.25	2017-10-29 19:45 No Apparent Injury 2018-02-18 07:30 No Apparent Injury 2017-02-05 21:32 No Apparent Injury 2014-11-29 05:29 No Apparent Injury	0 0 1 0 0 0 1 0 0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Snowing Snowing Snowing	Snow/Slush Straight & Grade	Dark-No Street Lights Over Em	bankment - No Guardrail Present	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead		East West North South
7705 State Route 7706 State Route 7707 State Route	Jefferson 10 Jefferson 10 Jefferson 10	01 01 01	275.57 No E603571 275.58 No E343655 275.71 No E81850	2016-10-24 17:20 Possible Injury 2014-07-08 09:05 No Apparent Injury 2018-07-11 15:15 Possible Injury	1 0 2 0 0 0 1 0	Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Grade Dry Straight & Grade Dry	Daylight From sa Daylight Vehicle: Daylight Over Fm	me direction - both going straight - one stopped - rear-end Strikes Deer bankment - No Guardrail Present	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic	West East West East West East South South North
7700 Sales Rode	Jefferson 10 Jefferson 10	01	275.71 No E73678 275.90 No E73678 275.90 No E734100	2017-11-11 18:16 Suspected Serious In 2017-11-11 19:20 No Apparent Injury	jury 1 0 2 0 0 0 0 3 0	O Pickup Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related	Raining Raining	Wet Straight & Level	Dark-Street Lights On Entering	at angle		Going Straight Ahead Slowing	South South North South North North
7710 State Route 7711 State Route 7712 State Route	Jefferson 10 Jefferson 10 Jefferson 10 Jefferson 10	01	276.01 No E87338 276.05 No E37311 276.10 No E58432	2018-12-16 10:09 No Apparent Injury 2014-09-14 19:53 No Apparent Injury 2016-09-10 00:07 Possible Injury	0 0 1 0 1 0 1 0	0 Passenger Car 0 Passenger Car 0 Passenger Car	гижир, Panei Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Level	Dark-No Street Lights Vehicle : Dark-No Street Lights Vehicle :	me direction - both going straight - both moving - rear-end me direction - both going straight - both moving - rear-end Strikes Deer Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing	South North South North West East
7713 State Route 7714 State Route 7715 State Route	Jefferson 10 Jefferson 10	01	276.14 No E461961	2015-09-16 04:32 Possible Injury	1 0 1 0	0 Pickup,Panel Truck or Vanette under 10,000 lb		At Intersection and Not Related	Clear or Partly Cloudy Overcast Clear or Partly Cloudy	Dry Straight & Grade Wet Straight & Level	Dark-No Street Lights Vehicle: Dark-No Street Lights Vehicle: Daylight From sa Dark-No Street Lighte Entering	Databas Usars Strikes Deer me direction - both going straight - one stopped - sideswipe at angle	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic	West East Vehicle Stopped Vehicle Stopped West East Vehicle Stopped Were Stopped Vehicle Stopped
7716 State Route 7717 State Route	Jefferson 10 Jefferson 10 Jefferson 10 Jefferson 10	01 01 01	276.14 No E90446 276.30 No E82018	2019-03-13 09:31 No Apparent Injury 2018-07-22 12:26 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb		Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight From sa Daylight Vehicle	me direction - one right turn - one straight Strikes Deer	Going Straight Ahead	Going Straight Ahead Making Right Turn	South East West
7718 State Route 7719 State Route 7720 State Route	Jefferson 10 Jefferson 10 Jefferson 10	01 01 01	276.60 No E671457	2017-05-10 16:20 Possible Injury	1 0 1 0	0 Pickup, Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Vehicle : Daylight Roadwa Dark-No Street Lights Roadwa	Strikes Deer y Ditch y Ditch	Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East South North East West
7721 State Route 7722 State Route	Jefferson 10 Jefferson 10 Jefferson 10 Jefferson 10 Jefferson 10	01	276.80 No E48720 276.80 No E4287 276.90 No E49677	2015-05-20 23:23 No Apparent Injury 2015-12-20 09:56 No Apparent Injury	0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Overcast	Dry Straight & Level	Dark-No Street Lights Roadwa	y Ditch			West East North South
7723 State Route 7724 State Route 7725 State Route		01 01 01	278.90 No E496777 10 2496777 10 278.90 No E910747 10 278.90 No E910737 10 278.90 No E91073 10 278	2018-06-09 23:00 No Apparent Injury 2018-06-20 22:23 No Apparent Injury 2019-03-25 05:19 No Apparent Injury	0 0 1 0	0 Trous (Fraudots, variety) 0 Passenger C Tack or Vanette under 10,000 ib 0 Pickup, Panel Truck or Vanette under 10,000 ib 0 Pickup, Panel Truck or Vanette under 10,000 ib 0 Pickup, Panel Truck or Vanette under 10,000 ib 0 Pickup, Panel Truck or Vanette under 10,000 ib 0 Passenger Catr 0 Pickup, Panel Truck or Vanette under 10,000 ib 0 Passenger Catr 0 Pickup, Panel Truck or Vanette under 10,000 ib 0 Passenger Catr	-	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Level	Daylight Vehicle: Dark-No Street Lights Roadwa Dark-No Street Lights Vehicle: Dark-No Street Lights Vehicle:	y Ditch Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead		North South North South West East
7726 State Route 7727 State Route	Jefferson	01	279.95 No E99912 276.95 No E99122 276.96 No E99972	2019-02-08 20:10 No Apparent Injury 2018-11-12 00:30 No Apparent Injury 2018-11-12 00:30 No Apparent Injury	0 0 1 0	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Snowing Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Snow/Slush Straight & Grade Dry Straight & Level	Dark-No Street Lights Off Roadwa Dark-No Street Lights Roadwa	y Ditch y Ditch	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead		North South North South
7720 Sate Rode	Jefferson 10 Jefferson 10	01 01	277.00 No E81725 277.26 No E82359 277.33 No E97121	2018-07-08 15:20 No Apparent Injury 2018-07-21 14:40 Possible Injury 2019-10-15 11:28 No Apparent Injury	0 0 1 0 1 0 2 0 0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight From sa	me direction - both going straight - one stopped - rear-end		Stopped for Traffic	recrin South South Vehicle Stopped Vehicle Stopped North South
7731 State Route 7732 State Route	Jefferson	01	277.33 No E97121 277.40 No E50066 277.50 No E60011	2018-07-21 14:40 Possible Injury 2019-07-21 14:40 Possible Injury 2019-10-15 11:28 No Apparent Injury 2016-05-30 16:50 Suspected Minor Injur 2015-03-03 19:25 Possible Injury 2013-03-07 03:46 Suspected Minor Injury	ry 1 0 2 0	Pickup, Panel Truck or Vanette under 10,000 lb Motorcycle Passenger Car Carlon Carlo		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Grade Dry Straight & Level	Dark-No Street Lights Roadwa	y Ditch	Going Straight Ahead Overtaking and Passing Going Straight Ahead	Going Straight Ahead	West East West West East West
7734 State Route 7735 State Route	Jefferson 10 Jefferson 10 Jefferson 10 Jefferson 10	01	227.7.8 No 550515- 227.7.8 No 550515- 227.7.5 No 542476- 227.7.5 No 53025- 227.7.6 No 53025- 227.7.6 No 50025- 227.7.6 No 5	2018-09-25 06:46 No Apparent Injury 2014-11-29 11:34 Suspected Minor Injury	0 0 1 0 ry 1 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	loe Straight & Level Dry Straight & Level loe Straight & Grade Wet Straight & Grade	Dark-No Street Lights Earth Ba Dawn Vehicle : Daylight Earth Ba	in or Ledge Skikes Deer nk or Ledge	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead		South North North South East West
7736 State Route 7737 State Route 7738 State Poule	Jefferson 10	01 01	277.86 No 350067.7 278.06 No 6522400.7 278.07 No 659573.07	2014-07-04 17:56 No Apparent Injury 2016-03-01 13:18 No Apparent Injury 2019-08-14 02-45 Supported Man	0 0 1 0	Pickup, Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining	Wet Straight & Grade Wet Curve & Level Dry Curve * Level	Daylight Vehicle: Daylight Fallen tr Dark-No Street Lights Vehicle	Strikes Deer se hit by vehicle (on the road) westurned	Going Straight Ahead		West East South North East West
Deline Income	Jefferson 10		278.07 No E950730			I DOD, OF THE SECOND TO SECON	+		and a survividual	, Juive a Level			Going Straight Ahead		

7559 Other Contributing Circ Not Listed 30 7560 Distractions Outside Vehicle 33	WV_DRIVER GROUDE MV_DRIVER_CONTRIBUTING GROUDE LIMIT_1 Female Female WV_DRIVER_CONTRIBUTING CRCUMSTANCE_1_UNT_2 CRCUMSTANCE_2_UNT_2 CRCUMSTANCE_3_UNT_2 CRCUMSTANCE_3_UNT_2	MV_DRIVER AGE_UNIT_2	INV. DEVER BICYCLIST, CONTRIBUTING CROMSTANCE, 2, UNIT, 1 UNIT, 2 BICYCLIST, CONTRIBUTING CROMSTANCE, 2, UNIT, 1 CROMSTANCE, 2, UNIT, 1 CROMSTANCE, 2, UNIT, 1	BOYCLIST BOYCLIST BOYCLIST CONTRIBUTING BOYCLIST, CONTRIBUTING UNIT. 1 CRCUMSTANCE, 1_UNIT. 2 CRCUMSTANCE, 2_UNIT. 2	BICYCLIST_CONTRIBUTING CIRCUMSTANCE_3_UNIT_2	BICYCLIST AGE_UNIT_2	BICYCLIST PEDESTRIAN_CONTRIBUTING GENDER_UNIT_2 CIRCUMSTANCE_1_UNIT_2	PEDESTRIAN_CONTRIBUTING CIRCUMSTANCE_2_UNT_2 PEDESTRIAN_CONTRIBUTING CIRCUMSTANCE_3_UNT_2	PEDESTRIAN PEDES AGE_UNIT_2 GENDER	TRAN RRST_MPACT_LOCATION City_St_County_Rd_8_Misc_2010_forward_St Past Right Shoulder increasing Milepost Right Shoulder Deceasing Milepost Right Shoulder Deceasing Milepost Right Shoulder Deceasing Milepost
7983 Under Influence of Richael 7984 Other Contributing Dic Not Listed 7985 Apparently Faliqued 444 49 490 49	Female	68 N	Andrew Comment of the							Past Right Shoulder Increasing Mepost
9790 Oher Combining Cin Not Listed 33 7971 Oher Combining Cin Not Listed 33 7971 Oher Combining Cin Not Listed Institution 54 54 7972 Operating Delection Equipment Exceeding Ress Sale Speed Follow Too Closely 27 2737 Institution Exceeding Ress Sale Speed Follow Too Closely 17 17 17 17 17 17 17 1	Select S	42 M	the state of the s							Pass Right Shoulder Increasing Milegoot Lave 1 Derreasing Milegoot Auen Concessing Milegoot Shoulder Increasing Milegoot Shoulder Increasing Milegoot Shoulder Increasing Milegoot Pass Right Shoulder Increasing Milegoot
3786 Other Contributing Dis Not Listed Districtions Outside Vehicle 56	Francis	76 M	Able							Pass Richt Shoulder Increasing Milegoot Pass Right Shoulder Increasing Milegoot Jame 1 Increasing Milegoot Pass Right Shoulder Increasing Milegoot Pass Right Shoulder Increasing Milegoot Pass Right Shoulder Increasing Milegoot Right Shoulder Increasing Milegoot Right Shoulder Increasing Milegoot
1984	Made Female Mule Mule Mule Female Female Female Female Female Female									Pass Right Shoulder Decreasing Mignost Pass Right Shoulde
7991 Oher Contributing Cin Not Listed Distractions Outside Whicke 444 7992 Under Influence of Alcohol	Male	60 N								Lane 1 Decreasing Misepost Past Right Shoulder Decreasing Milepost Past Right Shoulder Increasing Milepost Past Right Shoulder Increasing Milepost
7600 None	Select	59 N	remale Asle Asle							Lane I horeasing Milespoot Pass Right Shoulder Decreasing Milespoot Lane 10 Decreasing Milespoot Past Right Shoulder Increasing Milespoot Past Right Shoulder Increasing Milespoot Past Right Shoulder Increasing Milespoot
7609 Apparently Fatigued 31 7610 Other Contributing Circ Not Listed Exceeding Reas. Safe Speed 25 7611 Apparently Fatigued 50 (Oseph) 16	Made	35 N 16 N	Andre de la companya del companya de la companya del companya de la companya de l							Past Right Shoulder Increasing Milegost Lane 10 Dereasing Milegost Past Right Shoulder Increasing Milegost Lane 10 Dereasing Milegost Past Right Shoulder Increasing Milegost
1915 Fateriolica	Male None Male Male	33 N	And the second s							Line 1 Docrassing Mapos I Right Shoulder Doceasing Mapost Right Shoulder Doceasing Mapost Right Shoulder Doceasing Mapost Right Shoulder Loreasing Mapost Park Right Shoulder Loreasing Mapost Right Shoulder Loreasing Mapost
	Male Female Adde Mele Mele Mele Mele Mele Mele Mele M									Regist Squal Storout S
7630 Other Contributing Circ Not Listed 770 7631 None 399	Female	83 N	Anice							Lane 1 Formassing Milegost Past Digits Toucher Increasing Milegost Lane 1 Formassing Milegost
7638 None 57 7639 None 22 7840 Distractions Outside Vehicle 67 7841 None 62 7842 Under Influence of Mochal 53 7843 Instruction Exceeding Reas. Safe Speed 33 7843 Instruction 52	Make									Pest Hody Shoulder Exhibition Missess Pest Hody Shoulder Foressens Missess Past Roys Towarder Foressens pill Report Past Roys Towarder Foressens pill Report Asen To Dersonsing Melegost Asen To Dersonsing Melegost Asen To Dersonsing Melegost Asen Towarder Foressens pill Report Past Roys Towarder Poersens pill Report Past Roys Towarder Poersens pill Report Past Roys Towarder Poersens pill Melegost
1766	Mode	60 N	onale .							Leve I horses in p Mapped Dest Phot Scouler horsesses Miscost Part May Scouler horsesses Miscost Part May Scouler horsesses Miscost Part May Scouler horsesses Miscost Part Right Scouler horsessing Miscost Part Right Scouler horsessing Miscost Leve I Decreasing Miscost Leve I Decreas
7855 None 63 7856 Apparently Acleep or Fatigued 78 7857 Other Contributing Circ Not Listed 45 7858 None 55 7859 Under Influence of Alcobol 78 7860 Under Influence of Alcobol 78 7860 Under Influence of Statedom 63 7860 Under Influence of Statedom 63	Female None	69 N 75 N 28 N	Asie							Lane 1 Decreasing Melegost Past Digit Stooder Increasing Melegost Lane 1 Decreasing Melegost Lane 1 Decreasing Melegost Amen 1 Decreasing Melegost Lane 1 Decreasing Melegost Lane 1 Decreasing Melegost Lane 1 Decreasing Melegost Lane 1 Newseal pMelegost Past Right Shoulder Increasing Melegost Past Right Shoulder Increasing Melegost Past Right Shoulder Increasing Melegost
7653 One Combinating Circ Not Listed 14	Made									Lane 1 Processing Milepost Lane 2 Past Right Mouder Decreasing Milepost Lane 1 Decreasing Milepost Past Right Mouder Decreasing Milepost Lane 1 Decreasing Milepost Past Right Mouder Decreasing Milepost
1969 Apparently Nation or Faligned Other Contributing Circ Not Listed Hastention 37 1970 Under Hindman of Alcohol Other Contributing Circ Not Listed Hastention 31 1971 Institution State	Made	56 F 54 F 17 M 29 F	Termids Femals Femals Femals Femals Femals Femals Femals Femals							Lane I homesing Milepost Plant Right Shoulder Decreasing Milepost Plant Right Shoulder Decreasing Milepost Ram Flory Shoulder Decreasing Milepost Lane I becreasing Milepost Lane I lecreasing Milepost Lane I lecreasing Milepost Lane I lecreasing Milepost Lane I becreasing Milepost Registration of the Committee Milepost Registration of the Committee Milepost Registration of the Committee Milepost Lane I lecreasing Milepost Lane I lecreasing Milepost
16	Male	63 F 28 M 27 F	nomation beta onnate to the to							Lane I horsexing Milepost Lane I horsexing Milepost Lane I Oborsexing Milepost Lane I Oborsexing Milepost Lane I Dorsexing Milepost Lane I lorsexing Milepost Lane I lorsexing Milepost Lane I lorsexing Milepost Lane I Voronzezing Milepost
25	Female	74 F	able bits bits bits bits bits bits bits bits							Lame 1 formassing Milepost Lame 1 Decreasing Milepost
7893 None 57 7994 Cycenting Defective Equipment Exceeding Reas. Safe Speed 31 7995 Other Contributing Circ Not Listed 65 7996 Under Hindmore of Dhage 40 7996 Lived Hindmore of Dhage 40 7997 Exceeding Reas. Safe Speed 65 7998 Lived Reas. Safe Speed 65 7998 None 65	Make Female None Female Mone Female Mone Female Mone M									Lane 1 Occreasing Milepost Lane 1 Occreasing Milepost Lane 1 Increasing Milepost Lane 1 Increasing Milepost Lane 1 Increasing Milepost Lane 1 Increasing Milepost Past Right Shoulder Increasing Milepost Past Right Shoulder Increasing Milepost Lane 1 Increasing Milepost Lane 1 Increasing Milepost Lane 1 Increasing Milepost Lane 1 Increasing Milepost And Milepost Increasing Milepost
7701 Oher Combining Circ Not Listed 40 7702 Exceeding Reas Safe Speed 27 7703 Exceeding Reas Safe Speed 27 7704 Institution Exceeding Reas Safe Speed 28 7705 Institution Exceeding Reas Safe Speed 39 7706 Institution Follow Too Closely 35 7706 Nove 40 7707 Nove 40 7708 Nove 40 7709 Nove 40 7709 Nove 40 7700 No	Made		date							See 1 Considered Methods Anni Considered Methods Past Right Shoulder Increasing Megost Dest Right Shoulder Increasing Megost Lane 1 Victoria Right Megost Dest Right Shoulder Increasing Megost Dest Right Shoulder Increasing Megost Dest Right Shoulder Decessing Megost
1709	Female		Asia Asia Asia Asia Asia Asia Asia Asia							Lane 1 Decreasing Misport Lane 1 Decreasing Misport Lane 1 Decreasing Misport Lane 1 Percensing Misport
1716 Folion 100 LOSely 47 7717 None 55 7718 None 55 7719 None 55 7719 None 55 7719 None 55 7719 Under Influence of Alcohol Apparently Asleep or Fatqued 52 7720 Apparently Asleep or Fatqued 122 7720 Under Influence of Alcohol 123	Made									Lane 1 Docreasing Mitepost Lane 1 Docreasing Mitepost Lane 1 Torreasing Mitepost Lane 1 Torreasing Mitepost Lane 1 Torreasing Mitepost Past Royal Shoulder Increasing Mitepost Past Royal Shoulder Increasing Mitepost Past Royal Shoulder Increasing Mitepost Lane 1 Foresing Mitepost
7724 None 37 7726 Exceeding Reas. Safe Speed 56 7727 Distraction Outside Whidele 72 7727 Distraction Outside Whidele 66 7728 Paparently Fatigued 46 7728 Fajor Scotely 40 7730 Apparently Fatigued 74 7731 Exceeding Reas. Safe Speed Improper Passing 44 Improper Passing 44	Female	29 M								Lane 1 increasers Mispost Para Right Stoudier Decreasing
	Mate									Pass Right Shoulder Increasing Milepost Lane 1 Decreasing Milepost Pass Right Shoulder Decreasing Milepost Lane 1 Normaning Milepost Lane 1 Normaning Milepost Lane 1 Decreasing Milepost

WA_STATE_PLANE SOUTH_X 2010_FORWARD WA_STATE_PLANE SOUTH_Y 2010_FORWARD			OL COL SE IMPAIRED	COL_AGE SURFACE CONDITIONS	COL_LIGHT CONDITIONS	COL LOCATION	GEOMETRY A	TUDY COL COL TYPE TEXT TEXT		MVIW CONDITIONS CO	L_SURFACE ONDITIONS Decoded COL_LOCATION Decoded	COL_RISKY BEHAVIOR	Intersection Emphasis Area_Name Area
61 1057280.84 977773.71 2017	1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object 3 Vehicles/Parties Struck Object	Suspected Serious Injury Apparently Asleep, Fatigued or III 25-3	No No Apparently Asleep, Fatigued or III	Dry Dayli Dry Dayli 25-34 Wet Dark	ght No No Street Lights/Street Lights Off No	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Curve & Grade Curve & Grade Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Other Contributing Circ Not Listed Distractions Outside Vehicle Apparently Asleep or Fatigued	Daylight Dark-No Street Lights/Street Lights Off		No Risky Driving Behaviors Involved Distracted Driving Apparently Asleep, Fatigued or III	
63 1057379.1 977901.11 2015	1 \tehicleParty	Property Damage Only Other/Unknown 35-4	No Under Influence of Alcohol No	Dry Dayli 45-54 Dry Dayli Wet Dayli	ght No	t at Intersection/Driveway/Roundabout	Curve & Grade Straight & Grade Curve & Grade Curve & Grade	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Inattention Under Influence of Alcohol/Drugs/Medication Other Contributing Circ Not Listed Inattention	Dayight	Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout	Inattention Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved	
66 1057486.89 978021.59 2016 67 1057785.07 978235.65 2017	1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object	Property Damage Only Inattention Und Property Damage Only Inattention 25-3 Property Damage Only Inattention Und	7.25 NO 5 No 7.25 No	Wet Dayli Wet Dayli Wet Dayli	ght No	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Curve & Grade Curve & Grade	No Vuinerable Road User Involved No Vuinerable Road User Involved No Vuinerable Road User Involved	Inattention	Daylight	Wet Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout	Inattention Inattention	
67 1057785.07 978235.65 2017 68 1057827.4 978263.5 2014 69 1057918.76 978316.98 2018 70 1057944.18 978330.31 2018	1 Vehicle/Party	Property Damage Only Operating Defective Equipment 65+ Fatality Other/Unknown 55-6 Property Damage Only Other/Unknown 25-3	No I No I No	Dry Dayli Wet Dayli Wet Daw	ght No ght No n/Dusk No	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Straight & Grade Curve & Grade Curve & Grade	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Operating Defective Equipment Other Contributing Circ Not Listed Other Contributing Circ Not Listed	Daylight Daylight Dawn/Dusk	Dry Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	
71 1057944.18 978330.13 2018 72 1058466.37 978459.24 2019 73 1058550.97 978456.76 2019	1 Vehicle/Party Overturned 1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object	Minor or Possible Injury Other/Unknown 45-5 Property Damage Only Operating Defective Equipment 25-3	No i No i i No r 25 No	Dry Dayli		t at Intersection/Driveway/Roundabout	Curve & Grade Curve & Grade Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Other Contributing Circ Not Listed Operating Defective Equipment Inattention	Dark-No Street Lights/Street Lights Off Daylight Daylight	Wet Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Inattention	
75 1059432.74 978203.87 2017	1 Vehicle/Party Struck Object	Property Damage Only Inattention Und Mnor or Possible Injury Inattention 25-3 Mnor or Possible Injury Speeding 35-4 Property Damage Only Other/Unknown 55-6	No No No		gnt No -No Street Lights/Street Lights Off No ght No -No Street Lights/Street Lights Off No		Straight & Grade Straight & Grade	No Vuinerable Road User Involved No Vuinerable Road User Involved No Vuinerable Road User Involved	Inattention Speeding Other Contributing Circ Not Listed	Dark-No Street Lights/Street Lights Off Daylight Dark-No Street Lights/Street Lights Off	Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout	Inattention Speeding No Risky Driving Behaviors Involved	
77 1059544.84 978166.4 2017 78 1059996.2 977932.26 2014	5 1 Vehicle/Party Struck Object 7 2 Vehicles/Parties Rear-End 1 Vehicle/Party Struck Object 3 Vehicle/Party Struck Object	Minor or Possible Injury Follow Too Closely 35-4 Property Damage Only Speeding 55-6	No No	Dry Dayli Wet Dark	ght Inti No Street Lights/Street Lights Off No	t at intersection/Driveway/koundabout	Straight & Grade Straight & Grade Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved	Follow Too Closely Speeding	Daylight Dark-No Street Lights/Street Lights Off	Dry Intersection Related but Not at Intersection Wet Not at Intersection/Driveway/Roundabout	Speeding Speeding	
80 1060196.5 977794.54 2016 81 1060260.65 977763.57 2015	9 3 Vehicles/Parties Rear-End 5 1 Vehicle/Party Struck Object 5 1 Vehicle/Party Struck Object 5 1 Vehicle/Party Struck Object	Property Damage Only Operating Defective Equipment 65+ Property Damage Only Speeding 25-3 Property Damage Only Speeding 35-4	NO No No	Wet Dark Wet Dark	-No Street Lights/Street Lights Off No -No Street Lights/Street Lights Off No	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Straight & Level Curve & Grade Curve & Grade	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Operating Defective Equipment Speeding Speeding	Dark-No Street Lights Off Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off	Dry Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Speeding Speeding	
83 1060364.01 977735.45 2015	1 Vehicle/Party Struck Object 1 Vehicle/Party Over Embankment - No Guardrail Present 1 Vehicle/Party Struck Object	Minor or Possible Injury Operating Defective Equipment 25-3 Fatality Under Influence of Alcohol/Drugs/Medication 25-3 Minor or Possible Injury Speeding 55-6	No Under Influence of Acohol No	25-34 Wet Dark	n/Dusk No -No Street Lights/Street Lights Off No ght No -No Street Lights/Street Lights Off No	t at Intersection/UniversityRoundabout t at Intersection/DrivewayRoundabout	Straight & Grade Curve & Grade Curve in Sag Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 66+	Operating Defective Equipment Under Influence of Alcohol/Drugs/Medication Speeding	Dawn/Dusk Ice.	\(\text{NsnowsStush}\) Not at Intersection/Driveway/Roundabout \(\text{Wet}\) Not at Intersection/Driveway/Roundabout \(\text{Wet}\) Not at Intersection/Driveway/Roundabout \(\text{Wet}\) Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication Speeding	
87 1060468.81 977730.41 2017	5 1 Vehide/Party Struck Object 1 Vehide/Party Struck Object 1 Vehide/Party Struck Object 1 Vehide/Party Struck Object	Minor or Possible Injury Speeding 55-6 Property Damage Only Ofter/Unknown 65+ Minor or Possible Injury Inattention 25-3 Property Damage Only Under Influence of Alcohol/Drugs/Medication 65+	No I No Under Influence of Acohol	Wet Dark 65+ Dry Dark	-No Street Lights/Street Lights Off No -No Street Lights/Street Lights Off No	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Curve & Grade Curve & Grade Curve & Grade	Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+	Specialing Other Contributing Circ Not Listed Inattention Under Influence of Alcohol/Drugs/Medication	Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off	Wet Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout	Speeding No Risky Driving Behaviors Involved Inattention Under Influence of Alcohol/Drugs/Medication	
88 1060679.16 977733.46 2018 89 1060730.8 977734.56 2016 90 106089.35 977737.96 2014 91 1061642.33 977732.45 2018	1 Vehicle/Party	Property Damage Only Other/Unknown 35-4 Property Damage Only Operating Defective Equipment 55-6 Property Damage Only Other/Unknown 45-5	No No No	Wet Dayli	-No Street Lights/Street Lights Off No ght No ght No	t at Intersection/Driveway/Roundabout	Straight & Grade Straight & Grade Straight & Grade	No Vuinerable Road User Involved No Vuinerable Road User Involved No Vuinerable Road User Involved	None Operating Defective Equipment None	Dark-No Street Lights/Street Lights Off Daylight Davlight	Wet Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	
91 1061642.33 977732.45 2018 92 1062114.58 977567.16 2016	1 Vehicle/Party Struck Object	Suspected Serious Injury Other/Unknown 35-4 Minor or Possible Injury Under Influence of Alcohol/Drugs/Medication 25-3	No Under Influence of Acohol Under Influence of Acohol		ght No -No Street Lights/Street Lights Off No -No Street Lights/Street Lights Off No -No Street Lights/Street Lights Off No	t at Intersection/Driveway/Roundabout	Curve & Grade Curve & Grade Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved	Other Contributing Circ Not Listed Under Influence of Alcohol/Drugs/Medication Under Influence of Alcohol/Drugs/Medication	Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off	Wet Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication Under Influence of Alcohol/Drugs/Medication	
95 1062418.65 977484.75 2017	8 1 Vehide/Party Struck Object 5 1 Vehide/Party Struck Object 1 Vehide/Party Struck Object 5 1 Vehide/Party Struck Object 5 1 Vehide/Party Struck Object	Property Damage Only Under Influence of Alcoholi/Drugs/Medication 25-3 Minor or Possible Injury Speeding 25-3 Property Damage Only Inattention Und Property Damage Only Distracted Driving 65+	I No r 25 No	Dry Dayli Wet Dark	ght No -No Street Lights/Street Lights Off No	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Curve & Grade Curve & Grade Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 66+	Speeding Inattention Distractions Outside Vehicle	Daylight Dark-No Street Lights/Street Lights Off Daylight	Dry Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout	Speeding Inattention Distracted Driving	
66 1062901.5 977652.69 2015 17 1062907.6 977657.18 2018 18 1062907.6 977657.18 2019	1 Vehicle/Party Struck Object 2 Vehicles/Parties Sideswipe 1 Vehicles/Parties Sideswipe 1 Vehicles/Parties Sideswipe 2 Vehicles/Parties Sideswipe	Property Damage Only Improper Passing 45:5 Property Damage Only Apparently Aeleep, Fatigued or III 25:3 Minor or Possible Injury Distracted Driving 65+	No Apparently Asleep, Fatigued or III	Dry Dayli Dry Dayli 25-34 Dry Dark	ght No No Street Lights/Street Lights Off No ght No	t at Intersection/DrivewayRoundabout t at Intersection/DrivewayRoundabout	Straight & Grade Curve & Grade Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	Improper Backing/Passing/Turning/Parking/Signaling Apparently Asleep or Fatigued Distractions Outside Vehicle	Daylight Dark-No Street Lights/Street Lights Off Dark-No Street Lights Off	Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Apparently Asleep, Fatigued or III	
1 1064517.13 978433.32 2014 1 1064496.75 978423.03 2017	1 Venicle/Party /Animai 2 Vehicles/Parties Turning	Property Damage Only Other/Unknown Und Minor or Possible Injury Inattention 45-5	r 25 No 1 No	Dry Dark	-No Street Lights/Street Lights Off At	Intersection and Not Related Driveway or Intersection	Curve & Grade Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved	None Inattention	Dayingin	Dry At Intersection and Not Related Dry At Driveway or Intersection	No Risky Driving Behaviors Involved Inattention	
1064496.57 978423.33 2019 14 1064721.02 978558.95 2018	3 2 Vehicles/Parties Angle 2 Vehicles/Parties Angle 3 Vehicles/Parties Angle 4 Vehicle/Party Struck Object	Mnor or Possible Injury Inattention 45-5 Property Damage Only Disregard Traffic Control Device/Sign 65+ Mnor or Possible Injury Other/Unknown 45-5	No No No	Dry Dayli	ght No	Driveway or Intersection Driveway or Intersection et at Intersection/Driveway/Roundabout	Curve & Grade Curve & Grade Straight & Grade	Mbtorcycle Older Road User 65+ No Vulnerable Road User Involved	Inattention Disregard Traffic Control Device/Sign Other Contributing Circ Not Listed	Dark-No Street Lights/Street Lights Off Dark-Street Lights On Daylight	Dry At Driveway or Intersection Wet At Driveway or Intersection Dry Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	
15 1065102.86 978988.66 2015 16 1065104.66 978993.08 2016 17 1065242.03 979154.6 2018	5 1 Vehicle/Party Struck Object 5 2 Vehicles/Parties Rear-End 5 3 1 Vehicle/Party Struck Object	Property Damage Only Speeding 25-3 Property Damage Only Inattention Und Property Damage Only Inattention 45-5	F No r 25 No F No	loe/Snow/Slush Dark Dry Dark Wet Dark	-No Street Lights/Street Lights Off No -No Street Lights/Street Lights Off No -No Street Lights/Street Lights Off No	t at Intersection/DrivewayRoundabout t at Intersection/DrivewayRoundabout t at Intersection/DrivewayRoundabout	Straight & Grade Straight in Sag	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Speeding Inattention Inattention	Dark-No Street Lights/Street Lights Off Ice. Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off	/Snow/Slush Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout	Speeding Inattention Inattention	
8 1065385.79 979304.9 2018	1 Vehicle/Party Animal	Property Damage Only Other/Unknown 25-3 Property Damage Only Apparently Asleep, Fatigued or III 25-3 Fatality Other/Liknown 25-3	No Apparently Asleep, Fatigued or III No	Dry Dark	-No Street Lights/Street Lights Off No n/Dusk No		Straight & Grade Curve & Level Curve & Grade	No Vulnerable Road User Involved	None Driver Physical Condition Other Contributing Circ Not Listed		Dry Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Apparently Asleep, Fatigued or III No Risky Driving Behaviors Involved	##
106574.3.63 979568.82 2018 1065866.62 979710.81 2017 2 106622.775 980119.2 2014	1 \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \		r 25 No		-No Street Lights/Street Lights Off No	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved	Inattention Speeding	Dark-No Street Lights/Street Lights Off Ice.	/Snow/Slush Not at Intersection/Driveway/Roundabout	Inattention Speeding	##
1066226.74 980123.8 2018 1066226.74 980123.8 2018	5 1 Vehicle/Party Struck Object 3 2 Vehicles/Parties Rear-End 1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object	Property Damage Only Inattention 55-6	No No No	Dry Daw Wet Dayi Wet Dayi	ght No	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Straight & Level Straight & Grade Curve & Grade	Motorcycle No Vulnerable Road User Involved No Vulnerable Road User Involved	Other Contributing Circ Not Listed Operating Defective Equipment Instantion	Dawn/Dusk Daylight Daylight Dark-No Street Lights/Street Lights Off	Dry Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Inattention	##
1067639.44 981262.84 2016	1 Vehicle/Party Struck Object	Property Damage Only Speeding Und Suspected Serious Injury Under Influence of Alcohol/Drugs/Medication 45-5 Minor or Possible Injury Speeding 65+ Unknown Unix	7.25 No Lunder Influence of Alcohol No	Wet Dark	ght No	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Straight & Grade Straight in Sag Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 66- No Vulnerable Road User Involved	Under Influence of Alcohol/Drugs/Medication Speeding	Davlight	Dry Not at Intersection/Driveway/Roundabout	Under Influence of Alcohol/Drugs/Medication Speeding	
1067988.16 981592.85 2015	I 1 Vehicle/Party Struck Object	Minor or Possible Injury Other/Unknown Und	own No r 25 No own No	Dry Dark Unknown Othe	-No Street Lights/Street Lights Off No r/Unknown No	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Curve & Grade Curve & Level Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Other Contributing Circ Not Listed None Other Contributing Circ Not Listed	Other/Unknown Oth Dark-No Street Lights/Street Lights Off Other/Unknown Oth	Wet Not at Intersection/Driveway/Roundabout er/Unknown Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout er/Unknown Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	
1068023.48 981632.38 2016 1068113.3 981778.87 2018 1068145.37 991967.77 2014 1068272.86 982334.05 2019	1 Vehicle/Party Struck Object	Property Damage Only Inattention 55-6 Property Damage Only Inattention 55-6 Minor or Possible Injury Speeding 45-5	No No No	Wet Dayli Wet Dark	gnt No ght No -No Street Lights/Street Lights Off No	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Curve & Grade Curve & Grade Curve & Grade	No Vuinerable Road User Involved No Vuinerable Road User Involved No Vuinerable Road User Involved	Inattention Inattention Speeding	Daylight Daylight Dark-No Street Lights/Street Lights Off	ner/Unknown Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout	Inattention Inattention Speeding	
1068621.49 982796.93 2016	1 Vehicle/Party Animal	Property Damage Only Other/Unknown 45-5 Property Damage Only Other/Unknown 55-6	No No	Dry Dayli Dry Dark	ght No No Street Lights/Street Lights Off No	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Straight & Grade Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	None None	Dark-No Street Lights/Street Lights Off	Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	
1069042.87 983533.58 2019	1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object 1 Vehicle/Party Animal 1 Vehicle/Party Animal	Minor or Possible Injury Other/Unknown 55-6 Property Damage Only Other/Unknown 65+	Apparently Asleep, Fatigued or III No No	Dry Dayli Dry Dayli	ght No	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Straight & Grade Curve & Grade Curve & Grade	Motorcycle Older Road User 65+	Apparently Asleep or Fatigued None None	Daylight Daylight Daylight	Dry Not at Intersection/Driveway/Roundabout	Apparently Asleep, Fatigued or III No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	
1069042.87 983533.58 2019 1069097.58 983622.2 2018 1069324.45 983925.66 2014 1069423.16 984053.74 2014	8 1 Vehicle/Party Struck Object 8 1 Vehicle/Party Struck Object or Debris On Road 9 1 Vehicle/Party Struck Object or Debris On Road 1 1 Vehicle/Party Struck Object or Debris On Road	Minor or Possible Injury Other/Unknown 65+ Property Damage Only Other/Unknown 35-4 Property Damage Only Other/Unknown Und	No I No r 25 No	Dry Dark Dry Dark	No Street Lights/Street Lights Off No No Street Lights/Street Lights Off No No Street Lights/Street Lights Off No	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Curve & Grade Straight & Grade Straight & Grade	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Other Contributing Circ Not Listed None None	Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off	Wet Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	
	1 Vehicle/Party Fire started in vehicle 1 Vehicle/Party Struck Object or Debris On Road 2 Vehicles/Parties Rear-End 1 Vehicle/Party Struck Object	Property Damage Only Other/Unknown Und	1 No 25 No 1 No	Wet Dark		t at Intersection/UniversityRoundabout t at Intersection/Driveway/Roundabout	Straight & Grade Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Operating Defective Equipment None Follow Too Closely	Daylight Dark-No Street Lights/Street Lights Off Daylight	Dry Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Follow Too Closely	
1070086.26 985508.32 2017	1 Vehicle/Party Struck Object	Property Damage Only Inattention Und	No. Under influence of Alcohol r 25 No.	Wet Dayli 35-44 Wet Daw Wet Dayli Dry Dayli	ght No	t at Intersection/Driveway/Roundabout	Curve & Level Curve & Grade Curve & Grade Curve & Grade	No Vuinerable Road User Involved No Vuinerable Road User Involved No Vuinerable Road User Involved	Under Influence of Alcohol/Drugs/Medication Inattention	Dawn/Dusk Daylight Daylight	Wet Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout	Under Influence of Alcohol/Drugs/Medication Inattention No Risky Driving Behaviors Involved	
1070065.81 985667.76 2015 1070001.78 985893.47 2019 1069967.24 985978.66 2018 1069945.21 986032.78 2018	5 1 Vehicle/Party Animal 9 1 Vehicle/Party Animal 1 2 Vehicles/Parties 5 1 Vehicle/Party Animal	Property Damage Only Other/Unknown 55-6 Minor or Possible Injury Other/Unknown Und Minor or Possible Injury Distracted Driving 65-6 Control Office Of	r 25 No	Dry Dayli Dry Daw Wet Dark	-No Street Lights/Street Lights Off At	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout Intersection and Not Related t at Intersection/Driveway/Roundabout	Curve & Grade	Motorcycle Older Road User 65+ No Vulnerable Road User Involved	None Distractions Outside Vehicle	Daylight Dawn/Dusk Dark-No Street Lights/Street Lights Off	Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout Wet At Intersection and Not Related Dry Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Distracted Driving No Risky Driving Behaviors Involved	
1069810.93 986304.01 2016	1 Vehicle/Party	Minor or Possible Injury Inattention 25-3	No Under influence of Acohol No	Dry Dayli 35-44 Wet Dawli Dry Dayli	gnt No n/Dusk No ght No -No Street Lights/Street Lights Off No	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Curve & Grade Curve & Grade	No Vulnerable Road User Involved Motorcycle	Under Influence of Alcohol/Drugs/Medication Inattention	Daylight Dawn/Dusk Daylight Dark-No Street Lights/Street Lights Off	Wet Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout	Under Influence of Alcohol/Drugs/Medication Inattention	
1069755.94 986559.64 2019 1069881.17 987377.19 2017 1069898.42 987495.22 2019	2 Vehicles/Parties Rear-End 1 Vehicle/Party Struck Object	Property Damage Only Follow Too Closely 25-3 Minor or Possible Injury Apparently Asleep, Fatigued or III 65+	Under Influence of Acohol No Apparently Asleep, Fatigued or III	25-34 Dry Dayki 65+ Dry Dayki	ght No	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Straight & Grade Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	Under Influence of Alcohol/Drugs/Medication Follow Too Closely Apparently Asleep or Fatigued	Daylight	Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout	Under Influence of Alcohol/Drugs/Medication Follow Too Closely Apparently Asleep, Fatigued or III	
5 1059898.42 987495.22 2019 7 1070075.91 988563.6 2019 8 1070214.98 989147.31 2017 1070346.4 989547.38 2017	1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object 7 Vehicle/Party Struck Object	Property Damage Only Inattention 35-4	No Apparently Asleep, Fatigued or III No	55-64 Dry Dayli Dry Dayli Dry Dayli	ght No	t at Intersection/DrivewayRoundabout t at Intersection/DrivewayRoundabout t at Intersection/DrivewayRoundabout	Straight & Level Straight & Level Straight & Level	No Vuinerable Road User Involved No Vuinerable Road User Involved No Vuinerable Road User Involved	Inattention Apparently Asleep or Fatigued Other Contributing Circ Not Listed	Daylight Daylight Daylight	Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout	Inattention Apparently Asleep, Fatigued or III No Risky Driving Behaviors Involved	
0 1070425.77 989739.33 2015 1 1070458.73 989804.45 2018	5 1 Vehicle/Party All Other Non-Collision 8 1 Vehicle/Party Struck Object 2 Vehicles/Parties Sideswipe	Property Damage Only Other/Unknown 55-6 Property Damage Only Distracted Driving Unkn	No own No r 25 No	Wet Daw Dry Dark	n/Dusk No No Street Lights/Street Lights Off No	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Straight & Level Straight & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved	None Unknown Distraction	Dawn/Dusk Dark-No Street Lights/Street Lights Off	Wet Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Distracted Driving Spending	
1070859.27 991033.51 2015 1070859.27 991033.51 2015	5 1 Vehicles/Parties Rear-End 5 1 Vehicle/Party Struck Object	Property Damage Only Follow Too Closely 65+ Property Damage Only Other/Unknown 55-6	No I No	Dry Dayli Dry Dayli Dry Dayli	ght No	t at Intersection/UniversityRoundabout t at Intersection/DrivewayRoundabout	Straight & Grade	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved	Speeding Follow Too Closely None		Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout	Follow Too Closely No Risky Driving Behaviors Involved	
1070859.27 991033.51 2015 1070883.31 991135.95 2016 1071004.82 991653.71 2014 1071003.52 991648.14 2015	5 1 Vehicle/Party Animal 5 1 Vehicle/Party Struck Object 5 2 Vehicles/Parties Sideswipe	Property Damage Only Other/Unknown 55-6 Minor or Possible Injury Apparently Asleep, Fatigued or III 65+ Property Damage Only Other/Unknown 45-5	Apparently Asleep, Fatigued or III No	65+ Dry Dayli Dry Dayli	ght No	t at Intersection/Driveway/Roundabout	Straight at Hillcrest Straight & Level Straight & Grade	No Vuinerable Road User Involved Older Road User 65+ No Vuinerable Road User Involved	Apparently Asleep or Fatigued Other Contributing Circ Not Listed	Daylight Daylight Daylight	Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Apparently Asleep, Fatigued or III No Risky Driving Behaviors Involved	
1071039.58 991801.8 2015 1071123.72 992160.34 2015 1071636.48 993700.55 2015 1071659.72 993747.74 2018	1 \ lehideParty Animal	Property Damage Only Ohen/Unknown 55-6 Minor or Possible Injury Under Influence of Alcohol/Drugs/Medication 55-6 Minor or Possible Injury Distracted Driving 55-6	No Under influence of Acohol No	65+ Dry Dark Dry Dark	No Street Lights/Street Lights Off No No Street Lights/Street Lights Off No No Street Lights/Street Lights Off No	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Straight & Level Straight & Level Straight & Grade Straight in Sag	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved	None Under Influence of Alcohol/Drugs/Medication Unknown Distraction	Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off	Wet Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication Distracted Driving	
1072311.48 995069.51 2014	1 Vehicle/Party All Other Non-Collision	Fatality Inattention 65+ Property Damage Only Other/Unknown 35-4 Property Damage Only Other/Unknown 35-4	No I No I No	Wet Dayli Wet Dark	ght No -No Street Lights/Street Lights Off No -No Street Lights/Street Lights Off No		Straight & Level	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Inattention Other Contributing Circ Not Listed Other Contributing Circ Not Listed	Daylight Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off	Wet Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	
1072311.48 995069.51 2017 1072547.04 995544.93 2015 1072650.21 995780.72 2018	I 1 Vehide/Party Struck Object or Debris On Road T 1 Vehide/Party Struck Object or Debris On Road 1 Vehide/Party Overturned 1 Vehide/Party Paintal	Minor or Possible Injury Other/Unknown 65+ Minor or Possible Injury Other/Unknown 65+ Property Damage Only Other/Unknown 35-4	No No No	Wet Dayli Oil Dayli Dry Dayli	ght No	t at Intersection/DrivewayRoundabout t at Intersection/DrivewayRoundabout t at Intersection/DrivewayRoundabout	Straight & Level Straight & Grade Curve & Level Curve & Grade	Older Road User 65+ Older Road User 65+ No Vulnerable Road User Involved	None Other Contributing Circ Not Listed None	Daylight Daylight Daylight	Wet Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout Oil Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	
1072824.31 996545.84 2014	5 1 Vehicle/Party Struck Object 1 Vehicle/Party Animal 1 Vehicle/Party Struck Object	Property Damage Only Under Influence of Alcohol/Drugs/Medication 35-4 Property Damage Only Ofter/Unknown 55-6 Minor or Proscible Injury Apparently Asless - Eatiqued or III 65-	No Apparently Asleep, Fatigued or III	35-44 Wet Dayli Dry Dayli 65+ Wet Dayli			Curve & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65:-	Under Influence of Alcohol/Drugs/Medication None Apparently Asleen or Fatigued	Daylight Daylight Daylight	Wet Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout		
1072897.92 997799.86.2017 1072908.34 997877.17 2017 1072918.3 997874.01 2019	I Vehicle/Party Struck Object 2 Vehicles/Parties Turning Nehicles/Parties Turning	Minor or Possible Injury Inattention 25-3	Under Influence of Acohol No	Dry Dayli	ght At	ersection Related but Not at Intersection Driveway or Intersection Driveway or Intersection	Straight & Level Straight & Level Straight & Level	No Vuinerable Road User Involved No Vuinerable Road User Involved No Vuinerable Road User Involved	Under Influence of Alcohol/Drugs/Medication Inattention Improper Backing/Passing/Turning/Parking/Signaling	Dark-No Street Lights/Street Lights Off Daylight	Dry Intersection Related but Not at Intersection Dry At Driveway or Intersection Dry At Driveway or Intersection	Under Influence of Alcohol/Drugs/Medication Inattention No Risky Driving Behaviors Involved	
1072918.3 997874.01 2019 1072919.19 998015.62 2015 1072946.36 998435.54 2017	2 Vehicles/Parties Turning 2 Vehicles/Parties Turning 3 Vehicles/Parties Rear-End 1 Vehicle/Party Struck Object	Property Damage Only Inattention 35-4	No r 25 No No	Dry Dayii Dry Dayii Dry Dayii	ght At	Driveway or Intersection at Intersection/Driveway/Roundahout	Straight & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved	Follow Too Closely Inattention	Daylight Daylight	Dry At Driveway or Intersection Dry Not at Intersection/Driveway/Roundabout	Follow Too Closely Inattention	
1072973.43 998908.91 2017 1072974.8 998908.17 2015	5 2 Vehicles/Parties Rear-End 2 Vehicles/Parties Angle 2 Vehicles/Parties Angle	Property Damage Only Fail to Yield 65+	r 25 Under Influence of Acohol No No	Under 25 Dry Dark Dry Dayli Dry Dayli	shows the street Lights /Street Lights Off Dright At ght At Street Lights On At Street	neway Related but Not at Driveway Driveway or Intersection Driveway or Intersection	Straight & Level Straight & Level Straight & Level	No Vuinerable Road User Involved Older Road User 65+ No Vuinerable Road User Involved	Under Influence of Alcohol/Drugs/Medication Fail to Yield Distractions Outside Vehicle Inattention	Daytight Daytight	Dry Driveway Related but Not at Driveway Dry At Driveway or Intersection Dry At Driveway or Intersection	Under Influence of Alcohol/Drugs/Medication Fail to Yield Distracted Driving	
1072974.8 998908.17 2015 1072961.02 998913.22 2015	3 Vehicles/Parties Angle 2 Vehicles/Parties Rear-End	Minor or Possible Injury Inattention 65+	No No No			Driveway or Intersection	Straight & Level Straight & Level Straight & Level	No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+	Inattention	Dark-Street Lights On	Dry At Driveway or Intersection Wet At Driveway or Intersection Dry At Driveway or Intersection	Inattention Inattention Inattention	
1072974.8 998908.17 2017 1072975.8 998915.31 2017 1072975.8 998915.31 2018	Z Vehicles/Parties Angle	Minor or Possible Injury Inattention 65+ Minor or Possible Injury Fail to Yield 65+ Property Damage Only Fail to Yield 55-6 Minor or Possible Injury Fail to Yield 65+	No i No No	Dry Dayli Dry Dayli Wet Dark Wet Dayli	ght Ati -Street Lights On Ati	Driveway or Intersection Driveway or Intersection Driveway or Intersection Driveway or Intersection	Straight & Level Straight & Level Straight & Level Straight & Level	Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+	Inattention Fail to Yield Fail to Yield Fail to Yield		Dry At Driveway or Intersection Dry At Driveway or Intersection Wet At Driveway or Intersection Wet At Driveway or Intersection	Fail to Yield Fail to Yield Fail to Yield	
1072975.8 998915.31 2018 1072974.85 998906.17 2018 1072975.32 996918.2 2018 1072970.12 996916.4 2019	3 2 Vehicles/Parties Angle 3 2 Vehicles/Parties Angle 9 2 Vehicles/Parties Angle	Minor or Possible Injury Fail to Yield 25:3 Property Damage Only Inattention Und Minor or Possible Injury Inattention 65+	I No r 25 No	Dry Dayli Wet Daw Dry Dayli	ght At n/Dusk At	Driveway or Intersection Driveway or Intersection Driveway or Intersection	Unknown Straight & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	Fail to Yield Inattention	Daylight Dawn/Dusk Daylight	Dry At Driveway or Intersection Wet At Driveway or Intersection Dry At Driveway or Intersection Dry At Driveway or Intersection Dry At Driveway or Intersection	Fail to Yield Inattention	
1072975.8 998915.31 2019 1072982.8 998997.85 2019	2 Vehicles/Parties Angle 2 Vehicles/Parties Rear-End	Property Damage Only Fail to Yield 65+ Property Damage Only Inattention 35-4	No No	Dry Dayli Wet Dayli	ght At No	Driveway or Intersection t at Intersection/Driveway/Roundabout	Straight & Level Straight & Level	Motorcycle and Older Road User 65+ No Vulnerable Road User Involved	Fail to Yield Inattention			Fail to Yield Inattention	
1041403.89 1000217.53 2019 1041569.03 1000204.24 2017 1041782.61 1000197.89 2016	9 2 Vehicles/Parties Turning 1 Vehicles/Parties Struck Object 2 Vehicles/Parties Rear-End		No I No r 25 No	loe/Snow/Slush Dark Dry Dark	ght At 1 -No Street Lights/Street Lights Off No -No Street Lights/Street Lights Off No	Driveway or Intersection it at Intersection/Driveway/Roundabout it at Intersection/Driveway/Roundabout	Straight & Grade Straight & Grade Straight & Grade	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Follow Too Closely Speeding Follow Too Closely	Daylight Dark-No Street Lights/Street Lights Off Ice. Dark-No Street Lights/Street Lights Off	Dry At Driveway or Intersection /Snow/Slush Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout	Follow Too Closely Speeding Follow Too Closely	
1042307.28 1000183.5 2016 1042676.92 1000172.42 2017 1042816.13 1000170.53 2019	5 1 Vehicle/Party Animal 7 1 Vehicle/Party Fire started in vehicle 8 3 Vehicles/Parties 8 Sideswipe	Property Damage Only Other/Unknown 65+ Property Damage Only Other/Unknown 55-6 Property Damage Only Operating Defective Equipment 25-3 Property Damage Only Other/Unknown 65+	No I No I No	Dry Dayli	gnt No		Straight & Grade Straight at Hillcrest Straight & Grade Straight & Level	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	None None Operating Defective Equipment Other Contributing Circ Not Listed	Daylight Daylight	Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	
1043150 89 1000158 21 2019	1 Vehicle/Party Animal 1 Vehicle/Party Struck Object	Property Damage Only Under Influence of Alcohol/Drugs/Medication 35-4	No Under Influence of Drugs and Apparently Asleep, Fatigued or III No	35-44 Dry Davi	-No Street Lights/Street Lights Off No aht No	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Straight & Level Straight & Grade Straight at Hillcrest	Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+	Other Contributing Circ Not Listed Under Influence of Alcohol/Drugs/Medication Speeding	Dark-No Street Lights/Street Lights Off Daylight	Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout /Snow/Slush Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication Speeding	
1044470.74 1000114.82 2017	1 Vehide/Party Struck Object 1 Vehide/Party Animal 1 Vehide/Party Struck Object 3 Vehide/Party Struck Object	Property Damage Only Speeding 65+ Property Damage Only OfficerUnknown 55-6 Mnor or Possible Injury Under Influence of Alcohol/Drugs/Medication Und Property Damage Only Inattention 65+	No r25 Under Influence of Acohol	Under 25 Dry Dark	n/Dusk No -No Street Lights/Street Lights Off No -No Street Lights/Street Lights Off No	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Straight & Level Straight & Level Straight at Hillcrest	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	None Under Influence of Alcohol/Drugs/Medication	Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off	Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication	
1044468.61 1000114.88 2019 1044711.53 1000107.12 2017 1045259.31 1000090.74 2018 1045681.01 1000076.79 2017	2 Vehicles/Parties Sideswipe 1 Vehicle/Party Animal 1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object	Property Damage Only Other/Unknown 354-8 Property Damage Only Speeding 25-3 Property Damage Only Speeding 25-3 Property Damage Only Speeding 25-3	No No	Dry Dayli Wet Dark Ice/Snow/Slush Dayli	No Street Lights/Street Lights Off No ght No Street Lights/Street Lights Off No	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Straight & Grade Straight & Grade Straight & Grade	No Vulnerable Road User Involved	Inattention Other Contributing Circ Not Listed Speeding	Daylight Dark-No Street Lights/Street Lights Off Daylight Ice.	Wet Not at Intersection/Driveway/Roundabout /Snow/Slush Not at Intersection/Driveway/Roundabout /Snow/Slush Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Speeding	
1046090.93 1000063.23 2014	1 Vehicle/Party Over Embankment - No Guardrail Present	Property Damage Only Inattention 55-6	No No	Vet Dayli	-No Street Lights/Street Lights Off No ght No	t at Intersection/Driveway/Roundabout	Straight & Grade Straight & Grade Straight & Grade Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved	Speeding Inattention Inattention None	Dark-No Street Lights/Street Lights Off Los.	Show Slush Not at Intersection Driveway Roundabout	Inattention Inattention	
1047323.98 999992.55 2017	2 Vehides/Parties Rear-End 1 Vehides/Party Animal 3 Vehide/Party Animal 3 1 Vehide/Party Over Embankment - No Guardrail Present 2 Vehides/Parties Angle Animal Angle Animal	Minor or Possible Injury Inattention 45-5 Property Damage Only OtherUnknown 55-6 Minor or Possible Injury OtherUnknown 65+ Suspected Serious Injury Improper Turn/Merge 55-6	i No No i No	Dry Dayi Dry Dayi Wet Dark	ght No -Street Lights On At	t at Intersection/Oriveway/Roundabout Driveway or Intersection	Straight & Grade Unknown Straight & Level	No Vuinerable Road User Involved Older Road User 65+ No Vuinerable Road User Involved	None Other Contributing Circ Not Listed Improper Turn/Merge	Daylight Dark-Street Lights On	Dry Not at Intersection/Driveway/Roundabout Wet At Driveway or Intersection	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	
1047872.21 999835.76 2017 1048422.06 999652.81 2018	3 Vehicles/Parties Rear-End 3 Z Vehicles/Parties Rear-End 1 T Vehicle/Party Animal	Property Damage Only Speeding Und Property Damage Only Follow Too Closely 25-3 Property Damage Only Other/Unknown 25-3	r 25 No 1 No 1 No	Wet Dark Wet Dayli Dry Dark	-No Street Lights/Street Lights Off No ght No -No Street Lights/Street Lights Off No	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Curve & Level Curve & Level Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Speeding Follow Too Closely None	Dark-No Street Lights/Street Lights Off Daylight Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off	Wet Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout	Speeding Follow Too Closely No Risky Driving Behaviors Involved	
1048871.36 999497.54 2016 1049068.25 999423.29 2015	5 1 Vehicle/Party Animal 5 1 Vehicle/Party Animal	Minor or Possible Injury Other/Unknown 25-3 Minor or Possible Injury Other/Unknown 45-5	No No No	Dry Dark	-No Street Lights/Street Lights Off At	intersection.univeway.coundabout intersection and Not Related	Straight & Level Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	None None Contains Defective Fauisment	Dark-No Street Lights/Street Lights Off	Dry Not at Intersection/Driveway/Roundabout Dry At Intersection and Not Related	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	##
1049071.847 999433.7234 2018 1049067.03 999433.63 2019	7 2 Vehicles/Parties Sideswipe 8 2 Vehicles/Parties Angle 9 2 Vehicles/Parties Turning	Property Damage Only Follow Too Closely 45-5	No Under Influence of Alcohol No	Dry Dayli		Driveway or Intersection	Straight & Level Straight & Grade Straight & Level	No Vuinerable Road User Involved No Vuinerable Road User Involved	Operating Defective Equipment Under Influence of Alcohol/Drugs/Medication Follow Too Closely	Daylight Dark-No Street Lights/Street Lights Off Daylight	Wet At Driveway or Intersection Dry At Driveway or Intersection Dry At Driveway or Intersection	No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication Follow Too Closely	##
	1 Vehicle/Party Acimal 1 Vehicle/Party Acimal 1 Vehicle/Party Struck Object 1 Vehicle/Party Struck Object	Property Damage Only Other/Unknown 55-6	No No No No Under Influence of Alcohol and Apparently Asleep, Fatigued or III Apparently Asleep, Fatigued or III		ght No ght No ght No N	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Straight & Level Straight & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	None None Under Influence of Alcohol/Drugs/Medication	Daylight Daylight	Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication	
1052374.49 998327.12 2015		Property Damage Only Under Influence of Alcohol/Drugs/Medication 25-3	Under Influence of Alcohol No				Straight & Level Straight & Level Straight & Level	No Vulnerable Road User Involved	Under Influence of Alcohol/Drugs/Medication None	Dark-No Street Lights/Street Lights Off	Dry Not at Intersection/Driveway/Roundabout	Apparently Asleep, Fatigued or III Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved	#=
1052873.61 998162.65 2015 1052873.61 998162.65 2018 1052873.61 998162.65 2018 1052873.61 998162.65 2019	5 1 Vehicle/Party Animal 5 1 Vehicle/Party Struck Object 1 Vehicle/Party Animal 6 1 Vehicle/Party Animal 6 1 Vehicle/Party Animal	Property Damage Only Apparently Asleep, Fatigued or III 25-3 Property Damage Only Other/Unknown 35-4	Apparently Asleep, Fatigued or III No No	25-34 Wet Dark Dry Dark Dry Dark	ght No -No Street Lights/Street Lights Off No	t at Intersection/DrivewayRoundabout t at Intersection/DrivewayRoundabout	Straight & Level Straight & Level	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Apparently Asleep or Fatigued None None	Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off	Wet Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout Wet Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout	Apparently Asleep, Fatigued or III No Risky Driving Behaviors Involved	##
1052873.61 998162.65 2019	1 \text{\te}\text{\te}\tinx}\\\ \text{\text{\text{\text{\text{\text{\text{\text{\text{\text{\text{\text{\text{\text{\texi}\text{\text{\text{\text{\te\tinte\text{\text{\text{\text{\text{\texi}\tint{\text{\texi}}\tex	Property Damage Only Speeding 65+ Property Damage Only Distracted Driving 45-5	No I No	loe/Snow/Slush Dark Dry Dark	-No Street Lights/Street Lights Off No -No Street Lights/Street Lights Off No -No Street Lights/Street Lights Off No	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Straight & Level Straight & Grade Straight & Level	No Vuinerable Road User Involved Older Road User 65+ No Vuinerable Road User Involved	None Speeding Distractions Outside Vehicle Driver Physical Condition	Dark-No Street Lights/Street Lights Off Ice. Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off	Dry Not at Intersection/Driveway/Roundabout /Snow/Slush Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Speeding Distracted Driving	
1052873.61 998162.65 2019 1053123.59 998078.86 2019 1053173.64 998062.08 2018 1053173.72 997065.02 2018			Apparently Asleep, Fatigued or III	35-44 Dry Dayli	gnt	t at intersection/Driveway/koundabout	Straight & Level	No Vulnerable Road User Involved	priver mysical condition	Dayigiit	pryor as entersection/Driveway/Roundabout	Apparently Asleep, Fatigued or III	\rightarrow
1053373.72 997995.02 2018	8 2 Vehicles/Parties Rear-End	Minor or Possible Injury Follow Too Closely 25-3	No Apparently Asleep, Fatigued or III	Dry Dayli 65+ Dry Dayli	ght No	t at Intersection/Driveway/Roundabout	Straight & Level Straight & Level	No Vulnerable Road User Involved Older Road User 65+	Follow Too Closely Apparently Asleep or Fatigued	Daylight Daylight		Follow Too Closely Apparently Asleep, Fatigued or III	\pm
1053373.72 997995.02 2018 1054659.49 997553.92 2018 1055024.18 997442.52 2019 1055376.7 997321.24 2016 1055874.72 997158.36 2015 1057274.41 996690.71 2017	1 VerbiceParty Shruck Chpect 2 VerbiceSParty Ream End 1 VerbiceSparty Shruck Object 1 VerbiceSparty Shruck Object 1 VerbiceSparty 1 VerbiceSpa	Minor or Possible Injury Follow Too Closely 25-3 Property Damage Only Apparently Asleep, Fatigued or III 65+ Minor or Possible Injury Speeding 45-5	No Apparently Asleep, Fatigued or III	Dry Dayli 65+ Dry Dayli Dry Dayli Under 25 Dry Dark	ght No ght No -No Street Lights/Street Lights Off No -No Street Lights/Street Lights Off No	t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout t at Intersection/Driveway/Roundabout	Straight & Level Straight & Grade Straight & Level Straight & Level	No Vulnerable Road User Involved Older Road User 65+ Mobroyde No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	Follow Too Closely	Daylight Daylight Daylight Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off Loc.	Dry Not at Intersection/Driveway/Roundabout Dry Not at Intersection/Driveway/Roundabout	Apparently Asleep, Fatigued or III Speeding Apparently Asleep, Fatigued or III Speeding	

Column	OBJECTID JURISDICTION	COUNTY CITY	PRIMARY BLOCK TRAFFICWAY NUMBER	INTERSECTING FROM TRAFFICWAY REP-POINT FROM FEE POINT POINT, NAME	MILEPOST A SR_ONLY HISTORY SUSPENSE_IND REPORT NUMBER	DATE TIME INJURY_TYPE	NUM NUM NUM NUM NUM INJ FAT VEH PEDS BIK	ES VEH_1_TYPE	VEH_2_TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD_SURFACE ROADWAY CHARACTERISTIC	LIGHTING CONDITION	FRST_COLLISION TYPE_OBJECT_STRUCK	VEH_1 ACTION	VEH_2 ACTION	VEH.1_COMPASS DIRECTION_FROM DIRECTI
	7739 State Route 7740 State Route	Jefferson Jefferson	101		278.18 No E739206 278.26 No E658009	2017-11-18 06:40 No Apparent Injury 2017-03-30 21:44 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb		At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Dawn V Dark-No Street Lights V	fehicle Strikes Deer fehicle Strikes Deer	Going Straight Ahead Going Straight Ahead	ALL BUILT	West East East West
	7741 State Route 7742 State Route 7743 State Route	Jefferson	101				0 0 1 0			Not at Intersection and Not Related		Dry Straight & Level	Dusk V	/ehicle Strikes Deer	Going Straight Ahead		South North South East West East North South South North
	7744 State Route 7745 State Route	Jefferson Jefferson	101		279.56 No 3630740 279.56 No 3630741	2016-07-16 14:29 No Apparent Injury 2016-07-16 14:57 Possible Injury	0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight F Daylight F	from same direction - both going straight - both moving - rear-end from same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead	Slowing	South North South North South North South North
	7746 State Route 7747 State Route	Jefferson Jefferson	101		279.57 No 3630738 279.70 No E823598			0 Passenger Car	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight F	rom same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead		South North South North South North South North
	7748 State Route 7749 State Route	Jefferson Jefferson	101		279.81 No E949457 279.82 No E941879	2019-08-11 18:56 No Apparent Injury 2019-07-19 13:41 No Apparent Injury	0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		At Intersection and Related Not at Intersection and Not Related		Dry Straight & Level Dry Straight & Level	Daylight G	Suardrail - Through, Over or Under Yehicle Strikes Deer	Making Left Turn Going Straight Ahead		North East East West
	7750 State Route 7751 State Route	Jefferson Jefferson	101		280.00 No E326145 280.01 No E858647	2014-05-04 07:24 Suspected Minor Injury 2018-11-07 12:54 No Apparent Injury	0 0 2 0	Motorcycle Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related		Dry Curve & Level Dry Curve & Level	Daylight V Daylight A	lehicle Strikes Deer Il other non-callision	Going Straight Ahead Going Straight Ahead	Going Straight Ahead	South North South North North South
	7752 State Route 7753 State Route 7754 State Route	Jefferson Jefferson	101		280.10 No E523500 280.50 No E943432	2016-03-10 08:50 No Apparent Injury 2019-07-24 14:19 Possible Injury 2016-07-06 21-17 Possible Injury	1 0 3 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy		Daylight A Daylight F	II other non-collision rom same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic	North South South South Vehicle Stop Foat Wart Fact Wart
Part	7755 State Route 7756 State Route		101		280.99 No E490023	2015-12-04 19:10 No Apparent Injury	0 0 1 0	Passenger Car Pickup.Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10.000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast	Wet Straight & Level	Dark-No Street Lights R	Roadway Ditth	Making U-Tum		North North South South South
Part	7757 State Route 7758 State Route	Jefferson Jefferson	101		281.03 No E895681 281.12 No E354856	2019-02-07 14:40 No Apparent Injury 2014-09-05 14:15 Possible Injury	0 0 1 0	0 Passenger Car		Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Wet Curve & Level Dry Straight & Level	Daylight G Daylight F	Guardrail - Face From same direction - both going straight - both moving - rear-end	Going Straight Ahead		South North South North North
	7759 State Route 7760 State Route	Jefferson Jefferson	101		281.40 No E840857	2018-09-22 15:00 No Apparent Injury	0 0 1 0	U Pickup,Panei Truck or vanette under 10,000 lb			Raining Clear or Partly Cloudy		Daylight V Daylight V	fehicle Strikes Deer fehicle overturned			South North South North
	7761 State Route 7762 State Route	Jefferson Jefferson	101		281.69 No E818481 281.82 No E687834	2018-07-14 14:59 No Apparent Injury 2017-06-24 10:55 No Apparent Injury	0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related		Dry Straight & Level	Daylight F Daylight V	rom same direction - both going straight - both moving - rear-end rehicle Strikes Deer	Going Straight Ahead Going Straight Ahead		South North South North North South
	7763 State Route 7764 State Route	Jefferson Jefferson	101		281.84 No E692024 281.91 No E874746			0 Truck Tractor & Semi-Trailer				Wet Straight & Level Wet Straight & Level	Daylight F Dark-No Street Lights J	rom same direction - both going straight - one stopped - rear-end			North South Vehicle Stopped Vehicle Stop South North
State Stat	7766 State Route	Jefferson	101		282.00 No E436212 282.00 No E450147	2015-06-22 10:30 No Apparent Injury 2015-08-07 03:12 No Apparent Injury	0 0 3 0	Pickup,Panel Truck or Vanette under 10,000 lb Truck (Flathad Van etc)	Pickup,Panel Truck or Vanette under 10,000 lb	Driveway Related but Not at Driveway Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Dry Straight & Level Dry Curve & Level	Daylight F	rom same direction - both going straight - one stopped - rear-end	Slowing	Stopped for Traffic	North South Vehicle Stopped Vehicle Stop
Part	7768 State Route 7769 State Route		101							Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level	Dark-No Street Lights B	Roadway Ditch	Going Straight Ahead		North South North South
Part	7770 State Route 7771 State Route	Jefferson Jefferson	101		282.09 No E716069 282.09 No E966710	2017-09-25 05:55 No Apparent Injury 2019-09-30 11:25 No Apparent Injury	0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Not at Intersection and Not Related		Wet Straight & Level Dry Straight & Level	Dark-No Street Lights F Daylight F	rom same direction - both going straight - both moving - rear-end rom same direction - both going straight - one stopped - sideswipe	Going Straight Ahead Going Straight Ahead	Slowing Stopped for Traffic	North South North South South North South Vehicle Stop
Part	7772 State Route 7773 State Route	Jefferson Jefferson	101		282.19 No E434041 282.20 No E429937	2015-06-15 09:50 No Apparent Injury 2015-05-29 15:43 Suspected Minor Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related At Driveway	Clear or Partly Cloudy Clear or Partly Cloudy				Going Straight Ahead Going Straight Ahead		North South North South
Part	7774 State Route 7775 State Route	Jefferson Jefferson	101		282.24 No E873387 282.26 No E973079	2018-12-08 00:40 Suspected Serious Injury 2019-10-20 19:48 No Apparent Injury	0 0 1 0	0 Motorcycle 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Overcast	Dry Curve & Level Dry Curve & Level	Dark-No Street Lights R Dark-No Street Lights D	Roadway Ditch Jomestic animal other (cat, dog, etc)	Going Straight Ahead Going Straight Ahead		North South South North
Part	7776 State Route 7777 State Route	Jefferson Jefferson	101		282.29 No E549317 282.31 No E420584	2015-05-27 15:42 Possible Injury 2015-04-24 12:50 No Apparent Injury	0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related				rom same direction - both going straight - both moving - rear-end	Going Straight Ahead		South North Vehicle Stopped Vehicle Stop South North South North County North County Vehicle County North County Vehicle County North North
Part	7779 State Route	Jefferson	101		282.31 No E963720 282.32 No E341675	2019-09-16 14:58 No Apparent Injury 2014-07-11 15:07 Suspected Minor Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car Passenger Car	At Driveway At Intersection and Related	Overcast	Wet Curve & Level	Daylight F	rom same direction - both going straight - one stopped - rear-end	Going Straight Ahead Making Left Turn	Stopped for Traffic	South North Vehicle Stopped Vehicle Stop West North North North South
State Stat	7781 State Route 7782 State Route	Jefferson Jefferson	101		282.33 No E429060 282.33 No E809507	2015-05-27 15:00 No Apparent Injury 2018-06-16 14:30 Suspected Minor Injury	0 0 1 0 2 0	Truck Tractor & Semi-Trailer Passenger Car	Passenger Car	At Driveway within Major Intersection	Clear or Partly Cloudy Clear or Partly Cloudy			netal digit Post	Making Right Turn Making Left Turn		West South Southwest Northwest North South
The content of the	7783 State Route 7784 State Route	Jefferson Jefferson	101		282.33 No E826955 282.33 No E972522	2018-08-10 17:20 Unknown 2019-10-19 12:35 Suspected Minor Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb			Clear or Partly Cloudy Raining	Dry Straight & Level Wet Straight & Level	Daylight L	Itility Pole	Making Left Turn		North South South West
The content of the	7785 State Route 7786 State Route	Jefferson Jefferson	101		282.33 No E973693 282.35 No E898848	2019-10-17 15:40 Possible Injury 2019-02-28 03:02 Suspected Minor Injury	3 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Dry Ice Straight & Level	Daylight F Dark-No Street Lights R	rom same direction - both going straight - one stopped - rear-end Roadway Ditch	Going Straight Ahead Going Straight Ahead	Stopped for Traffic	South North Vehicle Stopped Vehicle Stop South North
The content of the	7788 State Route 7789 State Route	Jefferson	101		282.40 No E444189	2015-07-13 06:19 Suspected Serious Injury 2015-07-13 06:19 Suspected Serious Injury	7 3 0 3 0	0 Passenger Car	Passenger Car Pickup Panel Teach as Manager	Not at Intersection and Not Related	Raining	Dry Curve & Grade Wet Curve & Level	Daylight A	rom opposite direction - both moving - head-on	Going Straight Anead	Going Straight Ahead	South North North South
The content of the	7790 State Route 7791 State Route	Jefferson Jefferson	101		282.56 No E378435 282.56 No E422004	2014-11-26 13:10 Possible Injury 2015-06-07 12:35 No Apparent Injury	1 0 3 0	Passenger Car Pickup Panel Truck or Vanette under 10,000 lb	Passenger Car Pickup Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Related	Overcast Overcast Clear or Partiv Clearly	Dry Curve & Level Dry Curve & Level Dry Curve & Crade	Daylight F	rom opposite direction - one left turn - one straight - from same direction - one left turn - one straight - from same direction - both going straight - hoth movion - trace-end			North
The content of the	7792 State Route 7793 State Route	Jefferson Jefferson	101		282.56 No E548994 282.56 No FR90264	2016-05-27 13:54 No Apparent Injury 2019-01-29 15:32 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Driveway within Major Intersection		Wet Straight & Level Dry Curve & Grade	Daylight E	ntering at angle	Making Left Turn Starting in Traffic Lane	Going Straight Ahead	North East South North East West South North
	7794 State Route 7795 State Route	Jefferson Jefferson	101		282.60 No E488699 282.64 No E563018	2015-11-18 06:40 Possible Injury 2016-07-11 07:42 No Apparent Injury	1 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Intersection Related but Not at Intersection	Overcast n Overcast	Wet Curve & Grade Unve & Grade	Dark-Street Lights On C Daylight G	Over Embankment - No Guardrail Present Guardrail - Face	Going Straight Ahead Making Right Turn	and a second second	North South South South
	7796 State Route 7797 State Route	Jefferson Jefferson	101		282.64 No E574511 282.75 No E702265	2016-08-14 21:29 Possible Injury 2017-08-08 14:20 No Apparent Injury	1 0 2 0	0 Motorcycle 0 Passenger Car	Passenger Car	At Intersection and Related At Intersection and Not Related		Dry Straight & Grade Dry Curve & Grade	Dark-No Street Lights E Daylight V	ntering at angle fehicle Strikes Deer	Going Straight Ahead Going Straight Ahead		East West South Vehicle Bac South North
	7798 State Route 7799 State Route	Jefferson Jefferson	101		282.76 No E504043 282.80 No E680589	2015-12-24 14:48 Possible Injury 2017-06-02 12:39 Suspected Minor Injury	1 0 2 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related Not at Intersection and Not Related		Wet Curve & Level Dry Straight & Level	Daylight F	rom same direction - both going straight - one stopped - rear-end koadway Ditch	Going Straight Ahead Going Straight Ahead	Stopped in Roadway	North South Vehicle Stopped Vehicle Stop South North Vehicle Stopped Vehicle Stop North North North Vehicle Stopped Vehicle Stop North No
	7801 State Route 7801 State Route 7802 State Pourte		101		282.90 No E582887	2018-09-08 20:10 No Apparent Injury 2018-09-08 20:10 No Apparent Injury 2018-09-09 20:22 No Apparent Injury	0 0 1 0	0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related		Dry Curve & Level Dry Curve & Level Dry Strainba 9 1 mm.	Dark-No Street Lights V	nesa arga rutst fehicle Strikes Deer fehicle Strikes Deer	Going Straight Ahead		North South
	7803 State Route 7804 State Route	Jefferson Jefferson	101		282.91 No E943337 283.35 No F499077	2019-07-22 08:27 No Apparent Injury 2015-12-20 05:35 Suspected Minor Injury	0 0 2 0	0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Driveway Not at Intersection and Not Related	Clear or Partly Cloudy Raining	Dry Curve & Level Wet Curve & Grade	Daylight F Dark-No Street Lights F	from same direction - both going straight - one stopped - rear-end loadway Ditch	Stopped for Traffic Going Straight Ahead	Going Straight Ahead	South North South North South North
	7805 State Route 7806 State Route	Jefferson	101		283.59 No E811933 283.93 No E601515	2018-06-24 15:40 No Apparent Injury 2016-10-19 22:00 No Apparent Injury	0 0 1 0	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Passenger Car		At Intersection and Not Related		Dry Straight & Level	Daylight G	Suardrail - Face	Going Straight Ahead		North South North South
	7807 State Route 7808 State Route	Jefferson Jefferson	101		283.93 No E800698 283.93 No E869730	2018-05-22 08:23 Suspected Minor Injury 2018-12-02 19:56 No Apparent Injury	1 0 2 0 0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related	Clear or Partly Cloudy Overcast	Dry Curve & Level Dry Straight & Level	Daylight F Dark-No Street Lights R	rom same direction - both going straight - one stopped - rear-end toadway Ditch	Going Straight Ahead Going Straight Ahead	Stopped in Roadway	South North South Vehicle Stop North South
	7809 State Route 7810 State Route	Jefferson Jefferson	101		283.95 No 3120483 284.25 No E969964	2014-07-01 09:45 Suspected Serious Injury 2019-10-12 18:46 No Apparent Injury	0 0 1 0			Not at Intersection and Not Related		Dry Curve & Level	Daylight F	rom opposite direction - both moving - head-on	Going Straight Ahead		Northwest Southeast Southeast Northwest South North
	7811 State Route 7812 State Route	Jefferson Jefferson	101		284.38 No E957868 284.40 No E997986	2019-09-06 06:51 No Apparent Injury 2019-12-23 19:26 No Apparent Injury	0 0 1 0	0 Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy Fog or Smog or Smoke		Dawn V Dark-No Street Lights R	Pehicle Strikes Deer Roadway Ditch	Going Straight Ahead Going Straight Ahead		North South South North
Part	7813 State Route 7814 State Route	Jefferson Jefferson	101		284.43 No E736788 284.50 No E488702	2017-11-11 16:54 Possible Injury 2015-11-21 08:53 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at intersection and Not Keraled	Clear or Partly Cloudy	Dry Curve & Level	Daylight T		Going Straight Ahead	Going Straight Ahead	South South North South North South North South
The content of the	7816 State Route	Jefferson	101		284.79 No E817257	2018-07-07 09:52 No Apparent Injury 2018-07-07 09:52 No Apparent Injury 2019-04-08 12:39 Surported Moor Injury	0 0 2 0	School Bus Dickup Panel Truck or Vanette under 10 000 lb.	Pickup,Panel Truck or Vanette under 10,000 lb		Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight F		Making U-Turn	Going Straight Ahead	South West South North
	7818 State Route 7819 State Route	Jefferson Jefferson	101		285.36 No E797884 285.49 No E892046	2018-05-11 13:19 Suspected Minor Injury 2018-05-11 13:19 Suspected Minor Injury 2019-02-08 11:40 Suspected Minor Injury	2 0 1 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb	Pickup, Parier Huck of Varietie diluer 10,000 ib	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Snowing	Dry Curve & Level Snow/Slush Curve & Level	Daylight R Daylight N		Going Straight Ahead Going Straight Ahead	Going Straight Arlead	North South South North
	7820 State Route 7821 State Route	Jefferson Jefferson	101		285.89 No E455590 285.95 No E434946	2015-08-24 18:49 No Apparent Injury 2015-06-14 15:50 Possible Injury	0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Level	Daylight R Daylight F	Roadway Ditch	Going Straight Ahead Making U-Turn	Going Straight Ahead	South North North North South
	7822 State Route 7823 State Route	Jefferson Jefferson	101		285.98 No E652831 286.00 No E546202	2017-03-18 16:50 No Apparent Injury 2016-05-18 05:12 No Apparent Injury	0 0 2 0 0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Related Not at Intersection and Not Related	Overcast	Wet Curve & Level Dry Straight & Level	Daylight E Dawn R		Making Left Turn Going Straight Ahead		West North North South South North
		Jefferson Jefferson	101		286.24 No E544690 287.10 No E579499	2016-05-16 15:30 No Apparent Injury 2016-08-22 06:15 No Apparent Injury	0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Dry Curve & Level	Dawn A	W other non-collision	Going Straight Ahead		North South South North
	7825 State Route 7827 State Route 7828 State Route	Jefferson Jefferson	101		287.13 No E485183 287.42 No E461962	2015-11-18 04:43 Possible Injury 2015-09-14 19:55 No Apparent Injury 2019-07-13 11-14 No Apparent Injury	0 0 1 0	Prokup,Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related	Raining	Wet Curve & Level Dry Straight & Level	Dark-No Street Lights R	earth Bank or Ledge Roadway Ditch Tire started in webide			South North South North South North
	7829 State Route 7830 State Route	Jefferson Jefferson	101		287.62 No E429058 287.74 No E298838	2015-05-30 14:10 Possible Injury 2014-01-06 12:23 Suspected Minor Injury	1 0 1 0	0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight R	Roadway Ditch	Going Straight Ahead Going Straight Ahead		South North North South
Column	7831 State Route 7832 State Route	Jefferson Jefferson	101		287.81 No E680198 287.83 No E594787	2017-06-08 05:34 No Apparent Injury 2016-10-11 19:35 No Apparent Injury	0 0 1 0	Passenger Car Truck Tractor & Semi-Trailer		Not at Intersection and Not Related At Intersection and Related		Wet Straight & Level Dry Straight & Level	Dawn R Dark-No Street Lights R	Roadway Ditch Roadway Ditch	Going Straight Ahead Making Right Turn		East West North Northwest
Property Property	7833 State Route 7834 State Route	Jefferson Jefferson	101		287.96 No E727029 287.93 No E323209	2017-10-15 12:11 Possible Injury 2014-04-20 07:50 No Apparent Injury	2 0 1 0 0 0 1 0	1 Motorcycle 0 Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight F	Pehicle - Pedalcyclist	Going Straight Ahead Going Straight Ahead		South North North South
Property Property	7835 State Route 7836 State Route	Jefferson Jefferson	101		288.30 No E773840 288.42 No E855036	2018-02-24 09:20 Suspected Minor Injury 2018-10-18 10:00 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Truck & Trailer	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Snowing Clear or Partly Cloudy	Snow/Slush Straight & Level Dry Curve & Level	Daylight C Daylight R	over Embankment - No Guardrail Present Roadway Ditch	Going Straight Ahead Going Straight Ahead	Going Straight Ahead	North South South North North
	7838 State Route 7839 State Route	Jefferson	101		288.94 No E661411 288.96 No E302963	2017-04-07 12:40 No Apparent Injury 2017-04-07 12:40 No Apparent Injury 2014-01-21 13:30 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10 000 lb		Raining Clear or Partly Cloudy	Wet Straight & Level	Daylight	ence		Making Left Turn	North South South West
	7840 State Route 7841 State Route	Jefferson Jefferson	101		288.99 No E912755 289.02 No E708358	2019-04-16 16:03 No Apparent Injury 2017-08-24 15:05 No Apparent Injury	0 0 1 0	O Truck Tractor & Semi-Trailer O Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Overcast	Dry Straight & Level	Daylight R	rire started in vehicle Roadway Ditch	Going Straight Ahead Going Straight Ahead		South North North South
Column C	7842 State Route 7843 State Route	Jefferson Jefferson	101							Not at Intersection and Not Related		Dry Straight & Level Dry Straight & Level	Daylight A Dark-No Street Lights R	Roadway Ditch	Going Straight Ahead Going Straight Ahead		North South North South
Column C	7844 State Route 7845 State Route	Jefferson Jefferson	101		289.26 No E672033 289.28 No E583683	2017-05-04 23:00 Unknown 2016-09-08 10:55 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Driveway Related but Not at Driveway	Unknown Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Level	Daylight R	toatoway Dittil			South North Vehicle Backing Vehicle Backing
	7847 State Route	Jefferson	101		289.35 No E832824 289.50 No E832824	2018-08-29 05:13 No Apparent Injury	0 0 1 0	Passenger Car		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Curve & Level	Daylight R	Roadway Ditch	Going Straight Ahead		South North North South
	7849 State Route 7850 State Route	Jefferson Jefferson	101		289.80 No E920710 289.80 No E920711	2019-04-28 17:40 No Apparent Injury 2019-04-28 17:40 Suspected Serious Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10.000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight R Daylight F	rom same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped in Roadway	South North Vehicle Stooped Vehicle Sto
	7851 State Route 7852 State Route	Jefferson Jefferson	101		289.84 No E930571 290.04 No E974781	2019-06-14 21:45 No Apparent Injury 2019-10-23 22:04 No Apparent Injury	0 0 1 0	0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related At Intersection and Not Related	Fog or Smog or Smoke	Dry Straight & Level Dry Curve & Grade	Dark-No Street Lights V Dark-No Street Lights G	fehicle Strikes Deer Suardrail - Through, Over or Under	Going Straight Ahead Going Straight Ahead		South North North South
	7853 State Route 7854 State Route	Jefferson Jefferson	101		290.40 No E353128 290.49 No E706658	2014-08-29 07:49 No Apparent Injury 2017-08-15 10:50 Suspected Serious Injury	0 0 1 0	Passenger Car Truck Tractor & Semi-Trailer	Passenger Car	Not at Intersection and Not Related At Driveway	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight F	Omestic animal other (cat, dog, etc) From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead	Slowing	North South North South North South
	7855 State Route 7856 State Route 7857 State Route	Jefferson	101		290.70 No E562728 290.75 No E836717	2018-09-06 07:32 Suspected Minor Injury 2018-09-06 07:32 Suspected Minor Injury	0 0 1 0	U Passenger Car 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Overcast	Dry Straight & Level Dry Straight & Level	Dawn B	Roadway Ditch	Going Straight Ahead		South North South North Couth North
	7858 State Route 7859 State Route		101		291.00 No E430566 291.10 No E850454	2015-06-04 07:40 No Apparent Injury 2017-03-04 17:35 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related	Overcast Overcast	Dry Curve & Level Dry Straight & Level	Daylight T	ree or Stump (stationary)	Going Straight Ahead Going Straight Ahead		North South North South
The column Column	7860 State Route 7861 State Route	Jefferson Jefferson	101		291.52 No E422040 291.69 No E856924	2015-04-27 13:48 Suspected Minor Injury 2018-11-04 12:23 No Apparent Injury	3 0 2 0	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Passenger Car	Truck Tractor & Semi-Trailer	At Driveway Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Grade	Daylight E	ntering at angle fehicle Strikes Deer	Making Left Turn Going Straight Ahead	Going Straight Ahead	West North North South
The column Column	7862 State Route 7863 State Route	Jefferson Jefferson	101		292.02 No E731136 292.34 No E378000	2017-11-04 09:05 Possible Injury 2014-11-23 19:07 No Apparent Injury	1 0 1 0	0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Dark-No Street Lights G	Suardrail - Face			South North North South
The column Column	7864 State Route 7865 State Route	Jefferson Jefferson	101		292.69 No E701265 292.76 No E347059	2017-08-11 10:45 Suspected Serious Injury 2014-07-30 18:51 Suspected Minor Injury	1 0 3 0	0 Motorcycle 0 Motorcycle	Motorcycle	At Intersection and Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level	Daylight F	rom same direction - both going straight - one stopped - sideswipe	Going Straight Ahead Going Straight Ahead	Stopped for Traffic	South North South North North South
The column Column	7867 State Route 7868 State Route	Jefferson	101		292.97 No E761217	2018-01-22 07:50 No Apparent Injury 2018-01-22 07:50 No Apparent Injury 2014-07-03 16:19 No Apparent Injury	0 0 1 0	Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related		Ice Straight & Level	Dawn V		Going Straight Anead		South North North South
The column Column	7869 State Route 7870 State Route	Jefferson Jefferson	101		293.20 No E840860 293.75 No E840220	2018-09-14 07:55 No Apparent Injury 2018-09-14 07:55 No Apparent Injury 2018-02-25 15:25 No Apparent Injury	0 0 3 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Passenger Car Passenger Car	At Intersection and Not Related		Wet Straight & Level Dry Cups & Grade	Daylight F	From same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead	Stopped for Traffic	North South Vehicle Stopped Vehicle Stop South North Ushida Stopped Ushida Stopped
The column Column	7871 State Route 7872 State Route	Jefferson Jefferson	101		293.81 No E396796 293.81 No E943335	2015-01-25 18:21 No Apparent Injury 2019-07-21 19:14 No Apparent Injury	0 0 1 0	1 Passenger Car 0 Passenger Car		At Intersection and Not Related	Overcast Clear or Partly Cloudy				Going Straight Ahead		
Fig. Sept. Sept.	7873 State Route 7874 State Route	Jefferson Jefferson	101		293.89 No E428512 293.90 No E338795	2015-05-26 16:00 Suspected Minor Injury 2014-06-25 20:48 No Apparent Injury	1 0 1 0	0 Passenger Car 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Level Dry Straight & Level	Daylight U Daylight V	Itility Pole fehicle Strikes Deer	Going Straight Ahead Going Straight Ahead		North South North South
18 18 18 18 18 18 18 18	7875 State Route 7876 State Route	Jefferson Jefferson	101		293.98 No E571213 293.99 No E361397	2016-07-30 16:00 No Apparent Injury 2014-09-28 09:51 No Apparent Injury	0 0 2 0	0 Passenger Car 0 Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Intersection Related but Not at Intersection At Intersection and Related	n Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight F Daylight F	rom same direction - both going straight - one stopped - rear-end rom same direction - one right turn - one straight	Going Straight Ahead Overtaking and Passing	Stopped for Traffic Making Left Turn	South North South West
18 18 18 18 18 18 18 18	7877 State Route 7878 State Route	Jefferson	101		294.40 No E736790	2016-06-21 10:54 No Apparent Injury 2017-11-15 23:10 No Apparent Injury	0 0 2 0	Prickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	rruck Tractor & Semi-Trailer	At Intersection and Not Related	Overcast	Ury Straight & Level Wet Curve & Level	Dark-Street Lights On T	rom same direction - one left turn - one straight ree or Stump (stationary)	Going Straight Ahead	Overtaking and Passing	reorth South North South North South
No. Control Control	7880 State Route 7881 State Route	Jefferson	101					o i assenger oar		At Intersection and Related	Raining	Wet Straight & Level	Dark-Street Lights On N	Metal Sign Post	Going Straight Ahead	Quine Diminks (*)	North South
Part	7882 State Route 7883 State Route	Jefferson Jefferson	101		294.59 No E856808 294.70 No E856808	2018-10-28 23:54 Dead at Scene 2018-03-12 16:50 No Apparent Injury	1 1 2 0		Dickup Panel Truck or Vanette under 10 000 lb		Clear or Partly Cloudy Clear or Partly Cloudy	Wet Curve & Level Dry Straight & Level	Dark-Street Lights On B	Building	Going Straight Ahead Merging (Entering Traffic)		North South South North North
Part	7884 State Route 7885 State Route	Jefferson	101		294.74 No E908751 294.99 No E672320	2019-03-24 03:30 No Apparent Injury 2017-04-19 12:30 Possible Injury	0 0 1 0	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Passenger Car	Passenger Car	Not at Intersection and Not Related	Clear or Partly Cloudy Overcast	Dry Curve & Level Wet Straight & Level	Dark-Street Lights On R Daylight F	Coadway Ditch from same direction - both going straight - both moving - rear-end	Going Straight Ahead Slowing		North South South North South North North
Part	7886 State Route 7887 State Route	Jefferson Jefferson	101		295.00 No E793647 295.02 No E414169	2018-04-30 06:19 No Apparent Injury 2015-04-05 09:54 No Apparent Injury	0 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	At Intersection and Related	Raining Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Grade	Daylight R Daylight F	Roadway Ditch from same direction - both going straight - one stopped - rear-end	Going Straight Ahead		South North
Part Dear Count Jefferson 1015 244495	7888 State Route 7889 State Route	Jefferson Jefferson	101S128463 101S128463		0.17 No E410835 0.19 No E799816	2015-03-24 21:28 No Apparent Injury 2018-05-18 03:30 Suspected Minor Injury	0 0 1 0	Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Wet Curve & Grade Dry Curve & Grade	Dark-Street Lights Off R	Roadway Ditch Roadway Ditch	Going Straight Ahead		East West East West
Part Dear Count Jefferson 1015 244495	7890 State Route 7891 State Route	Jefferson	1015128463 1015128463		0.21 No E517715 0.31 No E432063	2016-02-15 04:26 Suspected Serious Injury 2015-06-07 04:05 Suspected Minor Injury	2 0 1 0	u Prickup, Panel Truck or Vanette under 10,000 lb O Pickup, Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related	Clear or Partly Cloudy	ovet Curve in Sag Dry Curve & Grade	Dark-Street Lights On S	oweet Light Hole or Base Metal Sign Post	Going Straight Ahead Merging (Entering Traffic)		East West East North
Part	7893 State Route 7894 State Route		101S128495 101S128496		0.08 No E415053 0.09 No E426707	2015-04-03 07:17 No Apparent Injury 2015-06-18 23:38 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Curve & Level	Dark-Street Lights On R	Roadway Disch	Going Straight Ahead		West North
Part Control Part Part	7895 State Route 7896 State Route	Jefferson Jefferson	104 104		0.22 No E868367 0.31 No E580497	2018-12-04 10:05 Possible Injury 2016-09-01 16:38 No Apparent Injury	1 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Not Stated	Passenger Car Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	loe Curve & Grade Dry Curve & Grade	Daylight F	rom same direction - all others rom same direction - both going straight - both moving - sideswipe		Going Straight Ahead Going Straight Ahead	West East West East Northwest South Northwest Southeast
PMS Description Descript	7897 State Route 7898 State Route	Jefferson Jefferson	104		0.32 No E441295 0.36 No E598215	2015-06-29 03:45 No Apparent Injury 2016-10-12 10:26 No Apparent Injury	0 0 1 0	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Grade Dry Curve & Grade	Dark-No Street Lights F Daylight V	ire started in vehicle Vood Sign Post	Legally Parked, Unoccupied Going Straight Ahead		East West
PMR State Poor Information 194 195 194 1	7899 State Route 7900 State Route	Jefferson Jefferson	104 104		0.37 No 3120480 0.37 No E923367	2014-06-01 18:15 No Apparent Injury 2019-05-17 19:50 Suspected Minor Injury	0 0 1 0	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Pickup,Panel Truck or Vanette under 10,000 lb		At Intersection and Related	Clear or Partly Cloudy Clear	Dry Straight & Grade Dry Curve & Grade	Daylight V Daylight N	fehicle Strikes Deer Metal Sign Post	Going Straight Ahead Going Straight Ahead		West East East
Part	7901 State Route 7902 State Route	Jefferson Jefferson	104		0.37 No E973077 0.42 No E472949	2019-10-19 21:06 No Apparent Injury 2015-10-13 12:17 No Apparent Injury	0 0 1 0	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Truck Tractor & Semi-Trailer		Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Dry Curve & Grade	Dark-No Street Lights N	Metal Sign Post	Going Straight Ahead	Ty .	East West East West
Page Search Page Search Page Search Page Search Page Search Page Page	7903 State Route 7904 State Route	Jefferson	104		0.46 No E690590	2014-11-29 10:15 Possible Injury 2017-07-09 17:05 No Apparent Injury	2 0 1 0	U Prickup, Panel Truck or Vanette under 10,000 lb Motorcycle	Danasas Co.	Not at Intersection and Not Related	Overcast	Dry Curve & Grade	Daylight E	arm sank or Ledge fehicle Strikes Deer	Going Straight Ahead Going Straight Ahead	Da	west East East West
Part	7905 State Route 7906 State Route 7907 State Poute	Jefferson	104		0.50 No E882815	2019-01-14 07:15 Suspected Serious Injury 2019-01-14 07:15 Suspected Minor Injury 2014-11-29 15:45 No Apparant Injury	2 0 1 0	Passenger Car Pickup Panel Truck or Vanette under 10,000 lb	rassenger var		Clear or Parity Cloudy Clear or Parity Cloudy Clear or Parity Cloudy	loe Curve & Grade	Daylight F Daylight n	roun same uneuton - both going straight - both moving - rear-end koadway Ditch koadway Ditch	Going Straight Ahead Going Straight Ahead	Uner:	West East West West East West East
	7908 State Route 7909 State Route	Jefferson Jefferson	104		0.75 No E886392 0.75 No E9000E0	2019-01-14 08:05 Suspected Minor Injury 2019-03-08 20:30 No Apparent Injury	1 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Truck (Flatbad, Van, etc)	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Curve & Grade	Dark-No Street Lights V	lehicle Strikes Deer	Going Straight Ahead Going Straight Ahead	Legally Parked, Occupied	West East East West
	7910 State Route 7911 State Route	Jefferson Jefferson	104 104		0.77 No E380263 0.82 No E856923	2014-11-30 19:25 No Apparent Injury 2018-10-26 08:32 No Apparent Injury	0 0 1 0	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	lce Curve & Grade Wet Curve & Grade	Dark-No Street Lights R Daylight R	Roadway Ditch Roadway Ditch	Going Straight Ahead Going Straight Ahead		West East West East
	7912 State Route 7913 State Route	Jefferson Jefferson	104 104		0.91 No E424375 1.00 No E473202	2015-05-09 13:00 No Apparent Injury 2015-10-14 21:21 No Apparent Injury	0 0 2 0	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Passenger Car	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Overcast				Making U-Turn Going Straight Ahead		West West East East West East
1 1 1 1 1 1 1 1 1 1	7914 State Route 7915 State Route	Jefferson Jefferson	104		1.00 No E813013 1.01 No 2986219	2015-06-23 21:20 Possible Injury 2015-10-20 06:30 No Apparent Injury	2 0 2 0	U Passenger Car 0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Metorsula	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast	Ury Straight & Grade Wet Straight & Grade	Dark-No Street Lights V	rom same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead		East West East West West East West
	7917 State Route	Jefferson	104		1.25 No E937305	2019-06-25 17:30 No Apparent Injury 2017-10-15 08-16 No Apparent Injury	0 0 2 0	0 Other	Passenger Car Pickup Panel Truck or Vanette and 40 coo =	Not at Intersection and Not Related At Driveway	Clear or Parity Cloudy	Dry Straight & Grade	Dark-Street Lights On C	one parked-one moving		Legally Parked, Unoccupied	West East West East

OBJECTID MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_1_UNIT_1	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_2_UNIT_1	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_3_UNIT_1	MV_DRIVER MV_D AGE GEN UNIT_1 UNI	DRIVER NDER NT_1 MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_1_UNIT_2	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_2_UNIT_2	MV_DRIVER_CONTRIBUTING MV_DRIVER GENER UNT_2 GENER UNT_2 GENER UNT_2 GENER UNT_2 GENER UNT_2 GENER UNT_2 GRICLIMSTANCE_1_UNT_1	BICYCLIST_CONTRIBUTING CIRCUMSTANCE 2_UNIT_1 BICYCLIST_CONTRIBUTING CIRCUMSTANCE_3_UNIT_1	BICYCLIST AGE GENDER UNIT_1 BICYCLIST_CONTRIBUTING CRCUMSTANCE_1_UNIT_2		BICYCLIST.CONTRIBUTING GRCUMSTANCE,3_UNIT,2 BICYCLIST AGE_UNIT,2 GENDER_UNIT,2	PEDESTRIAN, CONTRIBUTING CIRCUMSTANCE, 1_UNIT.2 PEDESTRIAN, CONTRIBUTING CIRCUMSTANCE, 1_UNIT.2 CIRCUMSTANCE, 2_UNIT.2 CIRCUMSTANCE, 3_UNIT.2		
7739 None 7740 None 7741 Exceding Reas. Safe Speed 7742 None			66 Male 64 Male 66 Male	None		58 Female						L	Lane 1 hremasing Mlepost Lane 1 Decreasing Mlepost Lane 1 Decreasing Mlepost Lane 1 Decreasing Mlepost
1741 Exceeding Ress. Sale Speed 1743 None 1743 Instension 1744 Exceeding Ress. Sale Speed 1745 Exceeding Ress. Sale Speed 1746 Exceeding Ress. Sale Speed 1747 Follow Too Closely 1747 Follow Too Closely	Exceeding Reas. Safe Speed Inattention		60 Female 43 Female 70 Female	ile None		63 Male 23 Female 62 Male							Lane 1 Decreasing Milepost
7746 Exceeding Reas, Safe Speed 7747 Follow Too Closely	Inattention		37 Female 34 Male	None None		23 Female 52 Mate 21 Female 66 Female							Lane 1 Decreasing Miepost
7748 Exceeding Reas. Safe Speed 7749 None 7750 None			18 Male 25 Male 41 Male									E L	Right Shoulder Decreasing Miepost Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost
7750 None 7751 Other Contributing Circ Not Listed 7752 None 7753 Follow Too Closely 7754 Other Contributing Circ Not Listed 7755 Over Contributing Circ Not Listed 7755 Over Contributing Circ Not Listed 7755 Over Contrib			53 Male 66 Female	None		57 Female 29 Maie							Lane 1 Increasing Miepost Lane 1 Increasing Miepost
7754 Other Contributing Circ Not Listed 7755 Other Contributing Circ Not Listed	Did Not Grant RW to Vehicle		69 Male 59 Male	None Foliow Too Closely None None None		24 Female						I IF	Lane 1 Decreasing Miepost Lane 1 Increasing Miepost Past Right Shoulder increasing Miepost
7756 Over Center Line 7757 Apparently Asleep or Fatigued 7758 Driver Interacting with Passengers, Anim	Exceeding Reas. Safe Speed Follow Too Closely		32 Male 29 Female 28 Male	None None		71 Male 53 Male						F	Lane 1 Increasing Miepost Right Shoulder Increasing Miepost Lane 1 Decreasing Miepost
7759 None 7760 Under Influence of Alcohol 7761 Follow Top Closely			71 Male 48 Male 21 Female	le None		29 Maie							Lane 1 Decreasing Miepost Lane 1 Increasing Miepost Lane 1 Increasing Miepost Lane 1 Increasing Miepost
7759 None 7760 Under Influence of Alcohol 7761 Follow Too Closely 7762 None 7763 Exceeding Reas. Safe Speed	Distractions Outside Vehicle		82 Male	None		61 Male							
7764 Follow Too Closely 7765 Under Influence of Alcohol 7766 Follow Too Closely	Other Contributing Circ Not Listed		41 Male 55 Female 34 Female	ile None		26 Male 63 Male						L L	Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost
7767 Driver Not Distracted 7768 Apparently As leep or Fatigued	Apparently III		25 Male 75 Male										Past Right Shoulder Increasing Milepost
7770 Exceeding Reas, Safe Speed 7771 Exceeding Reas, Safe Speed			38 Male 71 Male	None None		30 Male 42 Female						F	Past Right Shoulder Increasing Milepost Right Shoulder Increasing Milepost Lane 1 Decreasing Milepost
7772 Apparently Asleep or Fatiqued 7773 Inattention 7774 Under Influence of Alcohol	Exceeding Reas. Safe Speed		73 Male 84 Male 42 Male 31 Male										Past Right Shoulder Increasing Milepost Past Right Shoulder Increasing Milepost Past Right Shoulder Increasing Milepost Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost
7775 None 7776 Follow Too Closely	¥		31 Male 61 Female	ale None None		41 Male						L	Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost Lane 1 Increasing Miepost
7778 Inattention 7779 Follow Too Closely	Unknown Distraction	Follow Too Closely	75 Male 26 Female	None None		33 Female 26 Male 60 Female						L	Lane 1 increasing Milepost Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost
7775 None 7776 Foliow Too Closely 7777 Exceeding Reas. Sale Speed 7777 Exceeding Reas. Sale Speed 7777 Store Sale Speed 7779 Foliow Too Closely 7780 Did Not Gant Rif Vo Vehicle 7780 In proper TurnMerge 7782 Did Not Grant Rif Vo Vehicle 7781 None Too Combing Control 7783 Clore Combing Control 7783 Clore Combing Control 7784 Exceeding Reas. Sale Speed 7785 Clore Violo Co Cottoby			68 Female 51 Male 16 Male 59 Male	ile None		71 Male 36 Male							Lane 1 Increasing Milepost Part Right Shoulder Increasing Milepost
7783 Other Contributing Circ Not Listed 7784 Exceeding Reas. Safe Speed			21 Female	ile									- tax region conduced wenoustang wempoor Lane 1 Increasing Milepost Past Right Shoulder Increasing Milepost Past Right Shoulder Increasing Milepost
7787 Improper Turn/Merge	Inattention		67 Male 29 Female 63 Female	None		37 Male						E F	Lane 1 Decreasing Mlepost Past Right Shoulder Decreasing Mlepost Right Shoulder Increasing Mlepost
	Apparently Asleep or Fatigued		19 Male 80 Female	None None None		45 Female 31 Male 60 Male							Lane 1 increasing Miepost Lane 1 hicreasing Miepost Lane 1 hicreasing Miepost
7789	Over Center Line		80 Female 69 Male 47 Male 40 Male	None		60 Male 24 Female 54 Male							Lane 1 Increasing Miepost Lane 1 Decreasing Miepost
7793 Inattention 7794 Exceeding Reas. Safe Speed 7795 None	Did Not Grant RW to Vehicle					60 Male							Left Turn Lane horeasing Mispost Past Right Shoulder horeasing Mispost Decreasing Other Location Lane 1 Decreasing Mispost
7793 Inattention 7794 Exceeding Reas. Safe Speed 7795 None 7796 Under Influence of Alcohol 7797 None 7798 None			59 Femali 38 Femali 25 Male 26 Male 36 Male 57 Male 28 Male	Improper Backing		32 Female							
7799 Apparently Asleep or Fatigued 7800 Distractions Outside Vehicle			36 Male 57 Male 28 Male	None		56 Male						E F	Lane 1 Increasing Miepost Past Right Shoulder Decreasing Miepost Past Right Shoulder Decreasing Miepost Past Right Shoulder Decreasing Miepost
7801 None 7802 None 7803 None 7804 Under Influence of Alcohol	-		60 Female	ile		E3 Bhia						L	Lane 1 Increasing Miepost Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost Past Right Shoulder Decreasing Miepost
7804 Under Influence of Alcohol 7805 Other Driver Distractions Inside Vehicle	Apparently Asleep or Fatigued Follow Too Closely		67 Male 18 Female 68 Male			0.3 Matric							
7904 Other Insuerice to Proceedings of the Private Distractions inside Vehicle 7806 Other Contributing Circ Not Listed 19806 Inattention 19806 Unknown Distraction	Inattention Distractions Outside Vehicle	Follow Too Closely	60 Male 31 Male 28 Female	None		37 Female							Past Right Shoulder Increasing Milepost Lane 1 Decreasing Milepost Past Right Shoulder Increasing Milepost
7809 Operating Defective Equipment 7810 None	Under Influence of Drugs	Over Center Line	27 Male	None		58 Male							
7809			35 Male 25 Male 24 Female 55 Male			46 Female							Lane 1 Demeasing Miepost Lane 1 Demeasing Miepost Past Right Shoulder Decreasing Miepost Lane 1 homeasing Miepost
7814 Inattention 7815 Apparently III	Apparently Fatigued		72 Female 32 Male 61 Male	ile								F	Right Shoulder hicrassing Milepost Past Right Shoulder Decreasing Milepost Lane 1 Decreasing Milepost
7817 Improper Lutri/Merge 7817 Improper U-Turn 7818 Inattention	Driver Interacting with Passengers, Anim		18 Male 24 Male 55 Male	None		38 Male 65 Male						L L	_ane 1 Decreasing Miepost _ane 1 Decreasing Miepost Past Right Shoulder Increasing Milepost
7817 Improper U-Turn 7818 Inattention 7819 Exceeding Reas. Sale Speed 7820 Driver Interacting with Passengers, Anim 7821 Improper U-Turn 7821 Did Not (Fant RW to Vehicle	Over Center Line		55 Male 25 Female 53 Male	le None		26 Male						F	Lane 1 Decreasing Mispost Part Right Shoulder Increasing Mispost Lane Increasing Mispost Lane Increasing Mispost Lane Increasing Mispost
7822 Did Not Grant RW to Vehicle 7823 Apparently Asleep or Fatigued 7824 Inattention 7825 Apparently Asleep or Fatigued 7825 Apparently Asleep or Fatigued			28 Female	ile None		54 Male						I I I	Lane 1 Decreasing Milepost
7824 Inattention 7825 Apparently Asleep or Fatigued 7826 None			28 Female 65 Male 25 Male									F	Past Right Shoulder Increasing Milepost Past Right Shoulder Increasing Milepost Right Shoulder Increasing Milepost Right Shoulder Increasing Milepost Right Shoulder Increasing Milepost
7925 Spenierri y secop of rangeou 7927 South South South 7927 Inattention 7928 None 7929 Exceeding Reas, Safe Speed 7939 Apparently de Jean or Estimad	Exceeding Reas. Safe Speed		25 Male 22 Female 76 Male 20 Female	de								F	Past Right Shoulder Increasing Milepost Past Right Shoulder Decreasing Milepost Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost Past Right Shoulder Decreasing Milepost
			b1 Male									I I	Past Right Shoulder Decreasing Milepost
7831 Apparently Fatigued 7832 Improper Turn/Merge 7833 None 7834 Other Contributing Circ Not Listed 7835 Exceeding Reas. Safe Speed			21 Male 32 Male 56 Male 18 Male					Under Influence of Alcohol	Inattention	33 Male		E L	Past Right Shoulder Increasing Milepost Past Right Shoulder Increasing Milepost Lane 1 Decreasing Milepost Past Right Shoulder Increasing Milepost Past Right Shoulder Decreasing Milepost
7836 Other Contributing Circ Not Listed	Over Center Line		30 Male 53 Male 34 Male	None		71 Female						F	Past Right Shoulder Decreasing Milepost Past Right Shoulder Increasing Milepost Past Right Shoulder Increasing Milepost
7837 Apparently Asleep or Fatigued 7838 Inattention 7839 Under Influence of Alcohol	Exceeding Reas. Safe Speed Had Taken Medication		31 Female 55 Female			82 Male							
7841 Operating Detective Equipment 7841 Other Contributing Circ Not Listed 7842 Apparently Asleed or Fatigued	Inattention		56 Male 25 Female 69 Male	ile								F F	Lane 1 breasing Milepost Lane 1 Dereasing Milepost Past Right Shoulder Increasing Milepost Past Right Shoulder Decreasing Milepost
7843 Unknown Distraction	Imperor Postino		70 Male 39 Male	d-								F	Past Right Shoulder Decreasing Milepost Past Right Shoulder Increasing Milepost Past Right Shoulder Increasing Milepost Past Right Shoulder Increasing Milepost Past Right Shoulder Decreasing Milepost
1784 Oner Center Line 1784 Oner Center Line 1784 Oher Combuding Dic Not Listed 1784 National One Oher Combuding 1785 February 1785 February 1785 Pebruary 1785 Pebr	Inattention Exceeding Reas. Safe Speed		35 Female 58 Male	lie									Past Right Shoulder Decreasing Milepost Past Right Shoulder Decreasing Milepost
7849 Other Contributing Circ Not Listed 7849 Inattention 7850 Follow Too Closely	Exceeding Reas. Safe Speed		46 Male 20 Male 45 Female	ile None		87 Male						F F	Past Right Shoulder Decreasing Milepost Past Right Shoulder Increasing Milepost Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost
7851 None 7852 Apparently Asleep or Fatigued	Driver Interacting with Passengers, Anim		55 Male 21 Male 67 Female 52 Male	d-									Lane 1 Decreasing Milepost Intersecting Road Increasing Milepost
7854 Inattention 7855 Other Contributing Circ Not Listed	Follow Too Closely	Over Center Line	43 Female	ile		71 Male							Lane 1 Increasing Miepost Lane 1 Increasing Miepost Past Right Shoulder Decreasing Miepost
7854 hastension 7855 Ohr Contributing Circ Not Listed 7856 hastension 7857 Linknown Distraction 7858 Apparently Acteop or Falgued 7859 Apparently Acteop or Falgued 7859 Operating Detective Equipment 7860 Operating Detective Equipment 7861 None 7862 None 7863 Distractions Outside Vehicle	Exceeding Reas. Safe Speed		50 Male 31 Male									F	Past Right Shoulder Decreasing Miepost Past Right Shoulder Increasing Miepost
7859 Driver Interacting with Passengers, Anim 7860 Operating Defective Equipment	Did Not Grant RW to Vehicle		36 Male 23 Male	None		49 Male						E	last Right Shoulder Increasing Milepost Lane 1 Decreasing Milepost
7862 None 7863 Distractions Outside Vehicle			23 Male 47 Female 75 Male 23 Male	-								P	Lane 1 Decreasing Mlepost Lane 1 Increasing Mlepost Lane 1 Increasing Mlepost Right Shoulder Increasing Mlepost
7864 Inattention 7865 None 7866 Apparently Asleep or Fatigued	Follow Too Closely		53 Male 68 Male 35 Male	Follow Too Closely		54 Male						l l	Lane 1 Decreasing Milepost Lane 1 Increasing Milepost Past Right Shoulder Increasing Milepost
7867 Exceeding Reas. Safe Speed 7868 Under Influence of Alcohol	Expending Days Cole Con. 1		40 Female	ile		40							Past Right Shoulder Increasing Milepost Past Right Shoulder Decreasing Milepost
7870 Exceeding Reas. Safe Speed 7871 None	Exceeding Reas. Safe Speed		21 Male 43 Male 47 Female	None None		40 Female 16 Male		Under Influence of Alcohol	Light Violation: No Lights/Fail to Dim	Non Motorist on Wrong Side of Road 61 Male			Lane 1 Increasing Miepost Lane 1 Decreasing Miepost Lane 1 Increasing Miepost
1982 Note 1982 N			43 Male 74 Female	ile								I I	Past Right Shoulder Increasing Miepost Past Right Shoulder Decreasing Miepost Lane 1 Increasing Miepost Lane 1 Increasing Miepost Lane 1 Increasing Miepost
7875 Follow Too Closely 7876 Inattention	Did Not Grant RW to Vehicle		32 Female	le None		27 Maie 51 Female 54 Maie							
7877 Inattention 7878 Under Influence of Alcohol 7879 Other Driver Distractions Inside Vehicle	Failing to Signal Exceeding Reas. Safe Speed		56 Male 22 Male 36 Male	None Inattention	Improper Passing	54 Male						E E	Lane 1 Decreasing Miepost Past Right Shoulder Decreasing Miepost Past Right Shoulder Increasing Miepost
7880 Other Contributing Circ Not Listed 7881 Disregard Stop Sign - Flashing Red	Hadas latinas - 72	Survey Day O. C. C.				17 Male							
7875 Fedou Too Closely 7876 Instruction 7877 Instruction 7877 Instruction 7877 Instruction 7877 One Too Closely 7879 One Too Closely 78	Under Influence of Drugs Did Not Grant RW to Vehicle	Exceeding Reas. Safe Speed	21 Male 38 Female 41 Female 53 Female	ile		31 Male						F L	Lane 1 Decreazing Miepost Past Right Shoulder Increasing Miepost Lane 1 Decreazing Miepost Past Right Shoulder Decreazing Miepost Past Right Shoulder Decreazing Miepost
7885 Other Contributing Circ Not Listed 7886 Other Contributing Circ Not Listed 7887 Inatteriors	Follow Too Closely		35 Male 24 Male 27 Male	Follow Too Closely		25 Female							Past Right Shoulder Decreasing Milepost
7888 Inattention 7889 Under Influence of Alcohol	Apparently Fatigued		23 Male 23 Male	None		20 Female							Lane 1 Increasing Miepost Past Left Shoulder On Ramp Decreasing Miepost Side of Mainline Past Left Shoulder On Ramp Decreasing Miepost Side of Mainline
7890 Apparently As leep or Fatigued 7891 Apparently As leep or Fatigued 7892 Inattention			23 Male 23 Male 36 Female 42 Male 21 Female	ele ele								F	Past Left Shoulder On Ramp Decreasing Milepost Side of Mainline Past Right Shoulder On Ramp Decreasing Milepost Side of Mainline Past Left Shoulder On Ramp Decreasing Milepost Side of Mainline Past Left Shoulder On Ramp Decreasing Milepost Side of Mainline Past Right Shoulder On Ramp Decreasing Milepost Side of Mainline
7893 Other Contributing Circ Not Listed 7894 Exceeding Reas. Safe Speed			51 Male 40 Female			10 Familie						F	Past Right Shoulder On Ramp Decreasing Milepost Side of Mainline Past Right Shoulder On Ramp Decreasing Milepost Side of Mainline
7896 Inattention 7897 None		Unknown Distraction		None		49 Female 26 Female						L	Lane 1 increasing Milepost Right Shoulder Decreasing Milepost
7898 inattention 7899 None 7900 Under Influence of Alcohol	Exceeding Reas. Safe Speed		24 Female 46 Female 49 Male	sie sie								L	in Median Decreasing Miepost Lane 1 Increasing Miepost In Median Decreasing Mileost
7901 Over Center Line 7902 Other Contributing Circ Not Listed			21 Female 39 Male	ile								in the second se	n Median Increasing Milepost Past Right Shoulder Increasing Milepost
7886 Other Constituting Circ Not Lated 7887 Institution 7887 Institution 7889 Under Inflamma of Alcohol 7899 Under Inflamma of Alcohol 7890 Apparently Assept or Faligued 7890 Asset Asset 7890 Asset Asset Asset 7890 Asset Asset Asset 7890 Asset Asset 7890 Asset 7800 Asset 7800 Asset 7800 Asset 7800 Asset 7800 7800 Asset 7800	Over Center Line Exceeding Reas. Safe Speed	Follow Too Closely	39 Male 70 Male 55 Male 38 Female	ale None		39l Male							In Median Increasing Mispost Part Right Shoulder Increasing Mispost Part Right Shoulder Increasing Mispost Lave 1 Decreasing Mispost Lave 1 Decreasing Mispost Lave 1 Decreasing Mispost
7906 Exceeding Reas. Safe Speed 7907 Exceeding Reas. Safe Speed	Other Driver Distractions Inside Vehicle		38 Femali 80 30 Male 74 Male			THE PROPERTY OF THE PROPERTY O							Laine I Lechasaria Mispost Past Right Shoulder Dernasing Mispost Past Right Shoulder Dernasing Mispost Past Right Shoulder Dernasing Mispost Right Shoulder Dernasing Mispost Laine I Decnasing Mispost Laine I Decnasing Mispost
7909 None 7910 Exceeding Reas. Safe Speed 7910 Exceeding Reas. Safe Speed			29 Female	ile								E L	Ignt snourcer Decreasing Milepost _ane 1 Decreasing Milepost Past Right Shoulder Decreasing Milepost
7911 Exceeding Reas. Safe Speed 7912 Improper U-Turn 7913 Mone	-		41 Male 20 Male 34 Male 77 Male	None		30 Male							Past Right Shoulder Decreasing Miepost Past Right Shoulder Decreasing Miepost Lane 2 Increasing Miepost Lane 2 Increasing Miepost
7914 Exceeding Reas. Safe Speed 7915 One				ile None		24 Female						L	Lane 1 Decreasing Mlepost Lane 1 Decreasing Mlepost Lane 2 Increasing Mlepost Lane 2 Increasing Mlepost
7916 Other Contributing Circ Not Listed 7917 None 7918 Improper Passing			71 Male 65 19 Male	Other Contributing Circ Not Listed None None		54 Male						P	Lane 2 Increasing Milepost Right Shoulder Increasing Milepost Lane 1 Decreasing Milepost
	•	*	. J present	+	•				•	+ +			

OBJECTID WA_STATE_PLANE SOUTH_X SOUTH_X 2010_FORWARD 2010_FORWARD CO	OL COL COL ARR MIX TYPE	COL COL_HUM SEVERITY FACTOR	COL COL COL, AGE IMPAIRED IMPAIRED	COL SURFACE CONDITIONS CONDITIONS	COL COL STU GEOMETRY AR	DY COL COL TYPE SEVERITY TEXT	y COL_HUMAN COL FACTOR_Decoded MVIW	COL_LIGHT COL_SI CONDITIONS CONDI Decoded Deco	IRFACE COL_LOCATION TIONS Decoded ided	COL_RISKY Intersection En	Emphasis Roadway Departure
	17 1 Vehicle Party Animal 18 2 Vehicles Partes Turning 17 1 Vehicle Party Animal 17 1 Vehicle Party Animal 12 Vehicles Parties Sideswipe	Property Damage Only OherUnknown 65 Property Damage Only OherUnknown 65 Property Damage Only Speeding 55 Property Damage Only OherUnknown 65 Property Damage Only OherUnknown 65 Property Damage Only OherUnknown 65	No	Dry Dawn/Dusk Dry Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off Dry Dawn/Dusk Dry Dawn/Dusk	Not at Intersection/Driveway/Roundabout Curve & Grade At Driveway or Intersection Straight & Level At Driveway or Intersection Straight & Level At Driveway or Intersection Straight & Level Straight & Level Straight & Level Straight & Level	Older Road User 65+	None	Dawn/Dusk Di Dark-No Street Lights/Street Lights Off Di Dark-No Street Lights/Street Lights Off Di Dawn/Dusk Di Dawn/Dusk Di Dawn/Dusk Lights/Street Lights Off Di Dawn/Dusk D	y Not at Intersection/Driveway/Roundabout ty At Driveway or Intersection by At Driveway or Intersection y Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Speeding No Risky Driving Behaviors Involved	No No No No
7744 1061935.23 989083.37 201 7745 1061935.23 989083.37 201 7746 1061962.57 989044.47 201 7747 1062045.6 989353.44 201	16 2 Vehicles/Parties Rear-End 16 2 Vehicles/Parties Rear-End 16 2 Vehicles/Parties Rear-End 18 2 Vehicles/Parties Rear-End	Property Damage Only Speeding 35 Minor or Possible Injury Speeding 65 Minor or Possible Injury Speeding 35	14 No No 14 No 14 No 15 No 16 No 16 No 17	Dry Daylight Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Speeding Speeding Speeding	Daylight Di Daylight Di Daylight Di Daylight Di	w/Slush Not at Intersection/Drivway/Roundabout by Not at Intersection/Drivway/Roundabout Not at Intersection/Drivway/Roundabout by Not at Intersection/Drivway/Roundabout Not at Intersection/Drivway/Roundabout	Speeding Speeding Speeding Speeding Follow Too Closely	No No No No
7748 1062132.35 987779.7 201 7749 1062140.22 987727.85 201 7750 1062198.41 986762.83 201 7751 1062194.31 986730.74 201	1 Vehicle/Party Struck Object 1 Vehicle/Party Rnimal 1 Vehicle/Party Animal 1 Vehicle/Party Animal 2 Vehicle/Partes Will Other Non-Collision 1 Vehicle/Partes Will Other Non-Collision 1 Vehicle/Partes Will Other Non-Collision 1 Vehicle/Partes Vehi	Property Damage Only Speeding Ur Property Damage Only Other/Unknown 25 Minor or Possible Injury Other/Unknown 35	ter 25 No	Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight	At Driveway or Intersection Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Curve & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Motorcycle No Vulnerable Road User Involved	Speeding None	Daylight Di Daylight Di Daylight Di Daylight Di Daylight Di	ny At Driveway or Intersection y Not at Intersection/Driveway/Roundabout ny Not at Intersection/Driveway/Roundabout ny Not at Intersection/Driveway/Roundabout	Speeding No Risky Driving Behaviors Involved	Yes No No No
7752 1062107.14 986284.27 201 7753 1061570.89 984224.07 201 7754 1060735.06 982808.41 201 7755 1059927.88 982422.97 201	16 1 Vehiclac/Party All Other Non-Collision 9 3 Vehiclacs/Parties Rear-End 16 2 Vehiclacs/Parties Rear-End 15 1 Vehicla/Party Struck Object		No No No 14 No 14 No 154 No 155 No 15	Wet Daylight Dry Daylight Dry Dark-No Street Lights/Street Lights Off Wet Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level	Older Road User 65+ Older Road User 65+ Older Road User 65+ No Vulnerable Road User Involved	Follow Too Closely Other Contributing Circ Not Listed Other Contributing Circ Not Listed	Daylight Di Daylight Di Dark-No Street Lights/Street Lights Off Di Dark-No Street Lights/Street Lights Off W	et Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Follow Too Closely No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No No Yes
	15 2 Vehides/Parles Sideswipe 1 Vehides/Parly	Property Damage Only Over Center Line 25 Property Damage Only Apparently Asleep, Fatigued or III 25 Minor or Possible Injury Distracted Driving 25 Property Damage Only Other Unknown 55	14 No	Dry Daykight Wet Daykight Dry Daykight Wet Daykight Wet Daykight	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Motorcycle Older Road User 65+	Distracted Driving None	Daylight Di Daylight W	nt Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Apparently Aeleep, Fatigued or III Distracted Driving No Risky Driving Behaviors Involved	Yes Yes No No
7762 1056350.68 980091.86 201 7763 1056299.7 980027.68 201	17 Vehicle Party Overturned	Property Damage Only Under Influence of Alcoholl Drugs/Medication 45 Property Damage Only Follow Too Closely Ur Property Damage Only Oher/Unknown 35 Property Damage Only Speeding 55 65	34 Under Influence of Alcohol 45-54 extended		Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	None Speeding		ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout	Under Influence of Acohol/Drugs/Medication Follow Too Closely No Risky Driving Behaviors Involved Speeding Follow Too Closely	No No No
7766 1055759.29 979347.3 201 7767 1055759.29 979347.3 201	18 1 Vehicle-Party Jackknife Trailer 9 2 Vehicles-Parties Head-Ch 15 3 Vehicles-Parties Rear-End 15 1 Vehicle-Party Amimal 1 Vehicle-Party Struck Object	Property Damage Only Follow Too Closely 25 Property Damage Only Other/Unknown 25	14	Wet Dark-No Street Lights/Street Lights Off Wet Dark-No Street Lights/Street Lights Off Dry Daylight Dry Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off	Driveway Related but Not at Driveway Straight & Level Not at Intersection/Driveway/Roundabout Curve & Level	No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+	Follow Too Closely Under Influence of Alcohol/Drugs,Medication Follow Too Closely Other/Unknown Apparently Relep or Fatigued	Daylight Di Dark-No Street Lights/Street Lights Off Di	n/ Not at Intersection/Driveway/Poundahout	No Risky Driving Behaviors Involved	Yes No No Ves
7769 1055660.88 979222.4.201 7770 1055464.88 978976.63 201 7771 1055480.1 978990.95 201 7772 1055157.61 978547.59 201	15. 1 Vehicle/Party Struck Object 17. 2 Vehicles/Parties Rear-End 19. 2 Vehicles/Parties Sideswipe 15. 1 Vehicle/Party Struck Object	Unknown Distracted Driving Ur. Property Damage Only Speeding 35 Property Damage Only Speeding 65 Property Damage Only Apparenty Asleep, Fatigued or III 65	nown No 14 No No	Wet Dark-No Street Lights/Street Lights Off Wet Dark-No Street Lights/Street Lights Off Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level Driveway Related but Not at Driveway Not at Intersection/Driveway/Roundabout Curve & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+	Unknown Distraction Speeding Speeding Apparently Asleep or Fatigued	Dark-No Street Lights/Street Lights Off W Dark-No Street Lights/Street Lights Off W Daylight Di Daylight Di	et Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout by Driveway Related but Not at Driveway	Distracted Driving Speeding Speeding Apparently Asleep, Fatigued or III	Yes No Yes Yes
7774 1055058.78 978303.8 201 7775 1055032.5 978178.57 201 7776 1055021.99 978023.86 201	15 1 Vehicle/Party Struck Object 18 1 Vehicle/Party Struck Object 19 1 Vehicle/Party Avinal 16 2 Vehicles/Parties Rear-End		No No No No No No No No No No	Dry Daylight Dry Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off Dry Daylight	At Driveway or Intersection Straight & Level	Older Road User 65+ Motorcycle No Vulnerable Road User Involved No Vulnerable Road User Involved	Inattention Under Influence of Alcohol/Drugs/Medication None Follow Too Closely	Daylight Di	y At Driveway or Intersection Driveway Foundation y At Driveway or Intersection y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout y At Driveway or Intersection	Inattention Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved Follow Too Closely	Yes Yes No No
7778 1055034.95 977938.25 201 7779 1055035.15 977952.87.201	15 2 Vehides/Parlies Rear-End 7 2 Vehides/Parlies Rear-End 9 2 Vehides/Parlies Rear-End 14 2 Vehides/Parlies Angle	Property Damage Only Speeding 35 Mnor or Possible Injury Instension 65 Property Damage Only Follow Too Closely 25 Minor or Possible Injury Fail to Yield 65	14 No	Dry Daylight Dry Daylight Wet Daylight Dry Daylight	Not at Intersection Driveway/Roundabout Curve & Level Driveway/Related but Not at Driveway Curve & Level At Driveway or Intersection Curve & Level At Driveway or Intersection Curve & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+	Inattention Follow Too Closely Fail to Yield	Daylight Di	ry Not at Intersection/Driveway/Roundabout ry Driveway Related but Not at Driveway et Al Driveway or Intersection ry At Driveway or Intersection	Speeding Inattention Follow Too Closely Fail to Yield	No No No
	15. 1 Vehide Party Struck Object 18. 1 Vehide Party Ower Embankment - No Guardrail Present 19. 1 Vehide Party Struck Object Struck Object	Unknown Other/Unknown 55 Minor or Possible Injury Speeding Ur	No	Dry Daylight Dry Daylight Dry Daylight Wet Daylight Wet Daylight	At Driveway or Intersection Curve & Grade M Driveway or Intersection Curve & Level At Intersection and Not Related Straight & Level At Driveway or Intersection Straight & Level	No Vulnerable Road User Involved	Speeding Circ Not Listed	Daylight D: Daylight D: Daylight D: Daylight W	ry At Intersection and Not Related et At Driveway or Intersection	No Risky Driving Behaviors Involved Fail to Yield No Risky Driving Behaviors Involved Speeding	No Yes Yes
7787 1055166.05 977530.12 201 7788 1055187.5 977492.93 201	19 2 Vehicles/Parties Rear-End 1 Vehicles/Party Struck Object 18 1 Vehicles/Party Struck Object or Debris On Road 15 3 Vehicles/Parties Head-On 1	Minor or Possible Injury Follow Too Closely 65 Minor or Possible Injury Other/Unknown 25 Property Damage Cnly Improper TurnWilder 55 Suspected Serious Injury Under Influence of Alcohol/Drugs/Medication Ur	No	Dry Daylight los/Snow/Slush Dark-No Street Lights/Street Lights Off Dry Daylight Wet Dawn/Dusk	A Driveway or Intersection Unknown Not at Intersection/Driveway/Roundabout Straight & Level At Driveway or Intersection Ourve & Grade Not at Intersection/Driveway/Roundabout Curve & Level	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Beat User Envolved	Under Influence of Alcohol/Drugs/Medication	Daylight Di Dawn/Dusk W	y A Driveway or Intersection Wislush Not at Intersection/Driveway/Roundabout y A Driveway or Intersection Not at Intersection/Driveway/Roundabout	Follow Too Closely No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication No Risky Driving Review Comments No Risky Driving Behaviors Involved	Yes No Yes
7791 1055866.76 976923.86 201 7792 1055867.8 976925.39 201	18 2 Vehicles Paries Sideswipe	Property Damage Only Over Center Line 656 Minor or Possible Injury Fall to Meld 656 Property Damage Only Follow Too Closely 45 Property Damage Only Improper Turn/Merge 35 Property Damage Only Instruction 66	No No 14 No 14 No 15 No	Wet Daylight Dry Daylight Dry Daylight Ury Daylight Wet Daylight Ury Daylight Ury Daylight	Not at Intersection Driveway/Roundabout Curve & Level At Driveway or Intersection Curve & Level At Driveway or Intersection Curve & Grade At Driveway or Intersection Straight & Level At Driveway or Intersection Curve & Grade	Older Road User 65+ Older Road User 65+ No Wilnerable Road User Involved No Wilnerable Road User Involved Older Road User 65+ Older Road User 65+	Fail to Yield Follow Too Closely Improper Turn/Merge	Daylight Di Daylight Di Daylight W	et Not at Intersection/Driveway/Roundabout y At Driveway or Intersection et At Driveway or Intersection et At Driveway or Intersection y At Driveway or Intersection y At Driveway or Intersection	No Risky Driving Behaviors Involved Fail to Yield Follow Too Closely No Risky Driving Behaviors Involved Inattention	No No No No
7794 1056041.39 976815.24 201 7795 1056150.73 976656.56 201 7796 1056149.92 976657.35 201	15 Vehicle Party Over Embankment - No Guardrail Present	Minor or Possible Injury Speeding 55 Property Damage Only Other Unknown 35 Minor or Possible Injury Under Influence of Alcohol/Drugs/Medication 25 Property Damage Only Other Unknown 25	No	Other Dark-Street Lights On Wet Daylight Dry Dark-No Street Lights/Street Lights Off Dry Daylight	Not at Intersection/DriewayRoundabout Curve & Grade Intersection/DriewayRoundabout Curve & Grade Intersection Related but Not at Intersection Curve & Grade At Drieway or Intersection Straight & Grade At Intersection and Not Related Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved Motorcycle No Vulnerable Road User Involved	Speeding None Under Influence of Alcohol/Drugs/Medication None	Dark-Street Lights On Other/U: Daylight W Dark-No Street Lights/Street Lights Off Di	nknown Not at Intersection/Driveway/Roundabout et Intersection Related but Not at Intersection ry At Driveway or Intersection	Speeding Speak Speeding Speak Speeding Speak Speeding Speak Speeding No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved	Yes Yes No
7798 1056355.31 976093.61 201 7799 1056338.9 975873.54 201 7800 1056282.58 975641.8 201 7801 1056186.614 975368.3275 201	15 2 Vehicles/Parties Rear-End 17 1 Vehicle/Party Struck Object 19 1 Vehicle/Party Struck Object 16 1 Vehicle/Party Animal	Minor or Possible Injury OhertUnknown 35 Minor or Possible Injury Apparently Aeleep, Fatigued or III 55 Property Damage Only Ostracted Driving 25 Property Damage Only Other/Unknown 25	14 No 155-64 160 175-64	Wet Daylight Dry Daylight Dry Daylight Dry Daylight Dry Dark-No Street Lights/Street Lights Off Dry Dark-No Street Lights/Street Lights Off	At Driveway or Intersection Curve & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Curve & Level	No Vulnerable Road User Involved	Apparently Asleep or Fatigued Distractions Outside Vehicle	Daylight W Daylight Di Daylight Di	et Al Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Apparently Asleep, Fatigued or III Distracted Driving No Risky Driving Behaviors Involved	No Yes Yes No
7802 1056190.22 975368.05 201 7803 1056173.76 975318 201 7804 1056548.02 973075.3 201	18 1 Vehicle/Party Animal 2 Vehicles/Parties Rear-End 15 1 Vehicle/Party Struck Object 18 1 Vehicle/Party Struck Object 18 1 Vehicle/Party	Property Damage Only Other/Unknown 55 Property Damage Only Other/Linknown 65	No No Inc. Under Influence of Alcohol and Apparently Asleep, Fatigued or III Under 25 No	Dry Daylight Wet Dark-No Street Lights/Street Lights Off		No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+	None None Under Influence of Alcohol/Drugs/Medication Distracted Driving	Dark-No Street Lights/Street Lights Off Di Dark-No Street Lights/Street Lights Off Di Daylight Di Dark-No Street Lights/Street Lights Off W Dark-No Street Lights/Street Lights Off Daylight Di	y Not at Intersection/Driveway/Roundabout y At Driveway or Intersection et Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication Distracted Driving	No No Yes Yes
7806 1056829.17 970047.05 201 7807 1056830.04 970048.88 201 7808 1056834.26 970031.57 201	16 1 Vehicle/Party Struck Object 2 Vehicles/Party Rear-End 3 Vehicles/Party Struck Object 4 2 Vehicles/Parties Head-On	Property Damage Only Other/Unknown 55 Mnor or Possible Injury Inattention 25 Property Damage Only Distracted Driving 25 Suspected Serious Injury Operating Defective Equipment 25	34 No	Wet Dark-No Street Lights/Street Lights Off Dry Daylight Dry Dark-No Street Lights/Street Lights Off Dry Daylight	Al Intersection and Not Related Straight & Level At Driveway or Intersection Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Curve & Level	No Vulnerable Road User Involved	Other Contributing Circ Not Listed Inattention Unknown Distraction	Dark-No Street Lights/Street Lights Off W Daylight Dr Dark-No Street Lights/Street Lights Off Dr Daylight Dr	et At Intersection and Not Related ty At Driveway or Intersection y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Inattention Distracted Driving No Risky Driving Behaviors Involved	Yes No Yes Yes
7812 1056809.04 967641.1 201 7813 1056736.83 967500.58 201	19 1 Vehicle/Party Animal 9 1 Vehicle/Party Animal 19 1 Vehicle/Party Struck Object 17 2 Vehicles/Parties Turning				Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved	None None Speeding Improper Backing/Pas sing/Turning/Parking/Signaling		y Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Speeding No Risky Driving Behaviors Involved	No No Yes No
7816 1056505.1 965605.53 201 7817 1056152 963719.03 201	15 1 Vehicle/Party Struck Object 18 1 Vehicles/Parties Struck Object 18 2 Vehicles/Parties Turning 19 2 Vehicles/Parties Turning	Property Damage Only Improper Turn/Merge 55 Minor or Possible Injury Improper Turn/Merge Ur	Apparently Asteep, Fatigued or III 25-34	Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Curve & Level At Intersection and Not Related Straight & Level At Driveway or Intersection Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved	Driver Physical Condition Improper Turn/Merge Improper Backing/Passing/Turning/Parking/Signaling	Daylight Di Daylight Di Daylight Di	y Not at Intersection/Driveway/Roundabout try At Intersection and Not Related by At Driveway or Intersection by Not at Intersection/Driveway/Roundabout	Inattention Apparently Asleep, Fatigued or III No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	Yes Yes No No
	18 Vehide Party Struck Object	Minor or Possible Injury Speeding 55		Dry Daylight loe/Snow/Slush Daylight Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved	Speeding Distracted Driving Improper Backing/Passing/Turning/Parking/Signaling	Daylight	y Not at Intersection/Driveway/Roundabout Wislush Not at Intersection/Driveway/Roundabout by Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Institution Speeding Distracted Driving No Risky Driving Behaviors Involved	Yes Yes Yes No
7823 1055872.48 959499.36 201 7824 1055750.19 958264.58 201 7825 1055242.94 953733.41 201	17 2 Vehicles/Parties Angle	Property Damage Only Fail to Yield Ur. Property Damage Only Papernety Asleep, Fatigued or III 252 Property Damage Only Inattention 25 Property Damage Only Apparenty Asleep, Fatigued or III 655 Manor or Possible Injury Other/Unknown 255	fer 25 No 25 44 Apparently Asleep, Fatigued or III 25-34 44 No 4 Apparently Asleep, Fatigued or III 65+ Apparently Asleep, Fatigued or III 65+	Wet Daylight Dry Dawn/Dusk Wet Daylight Dry Dawn/Dusk Dory Dawn/Dusk loo/Snow/Slush Dark-No Street Lights/Street Lights Off	Al Driveway or Intersection Curve & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Curve & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Fail to Yield Apparently Asleep or Fatigued Inattention Apparently Asleep or Fatigued Mone	Dawn/Dusk Di Daylight W Dawn/Dusk Di	et Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout	Fait v Yield Apparently /elsep, Fatigued or III Inattention Apparently /elsep, Fatigued or III Apparently /elsep, Fatigued or III No Risky Driving Behaviors Involved	Yes Yes No
7826 1055220.4 963284.12 (201 7827 1054991.95 952078.86 (201 7828 1055088.75 951229.2 (201 7829 1055144.22 951026.07 (201 7830 1055300.16 950455.01 (201	15 1 Vehicle Party Struck Object 5 1 Vehicle Party Struck Cbject 19 1 Vehicle Party Fire started in vehicle 15 1 Vehicle Party Struck Object 4 1 Vehicle Party Struck Object	Property Damage Only Institention Ur Property Damage Only Other/Unknown 65 Mnor or Possible Injury Speeding Ur	er 25 No	Wet Dark-No Street Lights/Street Lights Off Dry Daylight Dry Daylight Wet Daylight	Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved	Inattention None Speeding Apparently Asleep or Fatigued	Dark-No Street Lights/Street Lights Off Loe/Snor Dark-No Street Lights/Street Lights Off W Daylight Di Daylight Di Daylight W	tet Not at Intersection/Driveway/Roundabout by Not at Intersection/Driveway/Roundabout by Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout tet Not at Intersection/Driveway/Roundabout	Inattention In	Yes No Yes Yes
7831 1055405.56 950067.59 201 7832 1055433.45 949968.61 201 7833 1055477.96 949812.58 201	17 1 Vehicle/Party Struck Object	Property Damage Only Apparently Asleep, Fatigued or III Ur Property Damage Only Improper Turn/Nerge 25 Minor or Possible Injury Other/Unknown 55 Property Damage Only Other/Unknown Ur	ter 25 Apparently Asleep, Fatigued or III Under 25 J4 No III J4 No III J6 No III	Wet Dawn/Dusk	Not at Intersection/Driveway/Roundabout Straight & Level At Driveway or Intersection Straight & Level Intersection Related but Not at Intersection Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Motorcycle and Pedalcyclist No Vulnerable Road User Involved	Driver Physical Condition Improper Turn/Merge None	Dawn/Dusk W Dark-No Street Lights/Street Lights Off Di Daylight Di Daylight Di	ry At Driveway or Intersection ry Intersection Related but Not at Intersection Not at Intersection/Driveway/Roundahout	Apparently Asleep, Fatigued or III NO Risky Driving Behaviors Involved NO Risky Driving Behaviors Involved NO Risky Driving Behaviors Involved	Yes Yes No Yes
7835 1056772.48 947894.29 201 7836 1057161.71 947394.94 201 7837 1057633.07 946624.77 201 7838 1058401.53 944957.42 201	18 1 Vehicle/Party Over Embankment - No Guardrall Present 12 Vehicles/Parties Struck Object 14 1 Vehicle/Party Struck Object 17 1 Vehicle/Party Struck Object 17 1 Vehicle/Party Struck Object 18 19 19 19 19 19 19 19	Minor or Possible Injury Speeding 25 Property Damage Only Offset/Unknown 45 Property Damage Only Apparenty Asleep, Fatigued or III 25 Property Damage Only Inattention 25	No	loe/Snow/Slush Daylight Dry Daylight Dry Dark-No Street Lights/Street Lights Off Wet Daylight	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Straight & Level Driveway/Related but Not at Driveway Straight & Level	No Vulnerable Road User Involved	Speeding Other Contributing Circ Not Listed Apparently Asleep or Fatigued Inattention	Daylight lce/Snor Daylight Dirk-No Street Lights/Street Lights Off Di Daylight W	wSlush Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout et Driveway/Related but Not at Driveway	Speeding No Risky Driving Behaviors Involved Apparently Asleep, Fatigued or III Inattention	Yes Yes Yes Yes
7839 1058425.17 944906.15 201 7840 1058506.99 944731.73 201 7841 1058577.8 944574.26 201 7842 1058644.15 944429.2 201	14 2 Vehicles/Parlies Turning 1 Vehicle/Party Fire started in vehicle 17 1 Vehicle/Party Struck Object 16 1 Vehicle/Party Struck Object 18 19 19 19 19 19 19	Property Damage Only Operating Defective Equipment 55 Property Damage Only Other/Unknown 25 Property Damage Only Apparently Asleep, Fatigued or III 65	34 Under Influence of Alcohol and Medication 55-64 34 No	Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry Daylight	A Driveway or Intersection Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	Operating Defective Equipment Other Contributing Circ Not Listed Apparently Asleep or Fatigued	Daylight Di Daylight Di	y Al Driveway or Intersection Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout	Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Apparently Asleep, Fatigued or III	No No Yes Yes
7845 1058987.06 943266.97 201 7846 1059064.73 942906.12 201	77 Vehicle Party Struck Object	Property Damage Only Distracted Driving 65 Uniformatic Distracted Driving 35 Property Damage Only Oher Ulnknown Ur Property Damage Only Oher Ulnknown 35	14 No	Dry Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Grade Driveway/Related but Not at Driveway Straight & Level Not at Intersection/Driveway/Roundabout Curve & Level	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Other Contributing Circ Not Listed Other Contributing Circ Not Listed	Dark-No Street Lights/Street Lights Off Di	y Driveway Related but Not at Driveway Not at Intersection/Driveway/Roundabout	Distracted Driving No Risky Driving Behaviors Involved	Yes Yes Yes
7848 1058957.91 942132.12 201 7849 1057970.83 940900.64 201 7850 1057970.83 940900.64 201	18 1 Vehicle/Party Over Embankment - No Guardrail Present 19 1 Vehicle/Party Struck Object 19 2 Vehicles/Parties Reast-End	Property Damage Only Other/Unknown 45 Property Damage Only Inattention Ursus Suspected Serious Injury Follow Too Closely 45 Property Damage Only Other Unknown 55	14 No	Dry Dark-No Street Lights/Street Lights Off Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Curve & Grade Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved	Other Contributing Circ Not Listed Inattention Follow Too Closely	Daylight Di Dark-No Street Lights/Street Lights Off Di Daylight Di Daylight Di Dark-No Street Lights/Street Lights Off Di	ny Not at Intersection/Driveway/Roundabout ny Not at Intersection/Driveway/Roundabout ny Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Inattention Follow Too Closely No Risky Driving Behaviors Involved	Yes Yes No
7851 1057821.42 340729.95 201 7852 1057206.87 939878.2 7 7853 1057102.01 93905.8 201 7854 1057089.56 937536.53 201 7855 1057055.16 938423.89 201	19 Vehicle Party Animal	Property Damage Chiy Other/Unknown 45 Property Damage Chiy Other/Unknown 35	Under 25	Dry Dark-No Street Lights /Street Lights Off Dry Dark-No Street Lights /Street Lights Off Wet Daylight Dry Daylight Dry Dawn/Dusk	Al Intersection and Not Related Curve & Grade Not at Intersection Oriveway/Roundabout Straight & Level Not at Intersection Oriveway/Roundabout Straight & Level Not at Intersection Oriveway/Roundabout Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	None	Dark-No Street Lights/Street Lights Off Di Dark-No Street Lights/Street Lights Off Di Daylight W Daylight Di Daylight Di Daylight Di Daylight Di Daylight	yy Not at Intersection/Driveway/Roundabout yy At Intersection and Not Related et Not at Intersection/Driveway/Roundabout y At Driveway or Intersection yy Not at Intersection/Driveway/Roundabout	Apparently Asleep, Fatigued or III No Risky Driving Behaviors Involved Inattention No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	Yes No No Ves
7856 1057047.18 936199.88 201 7857 1057037.45 935843.57 201 7858 1056963.15 934845.19 201 7859 1056856.81 934326.38 201	18 1 Vehicle/Party Struck Object	Minor or Possible Injury Inattention 45 Unknown Distracted Driving Ur Property Damage Only Apparently Asleep, Fatigued or III 25 Property Camage Only Distracted Driving 35	I4 No nown No 2 44 Apparently Asleep, Fatigued or III 25-34 14 No	Dry Daylight Wet Dark-No Street Lights/Street Lights Off Dry Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Straight & Level	No Vulnerable Road User Involved	Inattention Unknown Distraction Apparently Asleep or Fatigued	Daylight Di Dark-No Street Lights/Street Lights Off W Daylight Di Daylight Di	y Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout	Inattention Distracted Driving Apparently Asleep, Fatigued or III Distracted Driving	Yes Yes Yes Yes
7860 1055365.62 932144.65 201 7861 1055940.66 931382.37 201 7862 1055083.11 929867.88 201 7863 1054413.58 928323.34 201	15 2 Vehicles/Parties Angle	Minor or Possible Injury Operating Defective Equipment Ut Property Damage Only Other/Unknown 45 Minor or Possible Injury Other/Unknown 55 Property Damage Only Distracted Driving Ur	ler 25 No No No In	Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry Dark-No Street Lights/Street Lights Off	AD Driveway or Intersection Straight & Grade Not at Intersection Driveway Roundabout Straight & Grade Not at Intersection Driveway Roundabout Straight at Hillcrest Not at Intersection Driveway Roundabout Straight at Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved	Operating Defective Equipment None None	Daylight Di Daylight Di Daylight Di Daylight Di Daylight Di Daylight Common Lighte (Street Lighte Off	ny At Driveway or Intersection y Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Distracted Driving	No No No Yes
7867 1053816.68 925108.6 201	17 3 Vehicles/Parlies Sideswipe 14 1 Vehicle/Party Animal 15 1 Vehicle/Party Struck Object 18 1 Vehicle/Party Overturned	Suspected Serious Injury Inattention 45 Minor or Possible Injury Often/Unknown 65 Property Damage Only Apparently Asleep, Fatigued or III 35 Property Damage Only Speeding 55	14 Apparently Asleep, Fatigued or III 35-44 No Io	lce/Snow/Slush Dawn/Dusk	At Driveway or Intersection Straight & Level Not at Intersection (Driveway Roundabout Straight & Level Not at Intersection (Driveway Roundabout Curve & Level Not at Intersection, Driveway Roundabout Straight & Level	No Vulnerable Road User Involved	Apparently Asleep or Fatigued Speeding	Dawii/Dusk ice/alloi	y At Driveway or Intersection Processing Noundation by Not at Intersection/Driveway/Roundatiout y Not at Intersection/Driveway/Roundatiout w/Slush Not at Intersection/Driveway/Roundatiout	Inattention No Risky Driving Behaviors Involved Apparently Asleep, Fatigued or III Speeding	Yes No Yes Yes
7869 1053896.58 923897.82 201 7870 1054128.35 920979.3 201 7871 105428.2 52 920894.8 201	14 1 Vehicle Party Struck Object	Property Damage Only Inattention Ur Property Damage Only Speeding 35 Property Damage Only Other/Unknown 45	ler 25 No	Wet Daylight Dry Daylight Dry Dark-No Street Lights/Street Lights Off	Not at Intersection Driveway/Roundabout Straight & Level At Intersection and Not Related Straight & Level At Driveway or Intersection Curve & Grade At Intersection and Not Related Straight & Level At Intersection and Not Related Straight & Level Related Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Pedaloycilist No Vulnerable Road User Involved No Vulnerable Road User Involved	Inattention Speeding None	Daylight W Daylight Di Dark-No Street Lights/Street Lights Off Di	y Not at Intersection/DrivewayRoundabout et Al Intersection and Not Related by At Driveway or Intersection by At Intersection and Not Related by At Intersection and Not Related	Under Influence of Alcohol/Drugs/Medication Inattention Speeding No Risky Driving Behaviors involved Apparently Veleep, Fatigued or III	No No No
7873 1054504.76 920353.25 201 7874 1054538.26 920311.95 201 7875 1054802.01 919995.43 201	19 Venice Party Struck Object	Minor or Possible Injury Apparently Asleep, Fatigued or III 65 Property Damage Only Other/Unknown 65 Property Damage Only Follow Too Closely 25	Apparently Asleep, Fatigued or III 65+	Dry Daylight Dry Daylight Dry Daylight	Al Intersection and Not Related Curve & Grade Not at Intersection Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Straight & Level Intersection Related but Not at Intersection Straight & Level At Driveway or Intersection Straight & Level	NO vulnerable Road User (65+ Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Apparently As leep or Fatigued None Follow Too Closely	Daylight Di Daylight Di	ny Not at Intersection/Driveway/Roundabout ny Not at Intersection/Driveway/Roundabout ny Intersection Related but Not at Intersection	Apparently Astep, Fatigued or III Apparently Ricep, Fatigued or III No Risky Driving Behaviors Involved Follow Too Closely Inattention	Yes No No
7877 1054848.98 919930.65 201 7878 1056212.74 918250.93 201 7879 1056378.13 918043.66 201 7880 1056749.84 917399.5 201	16 2 Vehicles/Parties Turning 1 1 Vehicle/Party Struck Object 19 1 Vehicle/Party Struck Object	Property Damage Only Inattention 55 Property Damage Only Under Influence of Alcohol/Drugs/Medication University Only Distracted Driving 35		Dry Daylight Wet Dark-Street Lights On Dry Daylight	At Driveway or Intersection Straight & Level At Intersection and Not Related Curve & Level Not at Intersection Driveway Roundabout Curve & Level At Driveway or Intersection Straight & Level	No Vulnerable Road User Involved	Inattention Under Influence of Alcohol/Drugs/Medication Distracted Priving	Daylight Di Dark-Street Lights On W Daylight Di Daylight Di Mark-Street Lights On W	ny At Driveway or Intersection et At Intersection and Not Related ny Not at Intersection/Driveway/Roundabout	Inattention Under Influence of Alcohol/Drugs/Medication Distracted Driving No Risky Driving Behaviors Involved	No Yes Yes Yes
7881 1056755 917392.59 201 7882 1056798.79 916815.96 201 7883 1056799.79 916753.31 201 7884 1056776.12 916603.68 201		Property Damage Only Inattention 35 Property Damage Only Speeding 45	14 NO	Wet Dark-Street Lights On Wet Daylight Wet Dark-Street Lights On Dry Daylight Dry Dark-Street Lights On	At Driveway or Intersection Curve & Level Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Curve & Level	No Vulnerable Road User Involved	Disregard Traffic Control Device/Sign Under Influence of Alcohol/Drugs/Medication Inattention Societing	Dark-Street Lights On W Daylight Di Dark-Street Lights On Di	et At Driveway or Intersection et At Driveway or Intersection et Not at Intersection/Driveway/Roundabout by Not at Intersection/Driveway/Roundabout by Not at Intersection/Driveway/Roundabout by Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication Inattention Speeding	No Yes Yes Yes
7885 1055613.79 916222.4 201 7886 1055562.36 916223.23 201 7887 1055470.35 916222.69 201 7888 1056828.66 964663.71 201	17 2 Vehicles/Parties Rear-End	Minor or Possible Injury Other/Unknown 35 Property Damage Only Other/Unknown Ur Property Damage Only Inattention 25 Property Damage Only Inattention Ur	14 No	Wet Daylight Wet Daylight Dry Daylight Wet Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Straight & Level Not at Intersection/Driveway/Roundabout Straight & Level At Driveway or Intersection Straight & Grade Not at Intersection/Driveway/Roundabout Curve & Grade	No Vulnerable Road User Involved	Other Contributing Circ Not Listed Other Contributing Circ Not Listed Inattention Inattention	Daylight W Daylight W Daylight D Daylight D Doylight D Doylight W	et Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout ry At Driveway or Intersection et Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Inattention Inattention	No Yes No Yes
7889 1056833.82 964799.89 201 7890 1056813.17 964880.73 201 7891 1056652.2 965399.93 201 7892 1056785.89 964592.72 201	18 1 Vehildel/Party Struck Object 15 1 Vehildel/Party Struck Object 15 1 Vehildel/Party Struck Object 14 1 Vehildel/Party Struck Object	Minor or Possible Injury Under Influence of Alcohol/Drugs/Medication Ur. Suspected Serious Injury Apparently Asleep, Fatigued or III 35 Minor or Possible Injury Apparently Asleep, Fatigued or III 35 Property Damage Only Inattention Ur.	ler 25 Under Influence of Alcohol and Apparently Asleep, Fatigued or III Under 25 44 Apparently Asleep, Fatigued or III 35-44 45 Apparently Asleep, Fatigued or III 35-44 6er 25 No 9-7	Dry Dark-Street Lights On Wet Dark-Street Lights On Dry Dark-Street Lights On Wet Dark-Street Lights On	Not at Intersection/Driveway/Roundabout Curve & Grade Not at Intersection/Driveway/Roundabout Curve in Sag Not at Intersection/Driveway/Roundabout Curve & Grade Not at Intersection/Driveway/Roundabout Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Under Influence of Alcohol/Drugs/Medication Apparently As leep or Fatigued Apparently As leep or Fatigued Inattention	Dark-Street Lights On Di Dark-Street Lights On W Dark-Street Lights On Di	y Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout	Under Influence of Alcohol/Drugs/Medication Apparently Asleep, Fatigued or III Apparently Asleep, Fatigued or III	Yes Yes Yes Yes
7894 1056732.45 964701.93 201 7895 1056229.72 963853.83 201 7896 1056185.05 963327.3 201 7897 1056244.15 963257.4 8 201	15 Vehicle-Party Struck Object	Property Damage Only OfterUnknown Ur Property Damage Only Speeding 45 Mnor or Possible Injury Speeding 35 Property Damage Only Inattention 25	14 No 14 No 16 No 16 No 16 No 16 No 16 No 17 No 17 No 18 No	Wet Dark-Street Lights On loe/Snow/Slush Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Curve & Level Not at Intersection/Driveway/Roundabout Curve & Grade Not at Intersection/Driveway/Roundabout Curve & Grade Not at Intersection/Driveway/Roundabout Curve & Grade Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Speeding Speeding Inattention	Daylight Ice/Snor Daylight Di	et Not at Intersection/Driveway/Roundabout w/Slush Not at Intersection/Driveway/Roundabout by Not at Intersection/Driveway/Roundabout	Speeding Speeding Inattention	Yes Yes Yes Yes
7898 1056121.51 963008.48 201 7899 1056085.29 963002.29 201 7900 1056133.24 963024.42 201	15 Vehicle Party Fire started in vehicle	Property Damage Only Inattention Ur Property Damage Only Other/Unknown 45 Minor or Possible Injury Under Influence of Alcohol/Drugs/Medication Property Damage Only Oner Center Line Ur	nown No	Dry Dark-No Street Lights/Street Lights Off Dry Daylight Dry Daylight Dry Daylight Dry Daylight Dry Dark-No Street Lights/Street Lights Off	Not at Intersection/DrivewayRoundabout Curve & Grade Not at Intersection/DrivewayRoundabout Straight & Grade At Driveway or Intersection Curve & Grade	No Vulnerable Road User Involved	None Inattention None Under Influence of Alcohol/Drugs/Medication	Dark-No Street Lights/Street Lights Off Dr. Daylight Off Dr. Daylight Street Lights/Street Lights Off Dr.	ny Not at Intersection/Driveway/Roundabout ny Not at Intersection/Driveway/Roundabout ny At Driveway or Intersection	No Risky Driving Behaviors Involved inattention inattention No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication No Risky Driving Rehaviors (involved	No Yes No Yes
7902 1056012.09 962706.53 201 7903 1055993.45 962603.42 201 7904 1055990.78 962603.42 201	15 1 Vehicle/Party Struck Object 14 1 Vehicle/Party Struck Object 17 1 Vehicle/Party Arimal	Property Damage Only Other/Unknown 35 Mnor or Possible Injury Speeding 65 Property Damage Only Other/Unknown 55	14 No No Io	Dry Daylight loe/Snow/Slush Daylight Dry Daylight	Not at Intersection/Driveway/Roundabout Curve & Grade Not at Intersection/Driveway/Roundabout Curve & Grade Not at Intersection/Driveway/Roundabout Curve & Grade	No Vulnerable Road User Involved Older Road User 65+ Motorcycle	Other Contributing Circ Not Listed Speeding None	Daylight Doylight Doy	w/Slush Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Speeding No Risky Driving Behaviors Involved Speeding No Risky Driving Behaviors Involved	Yes Yes No
	18 2 Vehicles Paries Rear-End	Minor or Possible Injury Speeding 65 Property Damage Only Speeding 25 Minor or Possible Injury Speeding 65 Property Damage Only Other/Unknown 25	14 No	Dry Daylight cos/Snow/Slush Dawn/Dusk lcos/Snow/Slush Daylight lcos/Snow/Slush Daylight lcos/Snow/Slush Daylight Dry Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Curve & Grade	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Speeding	Daylight Ice/Snov	y Not at Intersection/Driveway/Roundabout WSlush Not at Intersection/Driveway/Roundabout WSlush Not at Intersection/Driveway/Roundabout WSlush Not at Intersection/Driveway/Roundabout by Not at Intersection/Driveway/Roundabout by Not at Intersection/Driveway/Roundabout	inamenon Speeding Speeding Speeding No Risky Driving Behaviors Involved	Yes Yes Yes
7910 1056712.78 961143.09 201 7911 1056850.4 961067.57 201 7912 1057410.59 960923.46 201	14 1 Vehicle/Party Struck Object	Property Damage Only Speeding 35 Property Damage Only Speeding Ur Property Damage Only Improper Turn/Merge 25 Property Damage Only Other/Unknown 65	14 No lo	ice/Snow/Slush Dark-No Street Lights/Street Lights Off Wet Day/ight Dry Day/ight Wet Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Curve & Grade Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Grade Not at Intersection/Driveway/Roundabout Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 66-	Speeding Speeding Improper Backing/Passing/Turning/Parking/Signaling None	Dark-No Street Lights/Street Lights Off lice/Sno Daylight W Daylight Di Dark-No Street Lights/Street Lights Off W	wSlush Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Speeding No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	Yes Yes No No
7914 1057793.72 960844.77 201 7915 1057924.14 960815.24 201 7916 1059154.8 960553.4 201	18 2 Vehicles/Parties Rear-End	Minor or Possible Injury Speeding Ur Property Damage Only Other/Unknown 25 Suspected Serious Injury Other/Unknown 65	ler 25 No	Dry Dawn/Dusk Wet Dark-No Street Lights/Street Lights Off Dry Daylight	Not at Intersection/DrivewayRoundabout Straight & Grade Not at Intersection/DrivewayRoundabout Straight & Grade Not at Intersection/DrivewayRoundabout Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved Motorcycle and Older Road User 65+	Speeding None Other Contributing Circ Not Listed	Dawn/Dusk Dr. Dark-No Street Lights/Street Lights Off W Daylight Dr.	y Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout	Speeding No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No No Yes
7918 1059206.29 960542.88 201	19 2 Vehicles/Parties Struck Parked Vehicle 17 2 Vehicles/Parties Turning	Property Damage Only Improper Passing Ur	No ler 25 No	Dry Dark-Street Lights On Dry Daylight	Not at Intersection/Driveway/Roundabout Straight & Grade At Driveway or Intersection Straight & Grade	Older Road User 65+ No Vulnerable Road User Involved	None Improper Backing/Passing/Turning/Parking/Signaling	Daylight Di	y At Driveway or Intersection	No Risky Driving Behaviors Involved	No

OBJECTID JURISDICTION	COUNTY CITY	PRIMARY BLOCK TRAFFICWAY NUMBER	INTERSECTING DIST ML.or COMP. DIR REFERENCE TRANSPORTAL REPORT POINT, MALE MILEPOST IS SR. OM. Y REPORT POINT, MAKE MILEPOST IS SR. OM. Y REPORT NAME.	T DATE TIME MOST_SEVER	E NUM NUM NUM NUM NUM NUM E INJ FAT VEH PEDS BIKES	VEH_1_TYPE	VEH_2_TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD_SURFACE ROADWAY CHARACTERISTIC	LIGHTING CONDITION	RRST_COLLISION TYPE_OBJECT_STRUCK	VEH_1 ACTION	VEH_2 ACTION	VEH.1.COMPASS VEH.1.COMPASS VEH.2.COMPASS VEH.2.COMPASS
7919 State Route 7920 State Route 7921 State Route 7921 State Route 7922 State Route 7923 State Route 7924 State Route 7925 State Route 7925 State Route 7925 State Route	Jefferson 10 Jefferson 10	14	1.66 No E8678 1.91 No E9050 2.00 No F5220	0 2018-11-26 11:16 No Apparent Injury 4 2019-03-23 20:02 No Apparent Injury 9 2016-10-02 19:44 No Apparent Injury	0 0 1 0	Truck Tractor & Semi-Trailer Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Grade Dry Straight & Grade Dry Straight & Grade	Daylight Over Roadway I Dark-No Street Lights Vehicle Strikes Dark-No Street Lights Vehicle Strikes	Power Lines Deer Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead		East West East West Fact West
7922 State Route 7923 State Route 7924 State Route	Jefferson 10 Jefferson 10 Jefferson 10	14 14	2.10 No E9730	8 2019-10-19 18:16 No Apparent Injury	0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast Raining	Dry Curve & Grade Dry Curve & Grade	Daylight Vehicle Strikes Dark-No Street Lights Vehicle Strikes	Deer Deer	Going Straight Ahead Going Straight Ahead		West East West East East East East East East
7925 State Route 7926 State Route 7927 State Route	Jefferson 10 Jefferson 10	14				Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining Overcast Clear or Partly Cloudy	Wet Curve & Grade Wet Straight & Grade Dry Straight & Loyel	Dark-No Street Lights Mscellaneous Dark-No Street Lights Domestic anim Daylight From same dire Daylight Vehicle Strikes	al other (cat, dog, etc) sction - both going straight - both moving - rear-end Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	West East East West East West Maret Care
7926 Soate Route 7927 Soate Route 7928 Soate Route 7928 Soate Route 7930 Soate Route 7930 Soate Route 7930 Soate Route 7931 Soate Route 7932 Soate Route 7932 Soate Route 7932 Soate Route 7933 Soate Route 7934 Soate Route	Jefferson 10 Jefferson 10 Jefferson 10	14 14	3.03 No E4932 3.03 No E4932 3.05 No 2744 9 3.05 No 2744	2019-12-04 11:24 No Apparent Injury 2015-08-11 11:05 No Apparent Injury 2017-06-25 14:41 No Apparent Injury 2014-10-26 20:40 No Apparent Injury 2014-03-16 06:40 No Apparent Injury 2014-03-16 06:40 No Apparent Injury	0 0 2 0	Passenger Car Passenger Car Pickup, Panel Truck or Vanette under 10,000 lb	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast Clear or Partly Cloudy	Dry Straight & Grade Wet Straight & Level	Daylight From opposite Dark-No Street Lights Vehicle Strikes		Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	versi casi East West East West East West East West
7931 State Route 7932 State Route	Jefferson 10 Jefferson 10	14	3.07 No E-006 3.19 No E-440 3.36 No E-4887	2 2015-07-16 17:06 No Apparent Injury 2 2015-07-16 17:06 No Apparent Injury 3 2015-11-19 05:35 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	lce Straight & Grade Dry Straight & Grade lce Straight & Grade	Daylight Fire started in v		Going Straight Ahead Going Straight Ahead		
7934 State Route 7935 State Route	Jefferson 10 Jefferson 10	14	3.46 No E5309 3.63 No E9360	3 2016-03-31 04:40 No Apparent Injury B 2019-06-29 15:03 Suspected Serious	0 0 1 0 njury 2 0 2 0	Truck Tractor & Semi-Trailer Pickup,Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade	Dark-No Street Lights Guardrail - Face Daylight From same dire	ection - all others	Going Straight Ahead Going Straight Ahead Making U-Tum	Going Straight Ahead	
7935 State Route 7936 State Route 7937 State Route 7938 State Route	Jefferson 10 Jefferson 10 Jefferson 10	4 4 4	3.87 No E683 3.88 No E603 3.96 No E4887	5 2017-05-03 16:20 No Apparent Injury 2 2018-06-11 18:21 No Apparent Injury 9 2015-11-28 12:34 No Apparent Injury	0 0 1 0 0 0 1 0 0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Ownerset	Dry Straight & Grade Dry Straight & Grade Wet Straight & Grade	Daylight Fire started in v Daylight Roadway Ditch Daylight One car leaving	parked position	Making U-Turn Illegally Parked, Occupied Going Straight Ahead Merging (Entering Traffic)	Going Straight Ahead	East West East West West East West East West East West East Ea
7936 Saste Route 7930 Saste Route 7940 Saste Route 7940 Saste Route 7941 Saste Route 7942 Saste Route 7942 Saste Route 7943 Saste Route 7944 Saste Route 7944 Saste Route 7945 Saste Route 7945 Saste Route 7946 Saste Route 7946 Saste Route	Jefferson 10 Jefferson 10 Jefferson 10	4 4 4	4.20 No E8829 4.25 No E5898 4.31 No E6834	1 2016-09-05 17-41 Possible Injury 3 2016-10-16 04:30 No Apparent Injury 2 2015-11-14 19:20 No Apparent Injury 5 2016-08-07 11:13 Possible Injury 1 2017-02-24 14:00 No Apparent Injury	2 0 2 0 0 0 1 0 0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Raining Raining Clear or Partly Cloudy	Dry Straight & Grade Standing Water Curve in Sag Standing Water Straight in Sag	Daylight From same dire Dark-Street Lights On Guardrail - Face Dark-Street Lights On Guardrail - Face	ccton - both going straight - both moving - rear-end 2 2 3 3 5 5 6 7 7 8 7 8 8 8 8 8 8 8 8 8 8 8 8 8 8 8	Going Straight Ahead Making Left Turn	Slowing	East West East West West East West West East
7942 State Route 7943 State Route 7944 State Route	Jefferson 10 Jefferson 10 Jefferson 10	44	4.34 No E5799 4.34 No E6899 4.34 No E7364	5 2016-08-07 11:13 Possible Injury 1 2017-02-24 14:00 No Apparent Injury 9 2017-11-17 12:50 Possible Injury	0 0 2 0 1 0 1 0	Passenger Car Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related At Intersection and Related At Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Overcast		Daylight From same dire Daylight Entering at angi Daylight Over Embankm Dark-Street Lights On Tree or Stump (Going Straight Ahead Making Left Turn Slowing Going Straight Ahead	Stopped for Traffic Making Left Turn	West East Vehicle Stopped Vehicle Stopped North East West North West North North
7945 State Route 7946 State Route 7947 State Route	Jefferson 10 Jefferson 10 Jefferson 10	14 14	4.34 No E7537 4.34 No E9275 4.34 No E9876	9 2017-11-17 12:50 Possible Injury 3 2017-12-24 20:11 Possible Injury 7 2019-05-13 14:40 Suspected Serious 9 2019-11-21 04:40 Died in Hospital	njury 3 0 2 0 0 1 1 0	Passenger Car Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Not Related At Intersection and Not Related At Intersection and Not Related	Snowing Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade	Daylight From opposite	direction - both moving - head-on	Going Straight Ahead	Going Straight Ahead	West East West East East West East West
7947 State Route 7948 State Route 7949 State Route 7950 State Route 7951 State Route	Jefferson 10 Jefferson 10 Jefferson 10 Jefferson 10	14 14	4.34 No E9906 4.36 No E7270 4.43 No E6453	9 2019-11-21 04:40 Died in Hospital 2 2019-12-06 19:03 Suspected Minor Inj 7 2017-10-16 06:45 Possible Injury 8 2017-02-13 06:04 Suspected Minor Inj 8 2018-11-26 03:54 No Apparent Injury 2 2017-12-24 18:04 No Apparent Inj	10 1 0 1 0 1 1 1 1 1 1 1 1 1 1 1 1 1 1	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight in Sag Dry Straight & Grade Ice Straight & Grade	Dark-Street Lights On Guardrail - Thro Dark-Street Lights On Earth Bank or L Daylight From same dire Dark-No Street Lights Guardrail - Thro Dark-No Street Lights Vehicle Strikes	edge secion - all others ugh, Over or Under	Going Straight Ahead Going Straight Ahead Making U-Turn Overtaking and Passing	Going Straight Ahead	East West West West East West
7951 State Route 7952 State Route 7953 State Route	Jefferson 10 Jefferson 10 Jefferson 10	14 14				Prassenger Car Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining Raining Clear or Partly Cloudy				Going Straight Ahead Going Straight Ahead		East West East West West East
7952 State Route 7953 State Route 7954 State Route 7955 State Route 7956 State Route	Jefferson 10 Jefferson 10 Jefferson 10	14	4.81 No E7717 4.89 No E9462 5.00 No E5412	2 2018-07-15 12:23 Suspected Minor Inj 7 2018-02-21 19:12 No Apparent Injury 9 2019-08-01 15:40 No Apparent Injury 7 2016-05-01 15:34 Suspected Serious	0 0 1 0 0 0 2 0 niury 1 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Truck Tractor Motorcycle	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Snowing Clear or Partly Cloudy Clear or Partly Cloudy	Snow/Slush Straight & Grade Dry Straight & Grade Dry Straight & Level	Daylight Guardrail - Face Dark-No Street Lights Roadway Ditch Daylight Other Objects Daylight Vehicle overturn	ed bei	Overtaking and Passing Going Straight Ahead Going Straight Ahead Overtaking and Passing	Going Straight Ahead	East West East West West East East West East
7956 State Route 7957 State Route 7958 Sate Route 7958 Sate Route 7959 State Route 7959 State Route 7960 State Route 7961 State Route 7961 State Route 7963 State Route 7963 State Route 7963 State Route 7963 State Route	Jefferson 10 Jefferson 10 Jefferson 10	14 14	5.00 No E7537 5.20 No E9699 5.25 No E8699	7 2016-05-01 15:34 Suspected Serious 0 2017-12-24 18:15 Possible Injury 5 2019-10-12 19:54 No Apparent Injury 7 2018-10-16 14:45 No Apparent Injury	1 0 2 0 0 0 1 0 0 0 2 0	Passenger Car Dickup Panel Truck or Vanette under 10 000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Truck (Flatbad,Van,etc)	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Snowing Overcast Clear or Partly Cloudy	Snow/Slush Straight & Grade Dry Straight & Grade Dry Curve & Grade	Dark-No Street Lights From same dire Dark-No Street Lights Vehicle Strikes Daylight From opposite	sed oction - both going straight - one stopped - rear-end Deer direction - both going straight - sideswipe	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic Going Straight Ahead	East West East Vehicle Stopped West East West West West East East West
7960 State Route 7961 State Route 7962 State Route	Jefferson 10 Jefferson 10	14				Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining Sleet or Hail or Freezing Rain Clear or Partic Clouds	Wet Curve & Grade	Dark-No Street Lights Roadway Ditch Daylight From opposite Dark-No Street Lights Vehicle Strikes		Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped in Roadway	West East East West Vehicle Stopped Vehicle Stopped East West
7963 State Route 7964 State Route	Jefferson 10 Jefferson 10	14	5.54 No E4999 5.95 No E9982	7 2015-03-31 14:16 No Apparent Injury 9 2015-10-24 19:00 No Apparent Injury 1 2015-12-22 11:05 No Apparent Injury 4 2019-11-17 14:38 No Apparent Injury	0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb	Danasana Cas	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast	Dry Straight & Grade	Daylight Over Embankm Daylight Guardrail - Face	ent - No Guardrali Present	Going Straight Ahead Going Straight Ahead	Clausian	West West West East
7964 State Route 7965 State Route 7966 State Route 7967 State Route 7967 State Route	Jefferson 10 Jefferson 10	14	6.09 No E9111 6.11 No E3338	5 2019-10-16 15:13 No Apparent Injury 5 2019-04-10 19:01 No Apparent Injury 6 2014-12-30 00:00 Unknown	0 0 1 0	Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Cal	At Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Unknown Straight & Grade	Unknown Vehicle Strikes		Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing	Cabst Vest East Vest East East East East East East East Ea
7969 State Route 7970 State Route 7971 State Route	Jefferson 10 Jefferson 10 Jefferson 10	14	6.19 No E3900 6.21 No E9973 6.30 No E3951 6.00 No E3951		1 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Raining Fog or Smog or Smoke Overcast Clear or Partly Cloudy	Wet Straight & Grade Wet Straight & Grade Plant	Dark-No Street Lights Vehicle Strikes Dark-No Street Lights Roadway Ditch Dark-No Street Lights From same dire Dark-No Street Lights Vehicle Strikes	Deer action - both going straight - both moving - rear-end Deer Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	East West East West East West Meet East West
7972 State Route 7973 State Route 7973 State Route	Jefferson 10 Jefferson 10 Jefferson 10 Jefferson 10	4	6.90 NO E55/8	2016-06-27 09:28 No Apparent injury	0 0 1 0	Pickup,Panei Truck or vanette under 10,000 ib		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Fog or Smog or Smoke	Dry Straight & Level	Dark-Street Lights On Utility Pole		Going Straight Ahead Going Straight Ahead		Labit West East North South
7947 State Roote 7949 State Roote 7940 State Roote 7970 State Roote 7970 State Roote 7971 State Roote 7971 State Roote 7972 State Roote 7973 State Roote 7974 State Roote 7974 State Roote 7975 State Roote 7976 State Roote 7977 State Roote 7977 State Roote 7977 State Roote 7978 State Roote 7988 State Roote	Jefferson 10 Jefferson 10 Jefferson 10	14	7.00 No E6983 7.37 No E9833 7.50 No E7207	2017-07-24 18:28 No Apparent Injury 8 2018-09-25 23:55 No Apparent Injury 8 2017-10-07 21:12 Suspected Serious	0 0 1 0 0 0 1 0 njury 3 0 1 0	Prickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup Panel Truck or Vanette under 10.000 b Pickup Panel Truck or Vanette under 10.000 b Plassenger Cat Passenger Cat Passenger Cat Pickup Panel Truck or Vanette under 10.000 b Not Stated	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Overcast	Dry Straight & Level Dry Straight & Level Dry Curve & Level	Daylight Fire started in v Dark-No Street Lights Vehicle Strikes Dark-No Street Lights Tree or Stump (ahicle Deer stationary)	Going Straight Ahead Going Straight Ahead Going Straight Ahead		
7977 State Route 7978 State Route 7979 State Route	Jefferson 10 Jefferson 10 Jefferson 10	14	7.61 No E8047 7.67 No E4666 7.70 No E3384	3 2018-06-01 11:30 No Apparent Injury 2 2015-09-23 14:26 Dead at Scene 2 2014-09-20 15:07 Possible Injury	0 0 1 0 3 1 2 0 3 0 2 0	Prickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Curve & Grade Dry Straight & Level	Daylight From opposite Daylight From opposite		Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead Going Straight Ahead	West East East West East East West East
7980 State Route 7981 State Route 7982 State Route	Jefferson 10 Jefferson 10 Jefferson 10	14	7.89 No E7707 7.90 No E4850 7.91 No E3688	2018-02-18 15:30 No Apparent Injury 2 2015-07-19 17:33 Suspected Minor Inj 2 2014-10-19 08:45 No Apparent Injury	o 0 1 0 o o o o o o o o o o o o o o o o	ртскир,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Passenger Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level	Daylight Roadway Ditch Daylight From same dire	ection - both going straight - one stopped - rear-end ehicle All Other Non-Domestic Animal	Going Straight Ahead Going Straight Ahead	Stopped for Traffic	West East Vehicle Stopped Vehicle Stopped East West We
7983 State Route 7984 State Route 7985 State Route	Jefferson 10 Jefferson 10 Jefferson 10	14 14	8.00 No E9675 8.12 No E8569 8.17 No E8610	4 2019-10-05 21:23 No Apparent Injury 0 2018-10-27 14:45 Suspected Minor Inj 4 2018-10-27 16:11 No Apparent Injury	ory 1 0 2 0 0 1 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Not Stated	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast Overcast	Dry Straight & Level Dry Straight at Hillor Wet Straight & Grade	Daylight From opposite	All Other Non-Domestic Animal direction - both moving - head-on scion - both going straight - one stopped - rear-end	Going Straight Ahead Overtaking and Passing Stopped in Roadway	Going Straight Ahead Going Straight Ahead	West East East West East Vehicle Stopped Vehicle Stopped East East
7986 State Route 7987 State Route 7988 State Route	Jefferson 10 Jefferson 10 Jefferson 10	14 14	8.22 No E703	2017-08-21 17:31 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Truck Tractor & Semi-Trailer Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy Overcast	Wet Straight & Grade Dry Straight & Grade Wet Straight & Grade	Dank-No Street Lights From same dire	ction - both going straight - both moving - sideswipe ction - both going straight - both moving - sideswipe	Overtaking and Passing Overtaking and Passing Going Straight Ahead	Going Straight Ahead Overtaking and Passing	East West East West East West East West East West West
7989 State Route 7990 State Route 7991 State Route	Jefferson 10 Jefferson 10	4 4	8.57 No E5712 8.81 No E6932 8.85 No E6932	2 2016-07-19 17-56 No Apparent Injury 8 2017-07-16 12:45 Possible Injury 8 2019-09-17 15:16 No Apparent Injury 9 2014-01-08 12:48 No Apparent Injury 5 2014-05-08 17-33 Suspected Moor Injury	1 0 2 0	Motorcycle Dickup Panel Truck or Vanette under 10 000 lb	Motorcycle Pickup Panel Truck or Vanette under 10 000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Grade Dry Straight & Level	Daylight Guardrail - Face	1	Going Straight Ahead	Going Straight Ahead Stopped at Signal or Stop Sign	West East West East East West North Vehicle Stopped North Vehicle Stopped
7992 State Route 7993 State Route 7994 State Route	Jefferson 10 Jefferson 10	14	8.87 No E2988 8.87 No E3278 9.67 No E3278	9 2014-01-08 12:48 No Apparent Injury 5 2014-05-08 17:33 Suspected Minor Inj 0 2014-07-23 11:46 Suspected Minor Inj	0 0 2 0 ury 3 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related	Raining Raining	Wet Straight & Level Wet Straight & Grade	Daylight Entering at ang	le le	Making U-Turn Going Straight Ahead Making Left Turn Making Left Turn Going Straight Ahead	Going Straight Ahead Going Straight Ahead	North East West East North East East West
7988 Sate Route 7990 Sate Route 7990 Sate Route 7990 Sate Route 7991 Sate Route 7992 Sate Route 7992 Sate Route 7992 Sate Route 7994 Sate Route 7994 Sate Route 7995 Sate Route 7996 Sate Route 7996 Sate Route	Jefferson 10 Jefferson 10	14	6.87 No E3434 6.87 No E3594 8.87 No E3967	5 2014-05-08 17:33 Suspected Mnor Inj 0 2014-07-23 11:46 Suspected Mnor Inj 3 2014-09-16 14:50 Suspected Serious 7 2015-01-23 16:15 Possible Injury	njury 2 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Motorcycle Passenger Car	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Overcast	Wet Straight & Grade Dry Straight & Grade Wet Curve & Level	Dusk Entering at ang	le le	Going Straight Ahead Making Left Turn Making Right Turn	Stopped at Signal or Stop Sign Going Straight Ahead Slowing	East West Vehicle Stopped Vehicle Stopped North East East West East North North South
7997 State Route 7998 State Route 7999 State Route 8000 State Route 8001 State Route	Jefferson 10 Jefferson 10 Jefferson 10	14	0.5/ NO E4:25 8.87 No E4:40 0.87 No E4:58 0.87 No E4:58	2 2015-05-08 15:00 No Apparent Injury 4 2015-07-03 13:38 No Apparent Injury 9 2015-08-25 17:44 No Apparent Injury	0 0 2 0	Passenger Car Passenger Car Passenger Car Pickup Panel Truck or Vanette under 10,000 lb Passenger Car Pickup Panel Truck or Vanette under 10,000 lb Dissenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Car.	At Intersection and Not Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Grade Dry Straight & Grade Dry Straight & Grade	Daylight Entering at ang	sction - all others le le sction - one left turn - one straight	Making U-Turn Making Left Turn Making Left Turn	Going Straight Ahead Going Straight Ahead Going Straight Ahead Making Left Turn	West Yvest West North East East West North East East West
8000 State Route 8001 State Route 8002 State Route	Jefferson 10 Jefferson 10 Jefferson 10	4 4 4	B.87 No E4732 8.87 No E4741 8.87 No E4754	1 2015-10-20 16:00 No Apparent Injury 4 2015-10-01 20:04 Suspected Serious 5 2015-10-26 09:29 No Apparent Injury	njury 2 0 2 0 0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Raining	Dry Straight & Level Dry Straight & Level Wet Straight & Grade	Daylight From same dire Dark-Street Lights On From opposite Daylight Entering at ang	direction - one left turn - one straight le	Slowing Making Left Turn Making Left Turn	Going Straight Ahead Going Straight Ahead	North South North East West North East West North East West
8002 State Route 8003 State Route 8004 State Route 8005 State Route	Jefferson 10 Jefferson 10 Jefferson 10	14 14	8.87 No E4927 8.87 No E4929 8.87 No E5146	4 2015-12-08 15:51 Suspected Minor Inj 4 2015-12-08 16:19 No Apparent Injury 1 2016-02-08 16:33 Suspected Minor Inj	1 0 2 0 0 0 2 0 1ry 2 0 3 0	Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Truck (Flatbad, Van, etc) Pickup, Panel Truck or Vanette under 10,000 lb Passenger Car		Raining Raining Clear or Partly Cloudy	Wet Straight & Grade Wet Straight & Grade Dry Straight & Level	Dusk Entering at angi Daylight From opposite	le direction - one left turn - one straight	Making Left Turn Making Left Turn Making Left Turn	Going Straight Ahead Going Straight Ahead Going Straight Ahead	North East West East North Southeast East West West North East West
	Jefferson 10 Jefferson 10 Jefferson 10	14 14	8.87 No E5224 8.87 No E5275 8.87 No E5489	1 2016-03-01 09:35 No Apparent Injury 2016-03-22 11:35 No Apparent Injury 2 2016-05-30 14:19 Possible Injury 5 2016-07-15 16:15 No Apparent Injury 5 2016-11-24 13:00 No Apparent Injury	0 0 1 0 0 0 2 0 1 0 2 0	Dickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Not Related At Intersection and Related	Raining Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Grade Dry Straight & Grade Dry Straight & Grade Dry Straight & Grade	Daylight From same dire Daylight Entering at ang	ection - all others	Making Left Turn Making U-Turn Making Left Turn Making Left Turn	Going Straight Ahead Going Straight Ahead	North East East East East East Worth East East East West West
8019 State Route 8010 State Route 8011 State Route	Jefferson 10 Jefferson 10 Jefferson 10 Jefferson 10	14 14	8.87 No E5955 8.87 No E5175 8.87 No E6296	 2016-07-15 16:15 No Apparent Injury 2016-11-24 13:00 No Apparent Injury 2016-12-28 10:15 Suspected Serious 	0 0 2 0 0 0 2 0 njury 2 0 2 0	Not Stated Passenger Car Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Raining Clear or Partly Cloudy	Wet Straight & Level	Daylight Entering at ang	ie Ie	Making Right Turn	Making Left Turn Stopped at Signal or Stop Sign Going Straight Ahead	West North North East East North Vehide Stopped Vehide Stopped North East West East
8013 State Route	Jefferson 10 Jefferson 10 Jefferson 10	14 14	8.87 No E6568 8.87 No E6569 8.87 No E6569	5 2017-03-30 04:55 Suspected Serious 9 2017-03-24 11:04 Suspected Serious 4 2017-04-29 02:50 Suspected Minor Inj	njury 1 0 1 0 njury 1 0 2 0 ury 1 0 1 0		Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb		Raining Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Grade Wet Straight & Grade Dry Straight & Grade Dry Straight & Level	Daylight Entering at ang Dark-Street Lights On Wood Sign Pos	le .	Making Left Turn Making Right Turn Making Left Turn Going Straight Ahead	Going Straight Ahead	East North North East East West North South
8015 State Route 8016 State Route 8017 State Route	Jefferson 10 Jefferson 10 Jefferson 10	14 14	8.87 No E6775 8.87 No E7414	4 2017-05-29 15:59 Suspected Minor Inj 0 2017-11-26 14:04 Suspected Serious 2 2018-04-07 15:30 No Apparent Injury	njury 1 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Not Stated Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related	Clear or Partly Cloudy Other Raining	Dry Straight & Grade Wet Straight & Grade Wet Straight & Grade	Daylight Entering at ang Daylight Entering at ang	ie ie edge	Making Left Turn Making Left Turn Going Straight Ahead	Going Straight Ahead Going Straight Ahead	North East East West North East West East West East West East
8018 State Route 8019 State Route 8020 State Route	Jefferson 10 Jefferson 10 Jefferson 10	14	8.87 No E8416 8.87 No E8876 8.87 No E8877 8.87 No E9379	5 2018-09-22 22:20 No Apparent Injury 8 2019-01-18 20:06 No Apparent Injury 8 2019-07-07 11:09 No Apparent Injury 9 2019-07-01 12:12 No Apparent Injury 8 2019-08-26 19:49 No Apparent Injury 8 2019-08-26 19:49 No Apparent Injury	0 0 1 0 0 0 2 0 0 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Not Stated	At Intersection and Not Related At Intersection and Related At Intersection and Related	Raining Overcast Overcast	Dry Straight & Grade	Dark-Street Lights On Metal Sign Post Dark-Street Lights On From same dire Daylight Roadway Ditch	ction - both going straight - both moving - rear-end	Going Straight Ahead Slowing Going Straight Ahead Making Left Turn	Going Straight Ahead	East West East West East West West East West
8021 State Route 8022 State Route 8023 State Route	Jefferson 10 Jefferson 10	14	8.87 No E9403 8.87 No E9403 8.87 No F9503	3 2019-07-13 12:12 No Apparent Injury B 2019-08-26 19:49 No Apparent Injury 1 2017-07-23 20:34 No Apparent Injury	0 0 2 0 0 0 0 1 0 0 0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Other Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Grade Dry Straight & Grade Dry Straight & Grade	Daylight Entering at ang Dark-Street Lights On Metal Sign Post Dusk From same dire	le : : : scrion - hoth going straight - hoth moving - rear-end	Making Left Turn Going Straight Ahead Going Straight Ahead	Going Straight Ahead	North
80118 State Route 80118 State Route 80129 State Route 8020 State Route 8021 State Route 8022 State Route 8024 State Route 8025 State Route 8026 State Route 8027 State Route	Jefferson 10 Jefferson 10 Jefferson 10 Jefferson 10	14	8.97 No E9695 8.99 No E9253 9.00 No E4244	3 2019-10-01 19:42 No Apparent Injury 3 2018-07-31 16:35 Suspected Serious 6 2015-10-16 21:15 No Apparent Injury	0 0 2 0 njury 1 0 2 0	Pickips Pierell Track or Vanette under 10,000 th. Passenger Care Not Stasted Melonoptis Not Stasted Melonoptis Track Tracket & Semi-Trainer Passenger Car Track Tracket & Semi-Trainer	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb		Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Grade Dry Straight & Grade	Dusk From same dire Dark-No Street Lights From same dire Dark-No Street Lights Vehicle Strikes	sccion - both going straight - both moving - rear-end close of both going straight - both moving - sideswipe caccion - both going straight - both moving - sideswipe close - both going straight - both moving - sideswipe close - both going straight - both moving - sideswipe close - both going straight - both moving - sideswipe - beer	Changing Lanes Changing Lanes Going Straight Ahead	Going Straight Ahead Going Straight Ahead	West East West East West East West East West Fast East East
8028 State Route	Jefferson 10 Jefferson 10	14	9.00 No E7362 9.00 No E7362 9.00 No E7362	6 2016-08-01 14:30 No Apparent Injury 0 2017-11-13 05:30 No Apparent Injury 9 2015-07-05 18:00 No Apparent Injury	0 0 2 0	Truck Tractor & Semi-Trailer Passenger Car	Truck Tractor & Semi-Trailer Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy Raining Clear or Partly Cloudy			sction - both going straight - both moving - sideswipe Deer sction - both going straight - both moving - rear-end	Overtaking and Passing Going Straight Ahead Going Straight Ahead	Going Straight Ahead	East West East West East West East West East West East West
8030 State Route	Jefferson 10 Jefferson 10	4	9.01 No E4532 9.03 No E3673	7 2015-08-17 14:27 No Apparent Injury 3 2014-03-24 21:36 No Apparent Injury	0 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Bus or Motor Stage Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Grade	Daylight From same dire Dark-Street Lights On Vehicle Strikes	ection - all others	Backing Going Straight Ahead	Stopped for Traffic	Vehicle Backing Vehicle Backing Webicle Stopped Vehicle Stopped East West West Stopped Vehicle
8031 State Route 8032 State Route 8033 State Route 8034 State Route	Jefferson 10 Jefferson 10 Jefferson 10 Jefferson 10	4	9.12 No E9999 9.20 No E3470 9.20 No E3470 9.20 No E3470	1 2019-01-01 07:00 No Apparent Injury 1 2014-07-27 20:59 No Apparent Injury 2 2014-07-27 20:59 No Apparent Injury	0 0 1 0	Truck Tractor & Semi-Trailer Passenger Car	Trough, and much or various dross 10,000 so	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Dry Straight & Grade	Dark-No Street Lights Vehicle Strikes Dark-No Street Lights Vehicle Strikes	octon - both going straight - both moving - sideswipe Deer Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	East West East West
8036 State Route 8037 State Route	Jefferson 10 Jefferson 10 Jefferson 10 Jefferson 10	14	9.27 No E960-5 9.27 No E914-6 9.45 No E414-1	2015-11-28-06.04 No Apparent Injuly 2019-04-26 04:19 Suspected Serious 4 2015-07-05 17:04 Possible Injury	njury 1 0 1 0	Motorcycle	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade	Dark-Street Lights Off Vehicle Strikes Dark-No Street Lights Vehicle Strikes Daylight From same dire	Deer Deer scton - both going straight - one stopped - rear-end Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped in Roadway	West East West East Vehicle Stopped Vehicle Stopped
8039 State Route	Jefferson 10 Jefferson 10 Jefferson 10	14	9.57 No E7095 9.57 No E7095 9.70 No E6008	2 2017-08-30 13:30 No Apparent Injury 7 2016-10-19 20:10 No Apparent Injury	0 0 2 0	Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related	Clear or Partly Cloudy Raining	Dry Straight & Grade Wet Straight & Grade	Daylight From same dire Dark-No Street Lights Vehicle Strikes	crear- cotion - both going straight - both moving - rear-end Deer direction - both moving - head-on	Going Straight Ahead Going Straight Ahead Overtaking and Passing	Slowing	East West East West North South
8042 State Route 8043 State Route	Jefferson 10 Jefferson 10	14	9.70 No E7160 9.77 No 35011 9.80 No E7200 9.80 No E7200	2017-09-10 UZ:34 Suspected Serious 2016-11-15 09:34 Suspected Minor Inj 2017-10-08 05:59 Possible Injury 3018-07-30 201	nyony 1 U 2 0 ny 2 0 2 0 1 0 1 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Truck & Trailer	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Raining Overcast	Wet Straight & Grade	Daylight From opposite	direction - both going straight - sideswipe	Overtaking and Passing Going Straight Ahead	Going Straight Ahead Going Straight Ahead	West East West East West East Ford Management West East East West East East East East West East East East East East East East Ea
8043 State Route 8044 State Route 8045 State Route 8045 State Route 8046 State Route 8047 Crists Portin	Jefferson 10 Jefferson 10 Jefferson 10	14	9.90 No E931 9.85 No E3310 9.85 No E4532	2018-07-29 13:55 Possible Injury 2014-12-04 18:45 No Apparent Injury 2015-08-13 13:36 No Apparent Injury	1 0 1 0 0 0 1 0 0 0 2 0	Prickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Intersection and Related At Driveway	Clear or Partly Cloudy Raining Clear or Partly Cloudy	Dry Straight & Grade	Dark-No Street Lights Roadway Ditch Daylight Roadway Ditch Dark-No Street Lights Roadway Ditch Daylight From same dire	ction - both going straight - one stopped - rear-end	Going Straight Ahead Making Right Turn Going Straight Ahead	Stopped for Traffic	East West East North West East Vehicle Stopped Vehicle Stopped
Oune Route	Jefferson 10 Jefferson 10 Jefferson 10	14	9.87 No E4887	3 2015-11-20 05:45 No Apparent Injury	0 0 1 0	Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Overcast	Dry Straight & Grade Dry Straight & Grade	Daylight From same dire Dark-No Street Lights Miscellaneous	oction - both going straight - one stopped - rear-end Object or Debris on Road	Going Straight Ahead Going Straight Ahead	Stopped for Traffic	West East Vehicle Stopped Vehicle Stopped East West East West
8050 State Route 8051 State Route 8052 State Route	Jefferson 10 Jefferson 10 Jefferson 10	14 14		5 2017-10-23 06:50 No Apparent Injury 0 2015-07-19 17:31 No Apparent Injury 8 2016-03-08 21:14 No Apparent Injury 3 2018-02-22 05:39 No Apparent Injury			Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy Snowing	ice Straight & Grade	Daylight Roadway Ditch	Deer ction - both going straight - both moving - rear-end Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing	West East West East East West East
8991 State Route 8992 Sale Route 8993 Sale Route 8993 Sale Route 8993 Sale Route 8994 Sale Route 8994 Sale Route 8995 Sale Route 8996 Sale Route 8996 Sale Route 8998 Sale Route	Jefferson 10 Jefferson 10 Jefferson 10	14 14	10.15 No E5935 10.30 No E8302 10.33 No E4259	1 2016-09-19 14:41 Suspected Serious 1 2018-08-17 12:19 Suspected Minor Inj 0 2015-05-17 00:30 No Apparent Injury	njury 1 0 1 0 ury 2 0 2 0 0 0 1 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Truck Tractor & Semi-Trailer	-	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Overcast	Wet Curve & Grade Dry Straight & Level Wet Curve & Grade	Daylight Guardrail - Face Daylight Jackknife Traile Dark-No Street Lights Guardrail - Face	r r	Going Straight Ahead Going Straight Ahead Going Straight Ahead		East West West East East East West
8056 State Route 8057 State Route 8058 State Route	Jefferson 10 Jefferson 10 Jefferson 10 Jefferson 10 Jefferson 10	14	10.43 NO E4.00 10.36 NO E3.00 10.40 No E7.00 10.40 No E5.00 10.52 No E6.00	6 2014-12-11 22:27 No Apparent Injury 6 2017-10-28 20:58 No Apparent Injury 4 2017-06-16 23:10 No Apparent Injury	0 0 1 0	Truck Tractor & Semi-Trailer Passenger Car Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Other Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Wet Curve & Grade Dry Straight & Level Dry Straight & Level	Dark-No Street Lights Dark-No Street Lights Dark-No Street Lights Vehicle Strikes Dark-No Street Lights Miscellaneous Daylight From same dire	y vehicle (on the road) Deer Colject or Debris on Road	Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East East East West East East
8059 State Route 8060 State Route 9061 State Route	Jefferson 10	14	10.52	0 2017-09-03 17:30 No Apparent Injury 2 2014-02-18 17:54 No Apparent Injury 9 2018-02-08 16:41 Possible Injury	0 0 2 0	Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Truck Truckor & Sami,Truiter	Passenger Car Pickup Panel Truck or Vanette under 10 000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Overcast	Wet Straight & Grade	Dark-No Street Lights Roadway Ditch		Slowing U-Turn	Going Straight Ahead	East
8062 State Route 8063 State Route 8064 State Pourte	Jefferson 10 Jefferson 10 Jefferson 10 Jefferson 10	14	11.00 No E3470	0 2014-08-02 17:04 Possible Injury 0 2018-05-22 16:08 Suspected Minor Inj	2 0 2 0 ary 1 0 2 0	Passenger Car Motorcycle	Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Greel Dry Straight & Grade Dry Straight & Grade	Daylight From opposite Daylight From opposite Daylight From opposite	action - both going straight - both moving - rear-end direction - one left turn - one straight direction - both going straight - one stopped - sideswipe	Going Straight Ahead Making Left Turn Going Straight Ahead Overtaking and Passing	Slowing Going Straight Ahead Stopped for Traffic Making Right Turn	East South West East West East Vehicle Stopped Vehicle Stopped West East South
8065 State Route 8066 State Route 8067 State Route	Jefferson 10 Jefferson 10 Jefferson 10 Jefferson 10	14	11.29 No E6905 11.30 No E6925	9 2017-06-30 22:18 Unknown 8 2018-08-05 10:42 No Apparent Injury	0 0 1 0	Passenger Car	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Entering at angl Dark-No Street Lights Guardrail - Face Daylight Vehicle Strikes Dark-No Street Lights Vehicle Strikes		Overtaking and Passing Other* Going Straight Ahead Going Straight Ahead	Making Right Turn	East West East West East
8053 State Rocke	Jefferson 10 Jefferson 10 Jefferson 10	14	11.40 No E4387 11.43 No E8392 11.45 No E3474	5 2018-09-11 16:49 No Apparent Injury 5 2014-07-31 10:20 No Apparent Injury 7 2014-07-31 10:20 No Apparent Injury	0 0 1 0	Passenger Car	Pickup Panel Truck or Vanette under 10,000 lb Passenger Car Pickup Panel Truck or Vanette under 10,000 lb Not Stated Pickup Panel Truck or Vanette under 10,000 lb	Intersection Related but Not at Intersection At Intersection and Related At Intersection and Related	Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Grade Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight From same dire	ection - both going straight - both moving - rear-end le		Slowing Going Straight Ahead Going Straight Mead	West
8071 State Route 8072 State Route 8072 State Route	Jefferson 10 Jefferson 10 Jefferson 10 Jefferson 10	14	11.45 No E4412 11.45 No E6905 11.45 No E6988	7 2015-07-05 18:38 No Apparent Injury 5 2017-06-06 10:40 Possible Injury 8 2017-07-30 14:10 No Apparent Injury 7 2017-07-23 19:05 Suspected Serious	1 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb Not Stated Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Straight & Level Dry Straight & Level Dry Straight & Level	Daylight Entering at ang	ie ie ie	Making Left Turn Going Straight Ahead Going Straight Ahead Making Left Turn Etwa Left Turn	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	North
8074 State Route 8075 State Route	Jefferson 10 Jefferson 10 Jefferson 10 Jefferson 10	4	11.45 No E6988	7 2017-07-23 19:05 Suspected Serious	njury 2 0 3 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Passenger Car	Passenger Car	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Grade Dry Straight & Level	Dark-No Street Lights From opposite Daylight Metal Sign Post	direction - one left turn - one straight	Stopped at Signal or Stop Sign Making Left Turn Making Right Turn Making Left Turn	Going Straight Ahead	Vehicle Stopped Vehicle Stopped East West East South West East West South
8076 State Route 8077 State Route 8078 State Route	Jefferson 10 Jefferson 10 Jefferson 10	14					Motorcycle Other	At Intersection and Related At Intersection and Related At Intersection and Related At Intersection and Not Related	Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level	Daylight Entering at angle Daylight Entering at	e e		Going Straight Ahead Going Straight Ahead Making Left Turn	East West
8079 State Route 8080 State Route 8081 State Route	Jefferson 10 Jefferson 10 Jefferson 10	14 14	11.46 No E4599 11.48 No E6997 11.55 No E6054	2015-09-09 00:01 No Apparent Injury 0 2017-07-05 20:45 No Apparent Injury 0 2016-10-26 10:18 Suspected Serious	0 0 1 0 0 0 1 0 njury 2 0 2 0	Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb		Clear or Partly Cloudy Clear or Partly Cloudy Raining	Dry Straight & Level Dry Straight & Level Wet Straight & Level	Daylight From same dire Daylight From same dire	ection - both going straight - both moving - sideswipe	Overtaking and Passing Going Straight Ahead Going Straight Ahead Overtaking and Passing	Going Straight Ahead	East West East West East
8082 State Route 8083 State Route 8084 State Route	Jefferson 10 Jefferson 10 Jefferson 10 Jefferson 10	14 14	11.77 No E6010 11.85 No E4756 11.90 No E8505	3 2016-10-20 10:32 No Apparent Injury 9 2015-10-27 09:39 Possible Injury 6 2018-11-26 07:45 No Apparent Injury	0 0 1 0 1 0 2 0 0 0 1 0	Passenger Car Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining Overcast Raining	Wet Straight & Level Dry Straight & Level Wet Straight & Level	Daylight Earth Bank or L Daylight From same dire Daylight Vehicle Strikes	edge ection - both going straight - one stopped - rear-end Deer	Overtaking and Passing Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic	East West East Vehicle Stopped Vehicle Stopped West East
8085 State Route 8086 State Route 8087 State Route	Jefferson 10 Jefferson 10 Jefferson 10 Jefferson 10	14 14 14	12.00 No E3393 12.00 No E5633 12.00 No E5678	9 2014-07-23 11:27 No Apparent Injury 7 2016-07-06 13:45 No Apparent Injury 9 2016-07-31 16:08 No Apparent Injury	0 0 2 0 0 0 2 0 0 0 2 0	Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	Pickup Panel Truck or Vanette under 10,000 b Pickup Panel Truck or Vanette under 10,000 b Passenger Car Passenger Car Passenger Gar Pickup Panel Truck or Vanette under 10,000 b Pickup Panel Truck or Vanette under 10,000 b Pickup Panel Truck or Vanette under 10,000 b Truck Tractor & Semi-Trailer	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Wet Straight & Level Dry Straight & Grade Dry Straight & Level	Daylight From same dire Daylight From opposite	ection - both going straight - one stopped - rear-end direction - all others	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped at Signal or Stop Sign Going Straight Ahead Slowing	West East Vehide Stopped Vehide Stopped East West East East West East West West
OUOD State Route	Jellerson	14 14	12.00 No E6878 12.00 No E7160 12.00 No E8909	1 2017-06-30 13:07 Possible Injury 8 2017-09-19 14:18 Suspected Serious 9 2018-06-17 20:09 Suspected Minor Ini	7 0 4 0 njury 2 0 2 0 ary 1 0 1 0	Passenger Car Passenger Car Motorcycle	Pickup,Panel Truck or Vanette under 10,000 lb Truck Tractor & Semi-Trailer	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast	Dry Straight & Level Dry Straight & Level	Daylight From opposite	ection - both going straight - both moving - rear-end cotion - both going straight - both moving - rear-end direction - both moving - head-on red		Slowing Going Straight Ahead	West East West East East West East East West East
8091 State Route 8092 State Route 8093 State Route	Jefferson 10 Jefferson 10 Jefferson 10 Jefferson 10	14	12,20 No 28000 12,20 No 2768 12,21 No 2768 12,22 No 23,33 12,22 No 23,33 12,25 No 23,33 No 25,33 No 25,33	8 2017-09-19 14:18 Suspected Serious 9 2018-06-17 20:09 Suspected Minor Inj 3 2015-06-25 07:58 No Apparent Injury 8 2014-07-21 08:14 Dead at Scene 1 2017-12-01 12:58 No Apparent Injury	0 0 5 0	Truck Tractor & Semi-Trailer Truck Tractor & Semi-Trailer	Pickup,Panel Truck or Vanette under 10,000 lb Truck Tractor & Semi-Trailer	Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Overcast	Dry Straight & Level Dry Straight & Level Dry Straight & Level Dry Straight & Level Wet Straight & Grade	Daylight Guardrail - Lear Daylight From opposite	ding End direction - all others	Going Straight Ahead Going Straight Ahead Going Straight Ahead Making U-Turn	Going Straight Ahead Going Straight Ahead	West
8093 State Route 8094 State Route 8095 State Route 8096 State Route 8096 State Route 8097 State Route 8098 State Route	Jefferson 10 Jefferson 10 Jefferson 10 Jefferson 10 Jefferson 10	14	1226 No E7314 12.15 No E7340 12.25 No E600 12.49 No E6000 12.71 No E7104	1 2017-12-01 12:58 No Apparent Injury 8 2017-08-13 13:20 Suspected Serious 6 2017-06-27 14:48 Suspected Minor Inj 8 2017-09-28 19-38 Possible Injury	njury 1 0 2 0	Motorcycle Pickup,Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb	Pickup.Panel Truck or Vanette under 10,000 lb Passenger Car Pickup.Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry Straight & Level Dry Straight & Level Dry Cross & Level	Daylight From same dire Daylight From same dire Daylight From same dire Dark-No Street Lights Vehicle Strikes	scision - both going straight - both moving - rear-end scison - both going straight - one stopped - rear-end Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing Stopped for Traffic	
8097 State Route 8098 State Route	Jefferson 10 Jefferson 10	14	12.71 NO E7188 12.79 No E6018 12.79 No E9227	2 2016-10-21 17:10 No Apparent Injury 9 2019-05-14 15:19 Suspected Serious	0 0 1 0 njury 1 0 3 0	Pickup,Panel Truck or Vanette under 10,000 lb	Pickup,Panel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Intersection Related but Not at Intersection	Raining Clear or Partly Cloudy	Wet Curve & Level Dry Curve & Level	Daylight Roadway Ditch Daylight From same dire		Going Straight Ahead Going Straight Ahead	Stopped for Traffic	West East Vehicle Stopped Vehicle Stopped

TID MV_DRIVER_CONTRIBUTING MV_DRIVER_CONTRIBUTING GROUMSTANCE 1_UNIT_1 CROUMSTANCE 2_UNIT_1	MV_DRIVER_CONTRIBUTING MV_DRIVER MV_CRIVER AGE GE GE UNT_1 UNT_1 UNT_1	V_DRIVER GENDER	MV_DRIVER_CONTRIBUTING MV_DRIVER_CONTRIBUTING MV_DRIVER_CONTRIBUTING CRCUMSTANCE_1_UNIT_2 CRCUMSTANCE_2_UNIT_2	MV_DRIVER	AV_DRIVER BICYCLIST_CONTRIBUTING BICYCLIST_CONTRIBUTING CIRCUMSTANCE_1_UNIT_1 CIRCUMSTANCE_2_UNIT_1	BICYCLIST_CONTRIBUTING AGE	T BICYCLIST BICYCLIST_CONTRIBUTING BICYCLIST GENDER CRCUMSTANCE_1_UNIT_2 CIRCUMST	r_contributing	BICYCLIST_CONTRIBUTING BICYCLIST CIRCUMSTANCE 3_UNIT_2 AGE_UNIT	ST BICYCLIST PEDESTRIAN_CONTRIBUTION	G PEDESTRIAN_CONTRIBUTING	i PEDESTRIAN_CONTRIBUTING PEDESTRIAN PEDESTRIAN CIRCUMSTANCE_3_UNIT_2 AGE_UNIT_2 GENDER_UNIT_2	FRST_IMPACT_LOCATION City St. County Rd & Misc 2010 forward. St
None None None None None None None None	UNT_1 UNT_1 48 Male 90 Male 68 Fema	UNIT_1	GROUND (ARC) _ GRAT _ L	AGE ONL'E	UNT_2	UNIT_1	UNIT_1 GROUND PARSE_I_GRAFIE	7.402_1_041_1	GROUND FARE SOUTH AND SOUT		GIROGIIDI AROLLI LORINI		ane 1 Decreasing Miepost ane 1 Decreasing Miepost
None	52 Fema	male											ane 1 increasing Milepost
None None	44 Male 58 Fema	le le male le No											ane 1 Increasing Milepost ane 1 Decreasing Milepost ane 1 Increasing Milepost ane 1 Decreasing Milepost
Under Influence of Alcohol None Operating Defective Equipment				76 66	emale emale								
None	60 Fema 56 Male 62 Male	male No male le											ane 1 Decreasing Milepost ane 1 Decreasing Milepost ans 1 Decreasing Milepost ass Right Shoulder Increasing Milepost and 1 Increasing Milepost
Exceeding Reas. Safe Speed None Other Contribution Clar Nat Listed	60 Male 34 Male	le le le le le male No											ane 1 Increasing Milepost ast Right Shoulder Increasing Milepost ane 1 Decreasing Milepost ight Shoulder Increasing Milepost
Other Contributing Circ Not Listed Other Contributing Circ Not Listed Apparently Asleep or Fatigued	74 Male Did Not Grant RW to Vehicle	male No le	lone	28	ale								ane 1 Decreasing Milepost
Apparently Asleep or Fatigued Did Not Grant RW to Vehicle	63 Male 63 Fema	nale No	ione	49									ight Shoulder Decreasing Miepost ass Right Shoulder Increasing Miepost ane 1 Increasing Miepost
Hatterstein Follow Too Clorely Other Contributing Circ Not Listed Exceeding Ross. Safe Speed Operating Defective Equipment Exceeding Ross. Safe Speed Hatterstein Improper TumMerge Institute Ins	Over Center Line 62 Male 72 Male	le l	urie	38	TO THE PROPERTY OF THE PROPERT								ane 1 Decreasing Milepost ight Shoulder Decreasing Milepost ight Shoulder Increasing Milepost ane 1 Increasing Milepost
Inattention Improper Turn/Merge Inattention Unknown Distraction	17 Fema 64 Male 92 Fema	male No nale No	ione Ione	58 58	male ale								ane 1 Increasing Miepost ane 1 Decreasing Miepost ast Right Shoulder Decreasing Miepost ast Right Shoulder Decreasing Miepost
Unknown Distraction Exceeding Reas. Safe Speed Under Influence of Drugs Other Contributing Circ Not Listed Exceeding Reas. Safe Speed	75 Male 25 Male 22 Male	male le No	ione	77	ale								ane 1 Increasing Milepost
Other Contributing Circ Not Listed Exceeding Reas. Safe Speed Other Contributing Circ Not Listed Improper 1 Turn Inattention Improper Passing None	71 Male 83 Male	le le No	one	49	ale								ight Shoulder Decreasing Milepost dersecting Road Decreasing Milepost ane 1 Decreasing Milepost
Improper Passing None Exceeding Reas. Safe Speed													ane 1 Decreasing Mlepost ight Shoulder Increasing Mlepost ane 1 Decreasing Mlepost ast Right Shoulder Decreasing Mlepost
None Exceeding Reas: Safe Speed Improper Passing Exceeding Reas: Safe Speed Other Contributing Circ Not Listed	21 Male 16 Male	le le No	lone.	25	amaja.								ight Shoulder Increasing Milepost ass Right Shoulder Decreasing Milepost ane 1 Increasing Milepost
Exceeding Reas, Sale Speed	21 Male 53 Male	le No	cone	43									ane 1 Increasing Milepost ane 1 Decreasing Milepost
None Operating Handheld Cell Phone Under Influence of Alcohol				27	ale								ane 1 Increasing Miepost ane 1 Decreasing Miepost ast Right Shoulder Decreasing Miepost
Folion to Closely	89 Fema 60 Male	male No le male	lone	65	emale								ight Shoulder Increasing Milepost ane I Decreasing Milepost ast Right Shoulder Decreasing Milepost
Other Contributing Circ Not Listed Other Contributing Circ Not Listed Inattention Exceeding Reas. Safe Speed	56 Fema	male		63	iale							1 1 1	ight Shoulder Increasing Milepost
Other Contributing Circ Not Listed	33 Male	le No											ane 1 Decreasing Milepost ast Right Shoulder Increasing Milepost ane 1 Decreasing Milepost ane 1 Decreasing Milepost ane 1 Decreasing Milepost
None Under Influence of Alcohol	30 Male 33 Male 33 Male	le No	lone	19	isle								and * Lobi Ghanish Mepons and * Lobi Ghanish Mepons and * Lobi Ghanish Mepons and * Decreasing Mepons and * Decreasing Mepons and * Lobi Ghanish Mepons and * Lobi Ghanish Mepons
None Other Contributing Circ Not Listed	78 Fema 64 Fema	male male											ane 1 increasing Milepost last Right Shoulder Increasing Milepost
None None Other Contributing Circ Not Listed Other Contributing Circ Not Listed None Other Contributing Circ Not Listed None Exceeding Stated Speed Limit	35 Fema 22 Male 19 Male	male le											ane 1 Decreasing Miepost ane 1 Decreasing Miepost ast Right Shoulder Increasing Miepost
Operating Defective Equipment Other Contributing Circ Not Listed	41 Male 73 Male	le No	ione	53	emale								ast Right Shoulder Decreasing Milepost ane 1 Increasing Milepost
Exceeding Reas. Safe Speed Inattention Exceeding Reas. Safe Speed	Over Center Line 49 Male 38 Male 23 Fema	ie No le male No	one	65	ale emale								ane 1 Decreasing Milepost last Right Shoulder Increasing Milepost
Operating Defective Equipment None Non	51 Male 61 Male Improper Passing 65 Male	male No le le le No	lone	24	ale								ane 1 Increasing Miepost ane 1 Decreasing Miepost ane 1 Increasing Miepost ane 1 Increasing Miepost
Exceeding Stated Speed Limit Non Microstro wrong Side of Road None Instendion Improper Passing Other Contributing Circ Not Listed Did Not Grant RW to Vehicle None	53 Fema 19 Fema	male Ex	xceeding Reas. Safe Speed ther Contributing Circ Not Listed	32	emale								ane 1 Decreasing Milepost ane 1 Decreasing Milepost
Other Contributing Circ Not Listed Did Not Grant RW to Vehicle None Operating Defective Equipment	Improper Passing	male No	centing Ress. Safe Street ther Combising Circ Nat Listed too	50	aie								ane 1 Decreasing Milepost Light Shoulder Decreasing Milepost Light Shoulder Decreasing Milepost
Improper U-Turn Inattention Inattention Follow Too Closely	Did Not Grant RW to Vehicle 72 Male 55 Male	le No nale No	one one	65 70	ale sale emale								ane 1 Decreasing Milepost Mersecting Road Decreasing Milepost
Indirector FundMarge Indirector FundMarge Dear Marge Committee Double Clear Marge Committee Double Clear Marge Committee Double Committee Fedoro Too Consely Under Vindence of Alknob Improper TurnMarge				74	maie ale ale								ane 1 Increasing Milepost ane 1 Decreasing Milepost tensecting Road Decreasing Milepost
Did Not Grant RW to Vehicle Exceeding Reas. Safe Speed Operating Other Electronic Devices (comp Did Not Grant RW to Vehicle		male No male No male No male No		32	ale								tersecting Road Decreasing Milepost ane 1 Decreasing Milepost eft Turn Lane Decreasing Milepost ane 1 Decreasing Milepost ane 1 Decreasing Milepost
Operating Other Electronic Devices (comp Did Not Grant RW to Vehicle Follow Too Closely Under Influence of Alcohol improper Turn/Merge	27 Male 18 Fema	le No male No le No le No	one one	66 61 72 63	male								ane 1 Decreasing Mispost ane 1 Decreasing Mispost firm Lane Decreasing Mispost tensecing Road Decreasing Mispost
Under Influence of Alcohol Improper Turn/Merge Did Not Grant RW to Vehicle	Inattention 32 Male 88 Male	le No	unite lone lone	29	are lale lale								ane 1 Decreasing Milepost
Did Not Grant RW to Vehicle Institution Did Not Grant RW to Vehicle	42 Fema 33 Fema 75 Male	ile No maile No maile No ile No maile No ile No	ione lone lone lone	45 60 82	tale								ane 1 Increasing Milepost ane 1 Decreasing Milepost ane 1 Decreasing Milepost
Improper Turn/Merge Exceeding Reas. Safe Speed Improper U-Turn	76 Fema 62 Male	male le No	ione	26 52									ast Right Shoulder Increasing Milepost ane 2 Decreasing Milepost
Other Contributing Circ Not Listed Improper Turn/Merge Unknown Distraction	81 Fema	male No	one	62 44	ale ale								ane 1 Decreasing Milepost ane 1 Decreasing Milepost tersecting Road Decreasing Milepost
Improper Turn/Merge Other Contributing Circ Not Listed	Did Not Grant RW to Vehicle 83 Fema Over Center Line 63 Fema 52 Fema 52 Fema	male No	ione	76 83	ale								ane 1 Increasing Milepost last Right Shoulder Decreasing Milepost
Under Influence of Alcohol Exceeding Reas. Safe Speed Installmenton Did Not Grant RW to Vehicle Did Not Grant RW to Vehicle Direct Not Distracted Other Not Distracted Did Not Grant RW to Vehicle Direct Not Distracted			one ght Violation: No Lights/Fail to Dim		emale								ane 2 Decreasing Miepost ane 2 Decreasing Miepost ane 2 Decreasing Miepost are 1 Decreasing Miepost are 1 Decreasing Miepost are 1 Decreasing Miepost are 1 Decreasing Miepost by Miepost are 1 Decreasing Miepost are 1 Decreasing Miepost
Driver Not Distracted Other Contributing Circ Not Listed	42 Fema 62 Male	male le											itersecting Road Decreasing Milepost
Other Contributing Circ Not Listed Other Contributing Circ Not Listed Other Contributing Circ Not Listed None Improper Turn Merge Improper Turn Merge Improper Turn Merge Exceeding Ress. Safe Speed	52 Male 41 Fema 22 Male	le Un male le No male	Inknown Distraction	48	emale								ane 1 Decreasing Miepost ast Right Shoulder Increasing Miepost ane 1 Decreasing Miepost
Other Contributing Circ Not Listed Inattention	43 Fema	maie No	lone lone	57 77	male sale								ane 1 Increasing Milepost
Institution Did Not Grant RW to Vehicle None Did Not Grant RW to Vehicle None Did Not Grant RW to Vehicle	36 Fema 50 Male 62 Male	male No le No	CORP.		ale amale								ane 2 Increasing Miepost ane 1 Increasing Miepost ane 1 Increasing Miepost ane 1 Decreasing Miepost
None Follow Too Closely	85 Maie 37 Maie	le No	Cone	38	ale							L	ane 1 Decreasing Milepost ane 1 Increasing Milepost
Inattention Improper Backing None	74 Male 22 Fema 35 Male	nale No	ione	67	ale								ane 1 Increasing Milepost ane 1 Decreasing Milepost ane 1 Increasing Milepost
None None	64 Male 58 Fema 64 Male	male No le No male											ane 1 Decreasing Miepost ane 1 Increasing Miepost ane 1 Decreasing Miepost
None Follow Too Closely More Maren More	64 Male 65 Male 41 Male 42 Fema 22 Fema	le No	lone	35	isle								ane 1 Increasing Milepost ane 1 Increasing Milepost ane 1 Increasing Milepost ane 1 Decreasing Milepost
None Institution Follow Too Closely None	42 Fema 21 Fema 27 Male	male No	lone	57	emale								ane 1 Decreasing Milepost
Notice Under Influence of Alcohol Improper Passing Improper Passing Over Center Line Other Contributing Circ Not Listed Inattention Inattention	21 Male 66 Fema 36 Male	le No male No	one one	54 52	emale lale								ane 1 Increasing Milepost ane 1 Increasing Milepost ane 1 Decreasing Milepost ast Right Shoulder Decreasing Milepost
Other Contributing Circ Not Listed Other Contributing Circ Not Listed Exceeding Reas. Safe Speed Other Crontributing Circ Not Listed Exceeding Reas. Safe Speed Other Driver Distractions Inside Vehicle Follow Too Closely	64 Male 79 Fema	le le male le No le No le male	lone.	34	ale								ast Right Shoulder Increasing Milepost ast Right Shoulder Decreasing Milepost
Follow Too Closely None	18 Male 40 Male 55 Male	le No	lone	34 39	ale								ane 1 increasing Milepost ane 1 Increasing Milepost ane 1 Decreasing Milepost
None Follow Too Closely None	42 Male	le No	lone	32	ale								ane 1 Decreasing Miepost ane 1 Decreasing Miepost ane 1 Decreasing Miepost
None None Two Clasely Notes Tw	72 Fema 73 Fema 74 Fema 74 Fema 74 Fema 74 Fema 75 Fema 75 Fema 75 Fema 75 Fema 75 Fema 75 Mate 75	ie male											ast Right Shoulder Increasing Milepost
Other Contributing Circ Not Listed Exceeding Reas. Safe Speed Distractions Outside Vehicle Over Center Line None	75 Fema 30 Male 51 Male	le le											ight Shoulder Decreasing Milepost anne 1 heneasing Milepost ight Shoulder Increasing Milepost anne 1 heneasing Milepost
None Inattention	19 Male 67 Male 52 Fema	le nale Ins	sattention Follow Too Closely	64	ale								ight Shoulder Decreasing Mlepost ane 1 locreasing Mlepost ane 1 Decreasing Mlepost
Imploper Coronal Industrial Indus	40 Male 57 Male	le No	one										ast Right Shoulder Increasing Milepost
None Did Not Grant RW to Vehicle Improper Passing	68 Fema 34 Male 44 Male	iiaie No le No	unie lone lone	77 62 41 27	ane							1 1 1	ane 1 Increasing Milepost and Thereasing Milepost eft Turn Lane Decreasing Milepost ight Shoulder Increasing Milepost
Under Influence of Drugs Inattention None None	Unknown Distraction 33 Male 19 Fema 39 Male	le male le											ight Shoulder Increasing Milepost ann 1 Increasing Milepost ann 1 Increasing Milepost
Did Not Grant RW to Vehicle In thattention Did Not Grant RW to Vehicle Did Not Grant RW to Vehicle	30 Fema 86 Male	male No le No le No male No	lone cone	23 52	ale								ane 1 Increasing Milepost
Other Contributing Circ Not Listed Inattention Did Not Grant RW to Vehicle Did Not Grant RW to Vehicle	74 Male 59 Fema 59 Fema	male No	UNIVE COMPANY	35									ane 1 Decreasing Miepost
Other Contributing Circ Not Listed Inattention	46 Fema 69 Male Exceeding Reas. Safe Speed 63 Male	male No male No le No le No	lone lone	29 33	ale							į.	ane 1 Decreasing Milepost ane 1 Increasing Milepost ane 1 Increasing Milepost ant Right Shoulder Increasing Milepost ane 1 Decreasing Milepost
Instruction Did Not Grant RW to Vehicle	80 Male 75 Male	le No	ione cone		ale emale								
Improper Passing None None	58 Male 60 Male 32 Fema	le No le No le male	NOTE TO SERVICE TO SER	44									ane I Increasing Milepost ane I increasing Milepost ane I Decreasing Milepost
Exceeding Reas. Safe Speed Did Not Grant RW to Vehicle Exceeding Reas. Safe Speed Exceeding Reas. Safe Speed	Improper Passing	le No	one one	63 53	ale emale								ane 1 Increasing Milepost last Right Shoulder Decreasing Milepost
None Follow Too Closely	58 Fema 17 Male		one	62	emale								ane 1 Decreasing Miepost ane 1 Increasing Miepost ane 1 Increasing Miepost
Operating Defective Equipment Follow Too Closely Institution Exceeding Reas. Safe Speed	44 Male 38 Fema 51 Male	ie No male No le No	one one one	54 25 29	anale emale la								ane 1 Increasing Milepost ane 1 Increasing Milepost ane 1 Increasing Milepost
Institution Exceeding Reas. Safe Speed Other Contributing Circ Not Listed Other Contributing Circ Not Listed Other Contributing Circ Not Listed Institution Circ Not	22 Fema 66 Male	male No	ione	27	ale								ane 1 Increasing Milepost
Other Contributing Circ Not Listed Other Contributing Circ Not Listed Improper U-Turn Apparently III	21 Male 61 Male 42 Male	le No	lone	59	ale								ane 1 Decreasing Miepost ane 1 Increasing Miepost ignis Shoulder Decreasing Miepost ane 1 Decreasing Miepost
Inattention Follow Too Closely Inattention Follow Too Closely None	47 Fema 63 Male 26 Male	male No le No le	lone lone	59 70	ale ale								ane 1 Increasing Milepost ane 1 Increasing Milepost ane 1 Decreasing Milepost
Inattention Follow Too Closely Inattention Exceeding Reas. Safe Speed	27 Male 22 Male	le No	ione	79	ale								aine I Deutsasing emplost and I Deutsasing Mepost and I Increasing Mepost

OBJECTID WA_STATE_PLANE WA_STATE_PLANE SOUTH_X 2010_FORWARD COL			HUM COL CTOR AGE	COL IMPAIRED	COL_AGE COL SURFACE CONDITIONS	COL_LIGHT CONDITIONS	COL LOCATION	COL STUI	IEXT IEXT	Y COL_HUMAN COL FACTOR_Decoded MVIV	COL_LIGHT COL_S CONDITIONS COND Decoded Deco		COL_RISKY BEHAVIOR	Intersection Area_Name Emphasis Area De
7921 1063005.89 959734.77 2016 7922 1063499.29 959560.66 2014 7923 1063428.51 959592.63 2019	1 Vehicle Party	Property Damage Only Other/Unknown	45:54 65+ 65+ 45:54 45:54	No No No No	Dry Dry	Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout	Straight & Grade Straight & Grade Straight & Grade Curve & Grade Curve & Grade	No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	None None None None None	Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off Daylight Dark-No Street Lights/Street Lights Off D	tet Not at Intersection/Driveway/Roundabout ny Not at Intersection/Driveway/Roundabout ny Not at Intersection/Driveway/Roundabout ny Not at Intersection/Driveway/Roundabout ny Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved	No No No No
7927 1067430.8 956671.78 2015	1 Wehide Party	Property Damage Only Other/Unknown Property Damage Only Other/Unknown Property Damage Only Under Influence of Alcoh Property Damage Only Other/Unknown Property Damage Only Operating Defective Equ		No No Under Influence of Alcohol No No	Dry II	Dark-No Street Lights /Street Lights Off Dark-No Street Lights /Street Lights Off Daylight Daylight Daylight Dark-No Street Lights /Street Lights Off	Not at Intersection/Driveway/Roundabout	Straight & Grade Curve & Grade Straight & Grade Straight & Level Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	None None Under Influence of Alcohol/Drugs/Medication None Operating Defective Equipment	Dark-No Street Lights/Street Lights Off W Dark-No Street Lights/Street Lights Off W Daylight W Daylight D Daylight D	ry Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No No No Yes
7931 1068104.04 956170.02 2015	1 Venicle/Party Fire started in Venicle	Property Damage Only Speeding Property Damage Only Speeding Delective Equ Property Damage Only Moro or Possible Injury OtherUnknown Delective Country OtherUnknown	55-64 55-64 uipment 55-64 55-64 25-34	No No No No	Dry Ice/Snow/Slush I	Dawn/Dusk Daylight	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Straight & Grade Straight & Level Straight & Grade	No Vulnerable Road User Involved	None Speeding Operating Defective Equipment Speeding None	Daylight D Dark-No Street Lights/Street Lights Off Ice/Sno	w/Slush Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Speeding No Risky Driving Behaviors Involved Speeding No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No Yes No Yes No
7935 1069919.32 954813.82 (2019) 7936 1070974.05 954040.6 (2017) 7937 1070998.37 954012.93 (2018) 7938 1071428.2 953692.99 (2015)	Vehicles/Parties Turning Vehicle/Party Fire started in vehicle	Property Damage Only Suspected Serious Injury Property Damage Only Fail to Yield	45-54	No Under Influence of Drugs No Apparently Asleep, Fatigued or III No	45-54 Dry 45-64 Dry 55-64 Dry Wet	Daylight	Not at Intersection/Driveway/Roundabout	Straight & Grade	Older Road User 65+ No Vulnerable Road User Involved	Other Contributing Circ Not Listed Under Influence of Mochol/Drugs/Medication Other Contributing Circ Not Listed Apparently Asleep or Fatigued Fail to Yield	Dark-No Street Lights/Street Lights Off D Daylight D Daylight D	ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved Apparently Asleep, Fatigued or III Fail to Yield	Yes No No Yos Yes
7939 1072405.34 953081.26 2016 7940 1072642.02 952967.66 2016 7941 1072910.67 952843.05 2015	2 Vehicles/Parties Rear-End 1 Vehicle/Party Struck Object 1 Vehicles/Party Struck Object 2 Vehicles/Parties Rear-End	Mnor or Possible Injury Inattention Property Damage Only Other/Unknown Property Damage Only Operating Defective Equ Mnor or Possible Injury Inattention Property Damage Only Improper Turn/Merge	25-34	No No No No	Dry Standing Water Standing Water Dry	Daylight Dark-Street Lights On Dark-Street Lights On Daylight	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not Driveway or Intersection	Straight & Grade Curve in Sag Straight in Sag Straight & Grade Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Inattention	Daykight D Dark-Street Lights On Standin Dark-Street Lights On Standin Daykight D	ry Not at Intersection/Driveway/Roundabout g Water Not at Intersection/Driveway/Roundabout g Water Not at Intersection/Driveway/Roundabout ry At Driveway or Intersection	Inattention No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Inattention No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No Yes Yes No
7944 107309159 952762.87 2017 7945 1073074.05 952777.99 2017 7946 1073074.05 952777.39 2019 7947 1073074.05 952777.09 2019	1 Vehicle/Party Over Embankment - No Guardrail Present 1 Vehicle/Party Struck Object 2 Vehicles/Parties Head-On 1 Vehicle/Party Struck Object	Minor or Possible Injury Distracted Driving Minor or Possible Injury Speeding Suspected Serious Injury Under Influence of Alcoh Fatality Other/Unknown	65+ 65+ 65+ hol/Drugs/Medication 25-34 25-34	No No Under Influence of Drugs No	Dry Ice/Snow/Slush 25-34 Dry	Daylight Dark-Street Lights On Daylight Dark-Street Lights On	M Driveway or Intersection M Intersection and Not Related	Curve & Grade Curve & Grade Straight & Grade Straight & Grade	Older Road User 65+	Unknown Distraction Speeding Under Influence of Alcohol/Drugs/Medication Other Contributing Circ Not Listed	Daylight	ny At Driveway or Intersection ny At Driveway or Intersection wiSlush At Intersection and Not Related ny At Intersection and Not Related ny At Intersection and Not Related	Distracted Driving Speeding Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved	Yes Yes Yes
7952 1073665.65 952515.85 2017	2 Vehioles/Parties Turning 1 Vehiole/Party Struck Object 1 Vehiole/Party Animal 1 Vehiole/Party Struck Object	Mnor or Possible Injury Property Damage Only Property Damage Only Property Damage Only Speeding	65+ 45-54 25-34 25-34	No N	Ice/Snow/Slush Wet Ice/Snow/Slush	Daylight Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout	Straight in Sag Straight & Grade Straight & Grade Straight & Grade Straight & Grade	Older Road User 65+ Older Road User 65+ No Vulnerable Road User Involved	Improper Backing/Passing/Turning/Parking/Signaling Improper Backing/Passing/Turning/Parking/Signaling None Speeding	Daylight D Dark-No Street Lights/Street Lights Off Ice/Sno Dark-No Street Lights/Street Lights Off V Dark-No Street Lights/Street Lights Off Ice/Sno	ry At Intersection and Not Related Yellow Not at Intersection/Driveway/Roundabout WSlush Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout WSlush Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Speeding	No Yes No Yes
7956 1076270.19 951367.91 2016	1 Vehicle/Party Overturned	Minor or Possible Injury Property Damage Only Property Damage Only Property Damage Only Suspected Serious Injury Minor or Possible Injury Property Damage Only Other/Unknown Other/Unknown Other/Unknown Other/Unknown	Under 25 Under 25 45-54 Under 25 45-54	NO N		Daylight Dark-No Street Lights/Street Lights Off Daylight Daylight Dark-No Street Lights/Street Lights Off		Straight & Grade Straight & Grade Straight & Grade Straight & Level Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Motorocie No Vulnerable Road User Involved No Vulnerable Road User Involved	Improper Backing/Passing/Turning/Parking/Signaling Speeding Other Contributing Circ Not Listed Speeding Follow Too Closely	Daylight	ny Not at Intersection/Driveway/Roundabout	No Riisky Driving Behaviors Involved Speeding No Riisky Driving Behaviors Involved Speeding Follow Too Closely No Riisky Driving Behaviors Involved	Yes Yes Yes Yes No
7960 1077747.88 950612.04 2016	1 Vehicle/Party Struck Object	Property Damage Only Under Influence of Alcoh Property Damage Only Inattention Property Damage Only Other/Unknown	55-64 35-44 hol/Drugs/Medication 65+ 65+ 55-64	No Under Influence of Acohol No No	65+ Wet	Dark-No Street Lights/Street Lights Off Daylight Dark-No Street Lights/Street Lights Off Daylight Daylight	Not at Intersection/Driveway/Roundabout	Straight & Grade Straight & Grade Curve & Grade Curve & Grade Curve & Grade Straight & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+ No Vulnerable Road User 66+ Older Road User 65+ Older Road User 65+	None Cperating Handheld Cell Phone Under Influence of Alcohol/Drugs/Medication Inattention Other Contributing Circ Not Listed		et Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Distracted Driving Under Influence of Alcohol/Drugs/Medication Inattention No Risky Driving Behaviors Involved	Yes Yes No
7964 1080119.85 948246.79 2019 7965 1080339.63 948020.6 2018 7966 1080652.27 947700.15 2019 7967 1080705.07 947663.64 2014	Vehide/Party	Property Damage Only Other/Unknown Property Damage Only Other/Unknown Property Damage Only Inattention Property Damage Only Speeding Unknown Other/Unknown	65+ 55-64 25-34 25-34 Unknown	No No No No	Dry Dry	Daylight Daylight Daylight	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection and Not Related Not at Intersection/Driveway/Roundabout	Straight & Grade Straight & Level Unknown Straight & Level Straight & Grade	Older Road User 65+ No Vulinerable Road User Involved	Other Contributing Circ Not Listed Other Contributing Circ Not Listed Inattention Speeding Other Contributing Circ Not Listed	Daylight Daylight Daylight Daylight D	let Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout ry At Intersection and Not Related riknown Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Inattention Speeding No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	Yes Yes No Yes No No
7968 1081013.83 947353.22 2015 7969 1081090.23 947276.89 2019 7970 1081424.58 946942.93 2014 7971 1083294.08 945075.55 2017	1 Vehide/Party Animal 1 Vehide/Party Struck Object 2 Vehides/Parties Rear-End 1 Vehide/Party Animal 1 Vehide/Party Animal	Property Damage Only Mnor or Possible Injury Other/Unknown Property Damage Only Property Damage Only Property Damage Only Other/Unknown Property Damage Only Other/Unknown	35-44 25-34 hol/Drugs/Medication 35-44 25-34 85+	No No Under Influence of Alcohol No No No	35-44 Wet Dry	Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off Daylight	Not at Intersection/Driveway/Roundabout	Straight & Level Straight & Grade Straight & Grade Straight & Grade Straight & Level Straight & Level	No Wilnerable Road User Involved No Vilnerable Road User Involved No Wilnerable Road User Involved No Wilnerable Road User Involved Older Road User 68+ Older Road User 68+	Other/Unknown None Under Influence of Alcohol/Drugs/Medication None None	Dark-No Street Lights/Street Lights Off W Dark-No Street Lights/Street Lights Off W Dark-No Street Lights/Street Lights Off W Dark-No Street Lights/Street Lights Off D Dark-No Street Lights/Street Lights Off D	let Not at Intersection/Driveway/Roundabout let Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No Yes No No
7973 1083892.41 944480.63.2015 7974 1084024.91 944349.83.2017 7975 1085421.15 942950.9.2018 7976 1086284.34 942110.81.2017	1 Vehicle/Party Struck Object	Property Damage Only Property Damage Only Property Damage Only Property Damage Only Other/Unknown Property Damage Only Other/Unknown Property Damage Only Other/Unknown Property Damage Only Operating Defective Equ	55-64 35-44 Under 25 Under 25 Jipment 26-64	No No No No No	Dry Dry Dry Dry	Dark-Street Lights On Daylight Dark-No Street Lights/Street Lights Off Dark-No Street Lights Off	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Straight & Level Straight & Level Straight & Level Curve & Level Curve & Level	No Vulnerable Road User Involved	Notice Other Contributing Circ Not Listed Other Contributing Circ Not Listed None None Other Contributing Circ Not Listed Other Contributing Circ Not Listed Other Contributing Circ Not Listed Operating Defective Equipment	Dark-Street Lights On D Daylight D Dark-No Street Lights/Street Lights Off D Dark-No Street Lights/Street Lights Off D	y Not at Intersection/Drivway/Roundabout ny Not at Intersection/Drivway/Roundabout	No Risky Driving Behaviors Involved	Yes No No Yes
7978 1086604.41 941912.5 2015 7979 1086749.74 941849.69 2014 7980 1087711.06 941562.32 2018 7981 1087761.23 941543.32 2015	2 Vehides/Parties Sideswipe 2 Vehides/Parties Sideswipe 1 Vehides/Party Struck Object 3 Vehides/Parties Rear-End	Fatality Apparently Asleep, Fatig Minor or Possible Injury Inattention Property Damage Only Speeding Minor or Possible Injury Inattention	qued or III 65+ 45-54 35-44 Under 25	Apparently Asleep, Fatigued or III No No No No	65+ Dry Dry lce/Snow/Slush Dry	Daylight Daylight Daylight Daylight Daylight	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Curve & Grade Straight & Level Straight & Level Straight & Level	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Apparently Asleep or Fatigued Inattention Speeding Inattention	Daylight D Daylight D Daylight D Daylight Ice/Sno Daylight D	ny Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout w/Slush Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout	Apparently & leep, Fatigued or III Inattention Speeding Inattention	Yes Yes Yes No
7985 1089131.89 941156.97 2018	1 Vehicle/Parties	Property Damage Only Property Damage Only Mnor or Possible Injury Property Damage Only Other/Unknown	sipment 45:54 55:64 65+ 45:54 Under 25	No No No No	Wet Wet	Daylight Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Straight & Grade Straight & Level Straight at Hillcrest Straight & Grade Straight & Grade Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved Cidler Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	None Inattention		ry Not at Intersection/Driveway/Roundabout ny Not at Intersection/Driveway/Roundabout the Not at Intersection/Driveway/Roundabout let Not at Intersection/Driveway/Roundabout to Not at Intersection/Driveway/Roundabout ny Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Speeding No Risky Driving Behaviors Involved Inattention No Risky Driving Behaviors Involved	No No Yes No Yes
7989 1091148.98 940583.3 2016 7990 1092384.08 940229.86 2017 7991 1092590.78 940173.47 2019	1 Vehicle/Party Fire stands in vehicle 1 Vehicle/Party Struck Object 2 Vehicles/Parties Turning 2 Vehicles/Parties Rear-End	Property Damage Only Operating Defective Equ Minor or Possible Injury Improper Turn/Merge Property Damage Only Instention	35-44 65+ Lipment Under 25 65+ 55-64	No No No No	Wet Dry Dry	Dark-No Street Lights /Street Lights Off Daylight Daylight Daylight	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout & Driveway or Intersection	Straight & Grade Straight & Level Straight & Grade Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User 65+ No Vulnerable Road User 66+ Motoroycle and Older Road User 66+ No Vulnerable Road User Involved	None Operating Defective Equipment Improper Backing/Passing/Turning/Parking/Signaling Inattention		tet Not at Intersection/Driveway/Roundabout ny Not at Intersection/Driveway/Roundabout ny Not at Intersection/Driveway/Roundabout ny Ab Driveway or Intersection	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Inattention	Yes No Yes No No No
7993 1092684.83 940146.34 2014 7994 1092684.83 940146.34 2014 7995 1092684.83 940146.34 2014 7996 1092684.18 940144.06 2015	2 \ \text{WhitdessParties} Angle	Property Damage Only Improper Turn/Merge Minor or Possible Injury Fail to Yield Other/Unknown Suspected Serious Injury Fail to Yield Minor or Possible Injury Speeding	65+ Under 25 65+ 65+ 25:34	No No No No	Wet I	Daylight Daylight Daylight	N Driveway or Intersection	Straight & Level Straight & Grade Straight & Grade Straight & Grade Curve & Level	Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+ Motorcycle and Older Road User 65+ No Vulnerable Road User Involved	Improper Turn/Merge Fail to Yeld Other Contributing Circ Not Listed Fail to Yeld Speeding		let At Driveway or Intersection et At Driveway or Intersection et At Driveway or Intersection ry At Driveway or Intersection et At Driveway or Intersection	No Risky Driving Behaviors Involved Fail to Yield No Risky Driving Behaviors Involved Fail to Yield Speeding	No No No No
7997 1092683.87 940145.3 2015 7998 1092684.83 940146.34 2015 7999 1092684.83 940146.34 2015 8000 1092686.67 940144.06 2015 8001 1092684.83 940146.34 2015	2 Vehicles/Parties Turning 2 Vehicles/Parties Angle 2 Vehicles/Parties Angle	Property Damage Only Suspected Serious Injury Under Influence of Nicoh	Under 25 25:34 Under 25 45:54 hol/Drugs/Medication 25:34		Dry Dry Dry	Daylight	N Intersection and Not Related N Driveway or Intersection Driveway or Intersection N Driveway or Intersection N Driveway or Intersection	Straight & Grade Straight & Grade Straight & Grade Straight & Grade Straight & Level Straight & Level	No Vulnerable Road User Involved	Operating Other Electronic Devices (comp	Daylight D Daylight D Daylight D Daylight D Daylight D Dark-Street Lights On D	ny At Intersection and Not Related ny At Driveway or Intersection	Distracted Driving Fail to Yield Fail to Yield Fail to Yield Follow Too Closely Under Influence of Alcohol/Drugs/Medication	No No No No
8002 1092684.83 940146.34.2015 8003 1092682.31 940145.21 2015 8004 1092684.83 940146.34.2015 8005 1092684.83 940146.34.2016 8006 1092684.83 940146.34.2016 8006 1092681.75 940142.33.2016	2 Vehicles/Parties Angle	Property Damage Only Fail to Yield Mnor or Possible Injury Fail to Yield Property Damage Only Fail to Yield Mnor or Possible Injury Inattention	65+ 35-44 25-34 65+ 85+	No No No No No	Wet Wet Wet Dry	Daylight Dawn/Dusk Dawn/Dusk Daylight Daylight	N Driveway or Intersection	Straight & Grade Straight & Grade Straight & Grade Straight & Grade Straight & Level Straight & Grade	Older Road User 65+ No Wilnerable Road User Involved No Wilnerable Road User Involved Older Road User 65+ Older Road User 65+ Older Road User 65+	Fail to Yield Fail to Yield Fail to Yield Inattention Improper Turn/Merge	Daylight V. Dawn/Dusk V.	let At Driveway or Intersection ly At Driveway or Intersection	Fail to Yield Fail to Yield Fail to Yield Fail to Yield Inattention No Risky Driving Behaviors Involved	No No No No
8007 1092684.83 940146.34 2016 8008 1092676.41 940140.2 2016 8009 1092685.78 940142.81 2016 8010 1092684.83 940146.34 2016	2 Vehides/Parties	Property Damage Only Improper Turn/Merge Property Damage Only Improper Turn/Merge Inattention Property Damage Only Other/Unknown Property Damage Only Other/Unknown Property Damage Only Improper Turn/Merge I	55-64 25-34 55-64 65+	No No No No No	Dry Dry Dry Wet	Daylight Daylight Daylight Daylight Daylight	N Intersection and Not Related Driveway or Intersection	Straight & Grade Straight & Grade Straight & Level Straight & Level	No Vulnerable Road User Involved	Improper Backing/Passing/Turning/Parking/Signaling Inattention Other Contributing Circ Not Listed Improper Turn/Merge	Daylight D Daylight D Daylight D Daylight D Daylight W	ny At Intersection and Not Related ny At Drivewayor Intersection y At Drivewayor Intersection let At Drivewayor Intersection let At Drivewayor Intersection	No Risky Driving Behaviors Involved Inattention No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No No No
8014 1092684.83 940146.33 2017	2 Venicles/Parties /Angle 1 Vehicle/Party Struck Object	Suspected Serious Injury Suspected Serious Injury Suspected Serious Injury Minor or Possible Injury Suspected Serious Injury Minor or Possible Injury Suspected Serious Injury Suspected Serious Injury Suspected Serious Injury Fail to Yield	55-64 45-54 45-64 55-64 45-64	No No Under Influence of Alcohol No	Under 25 Dry I	Daylight Dark-Street Lights On Daylight Dark-Street Lights On Daylight Daylight	M Driveway or Intersection	Straight & Grade Straight & Grade Straight & Grade Straight & Level Straight & Level Straight & Grade Straight & Grade	No Vulnerable Road User Involved	Fail to Yield	Dark-Street Lights On W Daylight D	let At Driveway or Intersection ty At Driveway or Intersection	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Fail to Yield Under Influence of Alcohol/Drugs/Medication Inattention Eatl to Yield	Yes No Yes No
8018 1092684.83 940146.34 2018 8019 1092681.02 940142.59 2019 8020 1092684.83 940146.34 2019	1 Vehicle/Party Struck Object	Jaspeched Geriloda Injuly Paria in Federal Property Damage Only Other/Unknown Property Damage Only Other/Unknown Property Damage Only Other/Unknown Property Damage Only Improper Turn/Merge	45-64 35-44 55-64 45-54 35-44 Under 25	No No	Dry I	Dark-Street Lights On Daylight	N Driveway or Intersection N Driveway or Intersection N Intersection and Not Related N Driveway or Intersection N Driveway or Intersection	Straight & Grade Straight & Grade Straight & Grade Straight & Grade	No Vulnerable Road User Involved	Other Contributing Circ Not Listed None	Dark-Street Lights On Daylight D	et At Driveway or Intersection let At Driveway or Intersection let At Intersection and Not Related by At Driveway or Intersection ly At Driveway or Intersection	No Risky Driving Behaviors Involved	Yes Yes No Yes
8022 1092677.73 940144.52 2019 8023 1093034.03 940043.41 2017 8024 1093209.31 93989.69 2019 8025 1093294.15 939989.49 2018	1 Vehicles/Party Struck Object 2 Vehicles/Parties Rear-End 2 Vehicles/Parties Sideswipe 2 Vehicles/Parties Sideswipe 3 Vehicles/Parties Sideswipe	Property Damage Only Other/Unknown Property Damage Only Other/Unknown Property Damage Only Other/Unknown Suspected Serious Injury Inattention	05+ 35-44 65+ 35-44		Dry Dry Dry Dry	Dawn/Dusk Dawn/Dusk Dark-No Street Lights/Street Lights Off	N Driveway or Intersection N Driveway or Intersection Not at Intersection:DrivewayRoundabout Not at Intersection:DrivewayRoundabout Not at Intersection:DrivewayRoundabout Not at Intersection:DrivewayRoundabout	Straight & Grade Straight & Grade Straight & Grade Straight & Grade Straight & Level Straight & Grade	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+ Motorcycle	Other Contributing Circ Not Listed Other Contributing Circ Not Listed	Daylight D Dark-Street Lights On D Dark-Street Lights On D Dawn/Dusk D Dark-No Street Lights/Street Lights Off D Dark-No Street Lights/Street Lights Off D Dark-No Street Lights/Street Lights Off D Daylight D Dark-No Street Lights/Street Lights Off D Daylight D Daylight D Daylight D D	ry At Driveway or Intersection ry At Driveway or Intersection ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Inattention	Yes No Yes
8027 1093344.97 939954.74 2016 8028 1093345.96 939954.45 2017 8029 1093395.75 939940 2015 8030 1193395.75 939940 2015	1 Vehicle/Party Animal 2 Vehicles/Parties Rear-End 1 Vehicles/Parties Rearies	Property Damage Only Inattention	45-54 55-64 65+ 35-44 65+	No No	Dry Wet Dry Dry	Dark-No Street Lights/Street Lights Off Daylight Daylight	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Straight & Grade Straight & Grade Straight & Level Straight & Level Straight & Level	No Wilnerable Road User Involved No Wilnerable Road User Involved Older Road User 65+ No Wilnerable Road User Involved Older Road User 65+ Older Road User 65+	None Fail to Yield None Follow Too Closely Instension	Dark-No Street Lights/Street Lights Off W Daylight D	let Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Fail to Yield No Risky Driving Behaviors Involved Follow Too Closely Inattention	Yes No No
8031 1093498.53 939910.17 2014 8032 1093832.47 939794.44 2016 8033 1093932.35 939746.54 2019 8034 1094303.2 939629.78 2014	1 Vehicle/Party Animal	Property Damage Only Other/Unknown Property Damage Only Fail to Yield Property Damage Only Other/Unknown Property Damage Only Other/Unknown Property Damage Only Other/Unknown	Under 25 35-44 55-64 55-64 55-64	No	Dry Dry Dry	Dark-Street Lights On Daylight Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Straight & Grade Straight & Level Unknown Straight & Grade Straight & Grade	No Vulnerable Road User Involved	None Fail to Yield None None None	Dark-Street Lights On D Daylight D Dark-No Street Lights/Street Lights Off D Dark-No Street Lights/Street Lights Off D Dark-No Street Lights/Street Lights Off W	ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout tet Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Fail to Yield No Risky Driving Behaviors Involved	No Yes No No
8036 1094570.59 939274.76 2019 8037 1095175.7 938540.25 2015 8038 1095241.73 938337.27 2014 8039 1095576.09 938050.74 2017	1 Vehide/Party	Suspected Senous Injury Other/Unknown Inattention Property Damage Only Other/Unknown Property Damage Only Inattention	55-64 35-44 35-44 Under 25	No No No	Dry Dry Dry Dry Dry	Dark-No Street Lights/Street Lights Off Daylight Dark-No Street Lights/Street Lights Off Daylight	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Straight & Grade Straight & Level Straight & Grade Straight & Grade	Motorcycle No Vulnerable Road User Involved	None	Dark-No Street Lights/Street Lights Off D Daylight D Dark-No Street Lights/Street Lights Off D Daylight D	ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout let Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Inattention No Risky Driving Behaviors Involved Inattention	No No No No
8043 1096346.13 937109.29 2017	2 Vehicles/Parties Sideswipe 1 Vehicle/Party Struck Object	Property Damage Only Other/Unknown Suspected Serious Injury Under Influence of Alcoh Minor or Possible Injury University Minor or Possible Injury Other/Unknown Minor or Possible Injury Other/Unknown Serious University Other/Unknown Serious Other	hol/Drugs/Medication Under 25 65+ 35-44 55-64	No Under Influence of Alcohol No No No No	Wet I	Dark-No Street Lights Off Dark-No Street Lights Off Dark-No Street Lights (Street Lights Off Dark-No Street Lights (Street Lights Off Dark-No Street Lights Off Dark-No Street Lights Off Daylight Dark-No Street Lights/Street Lights Off Daylight	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Straight & Grade	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	None Under Inlienne of Alcohol/Drugs/Medicason Improper Backing/Passing/Turning/Parking/Signaling Other Contributing Circ Not Listed Other Contributing Circ Not Listed	Dark-No Street Lights/Street Lights Off D Daylight V Dark-No Street Lights/Street Lights Off V	ny Not at Intersection/Driveway/Roundabout let Not at Intersection/Driveway/Roundabout let Not at Intersection/Driveway/Roundabout ny Not at Intersection/Driveway/Roundabout	Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Spending	Yes Yes Yes Yes
8047 1096581.58 936821.62 2015 8048 1096581.58 936821.62 2015	Vehicles/Parties Rear-End Vehicle/Party Struck Object or Debris On Road	Property Damage Only Speeding Property Damage Only Distracted Driving Property Damage Only Follow Too Closely Property Damage Only Other/Unknown Property Damage Only Follow Too Closely Property Damage Only Follow Too Closely	Under 25 35-44 55-64 55-64	No	Dry Dry Dry	Daylight Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Straight & Grade	No Vulnerable Road User Involved	Follow Too Closely None None	Daylight D Dark-No Street Lights/Street Lights Off D Dark-No Street Lights/Street Lights Off D	ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout	Distracted Driving Follow Too Closely No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No No No
8051 1097017.55 936290.27 2016 8052 1097017.55 936290.27 2018	1 Vehicle/Party Animal	Property Damage Only Other/Unknown Property Damage Only Speeding Suspected Serious Injury Internation Minor or Possible Injury Other/Unknown	55+ 25:34 35:44 65+	No No	Dry Ioe/Snow/Slush I	Dark-No Street Lights/Street Lights Off Daylight	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Straight & Grade Straight & Level Straight & Grade Curve & Grade Straight & Level Curve & Grade	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+	None Speeding Inattention	Daylight Ice/Sno Daylight V Daylight D	ry Not at Intersection/Driveway/Roundabout w/Slush Not at Intersection/Driveway/Roundabout let Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Speeding Inattention No Risky Driving Behaviors Involved	No Yes Yes
8056 1098337.1 934941,042014 8057 1098555.06 934845.88 2017 8058 1099139.75 934682.55 2017 8059 1099548.01 934570.98 2017	1 Vehicle/Party Struck Object or Debris On Road 1 Vehicle/Party Animal Vehicle/Party Struck Object or Debris On Road 2 Vehicles/Parties Turning	Property Damage Only Improper Turn/Merge	25-34 45-54 Under 25 65+ 45-54	No No No No	Dry Dry Dry	Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off Daylight	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Curve & Grade Straight & Level Straight & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 66+ No Vulnerable Road User Involved No Vulnerable Road User Involved	None None Improper Backing/Passing/Turning/Parking/Signaling	Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off Daylight D	ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout	Distracted Driving Behaviors Involved No Risky Driving Behaviors Involved	No No No No
8061 1100082.15 934425.18 2018 8062 1101587.87 934010.39 2014 8063 1101587.87 934010.39 2018 8064 1101639.4 933995.78 2017	2 Vehicles/Parties Angle	Property Damage Only Under Influence of Alcoh Minor or Possible Injury Inattention Minor or Possible Injury Other/Unknown Property Damage Only Improper Passing	55-64 65+ 25-34 35-44	Under Influence of Alcohol No No No No No No	Dry Dry Dry Dry Dry	Daylight Daylight Daylight	Not at Intersection/Driveway/Roundabout & Driveway or Intersection & Driveway or Intersection	Straight & Grade Straight & Level Straight & Level Straight & Grade Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ Motorcycle Motorcycle	Under Influence of Alcohol/Drugs/Medication Inattention Inattention Inattention None Instruction Ins	Dark-No Street Lights/Street Lights Off W Daylight D Daylight D Daylight D Daylight D Daylight D	tet Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout ry At Driveway or Intersection ry At Driveway or Intersection ry At Driveway or Intersection	Under Influence of Alcohol/Drugs/Medication Inattention Inattention No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	Yes No No Yes No
8065 1102927.55 933288.49 2017 8066 1102972.49 933258.59 2018 8067 1103393.61 932968.28 2015 8068 1103558.33 932867.35 2018	1 Vehide/Party Struck Object 1 Vehide/Party Animal 1 Vehide/Party Animal 1 Vehides/Parties Rear-End	Unknown Under Influence of Alcoh Property Damage Only Other/Unknown Property Damage Only Distracted Driving Property Damage Only Fail to Yield	hol/Drugs/Medication 25-34 Under 25 35-44 25-34 65+	Under influence of Drugs No No No No No No	25-34 Dry Dry Dry Dry Dry	Dark-No Street Lights/Street Lights Off Daylight Dark-No Street Lights/Street Lights Off	Not at Intersection.DrivewayRoundabout Vot at Intersection:DrivewayRoundabout not at Intersection:DrivewayRoundabout intersection Related but Not at Intersection at Driveway or Intersection	Straight & Level Straight & Level Straight & Level Straight & Grade Straight & Level	No Wilnerable Road User Involved Older Road User 65+	Under influence of Alcohol/Drugs/Medication None None Distracted Driving Fail to Yield	Dark-No Street Lights/Street Lights Off Daylight D Dark-No Street Lights/Street Lights Off D Dark-No Street Lights/Street Lights Off D Daylight D Daylight D	ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout ry Intersection Related but Not at Intersection ry At Driveway or Intersection	Under Influence of Alcohol/Drugs/Medication No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Distracted Driving Fail to Yield	Yes No No No No No
8072 1103636.7 932817.18 2017 8073 1103636.7 932817.18 2017	2 Vehicles/Parties Angle 2 Vehicles/Parties Angle 3 Vehicles/Parties Angle	Minor or Possible Injury Other/Unknown Property Damage Only Fail to Yield Suspected Serious Injury Other/Unknown	65+ 55-64 55-64 45-54 65+	No	Dry Dry Dry Dry Dry	Daylight Daylight Daylight Daylight Daylight	N Driveway of Intersection N Driveway of Intersection Driveway of Intersection N Driveway of Intersection	Straight & Level Straight & Level Straight & Level Straight & Level Straight & Grade	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+	Inattention Other Contributing Circ Not Listed Fail to Yield Other Contributing Circ Not Listed	Daylight D Daylight D Daylight D	ry At Driveway or Intersection	Inattention No Risky Driving Behaviors Involved Fail to Yield No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No No No
8077 1103631.82 932819.72 2019 8078 1103656.52 932802.33 2015 8079 1103678.14 932788.85 2015	2 Vehicles/Parties Angle 2 Vehicles/Parties 1 Vehicles/Parties 1 Vehicles/Party Fire started in vehicle 1 Vehicles/Party 1 Vehicles/	Property Damage Only Improper Turn/Merge Property Damage Only Under Influence of Alcoh Improper Turn/Merge Property Damage Only Inattention Improper Turn/Merge Inattention Improper Property Damage Only Other/Unknown	65+ 55-64 55-64	No	Dry Dry Wet	Daylight Daylight Dark-No Street Lights/Street Lights Off	N Driveway or Intersection N Driveway or Intersection N Driveway or Intersection N Driveway or Intersection N Intersection and Not Related	Straight & Level	Older Road User 65+ No Vulnerable Road User Involved Mobiroycle and Older Road User 65+ Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved	Under Influence of Alcohol/Drugs/Medication Improper Turn/Merge Inattention	Daylight	ry At Driveway or Intersection ry At Driveway or Intersection ry At Driveway or Intersection et At Driveway or Intersection ry At Intersection and Not Related	No Risky Driving Behaviors Involved Under Influence of Alcohol/Drugs./Medication No Risky Driving Behaviors Involved Inattention No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	Yes No No No No
8080 1103764.57 932732.06 2017 8081 1104070.27 932526.07 2016 8082 1105187.96 932339.14 2016	1 Vehicle/Party Animal 2 Vehicle/Parties Sideswipe 1 Vehicle/Party Struck Object	Property Damage Only Other/Unknown Suspected Serious Injury Property Damage Only Speeding Property Damage Only Speeding Property Damage Only Other/Unknown Other/Unknown	25-34 Under 25 Under 25 Under 25 55-64	Nn	Dry II	Daylight Daylight Daylight	k mersection and vor related Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Straight & Level	No Vulnerable Road User Involved	None Speeding Speeding	Daylight Daylight VI Daylight VI	y Not at Intersection/Driveway/Roundabout to Not at Intersection/Driveway/Roundabout et Not at Intersection/Driveway/Roundabout to Not at Intersection/Driveway/Roundabout y Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Speeding Speeding Speeding No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No Yes Yes
8085 1106393.12 932537.28 2014 8086 1106385.97 932536.09 2016 8087 1106385.97 932536.09 2016 8088 1106385.41 932536.09 2016	2 Vehides/Parties Rear-End 2 Vehides/Parties Sideswipe 2 Vehides/Parties Rear-End 4 Vehides/Parties Rear-End 4 Vehides/Parties Rear-End	Property Damage Only Follow Too Closely Property Damage Only Operating Defective Equ Property Damage Only Follow Too Closely Minor or Possible Injury Inattention	Under 25 sipment 35-44 35-44 45-54	No No No	Dry Dry Dry	Daylight Daylight Daylight Daylight Daylight	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Straight & Level Straight & Grade Straight & Level Straight & Level	No Vulnerable Road User Involved	Follow Toc Closely Operating Defective Equipment Follow Toc Closely Inattention	Daylight W. Daylight D. Daylight D. Daylight D. Daylight D.	let Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout ry Not at Intersection/Driveway/Roundabout	Follow Too Closely No Risky Driving Behaviors Involved Follow Too Closely Inattention	No Yes No No
8090 1106385.97 932536.09 2018 8091 1106445.26 932545.93 2015 8092 1106511.35 932556.89 2014 8093 1106698.74 932587.88 2017	1 Vehide/Party Overturned 5 Vehides/Party Sideswipe 1 Vehides/Party Struck Object 2 Vehides/Parties Sideswipe	Suspected Serious Injury Minor or Possible Injury Other Unknown Property Damage Only Other Unknown Other Unknown Property Damage Only Improper Turn/Merge Improper Turn/Merge	Under 25 65+ Under 25 55-64 35-44	No No No	Dry Dry Dry Wet	Daylight Daylight Daylight Daylight Daylight	Not at Intersection/Driveway/Roundabout	Straight & Level Straight & Level Straight & Level Straight & Level Straight & Grade Straight & Grade	No Vulnerable Road User Involved Motorcycle and Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved	Other Contributing Circ Not Listed Other Contributing Circ Not Listed Other Contributing Circ Not Listed Improper Backing/Passing/Turning/Parking/Signaling	Daylight D Daylight D Daylight D Daylight W Daylight W	ny Not at Intersection/DrivewayRoundabout ry Not at Intersection/DrivewayRoundabout ry Not at Intersection/DrivewayRoundabout ry Not at Intersection/DrivewayRoundabout et Not at Intersection/DrivewayRoundabout	No Risky Driving Behaviors Involved	Yes Yes Yes Yes Yes
8093 1106698.74 932587.98 2017 8094 1107167.25 932665.71 2017 8095 1108937.15 93299.92 2017 8096 1110091.8 93297.89 2017 8097 1110499.15 93286.42 2016 8097 1110498.55 93286.95 2019		Suspected Serious Injury Inattention Minor or Possible Injury Inattention Minor or Possible Injury Other/Unknown Property Damage Only Inattention Suspected Serious Injury Inattention	45-54 55-64 35-44 25-34 Under 25	rso No No No	Dry	Daylight Daylight Dark-No Street Lights/Street Lights Off Daylight	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Intersection Related but Not at Intersection	straight & Level Straight & Level Curve & Level Curve & Level	Motorcycle No Vulnerable Road User Involved No Vulnerable Road User Involved	None Inattention	Daylight Daylight Daylight Dark-No Street Lights/Street Lights Off D	ny Not at Intersection/Driveway/Roundabout ny Not at Intersection/Driveway/Roundabout ny Not at Intersection/Driveway/Roundabout let Not at Intersection/Driveway/Roundabout ny Intersection Related but Not at Intersection	Inattention Inattention No Risky Driving Behaviors Involved Inattention	No No No Yes

8100 State Route	1.00	INTERSECTING DIST FROM REF_POINT FT REF_	POINT_NAME	MILEPOST A HISTORY SUSPENSE INC			M NUM NUM F		VEH_2_TYPE	JUNCTION RELATIONSHIP	WEATHER	ROAD_SURFACE CONDITION	ROADWAY CHARACTERISTIC	LIGHTING CONDITION	RRST_COLLISION TYPE_OBJECT_STRUCK	VEH_1 ACTION	VEH_2 ACTION	VEH_1_COMPASS VEH_1_COM DIRECTION_FROM DIRECTION	PASS VEH_2_COMPASS DIRECTION_FROM DIRECTION_TO
8101 State Route	Jefferson 104 Jefferson 104 Jefferson 104			12.80 No 12.80 No 12.81 No	E706467 2017-08-27 12:15 E344429 2014-07-17 17:58			0 0 Passenger Car Passenger		At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry C	Curve & Level Curve & Level	Dark-No Street Lights Daylight Daylight	rom same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing Stopped for Traffic	West East West East West East	West East West Vehicle Stopped
## 8102 State Route ### 8103 State Route ### 8104 State Route ### 8105 State Route ### 8106 State Route	Jefferson 104 Jefferson 104 Jefferson 104			12.81 No 12.81 No 12.81 No	E404487 2015-02-28 15:10 E544686 2016-05-14 08:33 E957481 2019-09-04 17:50	Possible Injury Suspected Serious Injury No Apparent Injury	1 0 2 1 0 2 0 0 2	0 0 Pickup,Panel Truck or Vanette under 10,000 lb Passenger 0 0 Motorcycle Pickup,Pan 0 0 Passenger Car Pickup,Pan	er Car inel Truck or Vanette under 10,000 lb inel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Related	Overcast Clear or Partly Cloudy		Curve & Level Curve & Level Curve & Level		rom same direction - both going straight - one stopped - rear-end rom same direction - both going straight - one stopped - rear-end rom same direction - both going straight - one stopped - rear-end	Going Straight Ahead	Stopped for Traffic Stopped in Roadway Stopped in Roadway	West East West East West East	Vehicle Stopped Vehicle Stopped Vehicle Stopped Vehicle Stopped West Vehicle Stopped
8105 State Route 8106 State Route 8107 State Route	Jefferson 104 Jefferson 104 Jefferson 104			12.85 No 12.88 No 12.89 No	E568482 2016-07-26 13:40 E756758 2018-01-09 08:59 E702472 2017-08-17 15:42	No Apparent Injury Possible Injury No Apparent Injury	0 0 2	0 0 Motorcycle Pickup Part 0 0 Passenger Car Pickup Part 0 0 Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel 0 0 Passenger Car Passenger 0 0 Passenger Car	nel Truck or Vanette under 10,000 lb er Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Overcast Clear or Partly Cloudy	Dry 0		Daylight Daylight	from same direction - both going straight - one stopped - rear-end from same direction - both going straight - both moving - rear-end from same direction - both going straight - both moving - rear-end fscellaneous Object or Debris on Road	Going Straight Ahead Going Straight Ahead Changing Lanes Going Straight Ahead	Slowing Slowing	West East West West	West East West East
8108 State Route 8109 State Route	Jefferson 104 Jefferson 104			13.00 No 13.00 No	E342205 2014-07-13 19:55 E379723 2014-10-14 08:29 E488708 2015-13-01 08:20	No Apparent Injury Possible Injury Surported Minor Injury	0 0 2	0 Passenger Car 0 Passenger Car 0 0 Passenger Car 0 Pickup Panel Truck or Vanette under 10,000 lb Passenger Car 0 Pickup Panel Truck or Vanette under 10,000 lb Passenger Car 0 Pickup Panel Truck or Vanette under 10,000 lb Passenger Car 0 Pickup Panel Truck or Vanette under 10,000 lb Passenger Car 0 Pickup Panel Truck or Vanette under 10,000 lb Passenger Panel Pa	er Car er Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast	Dry	Straight & Level	Daylight	rom same direction - both going straight - one stopped - rear-end rom same direction - both going straight - both moving - rear-end rom opposite direction - both going straight - sideswipe	Going Straight Ahead Going Straight Ahead Overtaking and Passing	Stopped for Traffic Slowing Going Straight Ahead	West East West East	West Vehicle Stopped West East West East
8111 State Route 8112 State Route 8113 State Route	Jefferson 104 Jefferson 104			13.00 No 13.00 No	E499498 2015-12-2 05:25 E525496 2016-03-14 09:05	No Apparent Injury No Apparent Injury	0 0 1	0 0 Pickup,Panel Truck or Vanette under 10,000 lb 0 0 Pickup,Panel Truck or Vanette under 10,000 lb 0 0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Pickup,Panel Truck or Vanette under 10,000 lb	inel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Overcast	Wet 0	Curve & Grade Curve & Grade	Dark-No Street Lights Daylight	onlice power unice of the control of	Going Straight Ahead Going Straight Ahead	Slowing	West East West East	West East
8114 State Route 8115 State Route	Jefferson 104 Jefferson 104 Jefferson 104			13.00 No 13.01 No 13.06 No	E551956 2016-05-21 21:48 E653408 2017-03-19 18:00 E964817 2019-09-22 20:35	No Apparent Injury No Apparent Injury No Apparent Injury	0 0 1 0 0 2 0 0 1	Closup, Panel Truck or Vanette under 10,000 lb	er Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy Overcast	Dry S			coadway Ditch rom same direction - both going straight - both moving - rear-end allen tree hit by vehicle (on the road)	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing	East West East West East West	East West
8116 State Route 8117 State Route 8118 State Route	Jefferson 104 Jefferson 104			13.07 No 13.07 No	E365880 2014-10-09 20:30 E964816 2019-09-22 20:35 E987540 2019-01-24 08-57	Possible Injury No Apparent Injury No Apparent Injury	1 0 2 0 0 1	0 Passenger Car Passenger 0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb	er Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Dry S	Straight & Level Curve & Grade	Dark-No Street Lights Dark-No Street Lights	rom same direction - both going straight - one stopped - rear-end allen tree hit by whicle (on the road) Hises or Was Struck by Object from the Load of Another Vehicle	Going Straight Ahead Going Straight Ahead Overtaking and Passing	Stopped for Traffic Going Straight Ahead	East West East West	Vehicle Stopped Vehicle Stopped
	Jefferson 104 Jefferson 104			13.14 No 13.16 No	E895684 2019-02-19 14:38 E679948 2017-06-04 17:52	No Apparent Injury No Apparent Injury	0 0 1	0 0 Passenger Car 0 0 Pickup,Panel Truck or Vanette under 10,000 lb	mail Track of Famous Groot To,000 to	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy					Going Straight Ahead	Cong diagnization	East West West East	Lands YFEGS
8121 State Route 8122 State Route 8123 State Route	Jefferson 104 Jefferson 104 Jefferson 104			13.18 No 13.20 No 13.20 No	E869514 2018-12-07 09:00 E606250 2016-11-07 17:40 E708357 2017-09-03 13:01	No Apparent Injury No Apparent Injury No Apparent Injury	0 0 1	O Passenger Car O Pickup-Panel Truck or Vanette under 10,000 lb O Pickup-Panel Truck or Vanette under 10,000 lb O Pickup-Panel Truck or Vanette under 10,000 lb O Passenger Car O Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy	Dry S	Curve & Grade Straight & Grade Straight & Grade	Daylight Dark-No Street Lights Daylight	ine parked-one moving arth Bank or Ledge elsicle Strikes Deer ehicle Strikes Deer	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead		East West East West East West	
8124 State Route 8125 State Route 8126 State Proute	Jefferson 104 Jefferson 104			13.20 No 13.21 No	E964815 2019-09-22 20:35 E716397 2017-09-27 18:55 E936701 2019-07-03 07:38	No Apparent Injury Possible Injury No Apparent Injury	0 0 1	O Passunger Cott O Passunger Car O Pickup Panel Truck or Vanette under 10,000 ib O Pickup Panel Truck or Vanette under 10,000 ib O Pickup Panel Truck or Vanette under 10,000 ib O Pickup Panel Truck or Vanette under 10,000 ib O Pickup Panel Truck or Vanette under 10,000 ib			Overcast Clear or Partly Cloudy	Wet	Curve & Grade Straight & Grade	Dark-No Street Lights	allen tree hit by vehicle (on the road)	Going Straight Ahead Going Straight Ahead Going Straight Ahead		East West West East East West	
8127 State Route 8128 State Route	Jefferson 104 Jefferson 104			13.21 No 13.21 No	E964818 2019-09-22 20:35 E964819 2019-09-22 20:35	No Apparent Injury No Apparent Injury	0 0 1	0 0 Pickup,Panel Truck or Vanette under 10,000 lb 0 0 Pickup,Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related	Overcast Overcast	Wet 0	Curve & Grade Curve & Grade	Dark-No Street Lights Dark-No Street Lights	allen tree hit by vehicle (on the road) allen tree hit by vehicle (on the road)	Going Straight Ahead Going Straight Ahead		East West East West	
8129 State Route 8130 State Route 8131 State Route	Jefferson 104 Jefferson 104 Jefferson 104			13.26 No 13.26 No 13.27 No	E975879 2018-12-23 04:33 E948525 2019-08-04 10:11 E538844 2016-04-19 19:10	No Apparent Injury Suspected Minor Injury	0 0 1	Default Pariel Track or Waterille under 10,000 ib Default Pariel D	inel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Dry C	Straight & Grade Curve & Grade Straight & Grade	Dark-No Street Lights Daylight Daylight	loadway Dilch ehicle Strikes Deer rom same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Going Straight Ahead	East West East West	East West
8132 State Route 8133 State Route 8134 State Route 8135 State Route	Jefferson 104 Jefferson 104 Jefferson 104			13.30 No 13.35 No	E567220 2016-07-19 11:10 E608858 2016-11-09 08:00 E343233 2014-07-18 13-52	No Apparent Injury No Apparent Injury No Apparent Injury	0 0 2	0 0 Truck Tractor Pickup,Par 0 0 Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Par 0 0 Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Par	anel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy					Changing Lanes	Going Straight Ahead Stopped for Traffic	East West East West West	East West
8135 State Route 8136 State Route	Jefferson 104 Jefferson 104			13.46 No 13.50 No	E806706 2018-06-09 12:50 E526723 2016-03-16 14:02	No Apparent Injury Possible Injury	0 0 1	0 0 Passenger Car Passenger 0 0 Passenger Car Passenger	er Car	Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry 5	Straight & Grade Straight & Grade	Daylight Daylight	ehicle Strikes Deer rom same direction - both going straight - one stopped - rear-end shicle Strikes Deer rom same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing	East West West East	West East
8137 State Route 8138 State Route 8139 State Route	Jefferson 104 Jefferson 104 Jefferson 104			13.51 No 13.51 No 13.56 No	E747603 2017-12-12 17:21 E766305 2018-02-04 04:20	No Apparent Injury No Apparent Injury No Apparent Injury	0 0 1	O Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Raining	Other C	Straight & Grade Curve & Grade Straight & Grade	Dark-No Street Lights Dark-No Street Lights Dark-No Street Lights	rom same direction - both going straight - both moving - rear-end coadway Ditch coadway Ditch allen tree hit by vehicle (on the road)	Changing Lanes Going Straight Ahead Going Straight Ahead		East West East West West	
8140 State Route 8141 State Route 8142 State Route 8143 State Route	Jefferson 104 Jefferson 104			13.60 No 13.62 No 13.64 No	E581063 2016-08-31 12:50 E311294 2014-02-26 21:55 E538399 2016-04-16 13-44	No Apparent Injury No Apparent Injury No Apparent Injury	0 0 1	0 0 Passenger Car 0 0 Passenger Car Pickup,Par 0 0 Passenger Car Pickup,Pan 0 0 0 Pickup,Pan 10 0 0 0 b 0 0 0 0 0 0 0 0 0 0 0 0 0 0	inel Truck or Vanette under 10,000 lb	Not at Interrection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Overcast	Dry S	Straight & Grade Straight & Grade Straight & Grade	Daylight Dark-No Street Lights Daylight	ehicle Strikes Deer rom same direction - both going straight - both moving - sideswipe rom same direction - both going straight - both moving - rear-end rom same direction - all others	Going Straight Ahead	Going Straight Ahead	West East East West Fast West	East West
8144 State Route	Jefferson 104 Jefferson 104 Jefferson 104			13.65 No 13.65 No	E825105 2018-07-29 12:58 E827878 2018-08-10 09:27	No Apparent Injury No Apparent Injury	0 0 2	0 0 Passenger Car Pickup,Par 0 0 Passenger Car Pickup,Par	inel Truck or Vanette under 10,000 lb inel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy					Changing Lanes Going Straight Ahead Making U-Tum Going Straight Ahead	Going Straight Ahead Going Straight Ahead Slowing	East East East West	East West East West
8145 State Route 8146 State Route 8147 State Route	Jefferson 104 Jefferson 104 Jefferson 104			13.66 No 13.66 No 13.67 No	E544843 2016-05-14 14:10 E740137 2017-10-15 19:40 E728580 2017-10-15 19:44	No Apparent Injury Suspected Minor Injury Suspected Minor Injury	0 0 2 2 0 2 3 0 2	0 0 Passenger Car Passenger 0 0 Passenger Car Passenger 0 0 Plickup Panel Truck or Vanette under 10 000 lb Pickup Par	er Car er Car inel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy Clear or Partly Cloudy	Dry S	Straight & Grade Straight & Grade Straight & Grade	Daylight Dark-No Street Lights Dark-No Street Lights	rom same direction - both going straight - both moving - rear-end rom same direction - both going straight - one stopped - rear-end rom same direction - both going straight - both moving - sides wipe	Going Straight Ahead Going Straight Ahead Overtaking and Passing	Stopped for Traffic Changing Lanes	West East West East East West	West East Vehicle Stopped East West
8148 State Route 8149 State Route 8150 State Route	Jefferson 104 Jefferson 104 Jefferson 104			13.70 No 13.73 No	E394038 2015-01-17 21:22 E807642 2018-06-08 23:35	No Apparent Injury No Apparent Injury	0 0 1	O Pickup, Panel Truck or Vanette under 10,000 lb O Pickup, Panel Truck or Vanette under 10,000 lb O Passenger Car O O Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb Pickup, Panel Truck or Vanette under 10,000 lb		Not at Intersection and Not Related Not at Intersection and Not Related	Raining Overcast Clear or Partly Cloudy			Dark-No Street Lights Dark-No Street Lights Daylight		Going Straight Ahead Going Straight Ahead	Going Straight Ahead	East West East West Vehicle Stopped East	East West
8151 State Route 8152 State Route 8153 State Route	Jefferson 104 Jefferson 104			13.76 No 13.76 No 13.76 No	E354431 2014-08-30 16:12 E361574 2014-09-25 18:57	Suspected Minor Injury Suspected Minor Injury	4 0 2 2 0 2	0 0 Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Par 0 0 Pickup,Panel Truck or Vanette under 10,000 lb Passenger	inel Truck or Vanette under 10,000 lb er Car	At Intersection and Related At Intersection and Related	Overcast Raining Fog or Smog or Smoke	Dry 0	Curve & Grade Curve & Grade	Daylight Dusk	intering at angle intering at angle	Going Straight Ahead Making Left Turn Going Straight Ahead	Going Straight Ahead Going Straight Ahead Stopped for Traffic	North South North East	East West East West
8153 State Route 8154 State Route 8155 State Route 8156 State Route	Jefferson 104 Jefferson 104 Jefferson 104			13.76 No 13.76 No 13.76 No	E384712 2014-12-14 11:40 E390059 2014-12-29 16:27 E399317 2015-02-05 11:45	Suspected Minor Injury Suspected Serious Injury No Apparent Injury	2 0 2 7 0 2 0 0 2	0 0 Pickup Panel Truck or Vanette under 10,000 lb Passengeger 1 0 0 Pickup Panel Truck or Vanette under 10,000 lb Passengeger 1 0 0 Pickup Panel Truck or Vanette under 10,000 lb Passenger Cal 0 Pickup Panel Truck or Vanette under 10,000 lb Pickup Panel Truck or Vanette under 10,000 lb Passenger Cal 0 Pickup Panel Truck or Vanette under 10,000 lb Passengeger 1 0 Pickup Panel Truck or Vanette under 10,000 lb Passengeger 1 0 Pickup Panel Truck or Vanette under 10,000 lb Passengeger 1 0 Pickup Panel Truck or Vanette under 10,000 lb Passengeger 1 0 Pickup Panel Truck or Vanette under 10,000 lb Passengeger 1 0 Pickup Panel Truck or Vanette under 10,000 lb Passengeger 1 0 Pickup Panel Truck or Vanette under 10,000 lb Passengeger 1 0 Pickup Panel Truck or Vanette under 10,000 lb Passengeger 1 0 Pickup Panel Truck or Vanette under 1 0 Pickup	er Car inel Truck or Vanette under 10,000 lb inel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy	Dry C	Straight & Grade Curve & Grade Curve & Grade	Daylight Dusk Daylight	rom same direction - both going straight - one stopped - rear-end rom opposite direction - one left turn - one straight ntering at angle	Going Straight Ahead Making Left Turn Making Left Turn	Stopped for Traffic Going Straight Ahead Going Straight Ahead	North South North East North East	Vehicle Stopped Vehicle Stopped East West East West
8157 State Route	Jefferson 104 Jefferson 104 Jefferson 104 Jefferson 104			13.76 No 13.76 No	E407105 2015-03-07 15:06 E424378 2015-05-12 14:02	No Apparent Injury No Apparent Injury	0 0 2 0 2	0 0 Pickup,Panel Truck or Vanette under 10,000 lb Passenger 0 0 Pickup,Panel Truck or Vanette under 10,000 lb Passenger	er Car	At Intersection and Related At Intersection and Related	Raining Clear or Partly Cloudy Clear or Partly Cloudy	Dry S	Curve & Grade Straight & Grade	Daylight I Daylight I	ntering at angle ntering at angle	Making Left Turn Making Left Turn	Going Straight Ahead Going Straight Ahead	North East North Southeast	East West East West
8158 State Route 8159 State Route 8160 State Route	Jefferson 104 Jefferson 104 Jefferson 104			13.76 No 13.76 No 13.76 No	E474165 2015-10-21 20:00 E478252 2015-11-01 13:47	No Apparent Injury Suspected Minor Injury	0 0 1 4 0 5	0 Pickup,Panel Truck or Vanette under 10,000 lb Passenger 0 Pickup,Panel Truck or Vanette under 10,000 lb Passenger 0 Pickup,Panel Truck or Vanette under 10,000 lb 1 O Pickup,Panel Truck or Vanette under 10,000 lb 1 O Passenger Car	inel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Not Related At Intersection and Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Dry S Dry S	Straight & Grade Straight & Grade Straight & Grade		ntering at angle chicle Strikes Deer intering at angle	Making Left Turn Going Straight Ahead Making Left Turn	Going Straight Ahead Going Straight Ahead	West East North East	West East
8161 State Route 8162 State Route 8163 State Route 8164 State Route	Jefferson 104 Jefferson 104 Jefferson 104			13.76 No 13.76 No 13.76 No	E493488 2015-12-09 15:47 E521670 2016-02-20 12:28 E523336 2016-03-07 23:45	No Apparent Injury Suspected Minor Injury No Apparent Injury	0 0 2 3 0 2 0 0 2	0 0 Passenger Car Pickup.Par 0 0 Passenger Car Pickup.Par 0 0 Passenger Car Not Stated Not Stated Not Stated Not Stated	inel Truck or Vanette under 10,000 lb inel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Not Related	Overcast Clear or Partly Cloudy Overcast	Dry	Straight & Grade Straight & Grade Straight & Grade Curve & Grade	Daylight	intering at angle intering at angle rom same direction - both going straight - both moving - sideswipe	Starting in Traffic Lane Making Left Turn	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Vehicle Stopped South North East East West	East West East West East West
8164 State Route 8165 State Route	Jefferson 104 Jefferson 104			13.76 No 13.76 No	E537228 2016-04-18 10:36 E549623 2016-05-31 15:41	Possible Injury No Apparent Injury	2 0 2 0 0 2	1 0 Passenger Car Pickup Par	ctor & Semi-Trailer inel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy	Dry 0	Curve & Grade	Daylight	ntering at angle intering at angle	Going Straight Ahead Making Left Turn Making Left Turn	Going Straight Ahead Going Straight Ahead Going Straight Ahead	North East North East	East West East West
8165 State Route 8166 State Route 8167 State Route 8168 State Route	Jefferson 104 Jefferson 104 Jefferson 104			13.76 No 13.76 No	E579517 2016-08-24 08:10	Possible Injury	1 0 1	0 0 Truck Tractor & Semi-Trailer		At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Raining		Straight & Grade Curve & Grade Curve & Grade		intering at angle II other non-collision Intering at angle	Making Left Turn Going Straight Ahead Making Left Turn	Going Straight Ahead Going Straight Ahead	West East North Southeast	East West
8169 State Route 8170 State Route 8171 State Route 8172 State Route	Jefferson 104 Jefferson 104 Jefferson 104			13.76 No 13.76 No 13.76 No	E637687 2017-01-30 18:21 E698875 2017-07-23 16:29 E698877 2017-07-22 16:30	No Apparent Injury No Apparent Injury No Apparent Injury	0 0 2 0 0 2 0 0 2	0 0 Passenger Car 0 0 Passenger Car 0 0 Pickup Parel Truck or Vanette under 10,000 Ib Pickup Parel 0 0 0 Passenger Car	inel Truck or Vanette under 10,000 lb inel Truck or Vanette under 10,000 lb inel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry C Dry S	Curve & Grade Straight & Grade Straight & Grade	Dark-No Street Lights Daylight Daylight	intering at angle intering at angle ntering at angle	Making Left Turn Making Left Turn Starting in Traffic Lane	Going Straight Ahead Going Straight Ahead Going Straight Ahead	North East Vehicle Stopped Vehicle Stopp Vehicle Stopped Vehicle Stoppe	East West d East West d East West
	Jefferson 104 Jefferson 104			13.76 No 13.76 No	E729976 2017-11-01 17:55 E760573 2018-01-11 16:10	No Apparent Injury Suspected Minor Injury	0 0 2 2 2 0 2	0 0 Passenger Car Pickup,Par 0 0 Pickup,Panel Truck or Vanette under 10,000 lb Passenger	inel Truck or Vanette under 10,000 lb er Car	At Intersection and Related At Intersection and Related	Overcast	Wet 0	Curve & Grade Curve & Grade	Dark-No Street Lights	ntering at angle intering at angle	Making Right Turn Making Left Turn	Stopped at Signal or Stop Sign Going Straight Ahead	East North North East	North East West
8174 State Route 8175 State Route 8176 State Route	Jefferson 104 Jefferson 104 Jefferson 104			13.76 No 13.76 No	E818138 2018-07-15 15:50 E820129 2018-07-20 18:16	Suspected Minor Injury Suspected Minor Injury	1 0 1 2 2 0 2	O Pickup,Panel Truck or Vanette under 10,000 lb Passenger O Pickup,Panel Truck or Vanette under 10,000 lb Passenger O Mped O Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Pickup,Pane	inel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry C	Curve & Grade Curve & Grade	Daylight Daylight I	intering at angle ehicle overturned intering at angle	Making Right Turn Making Right Turn Making Left Turn	Stopped at Signal or Stop Sign Going Straight Ahead	East North East North North East	East West
8177 State Route 8178 State Route 8179 State Route	Jefferson 104 Jefferson 104 Jefferson 104			13.76 No 13.76 No 13.76 No				0 0 Passenger Car Passenger	er Car		Clear or Partly Cloudy Overcast Clear or Partly Cloudy	Dry 0	Curve & Grade Straight & Grade Curve & Grade	Daylight Daylight Daylight	rom same direction - both going straight - both moving - rear-end intering at angle ntering at angle	Going Straight Ahead Merging (Entering Traffic) Making Left Turn	Going Straight Ahead Going Straight Ahead Going Straight Ahead	West East South East North East	West East East West East West
8181 State Route	Jefferson 104			13.76 No 13.76 No	E946394 2019-03-27 17-27	Possible Injury	2 0 2	0 0 Passenger Car Pickup,Par	er Car inel Truck or Vanette under 10,000 lb inel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Overcast Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry 0	Curve & Grade	Daylight I	intering at angle rom same direction - both going straight - one stopped - rear-end	Making Left Turn Going Straight Ahead	Going Straight Ahead Going Straight Ahead Stopped in Roadway	North East East West	East West East West
8182 State Route 8183 State Route 8184 State Route	Jefferson 104 Jefferson 104 Jefferson 104			13.77 No 13.81 No 13.84 No	E71781 2017-05-29 17:48 E715027 2017-09-19 17:15 E823009 2018-07-28 16:56	No Apparent Injury No Apparent Injury Suspected Serious Injury Possible Injury No Apparent Injury Possible Injury	0 0 2	0 0 Passenger Car Passenger 0 0 Motorcycle	er Car	Intersection Related but Not at Intersection Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry	Curve & Grade Curve & Grade Straight & Grade	Daylight	tom same direction - both going straight - both moving - rear-end tom same direction - both going straight - both moving - rear-end tuardrail - Face	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing Slowing	West East West East	West East
8185 State Route 8186 State Route 8187 State Route	Jefferson 104 Jefferson 104 Jefferson 104			13.85 No 13.90 No 14.00 No	E308491 2014-02-12 14:06 E797886 2018-05-12 11:06 E384021 2014-12-09 14:43	Possible Injury No Apparent Injury Possible Injury	2 0 2 0 0 3 4 0 4	0 0 Passenger Car Passenger 0 0 Pickup,Panel Truck or Vanette under 10,000 lb Passenger 0 0 Passenger Car Passenger 0 0 Passenger Car Pickup,Par 0 0 Passenger Car Pickup,Par		Not at Intersection and Not Related	Clear or Partly Cloudy	Dry	Curve & Grade Straight & Level Straight & Level		rom same direction - both going straight - one stopped - rear-end rom same direction - both going straight - one stopped - rear-end rom same direction - both going straight - one stopped - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Stopped for Traffic Stopped for Traffic Stopped for Traffic	West East West East West Fast	West East West Vehicle Stopped West Vehicle Stopped
8187 State Route 8188 State Route 8189 State Route	Jefferson 104 Jefferson 104			14.00 No 14.00 No	E437876 2015-06-22 10:45 E445887 2015-07-24 11:02	No Apparent Injury Possible Injury	0 0 3	0 0 Passenger Car Pickup,Par 0 0 Motorcycle	er Car inel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy Raining	Wet S	Straight & Level Straight & Grade	Daylight Daylight	rom same direction - both going straight - one stopped - rear-end ehicle overturned	Going Straight Ahead Going Straight Ahead	Stopped for Traffic Stopped for Traffic	West East West East	Vehicle Stopped Vehicle Stopped
8190 State Route 8191 State Route 8192 State Route 8193 State Route	Jefferson 104 Jefferson 104 Jefferson 104			14.00 No 14.07 No 14.30 No				0 Passenger Car	nel Truck or Vanette under 10,000 lb (bad, Van,etc)	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Other	Wet S Other	Straight & Level Straight & Level Straight & Level	Dawn Daylight Daylight	rom same direction - both going straight - both moving - rear-end iridge Rail - Face the parked-one moving	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing Legally Parked, Unoccupied	West East East West East West	West East Vehicle Stopped Vehicle Backing
8193 State Route 8194 State Route 8195 State Route	Jefferson 104 Jefferson 104 Jefferson 104			14.36 No 14.38 No 14.48 No					er Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Other Clear or Partly Cloudy	Wet	Straight & Level Straight & Level Straight & Level	Daylight I	rom same direction - both going straight - both moving - rear-end utdge Rail - Face rom opposite direction - both going straight - sideswipe	Going Straight Ahead	Slowing Going Straight Ahead	West East West East West East	West East East West
8194 State Route 8195 State Route 8196 State Route 8197 State Route	Jefferson 104 Jefferson 104			14.38 No 14.48 No 14.50 No 14.50 No	E302965 2014-01-25 13:22 E499989 2015-12-30 00:43 3120484 2014-08-04 12:35	No Apparent Injury No Apparent Injury	0 0 1	0 0 Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Wet	Straight & Level	Daylight Dark-Street Lights On	ridge Rail - Face	Going Wrong Way on Divided Hwy Going Straight Ahead Going Straight Ahead		West East East West	
8198 State Route 8199 State Route 8200 State Route	Jefferson 104 Jefferson 104 Jefferson 104			14.53 No 14.54 No 14.54 No	E361395 2014-09-27 13:50 E454092 2015-08-14 18:18	No Apparent Injury No Apparent Injury	0 0 2	0 0 Pickup,Panel Truck or Vanette under 10,000 lb 0 0 Pickup,Panel Truck or Vanette under 10,000 lb Passenger 0 0 Passenger Car	er Car	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Raining	Dry	Straight & Level Straight & Grade Straight & Level	Daylight	trawbridge Crossing Gate Arm rom same direction - both going straight - both moving - rear-end uridge Rail - Face	Going Straight Ahead Going Straight Ahead Going Straight Ahead	Slowing	East West West East	East West
8201 State Route 8202 State Route 8203 State Route	Jefferson 104 Jefferson 104 Jefferson 104			14.55 No 14.58 No 14.59 No	E357215 2014-09-16 10:30 E386862 2014-12-23 15:30 E454467 2015-08-14 20:00	No Apparent Injury Suspected Serious Injury No Apparent Injury	0 0 1 5 0 4 0 0 2	Directory Partie 1 Trace or Vanette under 10,000 ib Passenger Directory Partie 1 Trace or Vanette under 10,000 ib Directory Partie 1 Trace or Vanette under 1 Trace or Vanette under 1 Tr	inel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy Raining Raining	Dry S Wet S	Straight & Level Straight & Level Straight & Level	Daylight Daylight Dusk	ndge Rail - Face rawbridge Crossing Gate Arm rom opposite direction - both going straight - sideswipe rom same direction - both going straight - both moving - rear-end	Going Straight Ahead Going Straight Ahead Going Straight Ahead Slowing	Going Straight Ahead Slowing	West East East West East West	West East East West
8204 State Route 8205 State Route	Jefferson 104 Jefferson 104 Jefferson 104			14.60 No 14.62 No	E685969 2017-06-27 15:42 E829085 2018-08-11 15:16	Possible Injury Suspected Minor Injury	1 0 2 2 2 2 0 2	0 0 Passenger Car Pickup,Par 0 0 Motorcycle Motorcycle	inel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy	Dry S Wet S	Straight & Level	Daylight Daylight	rom same direction - both going straight - both moving - rear-end rom same direction - both going straight - both moving - sideswipe	Going Straight Ahead Going Straight Ahead	Slowing Going Straight Ahead	East West East West	East West East West
8207 State Route 8208 State Route	Jefferson 116 Jefferson 116			0.00 No 0.00 No	E372073 2014-11-05 07:45 E465549 2015-09-19 13:20	No Apparent Injury No Apparent Injury	0 0 2	0 0 Pickup,Parier truck or Varietie under 10,000 lb Passenger 0 0 Passenger Car Pickup,Par	er Car unel Truck or Vanette under 10,000 lb	At Intersection and Related At Intersection and Related	Overcast Clear or Partly Cloudy	Dry	Straight & Level	Daylight	rom same direction - both going straight - both moving - rear-end ame direction both turning right one stopped rear end rom same direction both going straight - one stopped rear-end	Going Straight Ahead Making Right Turn Other*	Stopped at Signal or Stop Sign Stopped for Traffic	East North Vehicle Stopped West	East Vehicle Stopped Vehicle Stopped Vehicle Stopped
8209 State Route 8210 State Route 8211 State Route	Jefferson 116 Jefferson 116 Jefferson 116			0.00 No 0.06 No 0.12 No	E596130 2016-10-13 16:00 E932265 2019-06-13 15:00 E825107 2018-07-23 09:42	No Apparent Injury No Apparent Injury No Apparent Injury	0 0 3 0 0 2 0 0 2	0 0 Passenger Car Pickup.Par 0 0 Passenger Car Pickup.Par 0 0 Passenger Car Passenger Car	er Car inel Truck or Vanette under 10,000 lb inel Truck or Vanette under 10,000 lb inel Truck or Vanette under 10,000 lb er Car	At Driveway within Major Intersection At Driveway At Intersection and Related	Raining Clear or Partly Cloudy Clear or Partly Cloudy	Dry S	Straight & Level Straight & Grade Straight & Grade	Daylight Daylight Daylight	ntering at angle ntering at angle ntering at angle	Making Left Turn Making Left Turn Making Left Turn	Going Straight Ahead Going Straight Ahead Going Straight Ahead	North East North East South West	West East North East East West
8211 State Route 8212 State Route 8213 State Route 8214 State Route	Ideferson			0.16 No 0.46 No	E450198 2015-08-02 18:25 E946361 2019-07-26 18:37	No Apparent Injury No Apparent Injury Surposted Minor Injury	0 0 1	Displace Parties Trace or severe under 10,000 ib Passenger Displacement Disp	w Car	At Intersection and Related Not at Intersection and Not Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry C	Straight & Grade Curve & Grade Straight & Level Curve & Level	Daylight Daylight	intering at angle ehicle - Pedaltyclist ehicle Strikes Deer	Making Left Turn Stopped in Roadway Going Straight Ahead Making Left Turn		South West Vehicle Stopped Vehicle Stopp East West West North	East Was
8214 State Route 8215 State Route 8216 State Route 8217 State Route 8218 State Route	Jefferson 116 Jefferson 116			0.47 No 0.47 No 0.52 No	E414171 2015-02-28 17:08 E680588 2017-06-03 07:02	No Apparent Injury No Apparent Injury	0 0 2	0 0 Passenger Car Passenger 0 0 Pickup,Panel Truck or Vanette under 10,000 lb Passenger	er Car	At Intersection and Related Not at Intersection and Not Related	Clear or Partly Cloudy	Dry C	Straight & Level Curve & Level	Daylight Dawn	rom opposite direction - one left turn - one straight intering at angle tility Pole	Going Straight Anead	Going Straight Ahead Slowing	West East East West	West East
8217 State Route 8218 State Route 8219 State Route	Velistron			0.67 No 0.71 No 0.88 No	E789754 2018-04-15 15:36 E925427 2019-05-25 12:15 E764630 2018-01-30 15:45	No Apparent Injury No Apparent Injury Possible Injury	0 0 2 0 0 1 1 0 2	0 0 Pickup,Panel Truck or Vanette under 10,000 lb Passenger 0 0 Pickup,Panel Truck or Vanette under 10,000 lb 0 0 Pickup,Panel Truck or Vanette under 10,000 lb Passenger	er Car	At Driveway Not at Intersection and Not Related At Driveway	Overcast Clear or Partly Cloudy Raining Raining	Wet	Straight & Level Straight & Grade	Daylight	intering at angle ehicle Strikes Deer fom opposite direction - one left turn - one straight	Going Straight Ahead Making Left Turn Going Straight Ahead Making Left Turn	Going Straight Ahead Going Straight Ahead	North East West East West North	East West West West
8220 State Route 8221 State Route 8222 State Route 8223 State Route 8223 State Route	Jefferson 116			0.89 No 0.98 No	E708749 2017-09-06 08:30 E544687 2016-05-06 13:32 E498432 2016-05-06	Suspected Minor Injury Suspected Serious Injury No Apparent Injury	2 0 2	0 0 Passenger Car Passenger 0 0 Passenger Car Pickup Passenger 0 0 Passenger Car Pickup Passe	er Car inel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related Not at Intersection and Not Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Raining	Dry S	Straight & Level Straight & Level	Daylight Daylight Dark-Street Lights On	tom opposite direction: both en turn chick assigned from opposite direction: both going staight - sideswipe from opposite direction - both going staight - sideswipe from opposite direction - both going staight - sideswipe free or Sturng lestionary)	Going Straight Ahead Going Straight Ahead Making Left Turn	Going Straight Ahead Going Straight Ahead	West East East West	East West West East
8223 State Route 8224 State Route	Jefferson 116 Jefferson 116 Jefferson 116			1.00 No 1.00 No 1.01 No	E#32821 2018-08-24 10:00 E999027 2019-12-26 14:47	No Apparent Injury No Apparent Injury No Apparent Injury	0 0 2 0 0 2	0 Pickup,Panel Truck or Vanette under 10,000 lb 0 Pickup,Panel Truck or Vanette under 10,000 lb Passenger 0 0 Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Passenger Car Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel Truck or Vanette un	er Car inel Truck or Vanette under 10,000 lb	Not at Intersection and Not Related At Intersection and Related	Raining Clear or Partly Cloudy Clear or Partly Cloudy	Dry	Straight & Level Straight & Level Straight & Level	Daylight	rom same direction - both going straight - one stopped - rear-end	Making Left Turn Going Straight Ahead Making Left Turn Going Straight Ahead	Stopped in Roadway	West East North East	West Vehicle Stopped East West
8224 State Route 8224 State Route 8225 State Route 8225 State Route 8226 State Route 8227 State Route 8227 State Route 8229 State Route 8229 State Route 8229 State Route 8230 State Route 8231 State Route	Jefferson 116 Jefferson 116			1.11 No 1.11 No	E375357 2014-11-13 14:32 E386973 2014-12-24 14:28 E443641 2015-07-09 13-12	No Apparent Injury No Apparent Injury No Apparent Injury	0 0 2 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	Distance Parel Track or Varente under 10,000 tb. Passenger Distance Parel Track or Varente under 10,000 tb. Passenger Distance Parel Track or Varente under 10,000 tb. Passenger Distance Parel Track or Varente under 10,000 tb. Passenger Distance Parel Track or Varente under 10,000 tb. Passenger Distance Parel Track or Varente under 10,000 tb. Passenger Distance Parel Track or Varente under 10,000 tb. Passenger Distance Parel Track or Varente under 10,000 tb. Passenger Distance Parel Track or Varente under 10,000 tb. Passenger Distance Parel Track or Varente under 10,000 tb. Passenger Distance Parel Track or Varente under 10,000 tb. Passenger Distance Parel Track or Varente under 10,000 tb. Passenger Distance Parel Track or Varente under 10,000 tb. Passenger Distance Parel Track or Varente under 10,000 tb. Passenger Distance Parel Track or Varente under 10,000 tb. Passenger Distance Parel Track or Varente Under 10,000 tb. Passenger Distance Parel Track or Varente Under 10,000 tb. Passenger Distance Parel Track Organization (10,000 tb. Passenger Distance Parel Track Organization	er Car er Car inel Truck or Vanette under 10 000 %	At Intersection and Related At Intersection and Related At Intersection and Related	Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy Clear or Partly Cloudy	Dry	Straight & Level		intering at angle rom same direction - one right turn - one straight rintering at angle ritering at angle	Going Straight Ahead	Going Straight Ahead Making Right Turn Making Left Turn Going Straight Ahead	East West East West East West	East North North East South North
8228 State Route 8229 State Route	Jefferson 116			1.11 No 1.11 No 1.12 No	E709750 2017-09-06 15:40 E948902 2019-08-06 16:55	No Apparent Injury No Apparent Injury	0 0 2	0 0 Pickup,Panel Truck or Vanette under 10,000 lb Pickup,Panel 0 0 Pickup,Panel Truck or Vanette under 10,000 lb Passenger	nel Truck or Vanette under 10,000 lb	At Intersection and Related Intersection Related but Not at Intersection		Dry 5	Straight & Level Straight & Level Straight & Level	Daylight Daylight Daylight	ritering at angle Intelling	Going Straight Ahead Starting in Traffic Lane Going Straight Ahead	Going Straight Ahead Going Straight Ahead Stopped at Signal or Stop Sign Making Right Turn	East North East West	South North Vehicle Stopped Vehicle Stopped
8231 State Route 8232 State Route	Jefferson 116 Jefferson 116			1.14 No 1.15 No 1.21 No	E700229 2017-08-05 13:37 E796805 2018-05-10 11:23	Suspected Serious Injury No Apparent Injury	0 U 2 1 0 1 0 0 1	1 0 Pickup,Panel Truck or Vanette under 10,000 lb 0 0 Passenger Car	men muck or vanette under 10,000 lb	At Driveway Not at Intersection and Not Related Not at Intersection and Not Related	Snowing Clear or Partly Cloudy Overcast	Dry 0	Curve & Level	Daylight I	ehicle going straight hits pedestrian inear Curb	Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead	Making Right Turn	West East West East Southeast Northwest	West South
8232 State Route 8233 State Route 8234 State Route 8235 State Route	Jefferson 116 Jefferson 116 Jefferson 116 Jefferson 116			1.34 No 1.50 No 1.61 No	E405087 2015-03-02 08:50 E385031 2014-12-18 14:17 E962418 2019-09-17 17:46	No Apparent Injury No Apparent Injury No Apparent Injury	0 0 2	0	tbad,Van,etc)	At Intersection and Related Not at Intersection and Not Related Not at Intersection and Not Related	Overcast Clear or Partly Cloudy Overcast Overcast	Dry S Wet S Wet	Straight & Grade Straight & Grade Curve in San	Daylight Daylight Daylight	intering at angle ker Embankment - No Guardrail Present coadray Ditch	Backing Going Straight Ahead Going Straight Ahead	Slowing	Vehicle Backing Vehicle Backin East West East	g East West
8236 State Route 8237 State Route 8238 State Route 8239 State Route	Jefferson 116 Jefferson			1.63 No 1.75 No	E996078 2019-12-16 22:11 E896585 2019-02-19 17:11	No Apparent Injury No Apparent Injury	0 0 1	0 Passeriger Lab 0 Passeriger Lab 0 Passeriger Lab 0 Passeriger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Clear Raining			Dark-Street Lights On Daylight Daylight Daylight		Going Straight Ahead Going Straight Ahead Going Straight Ahead Going Straight Ahead		West East West East	
oza/ State Koute			1	1.76 No	E786324 2018-04-07 16:10	No Apparent Injury	U 0 1	u u Passenger Car		Not at Intersection and Not Related Not at Intersection and Not Related	Raining Clear or Partly Cloudy	vvet (Curve in Sag	Daylight	oadway.unch	Going Straight Ahead		west East	

OBJECTID	MV_DRIVER_CONTRIBUTING CIRCUMSTANCE_1_UNIT_1 MV_DRIVER_CONTRIBUTION CIRCUMSTANCE_2_UNIT_1	NG MV_DRIVER_CONTRIBUTING AGE UNIT_1	MV_DRIVER GRODER UNIT_1 ORDER_CONTRIBUTING CIRCUMSTANCE_2_UNIT_2 ORCUMSTANCE_2_UNIT_2.	MV_DRIVER AGE_UNIT_2	MV_DRIVER GENDER UNIT_2 BICYCLIST_CONTRIBUTING CIRCUMSTANCE_1_UNIT_1 BICYCLIST_CONTRIBUTING CIRCUMSTANCE_2_UNIT_1 CIRCUMSTANCE_3_UNIT_1	BICYCLIST AGE GENDER UNIT_1 UNIT_1 BICYCLIST_CONTRIBUTING GRCUMSTANCE_1_UNIT_2 BICYCLIST_CONTRIBUTING GRCUMSTANCE_1_UNIT_2 GRCUMSTANCE_2_UNIT_2	BICYCLIST_CONTRIBUTING BIC CIRCUMSTANCE_3_UNIT_2 AGE	CLIST BICYCLIST PEDESTRIAN_CONTRIBUTING CIRCUMSTANCE_1_UNIT_2	PEDESTRIAN_CONTRIBUTING CIRCUMSTANCE_2_UNIT_2 PEDESTRIAN_CONTRIBUTING CIRCUMSTANCE_3_UNIT_2	PEDESTRIAN PEDESTRIAN AGE_UNIT_2 GENDER_UNIT_	FIRST_IMPACT_LOCATION City_St_County_Rd_8_Misc_2010_forward_St
8099 Follo 8100 Follo	ow Too Closely ow Too Closely ention Exceeding Reas. Safe Speed	18 33	Male None Female None	19 30	Female Female						Lane 1 Increasing Miepost Lane 1 Increasing Miepost Lane 1 Increasing Miepost
8102 Exce	mention Exceeding Reas. Safe Speed seeding Reas. Safe Speed rating Defective Equipment Inattention	17 Exceeding Reas. Safe Speed 57	Femalie None	37	Maie Maie Maie						Lane 1 Increasing Milepost Lane 1 Increasing Milepost Lane 1 Increasing Milepost
8104 Inatte 8105 Inatte	ention Exceeding Reas. Safe Speed ention Follow Too Closely enting Reas. Safe Speed	27 24	Residence None Male None	55 63	Male Male						Lane 1 Increasing Milepost Lane 1 Increasing Milepost
8106 Exces 8107 None		55 23	Male None Nale	75	Male						Lane 1 Increasing Milepost Lane 1 Increasing Milepost
8108 Inatte 8109 Inatte	e e Other Driver Distractions Inside Ve ention Distractions Cutside Vehicle or Driver Distractions Inside Vehicle Improper Passing	Follow Too Closely 34	Female None Male None Female None	31 36	Female						Lane 1 Increasing Miepost Lane 1 Increasing Miepost Lane 1 Increasing Miepost
8111 None 8112 Follo	e over Too Closely er Influence of Alcohol	23	Peniate None Male None		Female Female						Lane 1 Increasing Milepost Lane 1 Increasing Milepost Lane 1 Increasing Milepost
8113 Unde 8114 Inatte	er Influence of Alcohol ention Follow Too Closely	29 18	Female Male None		Male						Past Right Shoulder Increasing Milepost Lane 1 Decreasing Milepost
8115 None 8116 Inatte	to Basebane Concentral minion Follow Too Closely e entron Exceeding Reas. Safe Speed entron Exceeding Reas. Safe Speed entron Contributing Circ Not Listed	36 23	Male Female None		Female Female						Lane 1 Decreasing Milepost
8117 None 8118 Other	e or Contributing Circ Not Listed	49 31	Male None	81	Male						Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost
	eeding Reas. Safe Speed e peding Reas. Safe Speed	27	Male Female Mate								Right Shoulder Decreasing Miepost Lane 1 Increasing Miepost Past Right Shoulder Decreasing Miepost Lane 1 Decreasing Miepost
8122 Distra 8123 None	e seding Reas . Safe Speed ractions Outside Vehicle e	28	Male Female Male								
8124 None 8125 None	e e	43	Male Male								Lane 1 Decreasing Milepost Lane 1 Increasing Milepost
8126 None 8127 None	8	76	Male Male								Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost
8129 Unkn 8130 None	e nown Distraction		Male Male								Lane 1 Decreasing Milepost Past Right Shoulder Increasing Milepost Lane 1 Decreasing Milepost
8131 Inatte 8132 None	ention Follow Too Closely	31 57	Male None Male None	41 50	Female Male Male						Lane 1 Decreasing Mlepost Lane 1 Decreasing Mlepost Lane 1 Decreasing Mlepost
8133 None 8134 Follo	e ow Too Closely	62 56	Female Male None	16	Male						Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost
8135 None 8136 Inatte	e Follow Too Closely		Male Female None	68	Male San						
8132 None 8133 None 8134 Follo 8135 None 8136 Inatte 8137 Inatte 8138 Inatte 8139 None	tention Exceeding Reas. Safe Speed tention Exceeding Reas. Safe Speed tention Exceeding Reas. Safe Speed tention tenti	45 65 27	Maie Male Male								Past Right Shoulder Decreasing Milepost Past Right Shoulder Decreasing Milepost Lane 1 Decreasing Milepost
8140 None 8141 Inatte	e Did Not Grant RW to Vehicle	28 54	Female None	76	Male						Lane 1 Increasing Milepost Lane 2 Decreasing Milepost
8142 Other 8143 Impro	er Contributing Circ Not Listed		Male None Female None	73 32	Male Female State						Lane 1 Decreasing Milepost Lane 2 Decreasing Milepost
8144 Follo 8145 Exper	copies or dustrial was Too Closely with Too Closely bedding Reas. Safe Speed mention per Contributing Circ Not Listed Distractions Cutside Vehicle	34 24	Female		Female						Lane 2 Decreasing Milepost Lane 1 Increasing Milepost Lane 1 Increasing Milepost
8147 Other 8148 Inate	mention refrontributing Circ Not Listed Distractions Outside Vehicle mention	Improper Passing 58	Male None Male Mone Male Other Contributing Circ Not Listed Distractions Outside Vehicle Female	31 44	Male						Lane 1 locreasing Milepost Lane 2 Decreasing Milepost Past Right Shoulder Decreasing Milepost
8149 None 8150 Inatte	ention perition Did Not Grant RW to Vehicle	62 70	Female Male Female None								Lane 1 Decreasing Milepost
8151 Did N 8152 Did N	Not Grant RW to Vehicle Not Grant RW to Vehicle		Male None Male None	67	Male						Lane 1 Decreasing Mlepost Left Turn Lane Decreasing Mlepost Lane 2 Decreasing Mlepost
8153 Other 8154 Did N	Net Grant RW to Nethicle On Christ Districtions Inside Vehicle On Christ Districtions Inside Vehicle De Not Grant RW to Vehicle De Not Grant RW to Vehicle antion De Not Grant RW to Vehicle antion De Not Grant RW to Vehicle antion De Not Grant RW to Vehicle On Not Combining Clin Not Listed	71 22	Female None Male None Female None		Female						Intersecting Road Decreasing Milepost Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost
8156 Did N	Not Grant RW to Vehicle ention Did Not Grant RW to Vehicle Did Not Grant RW to Vehicle	72	Male None	57							Lane 2 Decreasing Melpost Left Turn Lane Decreasing Melpost
8158 Inatte 8159 Other	mention Did Not Grant RW to Vehicle or Contributing Circ Not Listed	32	Male None Female	62	Male						Lane 1 Decreasing Miepost Lane 1 Increasing Miepost Lane 1 Increasing Miepost Lane 1 Increasing Miepost
8160 Impro 8161 Inatte	roper Turn/Merge Other Contributing Circ Not Listed ention Did Not Grant RW to Vehicle		Female None Female None	72	Female Male						Left I urn Lane increasing Milepost
8162 Inatte 8163 None	entition Did Not Grant RW to Vehicle entition Did Not Grant RW to Vehicle e Did Not Grant RW to Vehicle entition Did Not Grant RW to Vehicle entition Did Not Grant RW to Vehicle		Male None	/1	Male Male Male						Lane 1 Decreasing Mlepost Lane 1 Decreasing Mlepost Lane 1 Decreasing Mlepost
8165 Inatte	Amnon Did Not Grant RW to Vehicle Not Grant RW to Vehicle To Contributing Circ Not Listed Did Not Grant RW to Vehicle Did Not Grant RW to Vehicle Entition Did Not Grant RW to Vehicle Did Not Grant RW to Vehicle Notice Not Listed	45	Female None	74	Male						Lane 1 Decreasing Milepost
8167 Other 8168 Inatte	er Contributing Circ Not Listed ention Did Not Grant RW to Vehicle	56	Male		Male Male Male						Lane 1 Decreasing Mlepost Past Right Shoulder Increasing Mlepost Lane 1 Decreasing Mlepost
8169 Inatte 8170 Other	emtion Out Not Grant RW to Vehicle or Contributing Circ Not Listed watherdon or Contributing Circ Not Listed batterision watherdon or Contributing Circ Not Listed batterision Words Side of Road Motorists on Words Side of Road	32 20	Male None Male None	24 53	Male Male Male Male Male Male Male Male						Left Turn Lane Decreasing Milepost Lane 1 Decreasing Milepost
8171 Other 8172 Non I	or Contributing Circ Not Listed Inattention Motorist on Wrong Side of Road Net Cone DMA: Spirite	31 17	Maie None Maie None	63	Male Female Address Ad						Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost Intersecting Road Decreasing Miepost
8174 Unde 8175 Exper	Not Grant RW to Website In Misson or A Vision De Nos Grant RW to Website De Nos Grant RW to Website antique mention De Nos Grant RW to Website mention de Modern of Medical materials The Control RW to Website The Co		Male	38	Male Male						Lane 2 Decreasing Milepost Intersecting Road Decreasing Milepost Intersecting Road Decreasing Milepost
8176 Inatte 8177 Open	ention Did Not Grant RW to Vehicle rating Defective Equipment		Male None Female None	78 77	Female Male						Lane 1 Decreasing Milepost Lane 1 Increasing Milepost
8178 Inatte 8179 Inatte	rating Defective Equipment pention Did Not Grant RW to Vehicle pention Did Not Grant RW to Vehicle per Influence of Alcohol Inattention	20 68 Did Not Grant RW to Vehicle 34	Male None Male None	76 71	Male						Lane 1 Decreasing Mlepost Lane 1 Decreasing Mlepost Lane 2 Decreasing Mlepost
8181 Other	er innuence of Auconol inamention r Contributing Circ Not Listed ention Follow Too Closely	19	Nasie None Female Follow Too Closely Male None		Natie Male Female						Lane 2 Decreasing Milepost Lane 2 Decreasing Milepost Lane 2 Decreasing Milepost
8183 Drive 8184 Other	er Interacting with Passengers, Anim Follow Too Closely	24	Female None Male	38	Male						Lane 1 Increasing Milepost Right Shoulder Increasing Milepost
8185 natte	ention Follow Too Closely Ention Follow Too Closely		Female None Female None	52 53	Female Male						Lane 1 Increasing Miepost Lane 1 Increasing Miepost
8187 Inatte 8188 Follo	mention Exceeding Reas. Safe Speed ow Too Closely or Contributing Circ Not Listed	Eating or Drinking 77 24	Male None Female None	52 65	Male San						Lane 1 Increasing Miepost Lane 1 Increasing Miepost Lane 1 Increasing Miepost
8190 Follo	ow Too Closely	20 20	Female Driver Not Distracted	62	Male						Lane 1 Increasing Milepost
8192 Other 8193 Inatte	ow Too Closely seding Reas. Safe Speed re Contributing Circ Not Listed Inattention ention Driver Adjusting Audio or Entertain		Male None Male None	60	Female						Right Shoulder Increasing Milepost Right Shoulder Decreasing Milepost Lane 1 Increasing Milepost
8194 Exces 8195 Unkn	ention Univer Adjusting Audio or Entertain ceding Reas. Safe Speed nown Distraction ention sedling Reas. Safe Speed sedling Reas. Safe Speed	18	Female Male None Male None		Male						Right Shoulder Decreasing Milepost
8197 Exces	seriori seding Reas. Safe Speed	55	Male								Lane 1 Decreasing Miepost Lane 1 Increasing Miepost Right Shoulder Decreasing Miepost Lane 1 Increasing Miepost
8199 Follo 8200 Inatte	redning Reas. Safe Speed ow Too Closely ention Exceeding Reas. Safe Speed	27	Male		Female						Lane 1 hereasing Milpost Lane 1 hereasing Milpost Right Shoulder hicreasing Milpost Lane 1 hereasing Milpost Lane 1 hereasing Milpost Lane 1 hereasing Milpost
8201 Disre 8202 Appa	egard Stop Sign - Flashing Red Inattention arently Asleep or Fatigued	59 29	Male None None		Male						Lane 1 Increasing Milepost
8203 Exper 8204 Smol	aretiny paregue or eduqued seeding Reas. Safe Speed Follow Too Closely seeding Reas. Safe Speed Follow Too Closely sention Follow Too Closely sention Follow Too Closely	54	Female None Male None Female Eveneting Page Safe Speed	57 57	Male Male Female Female Female Female Male Male Male Male Male Male Male M						Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost
8205 Exces 8206 Inatte	senting Reas Safe Speed Follow Too Closely ention Follow Too Closely ention Follow Too Closely	62	Female	26	Female Female Male						Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost
8208 Other 8209 Did N	er Contributing Circ Not Listed Did Not Grant RW to Vehicle	Driver Not Distracted 67	Female None	36	Female Female						Left Turn Lane Decreasing Milepost Lane 1 Increasing Milepost
8210 Other 8211 Inatte	er Contributing Circ Not Listed Did Not Grant RW to Vehicle tention Did Not Grant RW to Vehicle	70 82	Female	76 54	Male						Lane 1 Increasing Milepost
8212 None 8213 None	e Not Grant RW to Vehicle Inattention	69	Female			Unknown Distraction		Male			Lane 1 Decreasing Milepost Lane 1 Increasing Milepost Lane 1 Decreasing Milepost Lane 1 Decreasing Milepost
8215 Follor 8216 Appa	Not Grant Pik Ve Vehicle The Manager of Man	38 18	Female None	58	Female						
8217 Did N 8218 None	Not Grant RW to Vehicle	82	Mable Matie Driver Not Distracted Female Driver Not Distracted		Female						Past Right Shoulder Decreasing Milepost Lane 1 Decreasing Milepost Lane 1 Increasing Milepost
8219 Inatte 8220 Appa	iention Other Contributing Circ Not Listed	Non Motorist on Wrong Side of Road 57	Female None Female None More More None	66 43	Male						Lane 1 Decreasing Mlepost
8222 Exces 8223 De-	er Commonung Cife Not Listed seding Reas. Safe Speed er Interacting with Passengers. Anim. Editor Too Clorek	57 19	mare	46	Male Male Male						Lane 1 Decreasing Milepost Lane 1 Increasing Milepost Past Right Shoulder Decreasing Milepost Lane 1 Increasing Milepost
8224 Inatte 8225 Follow	ention Follow 100 Closely	26 22 28	Female	64 86 56	Mule Mule Mule Mule Mule Mule Mule Mule						Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost Lane 1 Decreasing Miepost
8226 Disre 8227 Disre	ecting Ren. Sele Speed relating New York Control Selection Select	38	Male None Male None	15	Female Female						Lane 1 Decreasing Miepost Lane 2 Decreasing Miepost
8228 Did N 8229 Unde	Not Grant RW to Vehicle er Influence of Drugs	56 20	Male None	44 73	Female Female Female Male						Lane 2 Decreasing Milepost Lane 1 Decreasing Milepost Lane 2 Decreasing Milepost Lane 1 Increasing Milepost
8230 Did N 8231 Inatte	NOT GRAFT KNV to Vehicle sention	64 75	Female					None		83 Male	Lane 1 Increasing Miepost Lane 1 Increasing Miepost Past Right Shoulder Decreasing Miepost
8233 Inatte 8234 Appa	arently /sleep or Fatigued Improper Backing arently /sleep or Fatigued		Male Female Fem		Male						Past Right Shoulder Decreasing Milepost Lane 1 Decreasing Milepost Past Right Shoulder Decreasing Milepost Past Right Shoulder Increasing Milepost
8235 Appa 8236 Unde	arently Alexep or Fatigued er Influence of Drugs Exceeding Reas. Safe Speed	61	Male								
8237 Inatte 8238 Appa	seredy fathers or Fedinged mention improper Blacking mentify history or Fedinged mentify history or Fedinged mentify history or Fedinged mentify history or Fedinged mention mentify history or Fedinged mention mentify fathers mentify fathe	71 32 Exceeding Reas . Safe Speed 38	Female Female Female Male Mule Mule								Past Right Shoulder Increasing Milepost Past Right Shoulder Decreasing Milepost Past Right Shoulder increasing Milepost
		Exceeding Reas, Sate Speed 38	INSIE	1			1	1	1	1 1	Past Kinnt Spoulder Increasing Milenost

	WA_STATE_PLANE WA_S SOUTH_X S 2010_FORWARD 2010	FORWARD YEAR MIX TYPE	COL SEVERITY	COL_HUM FACTOR	COL COL AGE IMPARED	COL_AGE SIMPAIRED CO	COL SURFACE ONDITIONS	COL_LIGHT CONDITIONS	COL LOCATION	COL STUDY GEOMETRY AREA	COL VUL_USER	COL COL TYPE SEVERITY TEXT TEXT	COL_HUMAN FACTOR_Decoded	COL MVIW	COL_LIGHT CONDITIONS Decoded	COL_SURFACE CONDITIONS Decoded	COL_LOCATION Decoded	COL_RISKY BEHAVIOR	Intersection Emphasis Roadway Area_Name Area Departure
8099 8100 8101	1110566.33 1110544.55 1110612.69	932839.9 2014 2 Vehicles/Parties Rear-End 932842.98 2017 2 Vehicles/Parties Rear-End 932815.67 2014 2 Vehicles/Parties Rear-End	Property Damage Only Property Damage Only Minor or Possible Injury	Follow Too Closely Follow Too Closely Inattention	Under 25 No 25-34 No 65+ No			Dark-No Street Lights/Street Lights Off Daylight Daylight	At Driveway or Intersection Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Straight & Level Curve & Level Curve & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+		Follow Too Closely Follow Too Closely Inattention	Dark-No Daylight Daylight	o Street Lights/Street Lights Off it	Dry Dry	At Driveway or Intersection Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Follow Too Closely Follow Too Closely Inattention	No No No
8102 8103	1110594.97 1110602.65	932840.05 2015 2 Vehicles/Parties Rear-End 932829.07 2016 2 Vehicles/Parties Rear-End	Minor or Possible Injury Suspected Serious Injury	Speeding Operating Defective Equipment	Under 25 No 55-64 No		Dry Dry	Daylight Daylight	At Driveway or Intersection At Driveway or Intersection	Curve & Level Curve & Level	No Vulnerable Road User Involved Motorcycle		Speeding Operating Defective Equipment	Daylight Daylight	t t		At Driveway or Intersection At Driveway or Intersection	Speeding No Risky Driving Behaviors Involved	No No
8104 8105 8106	1110990.58 1110791.06 1110930.52	932828.8 2019 2 Vehicles/Parties Rear-End 932742.39 2016 2 Vehicles/Parties Rear-End 932672 2018 2 Vehicles/Parties Rear-End	Property Damage Only Property Damage Only Minor or Possible Injury	Inattention Speeding	251-34 No Under 25 No 55-64 No		Dry [Daylight Daylight Daylight	At Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved		Inattention Speeding	Daylight Daylight Daylight	it it	Dry Dry Wet	At Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Inattention Inattention Speeding	No No
8107 8108 8109	1110992.12 1111475.48 1111475.48	93224.59.2017 1 Vehicle/Party Struck Object or Debris On Road 932318.85 2014 2 Vehicles/Parties 932318.85 2014 2 Vehicles/Parties 932318.85 2015 3 Vehicles/Parties Sideswipe	Property Damage Only	Other/Unknown Inattention Inattention	Under 25 No 25-34 No 55-64 No			Daylight Daylight Daylight	Not at Intersection, Driveway Roundabout Not at Intersection, Driveway Roundabout Not at Intersection, Driveway Roundabout Not at Intersection, Driveway Roundabout	Straight & Level Straight & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+		None Inattention Inattention	Daylight Daylight Daylight	I.	Dry Dry Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Inattention Inattention	No No No
8110	1111475.48 1111475.48 1111476.58	932218.85 2015 3 Vehicles/Parties Sideswipe 932318.85 2015 1 Vehicles/Party Overturned 932317.88 2016 2 Vehicles/Parties Reat-End		Inattention Distracted Driving Other/Unknown Follow Top Closely	65+ No Under 25 No 65+ No		Wet [Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout	Straight & Level Straight & Grade Curve & Grade Curve & Grade	Older Road User 65+ No Vulnerable Road User Involved Older Road User 65+		Distracted Driving None Follow Top Closely	Dark-No	o Street Lights/Street Lights Off o Street Lights/Street Lights Off	Wet	Not at Intersection/Driveway/Roundabout	Distracted Driving No Risky Driving Behaviors Involved Follow Top Closely	Yes Yes No
8111 8112 8113 8114	1111463.77 1111506.37		Property Damage Only Property Damage Only	Under Influence of Alcohol/Drugs/Medication Inattention	25-34 Under Influence of Alcohol Under 25 No	25-34	Wet Dry D	Dark-No Street Lights/Street Lights Off Daylight Daylight Daylight	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Straight & Grade Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved		Under Influence of Alcohol/Drugs/Medication Inattention	Dayigiii				Follow Too Closely Under Influence of Alcohol/Drugs/Medicatio Inattential Na Betton	n Yes No
8115 8116 8117 8118	1111781.13 1111768.84	932141.58 2019 1 Vehicle/Party Struck Object or Debris On Road 932110.18 2014 2 Vehicles/Parties Struck Object or Debris On Road 932118.57 2019 1 Vehicles/Parties Struck Object or Debris On Road 931924.85 2019 2 Vehicles/Parties Struck Object or Debris On Road 931924.85 2019 2 Vehicles/Parties Struck Object or Debris On Road	Minor or Possible Injury Property Damage Only	Other/Unknown Inattention Other/Unknown	35-44 No Under 25 No 45-54 No		Dry E	Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Curve & Grade Straight & Level Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved		nattention None	Dark-No Dark-No	o Street Lights/Street Lights Off o Street Lights/Street Lights Off	Dry Wet	Not at intersection/Drieway/Roundabout Not at intersection/Drieway/Roundabout	Inattention No Risky Driving Behaviors Involved	No No
8119 8120 8121 8122	1112083.26 1112083.26 1112177.89	3/31/24/26/2019 I Vehicle/Party 3/31/24/26/2019 I Vehicle/Party 3/31/24/26/2019 I Vehicle/Party 3/31/24/26/2019 I Vehicle/Party 3/31/24/2019 I Vehicle/Party 3/31/24/2016 I Vehicle/Party	Property Damage Only Property Damage Only	Other/Unknown Speeding Other/Unknown	25:34 NO 25:34 No 25:34 No				Not at Intersection/Driveway/Roundabout				Other Contributing Circ Not Listed Speeding None	Daylight Daylight	it it	Dry Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Speeding No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	Yes Yes
8123	1112274.31 1112372.81 1112371.29	931835.57 2018 1 Vehicle/Party Struck Object 931797.64 2016 1 Vehicle/Party Animal	Property Damage Only Property Damage Only	Speeding Distracted Driving Other/Unknown	35-44 No 25-34 No 65+ No	loe/i	Dry Dry Dry	Daylight Dark-No Street Lights/Street Lights Off Daylight	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Curve & Grade Straight & Grade Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+		Speeding Distractions Outside Vehicle None	Daylight Dark-No Daylight	t o Street Lights/Street Lights Off t				Yes No No
8124 8125 8126	1112380.48 1112420.87 1112422.85	931798.15 2017 1 Vehicle/Party Animal 931797.87 2019 1 Vehicle/Party Struck Object or Debris Cn Road 931781.42 (2017) 1 Vehicle/Party Animal 931780.77 2019 1 Vehicle/Party Animal		Other/Unknown Other/Unknown Other/Unknown	35-44 No 35-44 No 45-54 No		Wet Dry Dry Dry	Daylight Dark-No Street Lights/Street Lights Off Dawn/Dusk Daylight	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Straight & Grade Curve & Grade Straight & Grade Curve at Hillcrest	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved		None None	Dark-No Dawn/D	o Street Lights/Street Lights Off Dusk	Dry Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Rehaviors Involved	No No No
8127 8128	1112416.5 1112424.63	931779.77 2019 1 Vehicle/Party Struck Object or Debris On Road 931779.08 2019 1 Vehicle/Party Struck Object or Debris On Road	Property Damage Only Property Damage Only	Other/Unknown Other/Unknown	65+ No 65+ No			Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Curve & Grade Curve & Grade	Older Road User 65+ Older Road User 65+		None None	Dark-No	o Street Lights/Street Lights Off o Street Lights/Street Lights Off	Wet	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No
8129 8130 8131	1112677.42 1112677.42 1112734.2	931711.4 2018 1 Vehicle/Party Struck Object 931711.4 2019 1 Vehicle/Party Struck Object 931718.7 2019 1 Vehicle/Party Animal 931718.7 2016 2 Vehicle/Parties Rear-End	Property Damage Only Minor or Possible Injury	Distracted Driving Other/Unknown Inattention	Unknown No 55-64 No 25-34 No		Dry [Daylight	Not at Intersection/Driveway/Roundabout	Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved		None Inattention	Daylight Daylight	t	Dry Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Inattention	No No
8132 8133 8134	1112885.7 1113148.35 1113217.59	931677.09 2016 2 Vehicles/Parties Sideswipe 931651.75 2016 1 Vehicle/Party Animal 931646.7 2014 2 Vehicles/Parties Rear-End	Property Damage Only	Other/Unknown Other/Unknown Follow Too Closely	55-64 No 55-64 No 55-64 No		Wet E	Daylight Daylight	Not at Intersection, Driveway Roundabout Not at Intersection, Driveway Roundabout Not at Intersection, Driveway Roundabout Not at Intersection, Driveway Roundabout	Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved		None None Follow Too Closely	Daylight Daylight Daylight	ıt	Dry Wet Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Follow Too Closely	Yes No No
8135 8136 8137 8138	1113726.8 1113945.71 1113965.73	931601.56 2018 1 Vehicle/Party Animal 931556.44 2016 2 Vehicles/Parties Rear-End	Property Damage Only Property Damage Only Minor or Possible Injury Property Damage Only	Inattention	25:34 No 55:64 No 45:54 No	loed	Dry	Daylight Daylight Daylight Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout	Straight & Grade	No Vulnerable Road User Involved		None Inattention		t it o Street Lights/Street Lights Off	Dry Dry Ice/Snow/Slush	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Inattention	No No Yes
8139	1113989.07 1114252.03	931593.6 2016 1 Vehicle/Party Struck Object 931578.8 2017 1 Vehicle/Party Struck Object 931556.03 2018 1 Vehicle/Party Struck Object or Debris On Road	Property Damage Only Property Damage Only Property Damage Only	Inattention Inattention Other/Unknown	65+ No 25-34 No		Other [Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Straight & Grade Curve & Grade Straight & Grade	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved		Inattention None	Dark-No Dark-No	o Street Lights/Street Lights Off	Other/Unknown Wet	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Inattention No Risky Driving Behaviors Involved	Yes No
8140 8141 8142 8143	1114463 1114586.83 1114698.03	931537.74 2016 1 Vehicle/Party Animal 931527.01 2014 2 Vehicles/Parties Sideswipe 931516.95 2016 2 Vehicles/Parties Rear-End	Property Damage Only Property Damage Only	Other/Unknown Inattention Other/Unknown	25:34 No 45:54 No 25:34 No		Dry Dry Dry Dry	Daylight Dark-No Street Lights/Street Lights Off Daylight Daylight	Not at Intersection/DrivewayRoundabout Not at Intersection/DrivewayRoundabout Not at Intersection/DrivewayRoundabout	Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+		Inattention Other Contributing Circ Not Listed		o Street Lights/Street Lights Off		Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout		No Yes No
8144 8145	1114725.93 1114725.93 1114778.52	931514.95 2018 2 Vehicles/Parties Turning 931514.95 2018 2 Vehicles/Parties Rear-End 931510.39 2016 2 Vehicles/Parties Rear-End	Property Damage Only Property Damage Only	Improper Turn/Merge Follow Too Closely Speeding	55+ No 25-34 No Under 25 No						No Vulnerable Road User Involved		Improper Backing/Passing/Turning/Parking/Signaling Follow Too Closely Speeding	Daylight Daylight Daylight		Dry Dry Wet	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	No Risky Driving Behaviors Involved Follow Too Closely Speeding	No No No
8146 8147 8148	1114777.97 1114847.97 1115008.04	931510.39 2016 2 Vehicles/Paries Rear-End 931510.44 2017 2 Vehicles/Paries Rear-End 931503.77 2017 2 Vehicles/Paries Geswipe 931490.4 2015 1 Vehicle-Party Struck Object	Minor or Possible Injury	Inattention Other/Unknown Inattention	25-34 No 55-64 No 65-64 No		Dry	Daylight Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off Dark-No Street Lights/Street Lights Off	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Straight & Grade Straight & Grade Straight & Grade Curve & Grade	No Vuinerable Road User Involved No Vuinerable Road User Involved No Vuinerable Road User Involved Older Road User 65+		Inattention Other Contributing Circ Not Listed Inattention	Dark-No Dark-No	t o Street Lights/Street Lights Off o Street Lights/Street Lights Off o Street Lights/Street Lights Off	Dry Dry Wet	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Inattention No Risky Driving Behaviors Involved Inattention	No Yes Yes
8148 8149 8150 8151	1115128.5 1115316.85	931471.88 2018 1 Vehicle/Party Animal 931421.43 2014 2 Vehicles/Party Pople 931424.12 2014 2 Vehicles/Parties Pople	Property Damage Only Minor or Possible Injury Minor or Possible Injury	Other/Unknown Inattention	55-64 No 55+ No		Wet E	Dark-No Street Lights/Street Lights Off Daylight	Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Straight & Grade Straight & Grade Curve & Grade	No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+		None Inattention	Dark-No Daylight	o Street Lights/Street Lights Off	Wet Dry	Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Inattention No Risky Driving Behaviors Involved Inattention	No No
8152 8153	1115316.85 1115316.85	931421.43 2014 2 Vehicles/Parties Angle 931421.43 2014 2 Vehicles/Parties Rear-End	Minor or Possible Injury Minor or Possible Injury	Fail to Yield Distracted Driving	Under 25 No 65+ No		Wet [At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Curve & Grade Straight & Grade	No Vulnerable Road User Involved Older Road User 65+		Fail to Yield Distracted Driving	Daylight Daylight	Dusk	Wet	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Fail to Yield Distracted Driving	No No
8154 8155 8156	1115316.85 1115316.17 1115308.54	931421.43 2014 2 Vehides/Pariles Turning 931419.45 2015 2 Vehides/Pariles Angle 931425.79 2015 2 Vehides/Pariles Angle	Suspected Serious Injury Property Damage Only Property Damage Only	Fail to Yield Inattention Fail to Yield	Under 25 No 65+ No 65+ No		Dry Dry Dry Dry	Dawn/Dusk Daylight Daylight	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Curve & Grade Curve & Grade Curve & Grade	No Vuinerable Road User Involved Older Road User 65+ Older Road User 65+		Fail to Yield Inattention Fail to Yield	Dawn/D Daylight Daylight	Dusk it it	Dry Wet Dry	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Fail to Yield Inattention Fail to Yield	No No No
8157 8158 8159	1115316.85 1115315.54 1115316.85	931421.43 2015 2 Vehicles/Parties Angle 931417.64 2015 2 Vehicles/Parties Angle 931427.02 2015 1 Vehicles/Parties Angle 931427.02 2015 6 Vehicles/Parties Angle	Property Damage Only Minor or Possible Injury	Inattention Inattention Other/Unknown	85+ No 25-34 No 35-44 No			Daylight	At Driveway or Intersection At Driveway or Intersection At Intersection and Not Related	Straight & Grade	Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+		Inattention Inattention Other Contributing Circ Not Listed	Daylight		Dry Dry Dry	At Driveway or Intersection At Driveway or Intersection At Intersection and Not Related At Driveway or Intersection	Inattention Inattention No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No No
8160	1115295.1 1115293.06	931429.72 2015 2 Vehicles/Parties Angle	Property Damage Only	Improper Turn/Merge Inattention	65+ No 65+ No		Dry D	Daylight	At Driveway or Intersection At Driveway or Intersection	Straight & Grade	Older Road User 65+		Improper Turn/Merge Inattention Inattention	Daylight	ıt	Dry	At Driveway or Intersection	Inattention	No No
8161 8162 8163 8164	1115307.55 1115316.85 1115301.83	931424.59 2016 2 Vehicles/Parties Angle 931421.42 2016 2 Vehicles/Parties Sideswipe 931420.12 2016 2 Vehicles/Parties Angle		Inattention Other/Unknown Inattention	55+ No 55-64 No		Dry	Daylight Dark-Street Lights On Daylight	At Driveway or Intersection At Intersection and Not Related At Driveway or Intersection	Straight & Grade Straight & Grade Curve & Grade	Older Road User 65+ Older Road User 65+ No Vulnerable Road User Involved		None Inattention	Daylight Dark-Str Daylight Daylight	treet Lights On It	Dry Dry Dry	At Driveway or Intersection At Intersection and Not Related At Driveway or Intersection	Inattention No Risky Driving Behaviors Involved Inattention	Yes No
8165 8166 8167 8168	1115294.7 1115296.67 1115298.63	931432.86 2016 2 Vehicles/Parties	Minor or Possible Injury Minor or Possible Injury	Inattention Fail to Yield Other/Unknown	45-64 No Under 25 No 55-64 No		Dry Dry Dry Dry		At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Curve & Grade Straight & Grade Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+		Inattention Fail to Yield Other Contributing Circ Not Listed	Daylight Daylight Daylight	it	Dry Dry Dry	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Inattention Fail to Yield No Risky Driving Behaviors Involved	No No No
8169 8170	1115298.63 1115298.61 1115298.63	931428.99 2016 2 Vehicles/Parties Angle 931428.94 2017 2 Vehicles/Parties Angle	Minor or Possible Injury Property Damage Only	Inattention Inattention Other/Linknown	65+ No 25-34 No		Dry [Daylight Dark-No Street Lights/Street Lights Off	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Curve & Grade Curve & Grade	No Vulnerable Road User Involved		Inattention Inattention Other Contributing Circ Not Listed	Daylight	rt o Street Lights/Street Lights Off	Dry Dry	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Inattention	No No No
8171 8172	1115298.63 1115298.63 1115298.63	931428.99 2017 2 Vehides/Paries Angle 931428.99 2017 2 Vehides/Paries Angle 931428.99 2017 2 Vehides/Paries Angle 931428.99 2018 2 Vehides/Paries Angle	Property Damage Only Property Damage Only Property Damage Only	Non Motorist on Wrong Side of Road	Under 25 No 25:34 No Under 25 No		Dry Dry Dry Wet Dry Wet Dry	Daylight Dawn/Dusk	At Driveway or Intersection At Driveway or Intersection	Curve & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved		Other Contributing Circ Not Listed Non Motorist on Wrong Side of Road	Daylight Dawn/D	Dusk	Dry Wet	At Driveway or Intersection At Driveway or Intersection	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved	No No
8173 8174 8175 8176	1115298.63 1115295.37 1115298.54	931425.03 2018 2 Vehicles/Parties Angle 931432.63 2018 1 Vehicle/Party Operturned	Property Damage Only Minor or Possible Injury	Fail to Yield Under Influence of Alcohol/Drugs/Medication Speeding	90-94 NO Under Influence of Acohol Under 25 No		Dry Dry	Daylight Daylight	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Unknown	No Vulnerable Road User Involved No Vulnerable Road User Involved Motorcycle No Vulnerable Road User Involved		Fail to Yield Under Influence of Alcohol/Drugs/Medication Speeding	Daylight Daylight	o Street Lights/Street Lights Off it	Dry Dry	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Fail to Yield Under Influence of Alcohol/Drugs/Medication Speeding	n No No Yes
8176 8177 8178	1115293 1115298.62 1115298.63	931427.98 2018 2 Vehicles/Paries Angle 931428.98 2018 2 Vehicles/Paries Rear-End 931428.99 2018 2 Vehicles/Paries Angle	Minor or Possible Injury Property Damage Only Property Damage Only	Operating Defective Equipment Inattention	Under 25 No Under 25 No Under 25 No		Dry E	Daylight Daylight Daylight	At Driveway or Intersection At Intersection and Not Related At Driveway or Intersection				Inattention Operating Defective Equipment Inattention	Daylight Daylight Daylight	it it	Dry Dry Wet	At Driveway or Intersection At Intersection and Not Related At Driveway or Intersection	No Risky Driving Behaviors Involved Inattention	No No No
8177 8178 8179 8180 8181	1115298.63 1115298.54 1115298.54	931428.99 2018 2 Vehicles/Paries Angle 931428.99 2019 2 Vehicles/Paries Angle 931432.83 2019 2 Vehicles/Paries Angle 931432.83 2019 2 Vehicles/Paries Rear-End	Property Damage Only Property Damage Only Minor or Possible Injury Minor or Possible Injury	Inattention Under Influence of Alcohol/Drugs/Medication Other/Unknown	55+ No 25-34 Under Influence of Acohol Under 25 No	25-34	Dry [Daylight Daylight Daylight	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Curve & Grade Curve & Grade	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved		Inattention Under Influence of Alcohol/Drugs/Medication Other Contributing Circ Not Listed	Daylight Daylight Daylight	it it	Dry Dry Dry	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Inattention Under Influence of Alcohol/Drugs/Medicatio No Risky Driving Behaviors Involved	No No No
8182 8183 8184	1115325.64 1115531.33	931418.51 2017 2 Vehicles/Parties Rear-End 93133.51 2017 2 Vehicles/Parties Rear-End 931251.54 2018 1 Vehicles/Parties Wick Object 931223.6 2014 2 Vehicles/Parties Rear-End	Property Damage Only	Inattention Distracted Driving Other/Unknown	Under 25 No Under 25 No SS-64 No		Dry L	Daylight Daylight	Intersection Related but Not at Intersection	Curve & Grade	Motorcycle No Vulnerable Road User Involved		Inattention Distracted Driving Other Contributing Circ Not Listed	Daylight Daylight	t t	Dry Dry	Intersection Related but Not at Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout		No No Ver
8185	1115945.45 1115715.71 1115902.55	931070.13 2018 3 Vehicles/Parties Rear-End	Minor or Possible Injury Property Damage Only	Inattention Inattention	25-34 No 25-34 No		Dry Dry	Daylight	Not at Intersection/Driveway/Roundabout	Straight & Level	Motorcycle No Vulnerable Road User Involved No Vulnerable Road User Involved		Inattention	Daylight Daylight	ıt	Dry	Not at Intersection/Driveway/Roundabout	Inattention	No No
8186 8187 8188 8189	1116297.14 1116297.14 1116293.77	930721.61 2014 4 Vehicles/Parties Rear-End 930721.61 2015 3 Vehicles/Parties Rear-End 930720.6 2015 1 Vehicle/Party Overturned	Minor or Possible Injury	Inattention Follow Too Closely Other/Unknown	55+ No Under 25 No 25-34 No		Wet [Daylight Daylight	Not at intersection/Driveway/koundabout	Straight & Grade	Older Road User 65+ No Vulnerable Road User Involved Motorcycle		Follow Too Closely Other Contributing Circ Not Listed	Daylight Daylight Daylight	t .	Dry Wet	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Follow Too Closely No Risky Driving Behaviors Involved	No No Yes
8190 8191 8192	1116290.16 1116570.15 1117465.2	930727.75 2017 2 Vehicles/Parties Rear-End 930480.15 2014 1 Vehicles/Party Struck Object 929686.47 2016 2 Vehicles/Parties Struck Parked Vehicle	Minor or Possible Injury Minor or Possible Injury Property Damage Only	Follow Too Closely Speeding Other/Unknown	Under 25 No 35-44 No 65+ No		Dry Dry Down Dry	Daylight	Not at Intersection/DrivewayRoundabout Not at Intersection/DrivewayRoundabout Not at Intersection/DrivewayRoundabout	Straight & Level Straight & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Older Road User 65+		Follow Too Closely Speeding Other Contributing Circ Not Listed	Dawn/D Daylight Daylight	t	Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Follow Too Closely	No Yes Yes
8193	1117697.96 1117761.26 1118171.78	929482.64 2017 3 Vehicles/Parties Rear-End 929428.53 2017 1 Vehicle/Party Struck Object	Minor or Possible Injury Minor or Possible Injury	Inattention Speeding Distracted Driving	65+ No Under 25 No		Dry [Daylight Daylight	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Straight & Level	Older Road User 65+ No Vulnerable Road User Involved		Inattention Speeding Unknown Distraction	Daylight Daylight	t				No Yes Ver
8194 8195 8196 8197	1118247.2 1118247.2	929067 2018 6 Vehicles:Parties Sideswipe 929968 7 2014 1 Vehicle:Party Struck Object 928996.87 2015 1 Vehicle:Party Struck Object	Property Damage Only	Inattention Speeding	55+ No 55-64 No		Wet I	Daylight Daylight Dark-Street Lights On	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Straight & Level Straight & Level	No Vulnerable Road User Involved Older Road User 65+ No Vulnerable Road User Involved		Inattention Speeding	Daylight Daylight Dark-Str		Dry Wet	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Inattention Speeding	Yes Yes
8198 8199 8200 8201	1118364.21 1118403.21 1118390.42	928993.89 2014 1 Vehicle/Party Struck Object 928858.89 2014 2 Vehicles/Parties 928836.04 2015 1 Vehicle/Party Struck Object 928824.39 2014 1 Vehicle/Party Struck Object	Property Damage Only	Speeding Follow Too Closely Inattention Disregard Traffic Control Device/Sign	55-64 No 25-34 No 35-44 No		Dry [Daylight Daylight Daylight	Not at Intersection/DrivewayRoundabout Not at Intersection/DrivewayRoundabout Not at Intersection/DrivewayRoundabout	Straight & Grade	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved		Speeding Follow Too Closely Inattention	Daylight Daylight Daylight		Dry Dry Wet Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Follow Too Closely Inattention	No Yes
8201 8202 8203	1118442.21 1118559.21 1118601.06	928720.91 2014 4 Vehicles/Parties Sideswipe	Property Damage Only Suspected Serious Injury Property Damage Only	Apparently Asleep, Fatigued or III	55-64 No 25-34 Apparently Asleep, Fatigued or III 55-64 No	25-34	Wet D	Daylight Daylight Dawn/Dusk	Not at Intersection/Driveway/Roundabout	Straight & Level	No Vulnerable Road User Involved		Disregard Traffic Control Device/Sign Apparently As leep or Fatigued Speeding	Daylight Daylight Dawn/D	t t Dusk				Yes Yes No
8204 8205 8206	1118636.48 1118716.7 1118754.22	928687.95 2015 2 Vehides/Paries Rear-End 928652.56 2017 2 Vehides/Paries Rear-End 928953.56 2018 2 Vehides/Paries Sideswipe 92854.64 2016 4 Vehides/Paries Rear-End 92854.64 9	Minor or Possible Injury Minor or Possible Injury Minor or Possible Injury	Speeding Distracted Driving Speeding Inattention	45-54 No 55-64 No 55-64 No		Wet L	Daylight Daylight Daylight	Not at intersection/Driveway/koundabout	Straight & Level Straight & Level Straight & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved Motorcycle No Vulnerable Road User Involved		Smoking Speeding Inattention	Daylight Daylight Daylight	at at	Dry Wet Wet	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Driveway Related but Not at Driveway	Distracted Driving Speeding Inattention	No Yes No
8207 8208	1082224.22 1082224.22	991508.8 2014 2 Vehicles/Parties Rear-End 991508.8 2015 2 Vehicles/Parties Rear-End	Property Damage Only Property Damage Only	Inattention Other/Unknown	55-64 No 65+ No		Dry E	Daylight Daylight	At Driveway or Intersection At Driveway or Intersection	Straight & Level Straight & Level	No Vulnerable Road User Involved Older Road User 65+		Inattention Other Contributing Circ Not Listed	Daylight Daylight		Dry Dry	At Driveway or Intersection At Driveway or Intersection	No Risky Driving Behaviors Involved	No No
8209 8210 8211	1082224.96 1082511.76 1082870.04	991505.18 (2016 3 Vehicles:Parties Angle 991566.19 (2019 2 Vehicles:Parties Angle 991642.8 (2018 2 Vehicles:Parties Angle	Property Damage Only	Fail to Yield Other/Unknown Inattention	90-94 NO 65+ No 65+ No		Wet E Dry E Dry E	zayigiit	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Straight & Grade	No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+		Fail to Yield Other Contributing Circ Not Listed Inattention	Daylight Daylight	ıt	Dry Dry	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	No Risky Driving Behaviors Involved Inattention	No No No
8211 8212 8213 8214	1083050.54 1084483.36 1084521.34	991687.73.2015 2 Vehides/Partes Pedalcyclist 992377.9.1.2019 1 Vehides/Party Animal 992405.44.2014 2 Vehides/Parties Turning	Property Damage Only Property Damage Only Minor or Possible Injury	Other/Unknown Other/Unknown Fail to Yield	35-44 No 65+ No 35-44 No		Dry Dry Dry Dry	Daylight Daylight	At Driveway or Intersection Not at Intersection, Driveway (Roundabout At Driveway or Intersection	Straight & Level Curve & Level	Pedalcydist Older Road User 65+ No Vulnerable Road User Involved		None None Fail to Yield	Daylight Daylight Daylight	it it	Dry Dry Dry	At Driveway or Intersection Not at Intersection/Driveway/Roundabout At Driveway or Intersection	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Fail to Yield	No No No
8215 8216 8217 8218	1084543.55 1084762.96 1085561.86	992415.41 (2015 2 Vehicles/Parties Angle 992514.37 (2017 1 Vehicle/Party Struck Object 992554.93 (2018 2 Vehicles/Parties Angle	Property Damage Only	Follow Too Closely Apparently Asleep, Fatigued or III Fail to Yield Other/Unknown	Under 25 No 65+ Apparently Asleep, Fatigued or III 65+ No		Dry Dry Dry Dry	Daylight Dawn/Dusk	At Driveway or Intersection Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Straight & Level	No Vulnerable Road User Involved Older Road User 65+ Older Road User 65+		Follow Too Closely Apparently Asleep or Fatigued Fail to Yield	Dayigni Dawn/D	Dusk	Dry Dry	At Driveway or Intersection Not at Intersection/Driveway/Roundabout	Follow Too Closely Apparently Asleep, Fatigued or III Fail to Yield No Risky Driving Behaviors Involved	No Yes No
8218 8219	1085772.02 1086651.61	992515.81 2019 1 Vehicle/Party Animal 992329.33 2018 2 Vehicles/Parties Turning			55-64 No 25-34 No 85-64 Apparently for leap Estimated ***		Wet [Daylight Daylight	Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Straight & Grade Unknown	No Vulnerable Road User Involved No Vulnerable Road User Involved		None Inattention	Daylight Daylight Daylight	T .	Wet	Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Inattention	No No
8219 8220 8221 8222	1086680.38 1087145.5 1087242.52	992323.15 2017 2 Vehicles/Parties Sideswipe 9922323.77 2016 2 Vehicles/Parties Sideswipe 992193.67 2015 1 Vehicles/Party Struck Object	Property Damage Only	Apparently Asleep, Fatigued or III Other/Unknown Speeding	55-64 Apparently Asleep, Fatigued or III 55-64 No Under 25 No		Dry	Daylight Dark-Street Lights On	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Straight & Level Straight & Level Unknown	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved		Apparently Asleep or Fatigued Other Contributing Circ Not Listed Speeding	Daylight Daylight Dark-Str	t treet Lights On	Dry Wet	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Apparently Asleep, Fatigued or III No Risky Driving Behaviors Involved Speeding	Yes Yes Yes
8223 8224 8225 8226	1087248.83 1087283.98 1087807.95	992201.07 2018 2 Vehicles/Paries Rear-End 992188.33 2019 2 Vehicles/Paries Angle 992080.99 2014 2 Vehicles/Paries Turning 992080.17 2014 2 Vehicles/Paries Angle	Property Damage Only	Distracted Driving Inattention Follow Too Closely Disregard Traffic Control Device/Sign	25-34 No Under 25 No 25-34 No		Dry I	Daylight Daylight	Not at Intersection/Driveway/Roundabout At Driveway or Intersection		No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved		Distracted Driving Inattention Follow Too Closely	Daylight Daylight Daylight	ıt		Not at Intersection/Driveway/Roundabout At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection	Distracted Driving Inattention Follow Too Closely No Risky Driving Behaviors Involved	No No No
8226 8227 8228	1087811.77 1087817.23 1087817.23	992078.99 2015 2 Vehicles/Parties Angle	Property Damage Only Property Damage Only	Disregard Traffic Control Device/Sign Fail to Yield	35-44 No 45-54 No 55-64 No		Dry [Daylight Daylight Daylight Daylight Daylight	At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection At Driveway or Intersection				Follow Too Closely Disregard Traffic Control Device/Sign Disregard Traffic Control Device/Sign Fail to Yield	Daylight Daylight Daylight	at at	Dry	At Driveway or Intersection	No Risky Driving Behaviors Involved No Risky Driving Behaviors Involved Fail to Yield	No No No
8227 8228 8229 8230 8231	1087885.03 1087985.74	992078-99 2017 2 Vehicles/Parties Angle 992060.91 2019 3 Vehicles/Parties Rear-End 992030.28 2017 2 Vehicles/Parties Turning 992032.6 2017 2 Vehicles/Parties Pedestrian	Property Damage Only Property Damage Only	Under Influence of Alcohol/Drugs/Medication Fail to Yield Inattention	55-64 No Under 25 Under Influence of Drugs 55-64 No No.	Under 25 Ice/	Dry Dry Dry Snow/Slush Dry	Daylight Daylight	At Driveway or Intersection At Driveway or Intersection Not at Intersection/Driveway/Roundabout	Straight & Level Straight & Level Straight & Level	No Vulnerable Road User Involved No Vulnerable Road User Involved No Vulnerable Road User Involved Pedestrian and Older Road User 65+		Under Influence of Alcohol/Drugs/Medication Fail to Yield	Daylight Daylight	it it	Dry Ice/Snow/Slush	At Driveway or Intersection Intersection Related but Not at Intersection At Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Under Influence of Alcohol/Drugs/Medicatio Fail to Yield	No No
8231 8232 8233 8234	1087995.96 1088267.91 1088644.93	991857.32 2018 1 Vehicle/Party Struck Object	Suspected Serious Injury Property Damage Only Property Damage Only	Inattention Apparently Asleep, Fatigued or III Inattention	155+ No 155+ No 155+ Apparently Asleep, Fatigued or III 155+ No 15	65+	Dry Dry Dry Dry	Daylight	Not at Intersection/Driveway/Roundabout	Unknown Curve & Level Straight & Grade	Pedestrian and Older Road User 65+		Inattention Apparently Asleep or Fatigued Inattention	Daylight Daylight Daylight	rt it	Dry Dry	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Apparently Asleep, Fatigued or III Inattention	Yes No
8234 8235 8236	1089067.57 1089396.7 1089464.88	991280.17 2015 2 Vehicles/Paries Angle 990541.49 2014 1 Vehicles/Paries Angle 990541.49 2014 1 Vehicles/Party Struck Object 989988.91 2019 1 Vehicles/Party Struck Object Struck Object 1 Vehicles/Party 1 Vehicles	Property Damage Only	Inattention Apparently Asleep, Fatigued or III Apparently Asleep, Fatigued or III Under Influence of Alcohol/Drugs/Medication	65+ Apparently Asleep, Fatigued or III 65+ Apparently Asleep, Fatigued or III 55-64 Under Influence of Drugs	65+ 65+ 55-64	Wet [Daylight Daylight Dark-Street Lights On	At Driveway or Intersection Not at Intersection, Driveway Roundabout Not at Intersection, Driveway Roundabout Not at Intersection, Driveway Roundabout	Straight & Grade Curve in Sag Curve & Grade	Older Road User 65+ Older Road User 65+ No Vulnerable Road User Involved		Apparently As leep or Fatigued Apparently As leep or Fatigued Under Influence of Alcohol/Drugs/Medication	Daylight Daylight Dark-Str	it It Ireet Lights On	Drv	At Driveway or Intersection Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout	Apparently Asleep, Fatigued or III Apparently Asleep, Fatigued or III Under Influence of Alcohol/Drugs/Medicatio	Yes Yes Yes
8237 8238 8239	1089995.93 1090046.46 1090046.46	989654.62 (2019 1 Vehicle/Party Struck Object 989639.77 (2018 1 Vehicle/Party Struck Object 989639.77 (2019 1 Vehicle/Party Struck Object 989639.77 (2019 1 Vehicle/Party Struck Object	Property Damage Only Property Damage Only Suspected Serious Injury	Inattention Apparently Asleep, Fatigued or III Under Influence of Alcohol/Drugs/Medication	65+ No 25-34 Apparently Asleep, Fatigued or III 35-44 Under Influence of Acohol		Wet L	Daylight			Older Road User 65+ No Vulnerable Road User Involved No Vulnerable Road User Involved		Inattention Driver Physical Condition Under Influence of Alcohol/Drugs/Medication	Daylight Daylight	t t	Wet Wet Drv	Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout Not at Intersection/Driveway/Roundabout At Driveway or Intersection	Inattention Apparently Asleep, Fatigued or III Under Influence of Alcohol/Drugs/Mersonson	Yes Yes Yes
8240	1091254.55	989537.26 2014 1 Vehicle/Party Animal	Property Damage Only	Other/Unknown	55-64 No		Wet [Dark-Street Lights On	At Driveway or Intersection	Curve & Grade	No Vulnerable Road User Involved		None	Daylight Dark-Str	treet Lights On	Wet	At Driveway or Intersection	No Risky Driving Behaviors Involved	No

NGITAL)			







ADOPTING THE SIX YEAR TRANSPORTATION IMPROVEMENT PROGRAM (2021-2026)

THE BOARD OF CLALLAM COUNTY COMMISSIONERS finds as follows:

- 1. RCW 36.81.121 and WAC 136-15-010 requires the Board of County Commissioners to annually adopt a Six Year Transportation Improvement Program.
- 2. Pursuant to WAC 136-20-060 the County Engineer prepared a 2020 Bridge Inspection Report to inform bridge priorities in preparing the Six Year Transportation Improvement Program.
- Six Year Transportation Improvement Program projects consider traffic volumes, roadway conditions, geometrics, safety and accident history; bridge inspection report, local significance, and available funding.
- 4. W.A.C. 136-20-060 and W.A.C. 136-14-050 requires that the Board has the Engineer's Bridge Report and the Priority Array available to consider at the time of determining the program.
- 5. Clallam County Planning Commission held a work session on the Six Year Transportation Improvement Program (2021-2026) on October 21, 2020.
- 6. The Clallam County Department of Community Development Issued a determination of non-significance (DNS) on the Draft Six-Year TIP (2021-2026) following review of the environmental checklist under the State Environmental Policy Act (SEPA).
- 7. The Clallam County Planning Commissioner held a public hearing on the Six Year Transportation Improvement Program (2021-2026) on November 4, 2020, and recommended approval of the Six-Year Transportation Improvement Program.
- 8. Pursuant to RCW 36.81.121, a public hearing before the Board is required to be held so all taxpayers have a chance to comment on the proposed program.

NOW, THEREFORE, BE IT RESOLVED by the Board of Clallam County Commissioners, in consideration of the above findings of fact:

- 1. A public hearing was held on November 24, 2020 on the Six Year Transportation Improvement Program for 2021-2026.
- 2. That the attached Six Year Transportation Improvement Program is hereby adopted from 2021 through 2026.

PASSED AND ADOPTED this	24	day of
	THE BOTH	BOARD OF CLALLAM COUNTY COMMISSIONERS
	TAM COUNTY	Mark Ozias, Chair
ATTEST:	Commission Z	Randy Johnson
L. Opres	ANGELERINI	afrach
Loni Gores, CMC, Clerk of the	e Board	Bill Peach

Clallam County Six-Year (2021-2026) Transportation Improvement Program (TIP)

Guide to Codes and Acronyms

Road Functional Class

06=Rural Minor Arterial

16≃Urban Minor Arterial

19=Urban Local Access

07=Rural Major Collector

17=Urban Collector Arterial

08=Rural Minor Collector

09=Rural Local Access

Federal Fund Codes

FEMA=Federal Emergency Management Agency Hazard Milgation Program

FLAP=Federal Lands Access Program

HSIP=Highway Safety Improvement Program

STBG=Surface Transportation Block Program

Improvement Types

01=New construction

08=New Bridge Construction

02=Relocation Project

09=Bridge or Culvert Replacement (e.g., fish passage project)

03=Reconstruction

10=Bridge Rehabilitation

04=Major Widening

11=Minor Bridge Rehabiliation

05=Minor Widening

12=Safety/Traffic Operation

06=Other Enhancements

23=Transit

07=Resurfacing

32=Non-Motor Vehicle Project (e.g., Olympic Discovery Trail)

State Fund Codes

CAPP=County Arterial Preservation Program

RAP=Rural Arterial Program

TA=Transportattion Alternatives

TIA=Transportation Improvement Account

UATA=Urban Arterial Trust Account

RCO-FBRB=Recreation Conservation Office-Fish Berrier Removal Board

RCO-SRFB=Recreation Conservation Office-Salmon Recovery Funding Board

RCO=Recreation Conservation Office

Funding Status

S-Project selected for funding. Funding has been secured for all or some project phases.

P=Planned Project. Funding has not been fully determined & may be dependent on grant or other agency. LTAC=Lodging Tax Advisory Committee Grant (County)

P/U=Planned project (Unfunded)

Other Fund Codes (local funding)

REET=Real Estate Excise Tax (County)

LEKT=Lower Elwha S'Kisliam Tribe

PVT=Private Donation

Project Phase

PE=Preliminary Engineering, R/W=Right-of-Way; Const=Construction

Utility Codes

C=Cable; P=Power; T=Telephone; W=Water; S=Sewer; G=Gas; O=Other.

*Local Funding

This column in the table shows the amount of local funds which are used on a project

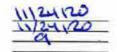
These funds come primarily from the property tax road levy, and the County's share of the

State Motor Vehicle Fuel Tax. They can also come from other local funding sources such as

the local Real Estate Excise Tax, lodging tax grants, Tribes, and other sources.

SIX YEAR TRANSPORTATION IMPROVEMENT PROGRAM 2021 TO 2026

Hearing Date : Adaption Date. Resolution No. Amendment Date:



		PROJECTIDENTIFICATION	-				1/2					PROJECT CO		DUSANDS C	F DOLLAR	5	and the same	-		
- 1	1		4			>		l l		FUNDS	NG SOURCE	INFORMATI	ON	0.00	Thomaseless		-			
2	L	A. PROJECT / ROAD NAME (ROAD LOG / BRIDGE NUMBER): PROJECT NUMBER B. BEGNINNG AND END MILEPOST (M.P.) C. DESCRIPTION OF WORK	五 2 2 3 3 4	- I	\ 0 \ ⊢	_ w 	PROJECT	MONTH	FEDERAL	FEDERAL	STATE	STATE	LOCAL	PROJECT	E	(LOCAL	IE SCHEQL AGENCY)	ALE		RALLY FUNDE JECTS ONLY
A S	B		մես a. >-	1	⊢∠ 0 w	- D	PHASE	YEAR PHASE	FUND	COST	OTHER FUND	FUNDS	PUNDS	PHASE TOTALS	1st	2nd	3rd	4th thru 6th		RWRES
•	E		3- ⊢	E9	⊢ ¬	24		STARTS		PHASE	.co5€			(See note 1	2021	2022	2023	2024-2026	ENVIR. TYPE	DATE MONTH/
	2	T	4	5	6	7	8	9	10	11	12	13	14		16	37	18	19	20	21
	1	Black Diamond Road (#31800)		S	0.48	CPTW	PE	845	HEND			- 1	30	30	20				EIS	Y
-		M.P. 266-M.P.3.40 [Hosre/Airce to Wellman Rd)	05	l.			RW		\$268K	E = 7	1		50	50	50				GE -	- "
		Widen shoulders, improve recoverable slopes, & potential separated path	(2				CONST	102		266	1		460	728		718	10		EA -	— 04
			32		1		TOTAL			268			540	808	70	718	10		NA.	
Т	2	Cadaborg Road (#94270)	03	S	0.75	C.P.T.W.S	Æ	ANA			RAP		30	30	30	1.7	12		EIB	Y
- 1		M.P. 1.01 to M.P. 1.76	07		23		RAN		1		472K		50	50	50				CE -	- "
П		Shoulder widering and resurtatong funded by Rural Artispel Program (RAP) with			1		CONST	5/21				472	289	741	701	10			EA -	- CA
	-51	County match		1			TO-AL					472	349	821	781	10	77		NA -	- "
T	3	Clettern Blough Bridge Replacement (#19406 BR1)	04	S	037	CPWTS	PE	820	STP(BR)	1.266				1.298	423	885			EI5	Y
-		On Frontier Street (#19400)	09				RW		5.193	87	1		_	8/	423	87			CE X	- '
- [MF 007 to MP 044	10	D			CONST			3.805			_	3,608			2.856	962		O/
1							TOTAL			5,193			_	5.193	423	952	2.656	952		- "
Т	4	Countywade Overlay Projects	07	В	0.57	CPTW	PE	1/21		7,122			144	144	74	24	24			
1		Overlay improvements at various intersections or red de sace (hypogenout the course)		11 *	-10		RAY						144	0	- /1	-	- /-		CE -	-
-				l .			CONST	601					1.066	1 058	176	176	176			
.1							TOTA.	041					1,290	1,200	200	200	200			DA DA
7	5	Dry Creek Road (#31870)	_	5	DEN	CPTW	PE	1)17		_	RAPS1.035K	90	110	200	200	5110	200			-
- 1		MP 0.00 to MP 0.68	03	"	0.00	OF 140	RAV	198			REET \$500K		120	300	_				E16	Υ.
- 1		Reconstruct Road to Major Collector Standards, Widon, Realign, Registe, and	05	1			CONST	5/20		_	LEKT \$125K		270	1,095	1,015	10			CE_	
П		and Resurface, Comparation of state, tribal, and local funding.	07	1			TOTAL	3/20			LEKE STZUK	1,035	800						EA _	DA
+	6	Edgewood Drive, Rife to Reddick Road (\$9.3710)		P	1.51	СРТУИ	PE	1.22			RAP	1,035	719	1,535	1.015	10	4		MA	-
П	•	MP. 0.67 in M.P. 2.38	03	"	1,21	C-146	RAV	1.92		_	\$2.4 mil.	-				119			E15	_ Y
1		Reconstruct Read to Major Collector Standards	05				CONST	5/23					227	227		227	-		CE _	_
1		VMden, Realign, Regrade, Resurface	07				TOTAL	3/23			Pending		2,378	2,378			1,016	1,362		LD4
+	7	Happy Valley Road (\$50300)	07	P	0.76	CPTW	PE						2.724	2 724		146	1,016	1,362		
-		MP. 0.74 to M.P. 1.44	03	,	0.10	Chalsh	RAV	1/24					50	50				50		Y
П		Recomment road north of McFarland Drive to west of 3rd. Avenue	05					1/24		_				0	11				CE _	
1		Widon Regrade, Drainage	07				CONST	1/25					500	500				500		Ov
+	8	Heath Road (#45860)	U/	-	4.50	DOTE	TOTAL	-				-	550	550				550		-
		M.P. 0.00 to M.P. 1.50		5	1.50	CPTW	PE	1/22		_			15	15		15			ES _	Y
П		Shoulderwidenry	D4				HOW!	1/22					:5	15		15			CE _	
		Silversi wer ii A	D5				CONST	5/22					85	85		65			EA _	_ 04
+	9	Hoko Fulls Bridge Repair (#91490 BHZ)	1	-	0.05	COTTAL	TOTAL						85	95		95			NA	
ı		Horo Road	11	2	40.03	РТүү	PE	1/24					25	25				25		_ Y
1		M.P. 6.66 to M.P. 6.91		1			RAW												CE _	
1							CONST	8-25					175	175				175		D#
+	40	Repair Expansion Joints, clean tridge, prepare and paint bearings, seal deck	-		-		TOTAL	1000					200	200			7	200		
	10	Hoko-Ozetle Road-Lower Hoto Wotland Coroplex Culvert Replacement	59	\$	0,31	CPTW	PE	1/21			RCO-FERS	200		200	200				EIS	. Y
		Hoko - Ozetis Road M. P. 3.67					RW				\$200K								CE X	
		Design for Replacement of Existing Understand Culvert with a Fish Passable Structure					CONST					i					1		EA	OA.
-							TOTAL			-		200		200	2:00				NA -	

Hearing Date : Adoption Date: Resolution No; Amendment Date 11-24-20

	T	PROJECT IDENTIFICATION	-					-		1000				OUSANDS	OF DOLLAR	5		777		
- 1.	E	A (NICHELL INDANERAL COADLOS INCIDES ALABOR, ACAUCATAMAN	7		1	≻				FUNDS	NG SOURCE	INFORMATI	ON		COLUMN TO SERVICE SERV		I THE			
٥	M N	A. PROJECT / ROAD NAME (ROAD LOG / BRIDGE NUMBER); PROJECT NUMBER B. BEGINNING AND END MILEPOST (M.P.) C. DESCRIPTION OF WORK	Σω	ισ =	⊒ <u>=</u>	_ w		LANCE CO.			STATE			Heaven transmi	180	(LOCAL		N.E		ECTS ONLY
L	M	C DESCRIPTION OF WORK	> 🗆	·	< 0	— ш	PROJECT	MONTH	FEDERAL	"ÉUERAL	QR.	STATE	LOCAL	PROJECT						
ŝl	6		Or CL	«	⊢ ≥	- 0	PHASE	YEAR PHASE	FUND	CDST	OTHER	FUNCS	FUNDS	PHASE	1st	2nd	3rd	4th thru 6th		R/W REQ
s	E		2-⊢	60	⊢ ¬	30		STRATS		PHASE	CODE			TOTALS (See fina 1	202-	2022	2023	2024-2026		DATE
-	2		-	- 5	8	7	8		10										TYPE	MONTHYR
-		Hoko - Ozette Road Johnson Creek Triple Culvert Replacement	09	5	0.01	CPTW	PE 5	1/21	10	11	17	13	14		16	17	18	19	20	21
		Hoko - Czotte Road M. P. S.4		"	40.	CP W	RW	1/21			ROOFBRB 199K	189		199	189			3	DE X	Y/N N
_		Replace Existing Undersized Culvert with a Larger Culvert or Replace with a Bridge			1	l .	CONST											_	EA .	DATE
-	12	Hoko - Corte Road - Taibol Creek Culvert Replacement	09		0.01		TOTAL					199		199	199				AT.	-
		Hoke - Ozetle Road M. P. 0.22	De	5	0.01	CPTW	PE RW	1/21		-	RCO-FBRB	107		197	187				is	YaN
- 1		Design for Replacement of Existing Undersized Culvert with a Fish Passable Structure					CONST				\$197K			1 1					CE X	N N
- 1		Condition section and the control of control of control and a least the control of contr					TOTAL		9		-			(60)				E		DATE
-	13	Joyce Predmont Road (192210)	92	5	4.00	CPTW	PE	400	FLAP	55.1	Pursue	197		197	197				VA.	-
- 1	100	MF, 0.00 to 4,3 (SR112 to East Beach Road)	03	3	4.00	CPTN	RW		5.487	364		$\overline{}$		564	250				15	YAN
- 1		Remnahura Read, Replace Culrents and Repaire to an Average wight of 26 feet	04	P	1	l	CONST	5/23	0,447	5.823	Tot- Credits	$\overline{}$	90		_	90	-		Æ _	
- 1		Project will be built by Western Federal Lands Highway Crusson.	07	-	1		TOTAL	5/23			for Local Majoh			5,848			3,423			DATE
. 1	14	Rudel Creek Cultert Replacement	09	S	0.04	CPTVV	PE	10/19		0.467	Malch RCD		1,013	7,500	250	90	3 4 2 3			_
	٠.	Cupos: Ranch Boad M.P. 0.24	90	. 5	D.G.	C-114	RAY	1/2/18			780K	80	20	100		_			13	- N-
- 1		Replace Existing Undersized Culvert with a Bridge, Fish passage empressment			1	1	CONST	6/71			7 BUK		- 7		770				E X	_
- 1	J., J.				1		TOTAL	6/1				700	76 98		776				A	DATE
-	15	Laird Road (FRJ110)	05	8	0.47	CPTW	PE .	1/20			RAP \$499K	760	13	976 10	776				JA,	-
- 1		M.P. 0.23 to 0.70	07	"	0.47	GF 110	RW	Hay			REFT 5770K	\vdash	12	10	_				F —	Y/N
- 1		Resertions and restrict funded by Rural Arterial Program (RAP) with County major.		l	1	1	CONST	5/20			iter - a grain	495	_	710	10	_	_			N.
_		Project construction completed in 2020		_		l	TOTAL	0.40				495	10	720	10			_		DATE
	16	Lateges at Read (#45358) and Voice of America Road (#45830)	02	- 0	0.21	CPTO	PE		FLAP	145	-	825	21		10		168		VA.	1
- 1		Lotzgesell M.P. 0.05 to M.P. 0.26, Valor of America M.P. 0.30 to 0.57	05	,	0.27	Dr. I.O	RAV		1.729	100		-	- 41	100		_	100		:Ка ЖЕ	AW
- 1		Retocate 8 widen 1440' of Yorke of America Rd, that provides access in Dungmass	08	l	0,2	l .	CONST		1,120	1,584	-		249	1,630			_			
- 1		Recreation Area & National Wildlife Refinge away from Stuff Replace abrupt 90 degree	07	l	l	l	TOTAL			1729	1		2/0	1,999			166	1,633 E		DATE
- 1		1410 hear entrers to seiler, curved transition from Michier-Dick Rd. in Lotzgesell Rg.	1,			1	10.12	_		Ires			210	1,3878			100	1.0-33	PK	-
		add left turn lane off of Littingasall at Park entrance; and wides shoulders				l .								- //						
all	17	Olympic Discovery Trail, Blyn to East County Line	32	S	0.30	PDW	PE	10/18					10	10				(5)	ie.	YAN
- 1		Clear grade and consolict 900-lest segment of buycle, pedestrian & equestrian	1	-	100		RW						10	1 "					E X	-
- 1		###-#er t/all from Diameted Pt, Ro to Old Gerdiner Rd, This will be a separated path				1	CONST	9/20			3		50	90	10			1		DATE
- 1		Within the exiting Right of Way of US 101, and complete the trail to washern County line,			1	l	TOTAL				11		60	60	10				IA -	-
		Includes exergination with Jefferson Co. for short trail to link to Old Gardiner Rd.																	~	-
uli	18	Olympic Discovery Trad, Electo River to Frastwerter Bey Rd, Qap	32	P	4.50	PTW	PE						100	100			100	E	5	Y/N
		Continue to plan route, buy Right of Way and construra bloyde, pessestian &		- 7			R.W	1/20			3		300	300	10	10	280		E X	
- 1		equestnan multi-use trail for the gap between the Elwha Priver Bridge to the new (2019)		- >]		CONST	9		70	1		1,100	1,100	1997		77.7	1.100 E		DATE
	2000	degment between Freshwater Bay Rd, and Thompson Rd					TOTAL			V. Ar			1,500	1,300	10	10	380			1
90	19	Olympic Discovery Trail, Thompson/Onella Rd to Gossett Rd Gap	32	P	7.00	PTW	PE						100	100			50			YAN
		Continue to plan multip, buy Right of Way & construct a bicycle/pedestrian/equestrian		P			RW			F			500	500	1D	-10	10			- A. C. C. C.
		wall for the gap between the Thompson Rd, & Gossett Rd, tree segments.		ρ]		CONST				1		2,800	2,800			- 10	2.000 =/		DATE
- 1			II 3		1	1	TOTAL						3 400	3,400	10	10	60			-

SIX YEAR TRANSPORTATION IMPROVEMENT PROGRAM 2021 TO 2026

Hearing Date : Adoption Date: Resolution No: Amendment Date: 11-54.50

F	1	PROJECT IDENTIFICATION	-							15,005,000		PROJECT D		OUSANDS O	F DOLLAR	5				
N	É	A DROUGHT AND ALLEM AND ALLEM AND ALLEMAN	2			>		/ 1		FUND	NG SOURCE	INFORMAT	ON	A CONTRACTOR OF THE PARTY OF TH	The state of the s		0.0			
Ċ C		A PROJECT / ROAD NAME (ROAD LOG / BRIDGE NUMBER); PROJECT NUMBER	last	l m	×	-									E	KPENDITUR	E SCHEOL	A.E	FEDER	RALLY FUNDED
F L		B. BECINIANG AND END MILEPOST (M.P) J. C. DEBCRIPT ON OF WORK	20	>		_ m					STATE		N	1 4		(LOCAL	AGENCY)		PRO	JECTS ONLY
N å	1 .	C DESCRIPT ON DE WORK	.> m	←	< €	- m	PROJECT	MONTH	FEDERAL	FEOERAL	OR	STATE	LOCAL	PROJECT		OF STREET	0.45777777			
o s	1 "		DE DE	<	⊢ Z	- n	PHASE	YEAR	FUND	COST	OTHER	PUNDS	FIJNDS	PHASE	Tist	2nd	3rd	4th thru 6th	N.	RAW REQ
NS	1 :		0 >	⊢	Om	-0		PHASE	CODE	BY	FUND			TOTALS					1	YAN
. 9				40	- J	20		STARTS		PHASE	CODE			(See nove 1	2021	2022	2023	2024-2026		DATE
1	2	1 1	- 4	5	6	7	8	9	10	- 11	12	13			10	17	- 10		TYPE	MONTHAR
Trail		Olympic Otscovery Trait, Waterline Road Section	32	S	3.50	PTW	PF	10/14	14		14	13	14 50	50	16	- 11	18	19	20 FIS	21 Y/N
		Complete survey socure remarking Right-of-Way, widen & pave existing grave!	-	5	1		RW	12/14					BD	80			30	-	CE -	X Y
		togging access road through the Lyre Pover Canyon provide paying trail connection	3	P	1	1	CONST	5/24		_			700	700		_	_	700		DATE
		between the Gosset/Waterine Rd, and LR, Crescent Seruce Rational bail segments		_	1	1	TOTAL						830	830			50			TALLE
Trail	21	Olympic Discovery Truit, Wary Clark Rd. to Forks "A" Rd. Gap	32	ρ	11.00	PTW	PE						50	50			- 34	700	EIS	YrN
		Continue to plan :cone, buy Right of Way A construct bicycle/pedestrantequestran	12.5	F	1		RW	7.1		-	1		500	500	10	10	10	30		X Y
		multi-user trail		P	1	1	CONST				1		2.450	2.450	- '0	- 10	IV	30		DATE
			1		1		TOTAL					- 5	3.000	3,000	10	10	10	30	NA -	DATE
Trail	22	Olympic Discovery Trait, Forks "A" Rd. to Calmach River Soat Launch Park	32	S	1.00	PTW	PE	1048	TA 3450K	85	PVT \$330K		10	75	75	10	10	20	EIS	Y/N
		Plan, Purchase Night of Way and Construct Paved Bioyole, Pedestrian, and	1 9	8	1		RW	10/16		_			145	145			_	_	_	X Y
1		Equestion (A.e) User Trol.		P	1	1	CONST	6/22		386	1		1,115	1,500		1,500	_	-	EA -	DATE
					1		TOTAL			450			1,270	1,720	75		0	- 0	NA.	
Trail	23	Olympic Descovery Trail, Forks to LaPush	9€	8	12.00	PTW	PE	1,023	FLAP		RCO \$905K		-210	495	- Fu	1,500	498		EIS	
		Plan. Pulchase Right of Way and Construct Paved Bicycle, Pedestrian, and	11	S	1		RW	791	5,153 million		PVT \$20K	905	302	1,207	725	482	1.00	_		X Y
	1	Equestran Multi-user Trail, Parking tof emprovements at Olympic National Park (ONP)	32	8	1		CONST		ONE \$200K	5.661	7. 02.51	540		5,681	120	792		5.001		DATE
		2nd and 3rd beach trailingeds, Begacheil bridge resurfaceur	-	_	1	1	TOTAL	- 411	Dia 4court	8.157		805	302	7.354	725	482	498			LIMIE
7	24	Bequim Dungeness and Woodcock Road Intersection Improvement	3	8	0.20	CPTW	PE	200	-SIP \$490K	45			5	50	30	402	190	100,0	EIS	YA
	1	MP 242 Ip MP 262	15		- 77		RW.	7/20		45				50	50				CE	100
		Roundabout intersection Improvement	6	l	1		CONST		STEG \$200K	SCD	1		77	807	517	10	_		FA -	DATE
	2.0		7		1		TOTAL		Pencino.	890			37	727	697	10			NA -	
- 8	26	Sot Duc Bridge Repair (#98090 BR1)	1T	P	0.05	СРТУУ	PE	8/24						50	441	10		10	EIS	YON
		Quillayubr Road					RW				1							100	CE X	- N
		MP 6.47 Io MP 6.52	1 1		1	1	CONST	8.25			1			250				250		DATE
		Réháb Portai and Partia Repaint				J	TOTAL							300				280		- CALL
7	26	Taylor - Cutoff Left Turp Lane		- 5	0,10	CPTVV	PE	1/24		2 = 3			20	20					DS	Y/N
	1	MP. 0,00 to M.P. 0.10	5			l .	RAY												CE	_
		Widen Road and Stripe a center left turn line	5		1		CONST	6/25			1		130	120				130	EA -	DATE
			7	:			TOTAL	-					150	150				150		
9	27	Towns Road (\$52278)	3	- 8	1.23	CPTW	PE	1/18			REET		70	70	10			-	Cis	Y/N
		M.P. 0.7615 M.P. 1.99	5				RAY	1/16					80	60	40				CE -	
		Reconstruct read between Woodcock Rd, & future align ment on top of retocated leves	6				CONST	677					460	460		450	.0		EA -	DATE
		Widen Regrade, Brainage	7				TOTAL						590	580	50	450	10		NA -	
9	28	Towns Road, No. (\$2270, \$2270A, \$2600)	2, 3, 5, 6	- 5	0.75	CPTW	PE	1/16			REEL						-		EIS .	YAN
		M.P. 1.89 to New Intersection with Sequim-Dungeness Way	4.7		100		RW	Iriá											CE -	
	1	Construct road segment on top of Dungoness River relocated leves and construct.	1				CONST	6-22			1		810	810		500	10		EA -	DATE
		roundabout at intersection with Sequen Dungeness Way.					TOTAL						810	610	-	800	10		N/A	

SIX YEAR TRANSPORTATION IMPROVEMENT PROGRAM 2021 TO 2026

Hearing Date : Adoption Date Resolution No: Amendment Cate 11-24-20

	1	PROJECT IDENTIFICATION	-							- 2.768SS	F	ROJECT CO	ISTS IN TH	OUSANDS O	F DOLLAR	5				
. 1	1		z	1		>				FUND	NG SOURCE	INFORMATI	ON		A CHARLES					
, C		6 6Economos os Money	의 조 (8 > 의	- n	- C 3	- m	PROJECT	MONTH	FEDERAL	FEDERAL	STATE OR	STATE	LOCAL	PROJECT	Đ	(LOCAL /	TO A THE PARTY OF	ALE.		NILY FUNDEC EGIS ONLY
A 5 4 S	M 8 8		# d d > ¥ ⊢	40 -	1 7 E	- B - 0 - 0	PMASE	YEAR PHASE STARTS	EGDE	COST BY PHASE	OTHER FUND CODE	FUNDS	FUNDS	P-WSE TOTALS (See note 1	19. 2021	2022	3ed 2023	4th thru 6th 2024-2026		RAW REQ Y/N DATE MONTH/Y/S
1	-2	1	4	5	6	7	8	9	10	31	12	13	14		16	17	18	19	20	21
ģ	29	Wisen Greek Road - Culvert Replacement Wisen Creek Road M.P. 0.13 and M.P. 0.38 and at Swede Road M.F. 0.09 Design for Replacement of Three Exacting Understazed Culverts with a Fish Passanile Structure	09	,5	0,01	CPTW	PE RW CONST TOTAL	1/20			RCC-ERF3	197	25	182	118				FIS GE EA	YM N DATE
107.00	30	Countywide Verious Improvements		P	6.70	DETEL	TOTAL					137	25	162	118				NA .	
nur.		Traffic control devices, upot safety, shoulder widening, gravings, switzing upgrades	UG.		0,3	CPTW	HAV				-		30	30	5	5	5	15	CE	YIN
		or other improvements,		l.			CONST						90	90	15	15	15	45	EA	DATE
_				-	-	-	TOTAL						120	120	20	20	20	60	NA T	
		GRAND TOTALS [Note: Totals includes the Federal Lands Access Program (FLAP) funded projects managed by Western Federal Lands Highway Diddion.)												45 550	5 848	5.713	8,707	20,201		

SIX YEAR TRANSPORTATION IMPROVEMENT PROGRAM 2021 TO 2026

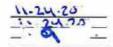
Hearing Oate .
Adoption Date:
Resolution No
Amendment Date.

11-24-20

	-	PROJECT IDENT/FICATION	-				1					PROJECT C	OSTS IN TH	OUSANDS (OF DOLLAR	RS				
	E	A. PROJECT/ROAD NAME (ROAD LOG/BRIDGE NUMBER), PROJECT NUMBER	4		1	>				FUNDS	NG SOURCE	INFORMAT	ION					The state of the s		
G L	M N	B, BEGINNING AND END MILEPOST (M.P.) J C, DESCRIPTION OF WORK	#. 2 % > ₩	⊃ ⊢	- F	_ o	PROJECT	MONTH	FEDERAL	FFNERAL	STATE	STATE	LOCAL	PROJECT	E	(LOGAL	AGENCY)	ULE		ALLY FUNDED ECTS ONLY
А	M		2 0	-	F Z	100	PHASE	YEAR	FUND	COST	OTHER	FUNDS	FLINDS	PHASE	184	240	3rd	41h thru 8th		R/W REQ
s	B		4.4	⊢	пω	H-0	1	PHASE	CODE	BY	FLND			TOTALS				***************************************		YAN
Я	R		5 ⊢	n)		30		STARTS		PHASE	COOE			(See note 1	2021	2022	2023	2024-2025	ENMR CYPE	DATE MONTHAR
1	2	3	4	5	6	7	8	9	10	11	12	13	14	THE PARTY	16	17	18	19	20	21
		PLANNED (UNFUNDED PROJECTS) - PROJECT NUMBERS 141 - USS																		
6	UI	Anderson Road (East) (#47990)	Q3	PU	0.45	CPTO	PE						40						EIS	YAN
		MP, 0.00 to MP, 0.45 (Coys Rd to Thorton Orles - east cressing)	05				RN						50						CE	
		Drainago, Wildon, Resurtacing, Polantial Rural Arterial Program Project,	07	1			CONST TOTAL						400						EA .	DATE
B.	UZ	Atterbarry Road (944300)	02	P/U	2.7A	CPTW	PE PE		_	-			490 150	460					A.P.	-
		MP 000 to MP 278	03	170	2.0		RAW			-			3CD			_	_		ENS	YAN
- 11		Reconstruct road between Sherburne Road & Mooker Road	05	1			CONST	-		-		_	2,380	1		_	_		EA -	DATE
		Widen Regratio Disinage - 3 Phases	07				TOTAL						7,780	2.780					NA -	
8	fina in the	Berr Road (North) (#43110)	02	PAU	0.54	CPTW	P€						50						EIS	_
		W.P. G.18 to M.P. C.82	0.3	1			RW						50	1 1		1 - 1			CF	
		Reconstruct rose north of SR 101 to was 90 day/se content	05			1	CONST						300						EA .	DATE
я	114	Milden, Regrade, Oramage - 2 Phases Brown Road (#53819)	07	PAU	0.50	CPSTWO	TOTAL						400	400					NA .	
	"	M.P. 0 67 to M.P. 1.47	06	100	0,50	CPSTMO	3.W	_		_		_	60 75		_		_		EIS	_ Y
		Sequim City Limits to Port Williams Road	32	1			CONST			_		_	336		_	_	_		CE _	DATE
		3R upgrade plus walkways-bikeway	1.				TOTAL						435	435			-		NA —	- DATE
7	US	Edgewood Drive, Reddick to City Limits (#93110)		PAU	0.61	CPTW	ME		- 7				50						EIS	YrN
		MP. 238 to MP. 299	03	1		1	P.W						50						(ZE	
		Reconstruct Road to Major Collector Standards	04	1		1	CONST						850						EA =	CATE
a	116	Widen, Region, Regrede, Resultabe Emits Creek Road (#16200)	97	PAL		DOTT I	TOTAL						950	950					NA.	-
- 1	~	MP. 0.00 to M.P. 0.27	96	Pill	0.27	CPTW	PE RAVI			_			63		_				EIS	Y/N
		Size Street to Front Street	32				CONST			_			60 400				_		ÇE	
-		3R upgrade plus walkwey/blurwsy	1	1			TOTAL						520	520					NA -	DATE
9	U7	Heath Road Connector to Gilbert Road (#48650)	112	PJU	0.08	CPTW	laE.						300	260					EI6	YAN
		Heath Road to Gittert Road, No. 45850	0.3	1	1.0	76	RW	1					300						CE	Η
		Construct Connector Road between Health and Cribert	05				CONST						1,300		5				EA .	DATE
	110	Carlsborg Transportation Study #13	Q7				TOTAL						1,900	1,900	4				VA	
	00	Hoko-Czette Road (891460) M.P. 19.5	02	P/U	0,08	PT	PE						10							
		Relocate ringo segment (~ 400 feet & less) away from side area, reconstruct and	03				CONST						10							
		resurtace. May need 40 to 50-feet of additional right-of-way.	9,				TOTAL			-		-	150 170	170						
7	U9	Hoko-Ozetle Road, No. 91480	02	P/U	1.01	PTW	PE						170	1/0					ES	YAN
- 1		Venous segments M.P. 0.00 to M.P. 21.31	93				SAN						100						CE —	N N
		Reconstruct mad away from Big River bank in 4 phases	05				CONST						/50						_	CATE
	1140	M.P. C.79-1.0, 22-24, 27 3.0, 41-44	07				TOTAL						970	970	- 5				NA -	1
200	U10	Noks-Ozetha Rood, No. 91460 M.P. 5.54 tu M.P. 5.84	03	PJU	0,30	CPT	PE						24		_	10 2			is	
		M.P. 3.54 to M.P. 3.84 'Big FM' in 'Borrow Sale'	05	((()			R/W		3				18		1	1	5		GE .	
		Rebuild to CityCounty Standards	0,				CONST		1	-			235						_	DATE
- 1		I consider to anytherating that thereta	_	1	-		TIOTAL	- /4					277	277		111		1	NUA.	_

SIX YEAR TRANSPORTATION IMPROVEMENT PROGRAM 2021 TO 2026

Hearing Date .
Adoption Cale:
Resolution No.
Amendment Date



F I PROJECT IDENTIFICATION	N .	-		1									DUSANDS C	F DOLLAR	rs es				
		2 m	1 - 8	τ 	- es	PROJECT	MONTHU	FEDERAL	FEDERAL	STATE OR	STATE	LOCAL	PROJECT	E	XPENDITUE (LOCAL	RE SCHEDI AGENCY)	ULE		ALLY FUNDED ECTS ONLY
A M- 05 B 18 E R		3 h	- × - ∞	1 7 0 m	- 0 -0	PHASE	YEAR FMASE STARTS	FJN⊒ G∆Ô€	ODST BY PHASE	OTHER FUND CODE	FUNDS	FUNDS	PHASE TOTALS (See note 1	761 2021	2022	3-6 2023	485 Circ (81 2024-2025	1	RAYRED YAN DATE MONTHAR
1 2 3		4	5	6	7	8	9.	10	11	12	13	14		16	17	18	19	20	21
9 U11 Kirk Road Extension		00	PAU	0.22	CP*	PE.						150						ĐS	Y
MP. 0.28 to MP. 0.50		C6				RAW						100						CE -	
Upgraria Fasting Road and	Extend to Atterberry Road	67		1		CONST				1		600						EA -	DATE
Cerisbona Transportation St		15				TOTAL				1		850	850		_		-	WA -	
9 U12 Lower Elwho Road (#3131	0)	(3	PAU	1.99	CPTW	Æ	-					100	500				1	EIB	Y/N
MP. 0.78 to MP. 2.78	-•	64		100		RAW			-	1		100	1				_	CE -	
Wicen, Drainage, Sidewalk,	Penave	06				CONST				1		1,556	1 1		_	_	-	EA -	DATE
amount, and anoger, ordered in	Tanger 1 to	67				TOTAL				4		1,758	1,756		_	_	-	WA -	Linie
9 U13 Marine Drive (047530)		05	PAU	0.70	CPTWG	PE.			-	_			1,136		_		-	IIIS	YAN
MP. 0.00 b MP. 0.72		06	l rid	0,72	SFING	RW			_			58 40	1		_		-	DE -	- T/M
Cays Read to Cline Sph Ro.	-1	07		11 1		CONST					_	533			_		-		-
Bluff Drainage Enhancemen		la.		1							_							EA	DATE.
9 U14 Mary Clark Road (#20108)	TE.					TO-AL						531	631	_				VA.	
		C2	PIU	0.28	PI	PE						10						EIS	
MP_2.32 to MP_2.60		05				RVW						25						tE	
Resign rood at Boar Spring	s Hatchery Columbi	07		L		CONST			2 3			150						EA _	DATE
						TOTAL						185	185					MA	
6 U15 Mill Road Traffic Island (P	4930)	05	PAU	0,1	CPTWO	P€						50						EIS	Y/N
MP 0.00		DQ.				RW						30						CE	
	to make Mili Road Right in-Right Out Intersection	07				CONST						110						ĒΑ	DATE
Contshore Transportation St						TOTAL			13 3			190	190	_ = :			6	44	
5 U16 Nill Road Extension to Bu	siness Park Loop (#44830)	05	P/U	0,1	CPTNO	PE						\$n						EIS	YaN
MF 0.57 to 0.73		ØĠ				RAY						50	1 1					CE .	
	he Industrial Business Park Loop	07		1	1	CONST				1		250						EA .	DATE
Cartsborg Transportation St	LOY F 5					TOTAL						350	350	= 3				NA T	
8 U17 MI, Angeles Road		02	F/Ų	0.85	CPTV	PE						75						EIE	Y/N
N.P. 0.47 to M.P. 1.12		an a				RAW				1		65	1 1				A.	CE	
Proposed Rural Arterial Pro	gram Project	04				CONST			S = 5			500						EA T	DATE
Widen, Realign, Regrede, R	eau lace	07				TOTAL						740	740					144	
7 LITE Old Dlympic Highway (694		03	PAU	267	CPTW	PE						100						EIS	_
M.P. 4.00 to 6.67		05	1	1		RAY						1912						DE -	-
	Resurface & Drainage from McDonald Creek.	07				CONST			-	1		920					_		-
Bridge to Cays Road.	The straight trace transcens at male					TOTA	-		7			1,020	1,020				1	NA -	
9 U19 Parkwood Boolevard Traff	ic Island (N44340)	05	PAU	0.1	CPTWO	PP	7					30						EIS	YIN
NP 0.00		06				RVW	-					50						CE -	1100
150 9 1000	to make Partwood Boulevard a Right n-	07				CONST					_	113			-	-		EA .	DATE
	bole Transportation Study #11D.					TOTAL			77			193	190		-	_	-	NA -	- Dale
rogs, con ancionadali, Quid	South that absorbers, search at are	80	1	I.	E.	HOTAL			-	į .		193	190					Jren	_

SIX YEAR TRANSPORTATION IMPROVEMENT PROGRAM 2021 TO 2028

Hearing Date : Adoption Date: Resolution No Amandment Date: 11.24.20

	1	PROJECT IDENTIFICATION	+							-				OUSANDS O	OF DOLLAR	₹5				
1	É	A. PROJECT (ROAD NAME (ROAD LOG / BRIDGE NUMBER), PROJECT NUMBER	z			>				FUNDI	NG SOURCE	INFORMAT	ICN			SCHEMOHEUR				ALLY FUNDED
C	M N	B. BEGINNING AND END MILEPOST (M.P.) C. DESCRIPTION OF WORK	> m > m	⊃ -	11 11	- w	PROJECT	MONTH/	FEDERAL	FEDERAL	STATE OR	STATE	LOCAL	PROJECT	, i		AGENCY)	U-E		ECTB ONLY
S	M 8 E		æ d. a.⊁		D T	- D	PHASE	YEAR PHASE STARTS	FUND CODE	COST BY PHASE	OTHER FUND COOL	rUNDS	TUNDS	PHASE FOTALS (See note 1	1sl 2021	2nd 2022	3rd 2023	4th thru 6th	ENVR.	R/W REQ Y/N DATE
_	R		-	-															TYPE	MONTHYR
1	2	3	4	5:	6	7	8	9	10	13	12	13	14		16	17	16	19	20	21
19	U20	Pioneer Road (#35820)	-02	PAU	0.39	CPTW	P€						50						EIS	Y/N
		MP 000 to MP 0.35	03	290			RAY			¥			100	1					CE _	_
		Reconstruct road between SR 101 & Baker Steel	05	1			CONST						400						EA	DATE
_		Wilden, Regrade, Dratinger - 2 Physics	07	-	_		TOTAL						560	560					NA	
9	U21	Quilleyute Road (#11200)	03	PrŲ	1,15	CPTW	PE						100						EIS	YIN
		M.P. 1.72 m 2.67	04	1			RW						100						GE.	_
		Quillayute Weather Station to Mine Smith Road	-07	1			CONST						675						EA	DATE
_	Time	Regrade Drain, Widen, Resurface	-				TOTA ₄						1,075	1,075					NA,	
Þ	022	Ruspion Road, East (#44730)	03	PAU	0.25	CPTWO							20						EIS	YIN
		M.P. 0.69 to M.P. 1.14	04	1			RAV	_					25				14		CE	_
		Cartsburg Road to Mill Road	a,	1			CONST					_	105			_			EA	DAT"
		Regrade, Oram, Widen, Resurtace		-			TOTAL			10 11			150	150					NA.	- 44
-	1,23	Russian Read Reundahout (444730)	03	P/U	0.25	CPTWO	PE						/8						EIS	_ ` `
		M.P. 0.69 (All intersection with Carlsborg Road and realigned Spath Road)	04	1			RAW						100						CE	-
		Construct a four-legged roundations with restigned Speth Road	07	1			CONST			1			560						EA	DATE
_	120.0	Carishorg Transportation Study #1		-			TOTA.					_	758	758					NA.	
7	024	Bequam-Dungeness Way (99330)	96	PU	3,52	NW	PE						29						EIS	YAN
		M.P. 1.00 to 4.62	07				RW						45						CE _	
		Sequim City Limits to Marine Drive, except Woodcook Road visualty					CONST						650						EA	DATE
		Керичя		<u> </u>	_	_	TOTAL						718	718					NA.	_
9	U25	Sparth Road (#45420)	03	PAJ	1 26	СРТЖО	PE						53						EIG	YIN
		MP 000 to MP 080	04				RAY						108						CE	
		Kitchen-Dick Road to Carleborg Road	07				CONST						500						EA	DATE
_		Regrade, Oram, Widen, Resurface					TOTA.						758	759					NA.	
ð	U26	Spath Reed to West Runnion Reed Connector Route (#45420)	03	PAU	1,26	CPTYVO	PE						100						EIS	Y/N
		M.P. 0.0 80 (Spoth Road) to M.P0 60 (West Hummon Hoad)	94	1			RW						90			7	in .		CE	
	0.9	Construct New Connector Segment between Spath and West Runnion Reads	07	1		1	CONST						800				19		EA	DATE
		Carisbong Transportation Study # 5			-		TOTA_						890	990					NA.	-
9	U27	Spencer Road Connector to Mill Road (#44669)	03	PAU	0.75	CPTWO	PE	- 1					300						EIS	YIN
		M.P. 1.50 to Mill Road	04				RW			8 8			300			1 3	47		CE	
		Construct New Connector Segment between Spencer Road and Mill Road	07				CONS"						1 584						EA.	DATE
		Cortsborg Transportation Study # 14				Latinia.	TOTAL			P 3		1 1	2,484	2,484			ý.	0	NA.	10
NA	U28	Sunland Non-motorized Trail	32	P/U	2.00	PTW	PE	11 7					40				11		EIS	Y/N
	11.03	Trial beside Taylor Blvd in Sunland for pedestrian access - Seq Dung Way to Holland Road		37.55520.5	20700		RW						40					-	CE	Y
							CONST						300						EA	DATE
							TOTA.			1 5	1		360	390			Ut-		NA.	

Hearing Date : Adoption Date : Resolution No. Amendment Oans 11-24-20

	ı	PROJECT IDENTIFICATION	-	1						- construction		PROJECT	OSTS IN TH	OUSANDS C	F DOLLAR	\$				
	T		z			>		7 8		FUNCE	VG SOURCE	INFORMAT	ION	3			vo - 11			
C L		A PROJECT / ROAD NAME (ROAD LOG / BRIDGE NUMBER), PROJECT NUMBER B. BEGINNING AND END MILEPOST (M.P.) U.C. DESCRIPTION OF WORK	25 NA 25 NA	pt ⊃ ⊢	⊀¢ ⊣⊢ Ε	- S	PROJECT	MONTHY	FEDERAL	FEDERAL	STATE	STATE	LOCAL	PROJECT	E	(LOCAL)	5 F 12 TY 1 T	JLE:		ALLY FUNDED JECTS ONLY
A 8 5	6 9		æ a a > = -	± 50	F Z □ W F Z	- 0 - 0	PHASE	YEAR PHASE STARTS	PLIND	EY FHASE	OTHER FUND CODE	FUNDS	PUNDS	PHASE TOTALS (See noor 1	1st 2021	2nd 2022	3rd 2023	4th thru 6th 2024-2026	ENVIR.	R/W REQ YIN DATE MONTH/YR
1	2	1	4	5	6	7	B.	9	10	11	12	13	14		16	17	18	19	20	2'
7	U29	Trout Creek Bridge Replacement (#\$1486 BR4) MP 17.18 to MP, 17.29 Hoko-Czette Road	09	P/U	D.10	PT	PE RAV CONST TOTAL						90 10 475 935	535					CE	Y/N DATE
т	USO	US 101 Underpasses of McDonald Creek Bridge (1941 20 BR1) Reconnect South Barr Road and Sherburg Road to North Skie of US 101 by extending those County Roads under the New McDouse of Creek US 101 Bridges	04 09 07	P/D	0.09	CPT	PE RAV CONST TOTAL						1,948 2,148	2.140					EIS CE EA	Y/N N DATE
9	U31	Weel Bridge (\$18759 BR1) Weel Russ, No. 18760 M.P. 0,32 to M.P. 0,38 (Weel Road, No. 16760) Residuos und renythen bridge	OH8	РЛ	0.06	CPTW	PE RAV CONST TOTAL						348 30 1,113 1,491	1.491					EIS	Y/N DATE
9	U32	Winterhork to West Runnion Connector (#44739) MP 0 60 - Inbarsection with West Runnion Read Winterhowk Street to West Runnion Read Conteque new Connector Segment between Winterhawk St. & W. Runnon Rd. Certsoorg Transportation Study # 12	03 04 07	PAU	1,25	CPTINO	PF RWI CONST TOYAL						90 20 90 160	160					EIS	DATE
		TOTAL UNFUNDED PROJECTS												27,011						

¹⁻Total project coats represents the total estimated cost of the project. The funding represented in the Six Year Transportation Improvement Program (TIP) may be less than the total project funding for two reasons. One, the project is underway and has excended money during the years prior to the first year of the TIP, Two, the project is planned for the later years of the Y-P and will not be completed by the final year of the TIP.

OIGITAL)			

Jefferson County Public Works Adopted 2021 - 2026 Six-Year Transportation Improvement Program













ADOPTED 2021 - 2026

Six-Year Transportation Improvement Program

Table of Contents

Letter of Introduction from the County Engineer/Public Works Directo	r1
Jefferson County Board of County Commissioners	
Notice of Public Hearing	2
Resolution Number 67-20	3
2021 - 2026 Six-Year Transportation Improvement (TIP) Program	
Project Location Map	4
Project Array	
Guide to Codes, Abbreviations and Acronyms	8
Past Accomplishments	10
Budget Forecasting, Funding Information & Analysis	
Geographic Distribution	
Top Prioritization Criteria	
Secured Funding Project List	
Category and Funding Type Charts	
Six-Year County Road Fund Assumptions & Projections	
Road Fund Chart	20
Annual Construction Plan	21
Bridge Report Highlights	22
FLAP Funded Projects	23
Access to ESRI ArcInfo StoryMap	25
Appendix A Annual Bridge Condition Report 2020	26
•	
Bridge Inventory List for East & West County Reference Materials & Definitions	
Appendix B Distribution List	51

Department of Public Works

Jefferson County, Washington

623 Sheridan Street Port Townsend, WA 98368 360-385-9160

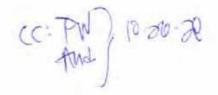
Welcome to the adopted 2021-2026 Six-Year Transportation Improvement Program (TIP) and its supporting materials. The Six-Year TIP is a rolling plan of proposed transportation projects that shows anticipated, partial, and/or total secured funding. The plan is annually updated to provide direction to transportation investments on Jefferson County's public roads and non-motorized inventory. The TIP development is a process that identifies and prioritizes future project phases to balance needs with financial resources, and schedules a six year planning period for reasonably viable projects.

A variety of capital preservation and improvement projects are included in the TIP but are reliant on receiving grants or federal investments such as Surface Transportation Program (STP) and state grants from CRAB, RCO, WSDOT and/or WDFW. The TIP prioritized list is highly influenced by the availability of these funds. The majority of the listed projects do not have secured funding as of TIP adoption. Historically, projects on Jefferson County's TIP have averaged over 80% funding from State and Federal sources; however, non-local transportation revenue sources usually fund specific types of improvements only on certain collector roads and trails. Federal and state grant program criteria can strongly influence which projects and features are accomplished. Often in smaller agencies, local match fund availability limits the number and size of grant applications. Local funds shown for this proposed Six-Year TIP average just \$197,000 per year and primarily come from the local Road Levy and Motor Vehicle Fuel Tax. Jefferson County has a limited tax base for its 400 road miles. Our transportation revenues are among the lowest per road mile in western Washington. The county road fund has seen a 25% overall reduction in annual operating revenue due to loss of federal land timber revenue in recent years. As a result, road fund reserves are projected to decline progressively each year.

The Six-Year TIP, like any other capital improvement plan, is only a plan for what should be done under the provision of adequate funding. It demonstrates typical or likely funding and/or a distribution of limited revenue sources. Decisions about advancing projects into design, acquisition, and construction are made through the county budgeting process. Since the Six-Year TIP is updated annually, it can be changed to reflect new needs, priorities, revenues, and revised budget assumptions.

If you would like more information, feel free to contact the Jefferson County Public Works office: 623 Sheridan Street, Port Townsend, WA (360)385-9160.

Monte Reinders, P.E. Public Works Director/County Engineer



COUNTY OF JEFFERSON STATE OF WASHINGTON

In the Matter of Adoption of the 2021-2026 Six-Year Transportation Improvement Program

RESOLUTION NO. 67 20

WHEREAS, State law obligates the legislative authority of each county to adopt on an annual basis a six-year transportation improvement program, holding a public hearing prior to adoption, the purpose of which is to "assure that each county shall perpetually have available advanced plans looking to the future for not less than six years as a guide in carrying out a coordinated transportation program" (RCW 36.81.121(1)); and,

WHEREAS, a multi-year analysis of the Road Fund was prepared covering the six-year program period using the County's best estimate of future revenues and expenditures in accordance with WAC 136-15-030; and,

WHEREAS, the Board of County Commissioners, Jefferson County, in the State of Washington, has reviewed the transportation project priority array, the road fund forecast and bridge condition report as prepared by the Department of Public Works, and has conducted a public hearing on the corresponding proposed six-year transportation improvement program;

IT IS HEREBY RESOLVED that the Board of County Commissioners does approve and adopt the Six-Year Transportation Improvement Program for the years 2021 through 2026 inclusive in accordance with WAC 136-15.

ADOPTED and signed this 26th day of October 2020.

ATTEST:

Carolyn Gallaway, Deputy Clerk

JEFFERSON COUNTY BOARD OF COMMISSIONERS

Greg Brotherton, Chair

David Sullivan, Member

Kate Dean, Member

NOTICE OF PUBLIC HEARING: PROPOSED SIX-YEAR TRANSPORTATION IMPROVEMENT PROGRAM

Notice is hereby given that a public hearing will be held by the Jefferson County Board of Commissioners on **Monday, October 26, 2020,** at **10:30** AM in the Commissioners' Chambers at the County Courthouse, 1820 Jefferson Street, Port Townsend, Washington, for the purpose of receiving written and verbal testimony about the adoption of the draft Jefferson County 2021-2026 Six-Year Transportation Improvement Program (TIP). "Each county shall perpetually have available advanced plans looking to the future for not less than six years as a guide in carrying out a coordinated transportation program, RCW 36.81.121(1)." Written public comment on the program will be received up and until the public hearing mailed to Jefferson County Commissioners' Office, PO Box 1220, Port Townsend, WA 98368 or emailed to jeffbocc@co.jefferson.wa.us.

The printed program is available for viewing at the Jefferson County Public Works office, 623 Sheridan Street, Port Townsend, open 8:00AM to 5:00PM, the Commissioners' office, 1820 Jefferson Street, Port Townsend, open 8:30AM to 4:30PM, and on the county website at www.co.jefferson.wa.us/444/6-Yr-TIP

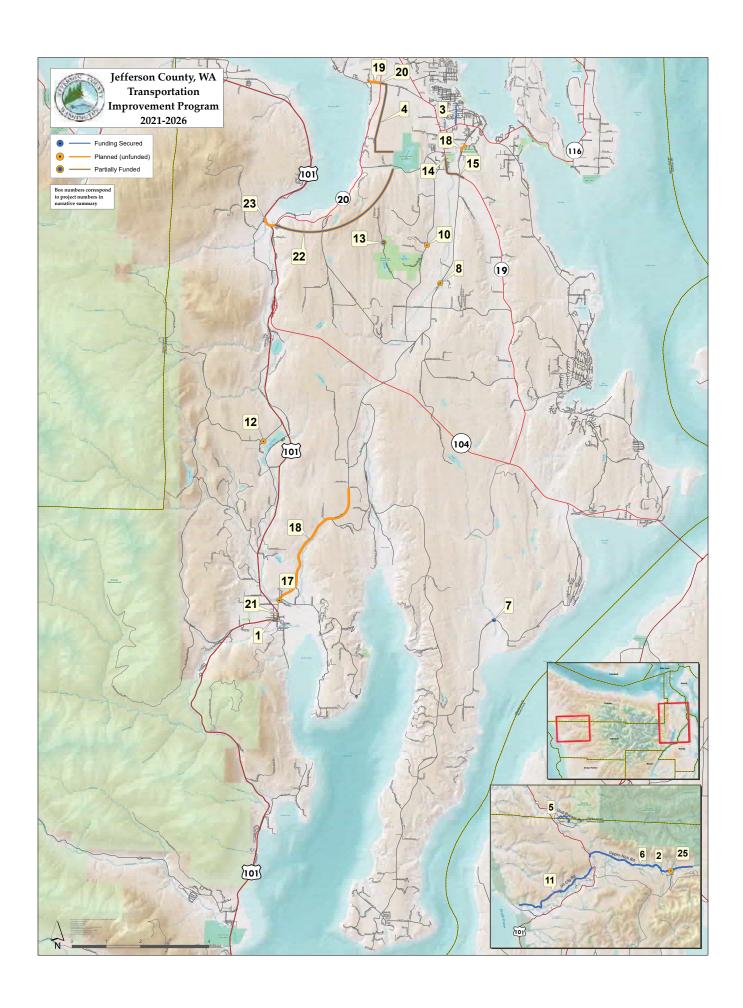
Jefferson County ensures full compliance with Title VI of the Civil Rights Act of 1964 by prohibiting discrimination against any person on the basis of race, color, sex or national origin in the provision of benefits & services resulting from its federally assisted programs & activities. The meeting site is ADA accessible. Accommodations for people with disabilities can be arranged with advance notice by calling (360)385-9100.

NOTE: Due to Covid-19, it is unknown if there will be restrictions on the Open Public Meeting Act on the date of this Hearing that will preclude public in-person attendance. Check the Jefferson County website at www.co.jefferson.wa.us for the current status and follow "Quick Links-Videos of Meetings streaming Live." to testify at the public hearing.

Greg Brotherton Chair

10/5/2020 Date

Board of Jefferson County Commissioners



SIX-YEAR TRANSPORTATION IMPROVEMENT PROGRAM 2021 - 2026

Hearing Date: 10/26/2020 Adoption Date: 10/26/2020 Resolution No. 67-20

_	_	T	г —		1		1			DDO II	CT COSTS	IN DOLLARS										
					ASSESS					PROJ		SOURCE INF						EXPEN	DITURE SCH	EDULE		
			ᇤ		SSE				FEDERA	L FUNDS	STATE		ORMATION									
FUNC. CLASS	TIP PROJECT NO.	PROJECT IDENTIFICATION A. Road Log Number - Bridge Number B. Beginning and End Milepost C. Project (No.) / Road Name D. Description of Work E. Total Cost (incl. prior/future)	IMPROVEMENT TYI	FUND STATUS	ENVIRONMENAL A	TOTAL LENGTH (mi.)	PROJECT PHASE	MONTH / YEAR FUND STARTS	FEDERAL FUND	FEDERAL COST BY PHASE	STATE FUND TYPE	STATE COST BY PHASE	OTHER REVENUES	LOCAL FUNDS	TOTAL	YEAR 1 2021	YEAR 2 2022	YEAR 3 2023	YEAR 4 2024	YEAR 5 2025	YEAR 6 2026	TOTAL 2021-2026
		US 101 & connected county roads MP 294.6 to MP 295.0																				
1		Quilcene Complete Streets - Phase 1 (1801933)	l			_	PE	1/13						10,000	10,000	10,000						10,000
00	1	Publicly driven ped/bike improvements involving: US 101, traffic calming, sidewalk, crosswalk & lighting improvements with a focus on connecting	28	S/P	E	0.50	R/W CN	3/20 6/21			PBP	695,350	52.416	0	747.766	747,766						747.766
		to the school campus. Total = \$997,500 (with \$52,416 from PIF)					Total	0/21		0	FBF	695,350	52,416	10,000	757,766	757,766	0	0	0	0	0	757,766
		91420 MP 0.00 to MP 12.04 (18019410)										,	·	,	,	·						,
		Upper Hoh Road Phase 2 - FLAP Project	06			12.04	PE								0							0
07	2	Local funds, total \$63,621,for infrastructure protection as performed by FHWA Western Federal Lands (WFL) division, thru FLAP program.	06	S	s	12.04	R/W CN	6/21	FLAP					10.000	10.000	5.000	5.000					10.000
		Total FLAP = \$14,029,621 (uses toll credits)					Total	0/21	I EAI	0		0	0	10,000	10,000	5,000	5,000	0	0	0	0	10,000
		63710 MP 0.00 to MP 0.46 (18020570)												,	,	·	,					,
09	3	Cedar Avenue, Ness' Corner (SR116) to Hayden Road Ped-Bike	28	s	E	0.61	PE	6/19			SRTS	17,805			17,805	17,805						17,805
09	3	WSDOT Safe Routes to School funds for ped/bike safety improvements including multi-use path, sidewalk, crosswalks, bike lanes, and RRFB.	28	5	E	0.61	R/W CN	6/21			SRTS	803,152			803,152	803,152						803,152
		Total = \$880,300					Total	0/21		0	OKTO	820,957	0	0	820,957	820,957	0	0	0	0	0	820,957
		850200 4 Corners Road to Anderson Lake State Park																				0
00		Olympic Discovery Trail (ODT) - Connection (1801989)	٠,	S/P	Е	2.70	PE R/W	1/17 1/20			RCO-RRG	38,070 231,500			38,070 231,500	38,070 231,500						38,070 231,500
100	4	PS&E, ROW (RCO RRG) and Construction (STBG & RCO WWRP) of OD segment between Jefferson Transit P&R and Anderson Lake State Park	28	5/P	_	2.70	CN	6/22	STP-R	1,189,020	WWRP	2,189,021			3,378,041	231,500		3.378.041				3,378,041
		Total Construction = \$1,189,020 (STBG) + \$2,189,021 (RCO WWRP)					Total	0,22	<u> </u>	1,189,020		2,458,591			3,647,611	269,570	0	3,378,041	0	0	0	3,647,611
		15100 MP 0.90 to MP 2.185 (18020460)																				
	_	Undi Road Bypass - FLAP Project Local funds, total \$33,000, to preserve access to ONP & USFS as	06	s		1.285	PE R/W	1/17	FLAP					13,336	13,336	6,668	6,668					13,336
1 03	3	performed by FHWA Western Federal Lands division, thru FLAP program.	"	3	•	1.203	CN	6/22	FLAP						0							0
		Total FLAP = \$2,034,407 (uses toll credits)					Total			0		0	0	13,336	13,336	6,668	6,668	0	0	0	0	13,336
		91420 MP 0.00 to MP 12.04 (18020560)							FLAP		RAP	61.400				6.400						61.400
07	6	Upper Hoh Road Pavement Preservation Pavement preservation & safety improvements along Upper Hoh Rd. to	06	s	Е	12.04	PE R/W	1/17	FLAP	U	KAP	61,400			61,400 0	6,400	55,000					61,400
		Hoh Rain Forest/ONP boundary performed by FHWA, WFL division.	21			-	CN	6/21	FLAP	0	RAP	347,000		0	347,000			347,000				347,000
_		Total = \$1,567,600 (FLAP) + \$408,400 (RAP)					Total			0		408,400	0	0	408,400	6,400	55,000	347,000	0	0	0	408,400
		41870 MP 4.71 (18020580) Thorndyke Road Culvert Replacement - Thorndyke Creek					PE	7/19						13,000	13,000	8,000	5,000					13,000
08	7	Replace parallel culverts at Thorndyke Creek with fish-passable crossing.	11	S/P	1	N/A	R/W	7710					5,000	5,000	10,000	10,000	0,000					10,000
		RCO will manage permits and design review for lead agency FBRB.					CN	7/21	NFPP	220,366	FBRB	1,645,410	70,000		1,935,776		1,935,776					1,935,776
-		Total = \$1,843,723(FBRB) +\$220,366(NFPP) +\$75,000(PIF)					Total			220,366		1,645,410	75,000	18,000	1,958,776	18,000	1,940,776	0	0	0	0	1,958,776
		Center Road Culvert Correction - Chimacum Creek					PE	7/21			FBRB	202,300		35,700	238,000	28,000	100,000	100,000	10,000			238,000
07	8	Replace non-conforming road culvert with fish passable culvert design	11	Р	1	N/A	R/W								0	.,,,,,		,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,	.,			0
		for Chimacum Creek salmon.					CN	6/25	NFPP	300,000	FBRB	1,997,500		52,500	2,350,000	60.05	460.00	460.00		2,350,000		2,350,000
\vdash		Total grants= \$2,497,500					Total			300,000		2,199,800	0	88,200	2,588,000	28,000	100,000	100,000	10,000	2,350,000	0	2,588,000
		Countywide Bridge Load Re-Rating					PL	1/21	STP-R	111,325				17,375	128,700	128,700						128,700
00	9	Federally required re-analysis of 22 county bridge load ratings for	18	s	Е	N/A	R/W								0							0
		accomodation of new specialized hauling vehicles by 12-31-22. Total STBG = \$111,325.50 (No Construction)					CN Total			111,325		•	0	17,375	0 128,700	128,700				0		128,700
\vdash		10tal 5 BG = \$111,325.50 (No Construction)					Total			111,323		U	U	17,375	120,700	120,700	U	U	U	U	U	120,700
		West Valley Road Culvert Replacement - Naylors Creek					PE	10/19	NFPP	7,425				6,250	13,675	6,250	1,225	6,200				13,675
09	10	Replace culvert at Naylor Creek with fish-passable crossing and realign	11	S/P	ı	N/A	R/W	0/01	MESS	460 10-	FBRB	000 11		3,000	3,000	3,000			700.05			3,000
		creek for 300ft. RCO will manage permits& design review for FBRB. Total Grants = (\$99,425+\$684,980)FBRB Plan&Restore +\$127,854 NFPP					CN Total	6/24	NFPP	120,429 127.854	FBRB	682,430 682,430	0	9,250	802,859 819,534	9,250	1,225	4,000 10,200	798,859 798,859	0	0	802,859 819,534
		13430 MP 8.50 to MP 10.98					Total			121,034		002,430	U	3,230	013,334	3,230	1,223	10,200	730,009	U		013,334
		Oil City Road Engineering Assessment - FLAP Project					PE	1/21	FLAP					30,000	30,000	15,000	15,000					30,000
09	11	Local funds for infrastructure protection, planning project performed	18	S	Е	2.48	R/W								0							0
		by FHWA Western Federal Lands division, thru FLAP program. Total FLAP = \$250,000 (uses toll credits)					CN Total			n		0	0	30,000	30,000	15,000	15,000	0	0	0	n	30.000
_		Total 1 E ii — \$200,000 (doco toli orodito)	_				I Jiai			U		U	U	30,000	30,000	13,000	10,000	U	U	U	0	50,000

SIX-YEAR TRANSPORTATION IMPROVEMENT PROGRAM 2021 - 2026

Hearing Date: 10/26/2020 Adoption Date: 10/26/2020 Resolution No. 67-20

_			1		45 1					DDC !	ECT COSTO	IN DOLLARS										
					ASSESS					PROJ		SOURCE INF						EXPEN	DITURE SCH	IEDULE		
			띮		SS				FEDERA	L FUNDS	STATE											
FUNC. CLASS	TIP PROJECT NO.	PROJECT IDENTIFICATION A. Road Log Number - Bridge Number B. Beginning and End Milepost C. Project (No.) / Road Name D. Description of Work E. Total Cost (incl. prior/future)	IMPROVEMENT TYI	FUND STATUS	ENVIRONMENAL A	TOTAL LENGTH (mi.)	PROJECT PHASE	MONTH / YEAR FUND STARTS	FEDERAL FUND TYPE	FEDERAL COST BY PHASE	STATE FUND TYPE	STATE COST BY PHASE	OTHER REVENUES	LOCAL FUNDS	TOTAL	YEAR 1 2021	YEAR 2 2022	YEAR 3 2023	YEAR 4 2024	YEAR 5 2025	YEAR 6 2026	TOTAL 2021-2026
		35290 MP 0.84 (18020310)																				
		Snow Creek Road Culvert Replacement					PE	1/20	FEMA	86,250	FEMAm	14,375		14,375	115,000	110,000	5,000					115,000
09	12	Replace and upsize a deteriorated metal culvert above Lake Leland	11	Р	E	N/A	R/W CN	6/21 6/22	FEMA FEMA	11,250 1,010,345	FEMAm FEMAm	1,875 168,391		1,875 168,391	15,000 1,347,127	15,000	1,347,127					15,000 1,347,127
		on an unnamed tributary w/o anadromous fish. Total FEMA (Hazard Mitigation Program + Local Funds) = \$1,292,486					Total	0/22	FEIVIA	1,010,345	FEMAIII	184,641	0	184,641	1,477,127	125,000	1,347,127	0	0	0	0	1,477,127
		51750 MP 1.82 (18020591)					Total			1,101,040		104,041		104,041	1,477,127	120,000	1,002,127		•			1,477,127
		Gibbs Lake Road Culvert Replacement - Naylors Creek					PE	10/19						8,625	8,625				8,625			8,625
09	13	Replace culvert at Naylor Creek with fish-passable crossing.	11	S/P	1	N/A	R/W								0							0
		RCO will manage permits and design review for lead agency FBRB.					CN	6/25	NFPP	105,152	FBRB	599,261		0	704,413				4,000	700,413		704,413
-		Total Grants= \$99,425(FBRB-secure)+\$606,592(FBRB)+\$105,152(NFPP) SR19 MP 9.56 to MP 10.1 (18020040)					Total			105,152		599,261	0	8,625	713,038	0	0	0	12,625	700,413	0	713,038
		Rhody Drive Ped-Bike Improvements - North Segment					PE	7/21	TAP	86,021				13,425	99,446	78,525	20,921					99,446
00	14	Pedestrain & bicycle facilities along SR19 connecting HJ Carroll Park/Rick	28	S/P	E	1.0	R/W			,					0	-,-10	,					0
		Tollefson Trail to finished improvements at Chimacum Schools Campus.					CN	6/22			SRTS	388,947			388,947		388,947					388,947
		Total secure grants = \$109,907 TAP					Total			86,021		388,947	0	13,425	488,393	78,525	409,868	0	0	0	0	488,393
		SR19 MP 8.94 to MP 9.56 (18020040) Rhody Drive Ped-Bike Improvements - South Segment					PE	7/21	TAP	165,223				22,226	187,449	69,858	117,591					187,449
00	15	Pedestrain & bicycle facilities along SR19 connecting the Chimacum	28	S/P	Е	1.0	R/W	7/22	TAP	37,137				5,796	42,933	09,030	42,933					42,933
		School Campus to the Chimacum Crossroads commercial corridor.			_		CN	6/23		01,101	SRTS	979,992		0,100	979,992		.2,000	979,992				979,992
		Total secured grant = \$152,093					Total			202,360		979,992	0	28,022	1,210,374	69,858	160,524	979,992	0	0	0	1,210,374
		850100 & 527109 (18019560)																				
00	146	Rick Tollefson Trail - Ballfield Connection	28	Р	Е	0.30	PE R/W	1/19						17,605	17,605		17,605					17,605
US	16	Old Hadlock Road and Chimacum Road intersection realignment with multi-use trail connecting the Rick Tollefson Trail to Bob Bates Fields.	20	Р	_	0.30	CN	6/24					140.000	32,965	172,965			172,965				172.965
		Total = \$208.269 (with \$140.000 from PIF)					Total	0/24		0		0	140,000	50,570	190,570	0	17,605	172,965	0	0	0	190,570
		93150 Center Road MP 14.60																				
		Little Quilcene River Bridge Replacement					PE	1/22	BRR	316,000					316,000		316,000					316,000
07	17	1955, 64-ft. concrete bridge with structural deficiencies is eligible for	11	Р	E	1.0	R/W CN	1/22 6/23	BRR BRR	50,000 2,583,981					50,000 2,583,981		50,000		2,583,981			50,000 2,583,981
		replacement through federal bridge program. Total = \$2,949,981					Total	6/23	BKK	2,583,981		0	0	0	2,583,981	0	366.000	0	2,583,981	0	0	2,583,981
		93150 MP 10.34 to MP 14.58					Total			2,040,001				Ů	2,040,001		500,000		2,000,001			0
		Center Road 2R					PE	1/23			RAP	53,800		6,086	59,886			59,886				59,886
07	18	Resurface 4.24 miles of Center Road	05	Р	E	4.24	R/W								0							0
		north from the Little Quilcene River.					CN	6/24			RAP	1,646,900		183,086	1,829,986			50.000	1,829,986			1,829,986
\vdash		Total grants = \$1,700,700 850200 S. Discovery and 4 Corners Roads (18019890)					Total			0		1,700,700	0	189,172	1,889,872	0	0	59,886	1,829,986	0	0	1,889,872
		Olympic Discovery Trail (ODT) Link					PE	7/21			PBP	126,813		10,000	136,813	26,013	50,400	60,400				136,813
00	19	Multi-use path connection from Milo Curry Trailhead of the ODT-Larry	28	Р	Е	0.62	R/W					33,904		.,.,.	33,904	.,	,	33,904				33,904
		Scott Trail to the Jefferson Transit Main Base/Park n Ride.					CN	6/22			PBP	354,541			354,541				354,541			354,541
\vdash		Total = \$535,258 60150 MP 4.52 to MP 4.84					Total			0		515,258	0	10,000	525,258	26,013	50,400	94,304	354,541	0	0	525,258
		South Discovery Road 3R					PE	4/23			RAP	118.800		8,200	127,000					127,000		127.000
08	20	Resurface County Road, widen shoulders, some re-alignment, remove	04,	Р	Е	.32	R/W	9/24			RAP	27,000		3,000	30,000					30,000		30,000
		clear zone hazards from Adelma Beach Road to SR20 intersection.	28				CN	6/26			RAP	622,800		69,200	692,000					,	692,000	692,000
		Total grant = \$768,600					Total			0		768,600	0	80,400	849,000	0	0	0	0	157,000	692,000	849,000
		US 101 & multiple county roads MP 294.54 to MP 295.03					DE.	7/05			DDD	404 772			404 750				00.000	454 750		404 750
00	21	Quilcene Complete Streets - Phase 2 Phase 2 project implementing plan to provide sidewalks, crosswalks, &	28	Р	Е	0.05	PE R/W	7/25			PBP	181,750			181,750				30,000	151,750		181,750
ľ	[bicycle lanes to connect school, community center & local businesses.	20	•	_	0.00	CN	1/26			PBP	1,160,000			1,160,000						1,160,000	1,160,000
		Total = \$1,341,750					Total	.,_0		0		1,341,750	0	0	1,341,750	0	0	0	30,000	151,750	1,160,000	
		850200 Anderson Lake State Park to HWY-101																				
		Olympic Discovery Trail (ODT) - Eaglemount	15,				PE	1/20			RCO-RRG	14,000			14,000	14,000						14,000
00	22	Complete a preferred route report, begin PS&E development, permitting	16, 28	S	Е	8.0	R/W	1/20 N/A			RCO-RRG	60,000			60,000	60,000						60,000
		and right-of-way appraisal and acquisition. Total = \$406,491 (Planning and Right-of-Way only)	20				CN Total	N/A		0		74,000	0	0	74,000	74,000	0	0	0	0	0	74.000
_		1.000 \$ 100,701 (Fidining and ragin-of-way only)					i Viai			U		7-4,000	U	U	, 4,000	7-4,000	U	U	U	U	U	7-4,000

SIX-YEAR TRANSPORTATION IMPROVEMENT PROGRAM 2021 - 2026

Hearing Date: 10/26/2020 Adoption Date: 10/26/2020 Resolution No. 67-20

					' 0					PROJ	ECT COSTS I	N DOLLARS			1							
					ASSESS							SOURCE INF						EXPEN	DITURE SCH	DULE		
			Ä		SSI				FEDERA	L FUNDS	STATE											
FUNC. CLASS	TIP PROJECT NO.	PROJECT IDENTIFICATION A. Road Log Number - Bridge Number B. Beginning and End Milepost C. Project (No.) / Road Name D. Description of Work E. Total Cost (incl. prior/future)	IMPROVEMENT TYF	FUND STATUS	ENVIRONMENAL AS	TOTAL LENGTH (mi.)	PROJECT PHASE	MONTH /YEAR FUND STARTS	FEDERAL FUND	FEDERAL COST	STATE FUND TYPE	STATE COST BY PHASE	OTHER REVENUES	LOCAL FUNDS	TOTAL	YEAR 1 2021	YEAR 2 2022	YEAR 3 2023	YEAR 4 2024	YEAR 5 2025	YEAR 6 2026	TOTAL 2021-2026
		850200 Discovery Bay Village to SR-20/HWY-101 Junction																				0
		Olympic Discovery Trail (ODT) Discovery Bay - Segment B					PE	7/23			RCO-RRG	300,000			300,000				50,000	250,000		300,000
0	23	ODT segment from existing South Discovery Bay ODT terminus	28	Р	E	.33	R/W	4/24			RCO-RRG	20,000			20,000					20,000		20,000
		(over Salmon & Snow Creeks) to SR 20/US 101 intersection.					CN	6/25			RCO-RRG	2,200,000			2,200,000						2,200,000	2,200,000
		Total = \$2,520,000					Total			0		2,520,000	0	0	2,520,000	0	0	0	50,000	270,000	2,200,000	2,520,000
																						0
		Countywide Safety Improvements					PE	1/22	HSIP	56,250				18,750	75,000		25,000		25,000		25,000	75,000
0	0 24	Systemic safety countermeasures applied to county network identified	21	Р	Е	N/A	R/W								0							0
		in a local plan and funded thru a bienniel WSDOT federal grant program.				-	CN	6/23	HSIP	240.000				60.000	300.000			150.000		150.000		300.000
		Total = \$375.000					Total			296,250		0		78,750	375,000	0	25,000	150,000	25.000	150,000	25.000	375,000
		91420 MP 9.7 to MP 9.8 (18019880)										_		10,100	010,000	_		,		,		0
		Upper Hoh Road Spruce Creek Mitigation					PE	11/18	ER	9,012				1,407	10,419	10,419						10,419
0	7 25	Environmental mitigation project in response	11	Р	Е	0.1	R/W							,	0	,						0
		to 2015 emergency repairs.				-	CN	6/22	ER	105.963				16.537	122,500		122.500					122.500
		Total = \$161,297					Total			114,975		0	0	17,944	132,919	10,419	122,500	0	0	0	0	132,919
														,								0
		Countywide Bridge Heavy Maintenance Program					PE	1/22	BRR	40.000				10.000	50.000			25.000		25.000		50.000
0	26	Protection of primary bridge components, seismic retrofits, etc. through	48	Р	Е	N/A	R/W			.,				-,	0			,,,,,,		.,		0
		a bienniel WSDOT administered federal grant program.			_		CN	6/23	BRR	240.000				60.000	300.000				150.000		150.000	300.000
		Total = \$350.000					Total			280.000		0		70,000	350.000	0	0	25.000	150,000	25.000	150,000	350,000
		Countywide Various Road Improvements								,				,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,	,			,	,	,	,	0
		Various projects countywide involving lighting, signage, intersection					PE							30,000	30,000	5,000	5,000	5,000	5,000	5,000	5,000	30,000
0	0 27	control, culvert replacement/rehabilitation, drainage, surfacing upgrades,		Р	E	N/A	R/W							0	0			·	·	,	,	0
		pedestrian and bicycle facilities, or other improvements using local funds.					CN							225,000	225,000	25,000	100,000	25,000	25,000	25,000	25,000	225,000
		Total = \$255,000					Total			0		0	0	255,000	255,000	30,000	105,000	30,000	30,000	30,000	30,000	255,000
		26,525,362								7,091,149		17,984,087	267,416	1,182,710	26,525,362	2,479,126	4,732,693	5,347,388	5,874,992	3,834,163	4,257,000	26,525,362
						Į.			<u> </u>					<u> </u>	<u>.</u>							
		2,292,979				ı	PE			877,506		1,129,113	n	286,360	2.292.979	578,708	740,410	256,486	128,625	558,750	30,000	2,292,979
		496.337					R/W			98.387		374.279	5.000	18.671	496,337	319,500	92.933	33.904	120,023	50.000	30,000	496.337
							CN			6,115,256		16,480,695	262,416	877,679	23,736,046	1,580,918	3,899,350	5,056,998	5,746,367	3,225,413	4,227,000	23,736,046
		23,736,046					Total			7,091,149		17,984,087	262,416	1,182,710		2,479,126	4,732,693	5,056,998	5,746,367	3,834,163		
		26,525,362					iotai			7,091,149		17,964,087	207,416	1,182,710	20,025,362	2,419,126	4,732,693	5,347,388	5,674,992	ა,ია4,163	4,257,000	20,020,362

2021-2026 TIP Adopted Page 3 of 3

Guide to Codes, Abbreviations and Acronyms

The following is a guide to codes used in the TIP project spreadsheet for Jefferson County recognized by state and federal agencies.

FEDERAL FUNCTIONAL CLASSIFICATION (FFC)

03	Rural Other Principal Arterial	09	Rural Local Access
06	Rural Minor Arterial	17	Urban Collector
07	Rural Major Collector	19	Urban Local Access
08	Rural Minor Collector	00	No Functional Classification, ex. trails

IMPROVEMENT TYPES per the Statewide Transportation Improvement Program (STIP) Categories

01	New construction roadway	18	Planning
03	Reconstruction - added capacity	19	Research
04	Reconstruction - no added capacity	20	Environmental only
05	4R Maintenance - resurfacing	21	Safety
06	4R Maintenance - restoration & rehabilitation	22	Rail/highway crossing
07	4R Maintenance - relocation	23	Transit
80	Bridge - new construction	24	Traffic Management/Engineering - HOV
10	Bridge replacement - added capacity	25	Vehicle Weight Enforcement
11	Bridge replacement - no added capacity	26	Ferry Boats
13	Bridge rehabilitation - added capactiy	27	Administration
14	Bridge rehabilitation - no added capacity	28	Facilities for Pedestrians & Bicycles
15	Preliminary Engineering	40	Special Bridge
16	Right-of-Way	48	Bridge Protection
17	Construction Engineering	49	Bridge Inspection

FUNDING STATUS

S = Secured (funded); P = Planned (unfunded); S/P = Partial secure funded (most likely by phases)

PROJECT PHASE (by STIP definition)

PL = Planning not leading to construction; PE = Preliminary Engineering & Design; R/W = Right-of-Way acquisition; an CN = Construction & Construction Engineering

Guide to Codes, Abbreviations and Acronyms

The following is a guide to abbreviations and acronyms used in the TIP project spreadsheet for Jefferson County.

FEDERAL FUND CODES (competitive funds applicable to Jefferson County)

BRR ER FEMA	Bridge Replacement and Rehabilitation, Program (aka "BRAC") Emergency Relief Federal Emergency Management Agency, Hazard Mitigation Program
FLAP	Federal Lands Access Program from WFL
HIP-R	Highway Infrastructure Program (2018 only)
HSIP	Highway Safety Improvement Program
NFPP	National Fish Passage Program
SRSII	Secure Rural School-Title II
SRTS	Surface Transportation Program-Safe Routes to Schools from Federal
STBG	Surface Transportation Block Grant Setasides
STP-R	Surface Transportation Program-Rural
TAP-R	Surface Transportation Program-Transportation Alternatives from RTPO
WAFH	Washington Forest Highways, Program

STATE FUND CODES (competitive funds applicable to Jefferson County)

ALEA	Aquatic Lands Enhancement Account
FBRB	Fish Barrier Removal Board
FEMAm	Federal Emergency Management Agency, Hazard Mitigation Program match, from State
NOVA	Non-Highway and Off-Road Vehicle Activities, Program
PBP	Pedestrian and Bicycle Program
PSAR	Puget Sound Acquisition and Restoration, Fund
RAP	Rural Arterial Program from CRAB
RRG	Recreation Resource Grant (managed by Recreation Conservation Office (RCO))
SRFB	Salmon Recovery Funding Board
SRTS	Safe Routes to School, Program from State
TIB	Transportation Improvement Board, Complete Streets Award or Urban Sidewalk
WWRP	Washington Wildlife Recreation Program

OTHER FUND CODES (applicable to Jefferson County)

CAPP	County Arterial Preservation Program
PIF	Public Infrastructure Fund
PILT	Payment in Lieu of Taxes

ENVIRONMENTAL ASSESSMENT

Applic	able to CRAB	Applicable to Federal Funding
Е	EXEMPT	Categorically Exempt CE or Documented Categorically Exempt DCE
1	INSIGNIFICANT	Environmental Assessment EA
S	SIGNIFICANT	Environmental Impact Statement EIS

PAST ACCOMPLISHMENT

Quilcene Center Road 2R & Complete Street

Resurface and Restore Center Road

MP14.61 to MP15.01

2020-2025 Adopted TIP

TIP Priority Array #4

Remove and replace layers of deteriorating asphalt pavement & replace sidewalks curb and ADA ramps.

Total Funding

\$788,000



Substantial Completion: August, 2020

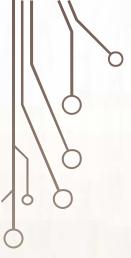
PAST ACCOMPLISHMENT

South Discovery Road, MP 3.18 – 4.58
Pavement Preservation
2019-2024 Adopted TIP
TIP Priority Array #3

Resurface and restore a 1.39 mile section of asphalt road to preserve the roadway structure and improve ride quality and safety. Minor shoulder widening, paving and edge finishing. Contractor: Lakeside Industries, Inc.

Substantial Completion: July, 2019
Total Funding \$549,631
State (RAP) \$450,000
Local \$99,631





PAST ACCOMPLISHMENT

OLYMPIC DISCOVERY TRAIL

South Discovery Bay — Segment B to Larry Scott Trail

2019 – 2024 Adopted TIP TIP Priority Array #4

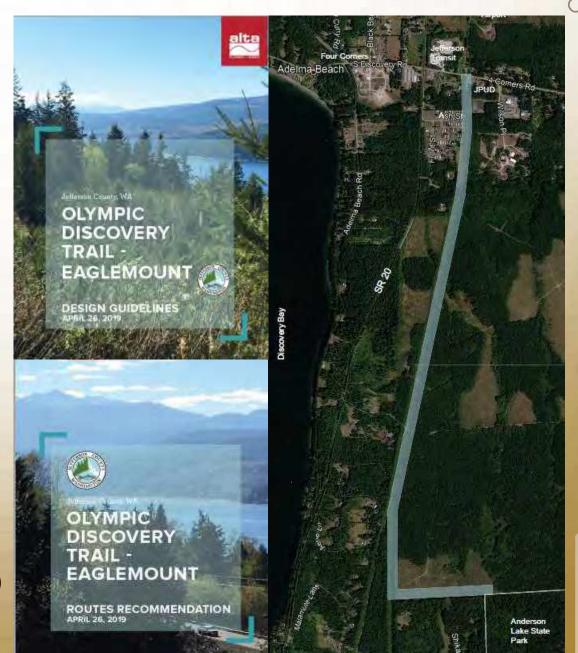
Complete a preferred route report, begin PS&E development, SEPA, and right-of-way appraisal and acquisition.

Preferred route report complete, April 2019 with design guidelines.

Preferred route selected from 4 Corners
Road to Anderson Lake Park, June 2019
Acquisition Phase continues, near completion

Total RCO Funds = \$1,000,000

Remaining for ROW = \$74,000 (2021-2026 TIP)



East Jefferson - 18 projects

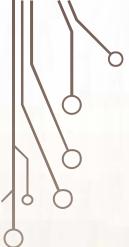
TRANSPORTATION IMPROVEMENT PROGRAM 2021 – 2026 27 PROJECTS

- o DESIGN (PE)
- o RIGHT-OF-WAY (ROW)
- o CONSTRUCTION (CN)

West Jefferson – 5 Projects



County wide – 4 Projects



TOP 7 CRITERIA
FOR
ESTABLISHING
TIP PRIORITY

Ranked #1	Criteria 16	GRANT AVAILABILITY:
	96.7%	Transportation infrastructure is eligible for grants, has a) an application filed to grant agencies, b) indication of grant availability, or c) secured grant funding.
Ranked #2	Criteria 5	SCHOOL PEDESTRIAN ROUTE:
	90.0%	Road segment is an authorized school pedestrian route sanctioned by the school transportation coordinator and needs sufficient pedestrian facilities to the school campus.
Ranked #3a	Criteria 14	COMMUNITY PLANNING/COMPREHENSIVE LAND USE CONSISTENCY:
	\$3.3%	Road segment provides mobility within and accessibility to an area of potential development which is land use consistent with zoning and approved codes. Road segment is identified in an adopted Growth Management Act sub-area or comprehensive plan for correction of deficiencies or improvements.
Ranked #3b	Criteria 17	ECONOMIC DEVELOPMENT:
	83.3%	Road segment, if not improved, will have a measurable and obvious impact on economic development or other rational measures of public necessity.
Ranked #4a	Criteria 2	CRASH HISTORY:
	\$0.0%	Road segment has significant hazard potential, defects or collisions. The inventory documents one or more serious injuries and fatalities to pedestrian, bicyclist or driver passenger within past five years.
Ranked #4b	Criteria 1	NON-MOTORIZED NEEDS:
	80.0%	Road or trail segment is on adopted alignment or non-motorized route map, frequented by non-motorized users, and needs sufficient active transportation facilities for the latent demand.
Ranked #4c	Criteria 6	TRANSIT AND SCHOOL BUS ROUTES:
	80.0%	Road segment supports fixed-route transit and/or school routes with numerous stops, and high volumes of users within one-mile of the school "safe-routes-to-school".

TIP PRIORITY ARRAY 2021 - 2026

Top 14 Projects with secured or partially secured funding.



Two Pending Grant Funded Projects as of October 2020

GRANT FUNDS PENDING FROM FEMA

12 35290 MP 0.84 (18020310) Snow Creek Road Culvert Replacement



TIP PROJECT NO.	PROJECT IDENTIFICATION A. Road Log Number - Bridge Number - Beginning and End Milepost B. Project / Road Name	To	otal Cost 2021 - 2026	S	ecured Funds 2021-2026
1	US 101 & connected county roads MP 294.6 to MP 295.0				
	Quilcene Complete Streets - Phase 1 (1801933)	\$	757,766	\$	705,350
2	91420 MP 0.00 to MP 12.04 (18019410) Upper Hoh Road Phase 2 - FLAP Project	\$	10,000	\$	10,000
3	63710 MP 0.00 to MP 0.46 (18020570) Cedar Avenue, Ness' Corner (SR116) to Hayden Road Ped-Bike	\$	820,957	\$	820,957
	850200 4 Corners Road to Anderson Lake State Park				
4	Olympic Discovery Trail (ODT) - Connection	\$	3,647,611	\$	269,570
5	15100 MP 0.90 to MP 2.185 (18020460)				
	Undi Road Bypass - FLAP Project	\$	13,336	\$	13,336
6	91420 MP 0.00 to MP 12.04 (18020560)		400 400		400 400
	Upper Hoh Road Pavement Preservation	\$	408,400	\$	408,400
7	41870 MP 4.71 (18020580) Thorndyke Road Culvert Replacement - Thorndyke Creek	•	1,958,776	\$	1,738,410
	Thomagae Road Gaivert Replacement - Thomagae Greek	Ψ	1,550,770	Ψ	1,730,410
9	Countywide Bridge Load Re-Rating	\$	128,700	\$	128,700
40	51410 MP 2.59 (18020591)			ľ,	The second second
10	West Valley Road Culvert Replacement - Naylors Creek	\$	819,534	\$	2,550
44	13430 MP 8.50 to MP 10.98				
11	Oil City Road Engineering Assessment - FLAP Project	\$	30,000	\$	30,000
13	51750 MP 1.82 (18020591)				
13	Gibbs Lake Road Culvert Replacement - Naylors Creek	\$	713,038	\$	8,625
14	SR19 MP 9.56 to MP 10.1 (18020040)	В			
1-7	Rhody Drive Ped-Bike Improvements - North Segment	\$	488,393	\$	99,446
15	SR19 MP 8.94 to MP 9.56 (18020040)		199		
	Rhody Drive Ped-Bike Improvements - South Segment	\$	1,210,374	\$	230,382
22	850200 Anderson Lake State Park to HWY-101				
	Olympic Discovery Trail (ODT) - Eaglemount	\$	74,000	\$	74,000/

TOTAL SECURED

4,539,726

BY PROJECT CATEGORY

EXAMPLE PROJECTS WITH SECURED AND ANTICIPATED FUNDING IN 2021

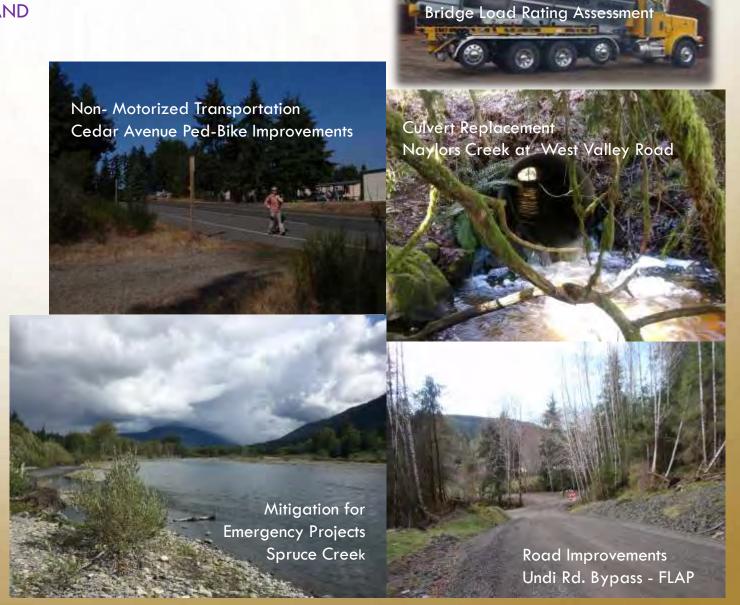
Road & Intersection Improvements

Culvert & Bridge Replacement / Repair/Heavy Maintenance

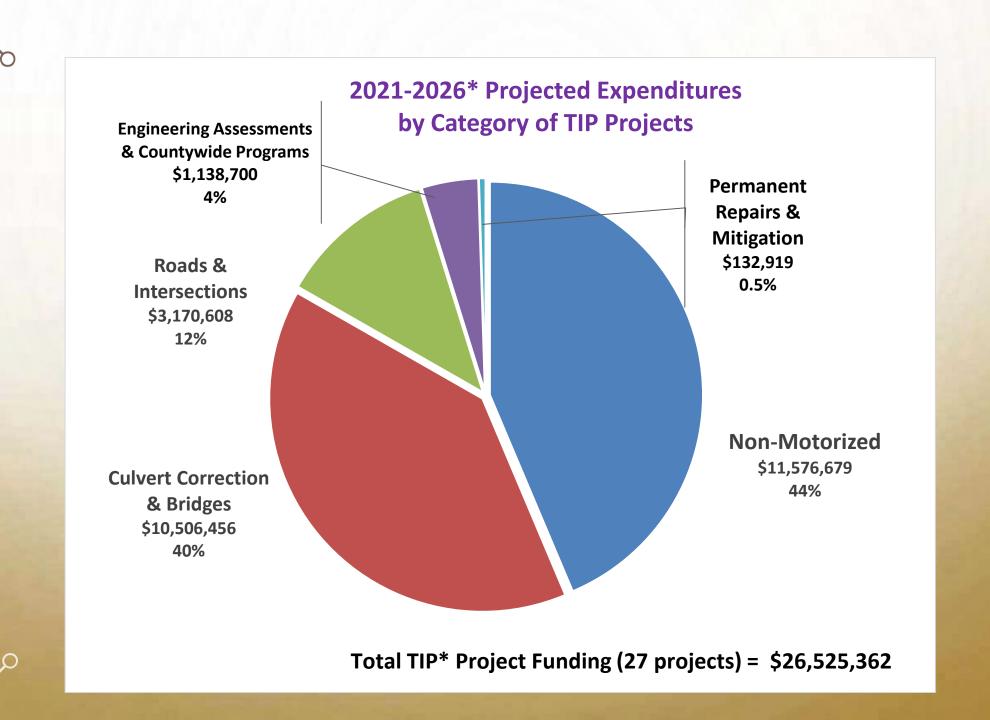
Non-Motorized Transportation

Assessments & Countywide Programs

Mitigation for Emergency Projects



Countywide programs



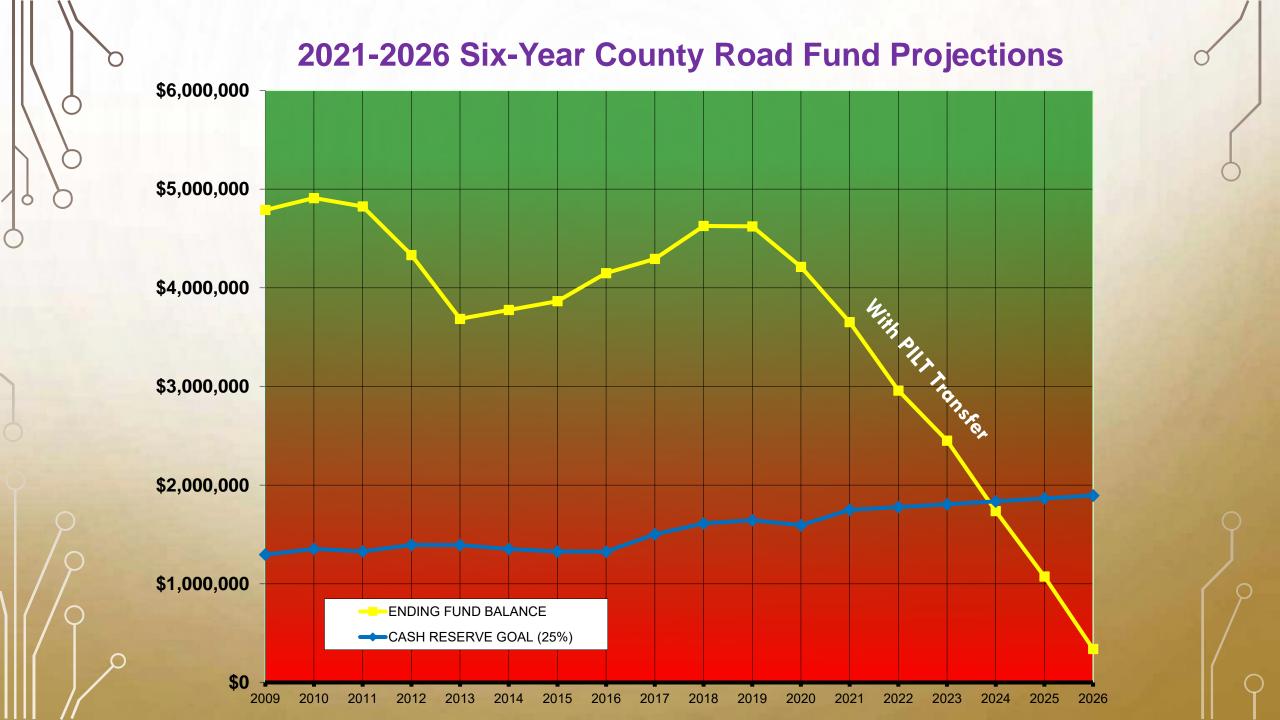
2021-2026* Projected Expenditures by Type of Funding **Federal** \$7,091,149 26.7% State Local \$17,984,087 \$1,182,710 67.8% 4.5% Other \$267,416 1.0% **Total TIP* Project Funding (27 projects) = \$26,525,362**

REVENUE ASSUMPTIONS

- O 2020 Projected Loss of \$300K MVFT
- O In 2021, MVFT Restored to pre-COVID Levels
- O Every Year \$450K PILT to Backfill SRS
- O Grant Funds Aim for 90% Project Total
- O 100% Federal managed bridge program
- O Local Funds are \$197K per year to Match Grants
- O PIF is excellent Last \$\$ for needed Match
- O Road Diversion Capped at \$720K per year
- O Maintain a 25% Operating Reserve until 2024

2021-2026 County Road Fund Projections

	2021 2020 000		ACTUAL	PROJ	BUDGET	TOTAL
	DESCRIPTION	Incr.	2019	2020	2021	2021-2026
	PROPERTY TAXES (+ NEW CN)	0.5%	4,548,564	4,634,222	4,628,139	28,118,267
	PROPERTY TAXES: 1% (I-747)	1.0%				235,612
	BANKED CAPACITY (Taken)	0.5%	0	0	0	0
	ROADS LEVY SHIFT		0	0	0	0
	DIVERSION (Traffic Enforcement)	1.0%	(720,000)	(720,000)	(720,000)	(4,320,000)
nes	TIMBER TAX	0.0%	219,200	136,717	200,000	1,200,000
Revenues	FEDERAL FOREST (SRS)	0.0%	403,540	363,190	85,000	510,000
	PILT TRANSFER (partial offset)	3.0%	86,172	131,344	425,211	2,750,439
atin	DNR TIMBER SALES	0.0%	212,927	140,026	125,000	750,000
Operating	FUEL TAX	0.5%	1,478,291	1,253,328	1,582,000	9,611,444
0	CAPA, TPA, MVA FUNDS	0.0%	254,688	244,943	267,000	1,602,000
	PERMITS & FEES	1.0%	203,765	184,754	153,000	941,258
	OTHER MISC. REVENUE	0.0%	38,059	29,108	3,500	21,000
	REIMBUSIBLE SERVICES		12,141	11,745	2,930	2,940
	TOTAL OPERATING REVENUES:		6,737,347	6,409,377	6,751,780	41,422,960



FIRST YEAR OF THE TIP IS THE DRAFT ANNUAL CONSTRUCTION PROGRAM

								7.6											
								.ieti	erson County 2021										
							A		estruction Pr VAC 136-16	rogram									
	CONSTR	ECTION DONK (IN	tal sum	of sulumn 13+ sulumn 14);	3 57A.B.	8.00									Date of 1	virname	mud Assessi	ment	
(C) 10(A)	COUNTY			OUNTY FORCES LIMIT: N Hotal sum of column 14%		48.55 (0,00									Ordi		Final Adop Inhitina Niin		
(0)	(12)	(3)		(4)				(5)	(6)	n	MX	to.i	:10)	7007	(12)	/(a):	(14) Equalitativ	:441	
Asonst Protesto	Road	Project None		Result Segment Fafores		1000	-	Project	Project Type	Farifona-etc.		Sourse of Pinels Other I			Right of	Consta	oller		
hrin Nil.	Jron No.	27,000,00	Roan?	Road Many	BMIP	EME	ASC	Designation of the last of the	Cedt	Asessant	Read Minds	Assemb	Paragram Somers	995.0f)	1995 #115	(Central)	Cuents Ferre:	Grand John (All 595)	
0.0	1,0	Contrare Complete Stress - Place 1	00502	Rone Neuri: HWY 10 Prim: Pank: Read To: Washington Street	204.61	(Enrice)	20	(i et	596	ř.	s latiff))	\$600,050 MC 552,410 MC	Notific White	510.05b		5217 AC		3757,766	
-	3	Opportfol, Boar 1945. 2 * 1 LAU* Project	91875	Break Name: 15 pc This 8d From: 166 Y 16d Tre CMP Resendany	9.00	1214	37	1838	le TVR	\$	\$5,500	20.30	Lucab Access Propositi	25,000	\$6.	20	SI-	23/200	
1	2	eR., 6-8. Csear Avenus Ped-Bike Improventino	627 n	Rand Name (Cop. Ass Prem; RX-116 Fq; Hayder Road	16141	s in	29	1191	749 749	P	424/02	\$71) HS2.00	50/72	917,600	\$0	\$350,153		5820,957	
	4	Clympic Dissector thats Connegation is Connected ALSP						-55%	Pay	F	\$3	料例を明め	8355	~styry)	Satistic			5296600	
*	+	How there I grown a Fland Project	13100	Shoul Some: Unit to thy etc. From: George Liniss The ASP's Democry	10.01	2 %	50	1.25	2R	E	26,649	\$5.00	Lend- According 15-15-15	45,565				35,569	
		Upper Hot Passions		Read Yames: Upper High Rd								\$14495200	6.Alt						
	*	Progression - TI A * Project	0.500	Print: IPAN III To: CNP Bourday	n/iz	1-16	17	12,04	28	Þ	50	30.00	Fedoral Limite Access Program	\$6,400				rasam	
	ā	Choragas Road Culvers Togelacordon - MP 4/91	4 82%	Road Name: Themloke 3.1 hmm 4.74 loca.72	779	4,7*	les	uoz.	17		S15,000	(30.00)	FRKE	\$5,000	\$10,200			\$18000	
0.0	6	Canta Heart Jahwar Etnesina Militarian Carta	33/30	Rand Martin: Conner Road Person: 1.26 Tech 201	848	2.49	100	0.006	175		890	925,003,50	even	\$24,000				s58.01b	
100	-a	Controvide Bridge Lead assistance						9886	roter	W.	\$1-,775	51)[5250.	SIEB)	\$128,720				5128,703	
-10	(8)	Nayton O ed. Curren To-laren an: Was Valley Road	SHIP	Rucol Maine: We a Valley Bill Promit Ages To: 2.35	2.53	2.00	.09	i du	Ur.		91,700	323300	FERR	\$6,250	=3.00%			(23)221	
-0.	440	Ot Ony Toold Engineering Newsonani	:480	Hotel Natura Cli Clip Read Treats 8.72 18: 10,95	v.5n	1091	16=	7.48	Oher		\$1500	3000	Folk in Lands Arres Prop. ac.	\$15(50)				518000	

- Provided copy for Board input.
- Adopted as part of biennial County budget
- Alternate years,adoption by ConsentAgenda
- O Sent to County Road
 Administration Board
 by 12-31-2020

UPDATED BRIDGE CONDITION REPORT 2020

Jefferson County Master Inventory

Total # of Bridges = 35 West County = 20

East County = 15

Total # of Permanently Closed Bridges = 2

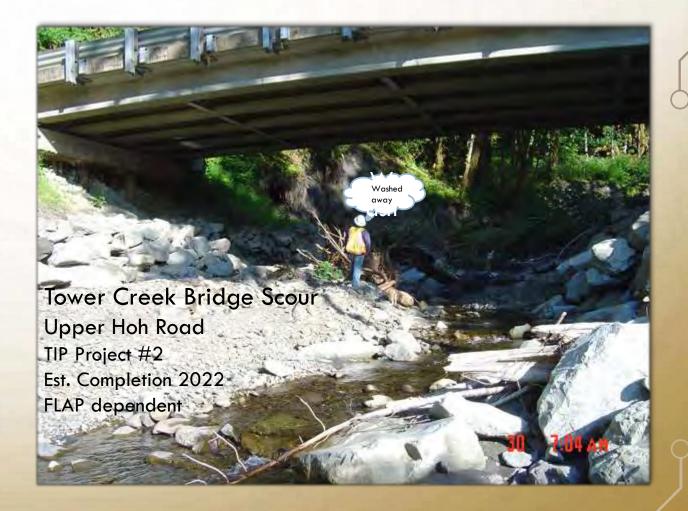
Total # of Active Bridges = 35 incl. pedestrian

Recommended Bridge Maintenance Activity at:

- O Hurst Creek Clearwater Road
 Steel Diaphragm Painting
- O Contractor's Creek Old Gardiner Road
 Pile Bracing and Stiffening
- O Cassel Creek Oil City Road Steel Girders Painting

Recommended Bridge Replacement at::

O Little Quilcene River – Center Road TIP #17



2013 FHWA Mandate

Specialized Hauling Vehicle (SHV) class will require new load ratings on 22 active bridges and potential new postings by Dec. 31, 2022

UPPER HOH ROAD – a FLAP Project

TIP Project #2
Regionally Significant
Est. Completion 2022
Total Project Budget = \$14,029,621 Uses Toll Credits

World famous Hoh Rainforest reached via 12 miles traveling this county road only way in & out Important to tourism economy for Greater Olympic Peninsula Nearly 300,000 visitors annually; 85% of road traffic or 315,000 trips per year (2018)

New bank washout every 2 to 3 years

Emergency expenditures average \$400,000/year

Route Jurisdiction Transfer to WSDOT considered

MOU with County, WSDOT, FHWA, NPS

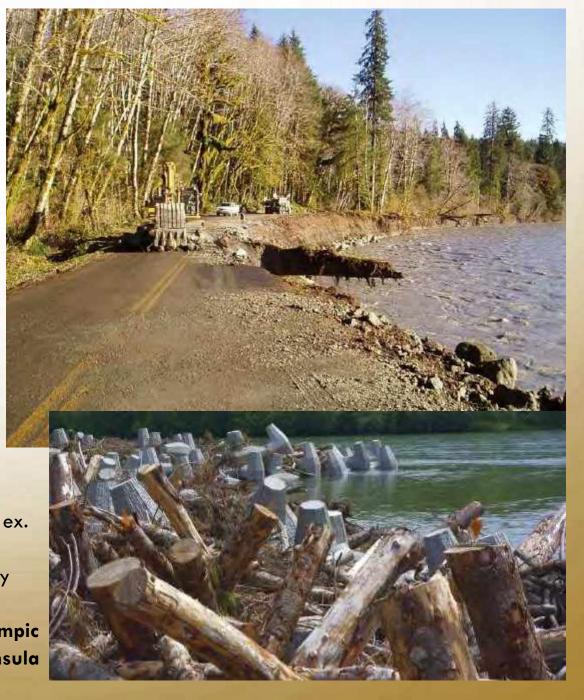
Federal Land Access Program (FLAP) investments

\$13 million allocated for bridges and bank protection, ex.

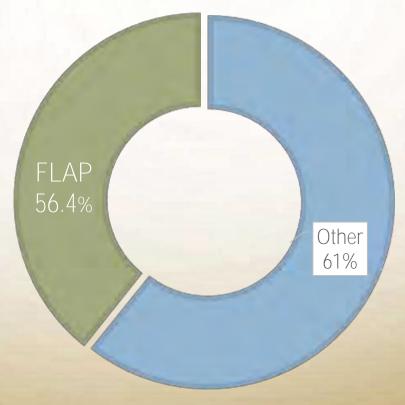
Tower and Canyon Creeks

\$2 million allocated for pavement/guardrail and safety improvements

First dolosse(s) permitted for bank protection on Olympic
Peninsula



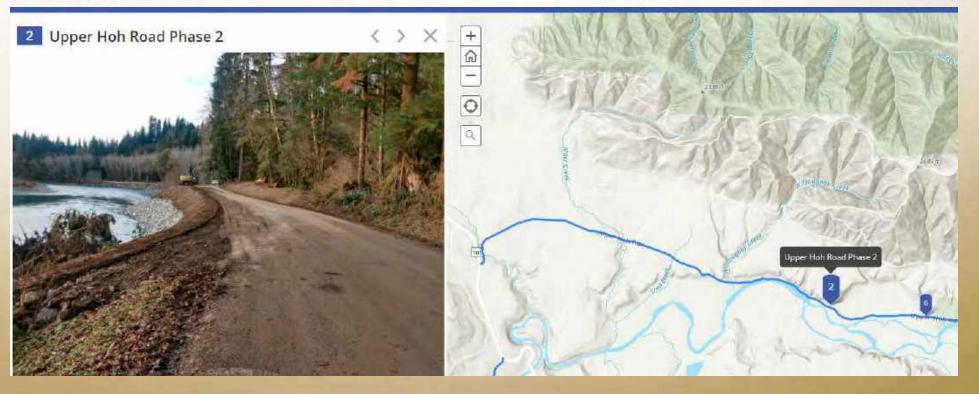
TOTAL FUNDING 27 TIP PROJECTS SECURE FLAP VS. NON-FLAP (OTHER) FEDERAL LANDS ACCESS PROGRAM, ADMINISTERED BY FHWA

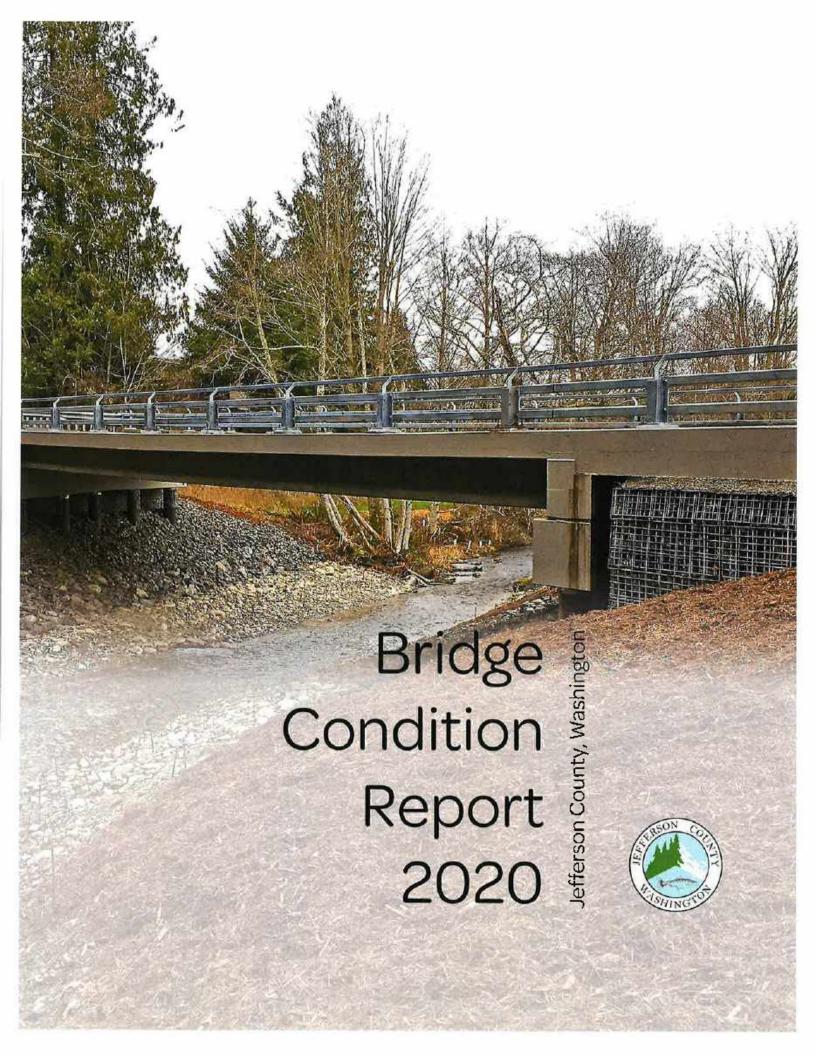


Total Project Funding, 27 projects = \$45,453,144

Interactive <u>STORYMAPPING</u> all 27 TIP projects with site specific pegs. Go to

https://arcg.is/1fO5n8





2020 Bridge Condition Report

Approved by:

Monte Reinders, P.E.

Public Works Director/County Engineer

- 9-24-2020

Prepared by: Mark Shouter

Mark Thurston, P.E.

Lead Bridge Inspector/Engineer III



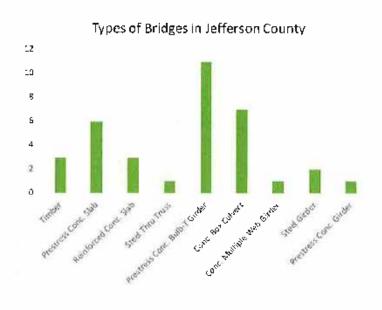
As required by WAC 136-20-060, "Each county engineer shall furnish the county legislative authority with a written report of the findings of the bridge inspection effort. This report shall be made available to said authority and shall be consulted during the preparation of the proposed six-year transportation program revision. The report shall include the county engineer's recommendations as to replacement, repair or load restriction for each deficient bridge. The resolution of adoption of the six-year transportation program shall include assurances to the effect that the county engineer's report with respect to deficient bridges was available to said authority during the preparation of the program. It is highly recommended that deficient short span bridges, drainage structures, and large culverts be included in said report."

INTRODUCTION

Jefferson County Public Works has 35 active bridges—see the attached Master Bridge List (Appendix A) for a listing of the county bridges. Of these, 33 bridges meet the federal definition of a bridge and are subject to the National Bridge

Inspections Standards. Two of the do bridges not meet the federal bridge definition. one because it is a pedestrian bridge, and one because it has less than a twenty foot span; nevertheless, these two bridges are on Jefferson County's biannual bridge inspection program. Jefferson County also has two permanently. closed

BRIDGE INVENTORY



bridges, which are not listed on the Master Bridge List.

BRIDGE INSPECTIONS

The National Bridge Inspection Standards (NBIS) and WAC 136-20-030 mandate that public agencies inspect and report on all bridges at least once every two years (routine inspection). Jefferson County staff performs all routine inspections. Special inspections are required for bridges that cannot be inspected adequately from the ground. For these bridges an Under-Bridge Inspection Truck (UBIT) is required. Steel bridges with fracture critical members may also require special inspections with the UBIT and/or other special equipment, Jefferson County has two bridges which require the UBIT inspection and one bridge which requires both the UBIT and the fracture critical inspection (identified in the attached Master Bridge List). Jefferson County currently has a contract with the Washington State Department of Transportation (WSDOT) to perform these inspections.

Jefferson County Public Works has split up the

Bridge Age by Decade

10

9

8

7

6

5

4

3

2

10

1350 1359 1960-1969 1970-1979 1980-1989 1990-1995 2000-2009 2010-2019

bridge inspections so that East Jefferson County bridges are inspected in odd-numbered years and West Jefferson County bridges are inspected in even-numbered vears. conformance with that schedule, lef-County ferson completed the biennial inspections for all West Jefferson County bridges in 2020. The

completed inspection reports were submitted to WSDOT, which verifies compliance with the NBIS and reports to the Federal Highway Administration (FHWA),

All bridges are assigned a sufficiency rating (SR), which is a calculated score based on numbers assigned to all of the bridge elements reviewed by the bridge inspector. The SR is a number from 0 to 100, with 100 being an entirely sufficient bridge, and 0 being an entirely insufficient bridge. For additional explanation of the sufficiency rating and other ratings see the attached WSDOT publication, Bridges and Structures - Ratings (Appendix B). The sufficiency rating is used to determine if a bridge is eligible for federal rehabilitation or replacement funding. Under the current funding requirements, to be eligible for replacement a bridge must be structurally deficient and have a sufficiency rating of less than 40: to be eligible for rehabilitation a bridge must be structurally deficient and have a sufficiency rating of less than 80. Currently, the county has one bridge that is classified as structurally deficient

and has a sufficiency rating less than 40: Little Quilcene Bridge on Center Road. Public Works plans to apply for bridge r epla cement funding as described under Programmed Projects.

As described in previous Bridge Condition reports, the Tower Creek Bridge is being monitored for potential scour problems

DEFICIENT BRIDGES



Tower Creek Bridge

associated with down-cutting of the streambed. The Tower Creek streambed experienced significant down-cutting in 2007-2008 due to migration of the Hoh River and heavy flows in Tower Creek. However, based on regular monitoring, the streambed elevation has been relatively stable since 2008. Public Works will continue to monitor this situation. The Tower Creek Bridge is scheduled to be replaced in 2021-2022 by the Federal Highway Administration Western Federal Lands Division, and is being funded entirely by a Federal Lands Access Program grant.

POSTED BRIDGES

All bridges are required to have a "Load Rating" calculation. The Load Rating establishes how much weight the bridge can carry for several standard configurations of vehicle axle loads. If the load rating calculation shows that a bridge cannot safely carry one or more of the legal vehicle axle load configurations, then the bridge must be posted with the appropriate load limits. Jefferson County currently has two bridges that are load posted, Meyers Bridge on Belfrage Road and Hemphill Creek Bridge on Dowans



Yarr Bridge

Creek Road. In addition, the load rating calculations for the Yarr Bridge indicate that should be posted for a number of legal vehicles. However, the bridge has been carrying normal traffic for appreciable an length of time and shows no distress. accordance with the Manual for Bridge Evaluation (published by the American Association of State Highway | and Transportation Officials), and as recommended by the WSDOT Local Programs : Bridge Engineer, Jefferson County is following a plan of increased inspection frequency as an acceptable alternative to load posting. **Public** Works inspects the Yarr Bridge on a 12 month frequency,



Underside of Hurst Creek Bridge

as opposed to the standard 24 month frequency, and intends to continue with this plan, provided that the bridge continues to show no signs of distress.

Similarly, the load rating calculations for the Little Quilcene Bridge on Center Road indicate that it should be posted for a number of legal loads, but this bridge has also been carrying normal traffic for an appreciable length of time and shows no distress. Based on the Manual for Bridge evaluation, the load rating document states that load posting is optional and should be at the County's discretion. The County has exercised this option and not posted the bridge. However, as of September 2020, FHWA has disagreed with the County's discretionary authority and has informed the County that the bridge must be posted. The County is complying with this request and intends to post the bridge.

A 2013 FHWA mandate requires that all bridges be load rated for a relatively new class of vehicle, the Specialized Hauling Vehicle (SHV). SHV's are closely-spaced multi-axle single unit trucks

introduced by the trucking industry in the last decade. Examples include dump trucks, construction vehicles, solid waste trucks and other hauling trucks. The FHWA has divided bridges into two categories, with the first category requiring updated load ratings by December 31, 2017, and the second category requiring updated load ratings by December 31, 2022. Prior to the 2013 mandate, Jefferson County aiready had eight bridges with load ratings that met the new FHWA requirements. Jefferson County has completed new load ratings for three bridges for the 2017 deadline. An additional twenty-two bridges will need to be re-load rated prior to the 2022 deadline. It is possible that some Jefferson County bridges will need to be load posted for SHV's after the load rating results are known.

OVERWEIGHT/ OVERSIZE VEHICLE PERMITTING PROCESS

Last year Public Works developed a new form and process for evaluating and issuing overweight/ oversize vehicle permits. A policy was written to help ensure that consistent procedures are used in processing permits, and a new, more detailed application form was developed. The new application form, along with associated webpage updates, was deployed last fall and has been used successfully to issue permits.

ORSOLETE BRIDGES

The Old Little Quilcene Bridge and the Maple Creek Bridge have been placed in obsolete status and removed from the Master Bridge List. Both are permanently closed to vehicular traffic. This action removes the requirement for biennial bridge inspection.

MAINTENANCE ACTIVITIES

Routine maintenance of the county's bridges is conducted by the road maintenance department. Typical routine maintenance consists of deck cleaning, brush clearing, and minor miscellaneous repairs.

In April 2019, the County applied for \$90,830 in Bridge Replacement Advisory Committee (BRAC) funds for a preventative maintenance project on the Hurst Creek Bridge, Clearwater Road milepost 2.66. The Hurst Creek Bridge has concrete girders with steel diaphragms between the girders at mid-span. The steel diaphragms are in need of repainting or replacement. In September 2019 we learned the project would not be funded through BRAC, so an alternative funding source will need to be found.

No emergency repairs or inspections were performed in 2020 at the time of this report.

EMERGENCY REPAIRS AND INSPECTIONS

No bridge projects were completed in 2020.

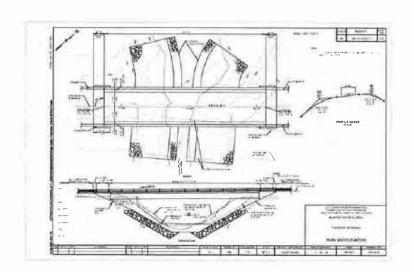
COMPLETED PROJECTS

Current projects include the following:

CURRENT PROJECTS

Upper Hoh Road/Olympic National Park Federal Lands Access Program (FLAP) Project:

Design is complete on a new 120 ft. long concrete bridge to replace the existing bridge at Tower Creek, Upper Hoh Road milepost 7.5. This project is being managed by the Federal Highway Administration Federal Western Lands Division, and is funded almost entirely by a FLAP



Preliminary Tower Creek Bridge drawing



Culvert at Thorndyke Creek

grant and toll credits. Construction is scheduled for 2021-2022.

Design is complete on a new 120 ft. long concrete bridge to replace the culvert at Canyon Creek, Upper Hoh Road milepost 10.0. This project is being managed by the Federal Highway Administration Western Federal Lands Division, and

is funded almost entirely by a Federal Lands Access Program grant and toll credits. Construction is scheduled for 2021-2022. However, federal funding for this project is uncertain and may delay the project.

The fish-barrier culverts at Thorndyke Road milepost 4.71 (Thorndyke Creek) will be replaced with a new bridge. Public Works has secured grant funding from the Fish Barrier Removal Board (FBRB) for project design and has completed the preliminary design process. Public Works applied for construction funds from the FBRB in 2020 and plans to construct the project in 2021-2022.

The fish-barrier culverts at West Valley Road milepost 2.59 (Naylor Creek) and Gibbs Lake Road milepost 1.82 (Naylor Creek) will be replaced with new structures most likely meeting bridge criteria. Public Works has secured grant funding from the FBRB for project design, and has completed the preliminary design process. Public Works applied for construction funds from the FBRB in 2020 and plans to construct the project in 2022-2023.

In addition to Current Projects, the following potential bridge projects are programmed in the 2020-2025 Transportation Improvement Program (TIP):

PROGRAMMED PROJECTS

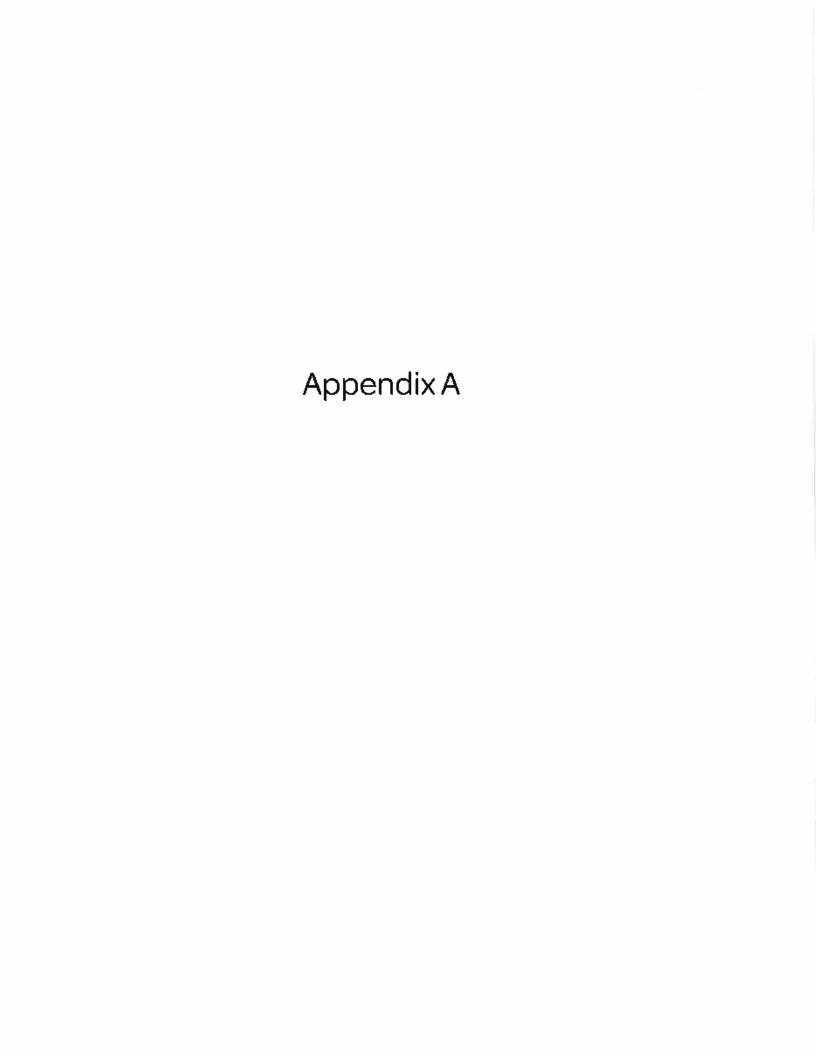
The culvert at Center Road milepost 3.26 (Chimacum Creek) will be replaced with a fish-passable structure, which will most likely meet the criteria for a bridge, given the stream width.

Re-load rating of 22 county bridges, as described previously in this document.

The Little Quilcene River Bridge on Center Road has been reclassified by the Washington State Department of Transportation as Structurally Deficient and the Sufficiency Rating was lowered from 60 to 22, based on a new load rating performed by the County in 2017. The load rating was required by the federal mandate to load rate bridges for specialized hauling vehicles, and the bridge was found to be deficient for some of these vehicles. Given the revised Sufficiency Rating and the Structurally Deficient classification, the bridge is now eligible for federal bridge replacement funds. Jefferson County intends to apply for bridge replacement funds in the next funding round, which will occur in early 2021.

Two or more heavy maintenance projects are anticipated during the period 2020-2025, and are identified as Countywide Bridge Improvements in the TIP. With 35 bridges in its inventory, and half of them around 50 years old, the county has to plan and budget for ongoing maintenance, in addition to capital projects. Potential maintenance projects include pile bracing/stiffening at Contractor's Creek Bridge, and repainting of the steel girders at Cassel Creek Bridge.

Hurst Creek Bridge steel diaphragm repainting/ replacement, as described under maintenance activities. RECOMMENDED PROJECTS



Jefferson County Public Works

Master Bridge List

LAST	UPDA	TED:	9/4/19
------	------	------	--------

Total number of bridges in inventory:	35
---------------------------------------	----

i short-span (non NBIS) bridge i permanently closed bridge

Total number of East Jefferson County Bridges 14 active bridges

1 pedestrian (non NBIS) bridge 1 permanently closed bridge

Jefferson County Public Works Master Bridge List East Jefferson County

PEDERAL STRUCTURE NUMBER	COUNTY NUMBER	NAME	ROAD NUMBER	ROADNAME	MILEPOST	LENGTH (FEET)	BRIDG	Е ТҮРК	VEAR BUILT	PRACIURE CRITICAL	SCOUR CRITICAL	LOAD POSTED	DATE OF LAST INSPECTION	SHFICIENCY RATING	PUNCTIONALLY OBSOLETE (FO) STRUCTURALLY DEFICIENT (SD)	INSPECTION KREQUENCY (MONTHS)	REMARKS
							Superstructure	Substructure									
7996700	15E	Flunts	634509	Hunt Rd.	0.15	27	Reinforced Conc.	Conc. Abutment w/ C.I.P. conc. piles	1971				4/16/19	89.68		24	
8165100	16E	Little Quilcene River	931507	Center Rd.	14.6	64	Reinforced Conc. Slab, Conc. Gurder	Conc. Pile cap. Prestress. Conc. piles	1955				4/16/19	28,21	SD	24	Eligible for bridge replacement funds.
8464900	17E	Yarr	931507	Center Rd.	3.44	25	Conc. Multiple Web Girder	Conc. Abutment w/ Prestress, Conc. piles	1955				4/9/20			12	
8320300	23E	Linger Longer	301309	Linger Longer Rd.	0.42	81	Prestress, Conc. Bulb-T Girder	Conc. Abutment w/ Conc. column over C.I.P. conc. piles	1969				4/16/19	63,91		24	
8 381400	24E	Rocky Brook	250008	Dosewallips Rd	3.01	47	Prestress, Conc. Bulb-T Girder	Conc. Abulment w/ Conc. Spread footing	1964				4/16/19			24	
8229900	25E	Meyers	503409	Belfrage Rd	0.12	30	Timber	Timber abutment, timber piles	1977			Х	6/26/19	74.85		24	
8 488200	26E	West Uncas	500209	West Uncas Rd.	1 89	61	Reinf, Conc. Slab	Cone. Abutment. Cone column over C.I.P. cone. pile	1964				4/16/19	83.26		24	
8 13 69 00	28E	Leland Creek	344009	Rice Lake Rd.	0.33	32	Prestress. Conc Slab	Conc. Abutment, Conc. spread footing	1987				4/16/19	88.10	FO	24	
8660000	29E	Contractors Creck	501409	Old Gardiner Rd	3.56	294	Tamber Deck, Timber Guders	Conc. Pile cap, steel piles	2000				8/4/20	87 42		24	UBIT EVERY 4 YEARS — LAST UBIT 8/4/2020

Jefferson County Public Works Master Bridge List East Jefferson County

Last Undated: 9/2/2020

PEDRAAL STRUCTURE NUMBER	COUNTY NUMBER	NAME	ROAD NUMBER	HOAD NAME	MILEFOST	LENGTH (FEET)	BRIDG	GE TYPE	VEAR BUILT	PRACTURE CRITICAL.	SCOUR CRITICAL.	LOAD POSTED	LAS JUSPECTIUN	SHEPICIENCY RATING	FUNCTIONALLY OBSOLETE (FG) STRUCTURALLY DEPICIENT (Sh)	INSPECTION FREQUENCY (MONTHS)	Remarks
							Superstructure	Substructure									
8754000	31E	Shine Creek	447608	South Point Rd.	0.25	70	Prestress. Conc. Slab	Conc. Abutment w/	2005				4/16/19	97.60		24	
8753900	32E	Donovan Creek	327508	Ti Quilcene Rd.	0.77	70	Prestress, Conc. Slab	Conc. Abutment w/ Conc. Pile	2006				4/16/19	87.36		24	
8753800	33E	Old Tarboo	357509	Old Tarboo Rd.	0.88	42	Prestress, Cone. Slab	Conc. Abutment w/ steel piles	2005				4/16/19	89.98		24	
8753800	38E	Andrews Creek	352909	Snow Creek Rd.	3.78	24	Conc. 3-sided Box Culvert	Conc. Spread Footing	2014				4/16/19	91.14		24	
8898600	41E	RTMT Pedestrian	352909	Rick Tollefson Mem. Trail	0.17	80	Steel Thru Truss	Cone, Spread Footing	2017				4/30/19	N/A		24	
8902400	42E	Wally Bowman	500209	West Uncas Rd.	0.81	80	Prestress Conc. Hulb-T Girder	Cone. Abutment w/ Cone. Pile	2018				4/16/19	98.95		24	

Jefferson County Public Works Master Bridge List West Jefferson County

PEDERAL STRUCTURE NUMBER	CULLYTY NUMBER	VANE	ROAD NUMBER	RDAD NAME	MILEPOST	(RNGTH (PERT)	RRIDG	е туре	YBAR BUHUT	PRACTURE CRITICAL	SCOUR CRITICAL	LOAD PUSTED	LAS INSPECTION	SUPPORNCY RATING	STRUCTURALLY DEFICIENT (SD) FUNCTIONALLY OBSOLETR (PD)	INSPECTION PREQUENCY (MONTHS)	REMARKS
							Superstructure	Substructure					4				
8451600	OIW	Fletcher Creek	911607	Quinault S Shore Rd.	3,19	7%	Prestaess, Conc. Bulb-T Girder	Conc. Abutment w/ C.I.P. conc. pile	1974				5/20/20	99,62		24	
8258500	02W	Barlow	134309	Oil City Rd,	9,75	64	Prestress, Conc. Slab	Cone. Abutment w/ C.1.P. Cone. Pile	2005				5/20/20	83.43		24	
81458/00	03W	Anderson Creek	134309	Oil City Rd.	4,44	87	Prestress, Conc. Bulb-7 Girder	Conc. Abulment w/ C.I.P. conc. pile	1974				5/20/20	91_91		24	
6280100	04 W	Hell Roaring Creek	914207	Opper Huh Rd	0.16	120	Prestress, Conc. Girder	Conc. Abutment w/ steel H p:le	1982				5/21/20	76,65		24	
8298800	05W	Alder Creek	914207	Upper Hoh Rd	2.07	67	Prestress Conc. Bulb-T Girder	Cure Abutment w/ CTP conc. pile	1972				5/21/20	90,21		24	
8.383600	06W	Rock Creek	914207	Upper Floh Rd.	6.49	83	Prestress, Cone, Bulb-T Girder	Conc. Abutment w/ C.L.P. conc. pile	1973				5/21/20	77,41		24	
8384000	07W	'I ower Creek	914207	Upper Hah Rd.	7,5	71	Prestress, Cone. Bulb-T Girder	Cone. Abatment w/ C.J.P. conc. grie	1973				5/21/20	78.81		24	
8391000	D8W	Queets River	107509	Clearwates Rd	0.6	844	Steel Plate Girder, C.I.J. cens. deck	Conc. Piers, Conc. Abutment w/ steel Hi pile	1988				7/11/18	98.61		24	UBIT EVERY 4 YEARS — LAST CBIT 7/11/2018
3 047 8 00	09W	Hemphiti Creek	146809	Dowans Creek Rd	2,3	31	Timber Deck, Timber Girders	Timber abuterent, amber piles	1977				5/20/20	76.55		24	
7966600	10W	Dowans Crock	146809	Dowans Creek Rd	0,52	81	Prestress Conc. Bub-T Girder	Conc. Abument w/ C.t.P. conc. pile	1974				5/20/20	94.63		24	

Jefferson County Public Works Master Bridge List West Jefferson County

PKRIZRA1. STRUCTURF NUMBER	COUNTY NUMBER	NAME	RDAD NUMBER	ROAD SAME	MILEPOST -	LENGTH (PRICT)	BRIDG	E TYPE	VRAR RUIT	FRALTURE CRITICAL	SCOUR CRITICAL	LOAD POSTED	LAS INSPECTION	SUFFICIENCY RATING	STRUCTURALLY DEFICIENT (SD) FUNCTIONALLY DBSOLETE (FO)	INSPECTION PREQUENCY (MONTHS)	REM4RKS
							Superstructure	Substructure									
8475700	11 W	Owl Creek	135109	Maple Creek Rd	1.29	69	Prestress Conc Stab	Conc. Abutment w/ steel H piles	1989				5/20/20	83,39	FO	24	
8253600	13W	Hurst Creek		Clearwater Rd.	2.66	67	Prestress Conc. Bulb-T Girden	Conc. Abutment, Conc. culman, Conc. Inoting	1969				5/20/20	98.82		24	Maintenance needed – paint/replace steel diaphragms
8339400	27W	Willoughby Creek		Upper Hoh Rd	3.52	60	Prestress Conc Bulb-T Girder	Conc. Pile cap w/ Prestress, Conc. piles	1962				5721720	45 53		24	
8 659900	30W	Cassel Creek	134309	Oil City Rd.	3 59	154	Steel Girder	Conc. Pile cap, steel piles	2000	х			7/9/18	80.32	FO	24	Fracture Crutical Special Inspection = WSDOT performs inspection with UBIT been ally Upcoming maintenance - paint steel perfors
8829800	34W	Pole Creek	914207	Upper Holi Rd	8.3	37	Conc. 3-sided Box Culvect	Conc. Spread Fooling	2016				5/21/20	80.20		24	
8845600	3SW	Dismal Creek	914207	Opper Holi Rd	9 19	22	Conc. 3-sided Box Culvert	Corte Spread Footlog	2011				5/21/20	73 19		24	
885350C	36W	Spruce Creek		Oppo Hob Rd,	9.7	22	Conc. 3-sided Box Culvert	Conc Spread Footing	2012				5/21/20	77.41		24	
8863300	37W	Alder Creek Tributary	914207	Opper Hinh Rd	2.15	25	Conc. 3-gided Box Culvert	Conc Spread Footing	2013				5/21/20	76.65		24	
8886600	39W	Upper Hoh Rd, MP 6,95	914207	L'pper Hob Rd	6.95	21	Conc. 3-sided Box. Culvert	Conc_Spread Footing	2016				5/21/20	82.69		24	
KG0307	41E	Donkey Creek	107509	Clearwater Rd	1 29	17	Conc 3-sided Box Culvert	Open-ended steel pipe pilling	2015				5/20/20			24	





Bridge Ratings

The safety of bridge structures in Washington State is ensured through a meticulous inspection system. All public bridge owners, such as WSDOT, Counties and Cities, follow the same bridge inspection procedures. The condition rating of all bridge decks, superstructures and substructures and other elements based on these inspections.

USDOT's Federal Highway Administration (FHWA) requires all public bridge owners (state, city, and county) to inspect and their bridges and report information including bridge condition ratings as part of their requirements in the National Bridge Inventory Standards (NBIS).

Important aspects of the NBIS were:

All states
 must
 perform
 periodic
 inspections
 of bridges
 greater than
 20 feet in
 span on at
 least a
 biennial
 basis.



SR6 Wiliapa River bridge



SR20 Deception Pass Bridge

- 2. Data collection was standardized and must be reported to FHWA.
- 3. Qualifications for inspection personnel were defined.
- 4. Training programs were developed and implemented.
- 5. The Bridge Replacement Program (BRP) was established to provide funding for bridge replacement on the system.

Structurally Deficient

Structurally deficient means that a bridge requires repair or replacement of a certain component. This may include cracked or spalled concrete, the bridge deck, the support structure, or the entire bridge itself. If the condition is such that it no longer is able to carry its intended traffic loads it may be weight restricted. Being structurally deficient does not imply that the bridge is in danger of collapse or unsafe to the traveling public. If a bridge is open then it is considered safe.

A bridge is classified as "Structurally Deficient" when bridge inspectors give either the superstructure, deck, and/or substructure a rating of four or less on a scale of zero to nine. WSDOT's poor condition category uses the same data, criteria, and rating scale.

WSDOT has 137 state owned bridges that are classified as structurally deficient as of Jan. 2015. A list of these bridges is available in pdf and web page and map format.

Functionally Obsolete

Functional obsolescence is assessed by comparing the existing design of each bridge to current standards.

A bridge can be categorized functionally obsolete a number of ways including: substandard bridge widths, low vertical clearance that can lead to repeated damage from over height trucks, load-carrying capacity, or flood potential.

There are 866 WSDOT bridges that are rated "Functionally Obsolete"



Interstate B2 Columbia River bridge near Umatilia

Good, Fair, Poor Condition Rating

Good: A range from no problems to some minor deterioration of structural elements.

Fair: All primary structural elements are sound but may have deficiencies such as minor section loss, deterioration, cracking, spalling, or scour.

Poor: Advanced deficiencies such as section loss, deterioration, cracking, spalling, scour, or seriously affected primary structural components. Bridges rated in poor condition may be posted with truck weight restrictions.

A summary of the WSDOT bridge network conditions is available in the 2014 Bridge Annual Report in the Gray notebook.

Copyright WSOOT @ 2015



Bridge inspectionDefinitions

What are "general condition ratings?" According to the National Bridge Inspection Standards (NBIS), condition ratings are used to describe an existing bridge or culvert compared with its condition if it were new. The ratings are based on the materials, physical condition of the deck (riding surface), the superstructure (supports immediately beneath the driving surface) and the substructures (foundation and supporting posts and piers). General condition ratings range from 0 (failed condition) to 9 (excellent). For detailed definitions, click here.

Which bridges are included in the NBI system? NBI structures are bridges or culverts that carry vehicular traffic and have an opening longer than 20 feet measured along the center of the roadway.

What bridges are not considered part of the NBI system? Non-NBI structures include bridges or culverts that carry vehicular traffic and are <u>equal to or less than</u> 20 feet measured along the center of the roadway.

VDOT exceeds the NBI standards by inspecting and documenting in our inventory <u>all</u> bridges regardless of their length and all culverts having an opening greater than 36 square feet.

What is a "structurally deficient" bridge? Bridges are considered structurally deficient if they have been restricted to light vehicles, closed to traffic or require rehabilitation. Structurally deficient means there are elements of the bridge that need to be monitored and/or repaired. The fact that a bridge is "structurally deficient" <u>does not</u> imply that it is likely to collapse or that it is unsafe. It means the bridge must be monitored, inspected and maintained.

How is "structural deficiency" determined? The condition of different parts of a bridge is rated on a scale of 0 to 9 (with 9 being "excellent" and zero being "failed"). A structurally deficient bridge is one for which the deck (riding surface), the superstructure (supports immediately beneath the driving surface) <u>or</u> the substructure (foundation and supporting posts and piers) are rated in condition 4 or less.

What makes a bridge structurally deficient, and are structural deficient bridges unsafe? The fact that a bridge is "structurally deficient" <u>does not</u> imply that it is likely to collapse or that it is unsafe. A "deficient" bridge is one with some maintenance concerns that do not pose a safety risk. A "deficient" bridge typically requires maintenance and repair and eventual rehabilitation or replacement to address deficiencies. To remain open to traffic, structurally deficient bridges are often posted with reduced weight limits that restrict the gross weight of vehicles using the bridges. If unsafe conditions are identified during a physical inspection, the structure must be closed.

What is a "functionally obsolete" bridge? A functionally obsolete bridge is one that was built to standards that are not used today. These bridges are not automatically rated as structurally deficient, nor are they inherently unsafe. Functionally obsolete bridges are those that do not have adequate lane widths, shoulder widths, or vertical clearances to serve current traffic demand, or those that may be occasionally flooded.

A functionally obsolete bridge is similar to an older house. A house built in 1950 might be perfectly acceptable to live in, but it does not meet all of today's building codes. Yet, when it comes time to consider upgrading that house or making improvements, the owner must look at ways to bring the structure up to current standards.

What is a "fracture-critical" bridge? A fracture-critical bridge is one that does not contain redundant supporting elements. This means that if those key supports fail, the bridge would be in danger of collapse. This <u>does not</u> mean the bridge is inherently unsafe, only that there is a lack of redundancy in its design.

What is a bridge's "sufficiency rating?" Sufficiency ratings were developed by the Federal Highway Administration to serve as a prioritization tool to allocate funds. The rating varies from 0 percent (poor) to 100 percent (very good). The formula considers structural adequacy, whether the bridge is functionally obsolete and level of service provided to the public.

History of Federal Bridge Inspection Program The federal bridge inspection program regulations were developed as a result of the Federal-Aid Highway Act of 1968 following the collapse of the Silver Bridge in Point Pleasant, West Virginia. The United States Secretary of Transportation established the National Bridge Inspection Standards (NBIS) to locate and evaluate existing bridge deficiencies to ensure the safety of the traveling public.

The 1968 Federal-Aid Highway Act directed the states to maintain an inventory of federal-aid highway system bridges. This was amended over time to establish criteria for NBIS bridges including:

- Defining the NBIS to bridges to those on the federal-aid highway system.
- Requiring inspections of bridges longer than 20 feet on all public roads
- Expanding bridge inspection programs to include special inspection procedures for fracturecritical members and underwater inspection



Bridge Condition Code key

Code	Description
N	NOT APPLICABLE
9	EXCELLENT CONDITION
8	VERY GOOD CONDITION No problems noted.
7	GOOD CONDITION Some minor problems.
6	SATISFACTORY CONDITION Structural elements show some minor deterioration.
5	FAIR CONDITION All primary structural elements are sound but may have some minor section loss (due to corrosion), cracking, spalling (deterioration of concrete surface) or scour (erosion of soil)
4	POOR CONDITION Advanced section loss, deterioration, spalling or scour.
3	SERIOUS CONDITION Loss of section, deterioration, spalling or scour have seriously affected primary structural components. Local failures are possible. Fatigue cracks in steel or shear cracks in concrete may be present.
2	CRITICAL CONDITION Advanced deterioration of primary structural elements. Fatigue cracks in steel or shear cracks in concrete may be present or scour may have removed substructure support. Unless closely monitored it may be necessary to close the bridge until corrective action is taken.
1	"IMMINENT" FAILURE CONDITION Major deterioration or section loss present in critical structural components or obvious vertical or horizontal movement affecting structure stability. Bridge is closed to traffic but corrective action may put back in light service.
0	FAILED CONDITION Out of service - beyond corrective action,

TIP Distribution List

Digital Copy unless where noted

	1		1			T
Annual Commons	Danaston aut 1	Damanton ant 2	Cambant	F		T:41a
Agency / Company	Department1	Department2	Contact	Fname	Lname	Title
Internal	511 514 626 (11 1 6 1)			ol :	6 11	
JC PW	File PW 030 (Hard Copy)			Chris	Spall	
JC PW	Display (Hard Copy)			Chelsie	Kilmer	
JC PW Director/County Engineer				Monte	Reinders	
JC PW Engineering Services Manager				Eric	Kuzma	
JC PW Transportation Planner				Wendy	Clark-Getzin	
JC PW Engineers	Engineers' Library (Hard Copy)			Chelsie	Kilmer	
JC Engineering Tech III ROW Specialist				Mara	Dotson	
JC PW Operations Manager	Port Hadlock Shop			Matt	Stewart	
JC BoCC	Commissioners Office			3	Commissioners	
JC County Administrator	Commissioners Office			Philip	Morley	
JC DCD Director	Community Development			Patty	Charnas	
JC Sheriff	Sheriff's Office			Joe	Nole	
JC Public Health Director	Public Health			Vicki	Kirkpatrick	
JC Auditor	County Courthouse			Rose Ann	Carroll	
JC Assessor	County Courthouse			Jeff	Chapman	
JC Treasurer	County Courthouse			Stacy	Prada	
RCW 36.81.121 Submissions	,					
County Road Administration Board (CRAB)			Main	John	Koster	Executive Director
County Road Administration Board (CRAB)			Secondary	Toni	Cox	Engineering Technician
County Road Administration Board (CRAB)			Secondary	Randy	Hart	Grant Program Manager
Washington State Department of Transportation	Olympic Region	Local Programs	Main	Bryan	Dias	Local Programs Engineer
Washington State Department of Transportation	HQ	Local Programs	Main	Huntley	Nancy	Project Prioritization Engineer
Washington State Department of Transportation	Olympic Region	Local Frograms	Main	John	Wynands	Regional Administrator
Washington State Department of Transportation	Olympic Region	Traffic Operations		Sarah	Ott	Traffic Operations Engineer
Optional Others	Olympic Region	Traffic Operations	Secondary	Saraii	Ott	Traffic Operations Engineer
			N Andre	T	N 4 a u a al i te la	Director
Jefferson County Library			Main	Tamara	Meredith	
Jefferson Conservation District			Main	Al	Clarks	Director
Chimacum School District			Main	Art	Clarke	Asst. Superintendent
Quilcene School District			Main	Gena	Lont	Director of Business & Finance
City of Port Townsend	Public Works Department		Main	Steve	King	Public Works Director
City of Port Townsend	Public Works Department		Secondary	Dave	Peterson	City Engineer
Jefferson County PUD #1			Main	Kevin	Streett	General Manager
FHWA	Western Federal Lands		Main	Andrew	Rasmussen	WA FLAP Program Manager
Olympic National Park			Main	Lisa	Turecek	Chief of Maintenance
Peninsula RTPO	Lead Planning Agency	Kitsap Transit	Main	Ed	Coviello	Land Use Planner
Transportation Improvement Board (TIB)			Main	Chris	Langhoff	Project Engineer
Jefferson Transit			Main	Jayme	Brook	Grants & Procurement Coordinator
Port of Port Townsend			Main	Eric	Toews	Deputy Director
Clallam County	Department of Public Works		Main	Steve	Gray	Transportation Program Manager
Kitsap County	Department of Public Works		Main	David	Forte	Transportation Planner
Mason County	Department of Public Works		Main	Dave	Smith	Transportation Engineer
Grays Harbor County	Department of Public Works		Main	Rob	Wilson	County Engineer
Island County	Department of Public Works		Main	Susan	Driver	Transportation Planner
Peninsula Trails Coalition			Main	Jeff	Bohman	President
Centurylink	Engineering		Main	Daniel	Beach	
Wave	Business & Legal Affairs		Main	Jim	Penney	Executive Vice President
Wave	Operations		Main	Jake	Crabtree	Operations Manager
Olympic Water & Sewer			Main	Greg	Rae	Operations Supervisor
Centurylink	1		Main	Melitta	Holland	Construction Project Administrator
PUD #1 of Mason County	<u> </u>		Main	Kristin	Masteller	
PUD #1 of Clallam County	+		Main	John	Purvis	Asst. General Manager
PUD #1 of Clallam County		1	Secondary	Mike	Hull	Asser General Manager
Jamestown Tribe	1	 	Main	Annette		Transportation Planner
	+				Nesse	
Hoh Tribe			Main	Wendy	Largent	Transportation Planner
Quinault Nation	+	-	Main	Michael	Cardwell	Landuse Planner
Quinault Nation			Secondary	Charles	Warsinske	DCD Director